



CITY OF WEST KELOWNA REGULAR COUNCIL AGENDA

Tuesday, September 26, 2023, 6:00 P.M.

COUNCIL CHAMBERS

2760 CAMERON ROAD, WEST KELOWNA, BC

Pages

1. CALL THE REGULAR COUNCIL MEETING TO ORDER

It is acknowledged that this meeting is being held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting is open to the public. All representations to Council form part of the public record. This meeting is being webcast live and will be archived on the City's website.

2. INTRODUCTION OF LATE ITEMS

3. ADOPTION OF AGENDA

4. ADOPTION OF MINUTES

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6. DELEGATIONS

6.1 Allyson Graf, President and CEO, YMCA of Southern Interior BC 15

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8. DIVISION REPORTS

8.1 CHIEF ADMINISTRATIVE OFFICER

8.1.1 Council Meeting Schedule 2024 17

Recommendation to Consider and Resolve:

THAT Council approve the 2024 City of West Kelowna Council Meeting Schedule.

8.1.2 Good Neighbour Bylaw Amendments 20

Recommendation to Consider and Resolve:

THAT Council adopt “City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023”; and

THAT Council adopt “City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023”; and

THAT Council adopt “Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023”.

8.1.3 State of Local Emergency 28

Recommendation to Consider and Resolve:

THAT Council endorse the extension of the State of Local Emergency for the McDougall Creek wildfire authorized by Mayor Gord Milsom extended on September 14, 2023, for a further week set to expire on September 21, 2023; and

THAT Council authorize a further week extension for the State of Local Emergency for the McDougall wildfire until September 28, 2023.

8.2 FINANCIAL SERVICES

8.3 DEVELOPMENT SERVICES

8.3.1 DP 22-26; Development Permit with Variances; 3401 Sundance Drive 30

Recommendation to Consider and Resolve:

THAT Council deny the Development Permit with Variances located at 3401 Sundance Drive (File: DP 22-26) and direct staff to close the file.

8.3.2 Application No. Z 17-02 – Application Extension (3290 Glenrosa Road, 3280 Glenrosa Road and Salmon Road) 179

Recommendation to Consider and Resolve:

THAT in accordance with Development Application Procedures Bylaw No. 0260, Council approve an additional extension of one (1) year for File: Z 17-02, subject to the reservoir costs being brought to 2023 standards.

8.3.3	Z 22-08; Zoning Amendment Bylaw No. 0265.12, 2023 (Second Reading); 2741 Auburn Road	245
	<u>Recommendation to Consider and Resolve:</u> THAT Council give second reading to Zoning Amendment Bylaw No. 0265.12, 2023; and THAT Council direct staff to schedule a public hearing regarding the proposed amendment bylaw.	
8.3.4	ALC Standing Resolution; 1221 Hudson Road, 3044 Sandstone Drive, and 2829 Inverness Road	289
	<u>Recommendation to Consider and Resolve:</u> THAT Council, pursuant to the <i>Agricultural Land Commission Act</i> , authorize the Director of Development Approvals, to directly forward to the Agricultural Land Commission with support, the following Non-Farm Use Applications: For all school and childcare related uses at Hudson Road Elementary (1221 Hudson Road), Shannon Lake Elementary (3044 Sandstone Drive), and Webber Road Community Centre (2829 Inverness Road) where: <ul style="list-style-type: none"> a. the proposal complies with the City of West Kelowna's Zoning Bylaw as amended or replaced from time to time; b. the proposal complies with the City of West Kelowna's Official Community Plan as amended or replaced from time to time; and c. the property is owned by the Board of School Trustees of School District No. 23 (Central Okanagan) 	
8.4	RECREATION AND CULTURE	
8.5	ENGINEERING / PUBLIC WORKS / PARKS	
8.5.1	Local Area Service Inclusion – 1135 Westside Road	294
	<u>Recommendation to Consider and Resolve:</u> THAT Council adopt Bylaw No. 0280.02 being “City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Bylaw No. 0280 2020”.	
8.5.2	Shannon Lake Road Active Transportation Corridor	299
	Information Report from the Director of Engineering and	

Operations.

8.5.3 Rose Valley Water Treatment Plant Update

Verbal update from the Director of Engineering and Operations.

8.6 CORPORATE INITIATIVES

8.7 FIRE RESCUE SERVICES

9. CORRESPONDENCE AND INFORMATION ITEMS

10. NOTICE OF MOTION

11. ADJOURNMENT OF THE REGULAR MEETING

The next Council meeting is scheduled for Tuesday, October 10, 2023.



CITY OF WEST KELOWNA
MINUTES OF THE SPECIAL MEETING OF COUNCIL

Tuesday, September 12, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen
Councillor Stephen Johnston (arrived at 11:25 a.m.)
Councillor Garrett Millsap (arrived at 11:31 a.m.)
Councillor Carol Zanon

Staff Present: Paul Gipps, CAO
Trevor Seibel, Deputy CAO / Deputy Corporate Officer
Allen Fillion, Director of Engineering / Operations
Warren Everton, Director of Finance / CFO
Sandy Webster, Director of Corporate Initiatives
Michelle Reid, Director of Human Resources
Jason Brolund, Fire Chief
Brad Savoury, Director of Legal Services
Brent Magnan, Director of Development Approvals
Corinne Boback, Legislative Services Manager/Corporate Officer

1. CALL THE SPECIAL COUNCIL MEETING TO ORDER

The Special Council meeting was called to order at 11:06 a.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting was open to the public. All representations to Council form part of the public record.

2. ADDITIONS OR CHANGES TO PROCEDURAL MOTION

3. ADOPTION OF AGENDA

It was moved and seconded

Resolution No. C301/23

THAT the agenda be adopted as presented.

CARRIED UNANIMOUSLY

4. PROCEDURAL MOTION

It was moved and seconded

Resolution No. C302/23

THAT Council close the meeting in accordance with Section 90(1) of the *Community Charter* for:

(c) labour relations or other employee relations;

(d) the security of the property of the municipality;

(e) the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality;

(g) litigation or potential litigation affecting the municipality; and

(k) negotiations and related discussions respecting the proposed provision of a municipal service that are at their preliminary stages and that, in the view of the council, could reasonably be expected to harm the interests of the municipality if they were held in public.

CARRIED UNANIMOUSLY

5. ADJOURNMENT OF THE SPECIAL COUNCIL MEETING

The Special Council meeting adjourned at 1:06 p.m.

MAYOR

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER



CITY OF WEST KELOWNA
MINUTES OF THE REGULAR MEETING OF COUNCIL

Tuesday, September 12, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen
Councillor Stephen Johnston
Councillor Garrett Millsap
Councillor Carol Zanon

Staff Present: Paul Gipps, CAO
Trevor Seibel, Deputy CAO / Deputy Corporate Officer
Allen Fillion, Director of Engineering / Operations
Warren Everton, Director of Finance / CFO
Sandy Webster, Director of Corporate Initiatives
Jason Brolund, Fire Chief
Brad Savoury, Director of Legal Services
Brent Magnan, Director of Development Approvals
Corinne Boback, Legislative Services Manager/Corporate Officer
Bob Dargatz, Development Manager/Approving Officer
Chris Oliver, Planning Manager
Rob Hillis, Engineering Manager
Mike Cain, Bylaw Services Manager
Mark Roberts, Parks Manager
Stacey Harding, Fleet Operations and Project Manager
Carla Eaton, Senior Planner - Long Range
Natasha Patricelli, Legislative Services Assistant

1. CALL THE REGULAR COUNCIL MEETING TO ORDER

The meeting was called to order at 1:36 p.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting was open to the public. All representations to Council form part of the public record. This meeting was webcast live and archived on the City's website.

2. INTRODUCTION OF LATE ITEMS

3. ADOPTION OF AGENDA

It was moved and seconded

Resolution No. C303/23

THAT the agenda be adopted as presented.

CARRIED UNANIMOUSLY

4. ADOPTION OF MINUTES

4.1 Minutes of the Public Hearing held Thursday, July 20, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C304/23

THAT the Minutes of the Public Hearing held Thursday, July 20, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

4.2 Minutes of the Special Council Meeting held Tuesday, July 25, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C305/23

THAT the Minutes of the Special Council Meeting held Tuesday, July 25, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

4.3 Minutes of the Regular Council Meeting held Tuesday, July 25, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C306/23

THAT the Minutes of the Regular Council Meeting held Tuesday, July 25, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

4.4 Minutes of the Special Council Meeting held Tuesday, August 29, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C307/23

THAT the Minutes of the Special Council Meeting held Tuesday, August 29, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

4.5 Minutes of the Special Regular Council Meeting held Tuesday, August 29, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C308/23

THAT the Minutes of the Special Regular Council Meeting held Tuesday, August 29, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

5. MAYOR AND COUNCILLOR'S REPORTS

Mayor Milsom on behalf of Council, thanked the community for staying strong and remaining compassionate during the McDougall Creek Wildfire, which remains out of control. The Emergency Operations Centre continues to work in consultation with the BC Wildfire Service, West Kelowna Fire Rescue, and other agencies to get those still on evacuation order safely home as soon as possible. Heartfelt condolences go out to all who have lost structures. Council also thanked West Kelowna Fire Rescue, BC Wildfire Service and many other responders who saved thousands of homes, ensured no loss of life and all who are now working to help our community to rebuild and recover.

Mayor Milsom congratulated Collette Beggs on completing her Local Government Administration Certificate with Capilano University.

- 5.1 **Regional District of Central Okanagan Highlights from the Thursday, August 3, 2023 Regional Board Meeting**
- 5.2 **Regional District of Central Okanagan Highlights from the Thursday, September 7, 2023 Regional Board Meeting**
- 5.3 **Rise and Report - Westbank Centre Park - Community Garden and Dog Park Relocation**

The Parks Manager provided a PowerPoint presentation and was available for questions and comments from Council.

6. DELEGATIONS

7. UNFINISHED BUSINESS

- 7.1 **A 21-08 w/ Variances, Subdivision and Non-Adhering Residential Use with Variances, 3716 and 3792 Carrall Road**

This item was deferred from the July 25, 2022 Regular Council meeting and was before Council for consideration.

It was moved and seconded

Resolution No. C309/23

THAT Council deny application (A 21-08) for a two-lot subdivision in the ALR; and

THAT Council deny application (A 21-08) for a Non-Adhering Residential Use; and

THAT Council deny the application for a variance to Zoning Bylaw No. 0265 S. 3.19.2 to increase the size of a Carriage House from 90m² to 131m² and to S.3.19.9 to include a patio greater than 0.6m in height for the dwelling at 3792 Carrall Road; and

THAT Council direct staff not to discharge the existing covenant LB386881 restricting the use of the accessory dwelling for agricultural purposes (Agricultural Worker Dwelling); and

THAT Council direct staff to close the file for application (A 21-08).

CARRIED UNANIMOUSLY

7.2 Councillor Johnston

At the July 25, 2023 Regular Council Meeting, Councillor Johnston provided the following Notice of Motion, which was before Council for consideration.

It was moved and seconded

Resolution No. C310/23

WHEREAS Council recognizes the housing affordability challenges faced by residents within our city and this province;

and WHEREAS Council seeks to reduce barriers and remove regulations that negatively impact housing affordability in our city;

and WHEREAS the City of West Kelowna Water Regulation Bylaw No. 274, 2021, Section 7.10 currently supports the installation of water meter pits where a parcel owner fails to provide adequate, convenient, and unobstructed access to the City;

THAT Council direct staff to prepare a report on water meters in pits with options to amend the City of West Kelowna Water Regulation Bylaw No. 274, 2021, to assess the merits of the requirement to have water meters installed within water meter pits for new residential construction; and

THAT Council direct staff to bring forward a report with options to amend the City of West Kelowna Water Regulation Bylaw No. 274, 2021 back to Council for first, second and third readings for the October 10, 2023 Council Meeting.

CARRIED; Mayor Milsom opposed

8. DIVISION REPORTS

8.1 CHIEF ADMINISTRATIVE OFFICER

8.1.1 Good Neighbour Bylaw Amendments

It was moved and seconded

Resolution No. C311/23

THAT Council give first, second, and third reading to the “City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023”; and

THAT Council give first, second, and third reading to the “City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023”; and

THAT Council give first, second, and third reading to the “Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023”.

CARRIED UNANIMOUSLY

8.1.2 Casa Loma Emergency Egress Project

It was moved and seconded

Resolution No. C312/23

THAT Council award the “Casa Loma Emergency Egress” civil construction contract to R&L Construction Ltd in the amount of \$1,186,713.10 (excluding GST); and

THAT Council approved additional budget of \$180,000 for contingency (\$100,000.00), archaeological monitoring (\$50,000.00), and geotechnical investigation and testing (\$30,000.00); and

THAT the City’s 84% share (\$835,800) of the original budget of \$995,000 for the “Casa Loma Emergency Egress” project be funded from the Capital Reserve; and

THAT the City’s 84% share (\$312,239) of the additional \$371,713.10 project costs be funded from the Future Expenditures Reserve.

CARRIED UNANIMOUSLY

8.1.3 State of Local Emergency Extension 2023

It was moved and seconded

Resolution No. C313/23

THAT Council endorse the extension of the State of Local Emergency for the McDougall Creek wildfire authorized by Mayor Gord Milsom extended on September 8, 2023, for a further week set to expire on September 14, 2023; and

THAT Council authorize a further week extension for the State of Local Emergency for the McDougall wildfire until September 21, 2023.

CARRIED UNANIMOUSLY

The meeting recessed at 2:40 p.m.

The meeting reconvened at 2:54 p.m.

8.2 FINANCIAL SERVICES

8.3 DEVELOPMENT SERVICES

8.3.1 Z 20-08; OCP and Zoning Amendment Bylaws No. 100.62, 265.16, and 265.17 (Rescind Third Reading, Give Third Reading as Amended and Adopt); 2211 Campbell Road

It was moved and seconded

Resolution No. C314/23

THAT Council adopt the City of West Kelowna Official Community Plan Amendment Bylaw No. 100.62, 2021;

THAT Council rescind third reading to the City of West Kelowna Zoning Amendment Bylaws No. 154.96 and 154.97, 2021;

THAT Council give third reading as amended to the City of West Kelowna Zoning Amendment Bylaws No. 265.16 and 265.17, 2023; and

THAT Council adopt the City of West Kelowna Zoning Amendment Bylaws No. 265.16 and 265.17, 2023.

CARRIED UNANIMOUSLY

8.3.2 P 21-01, Official Community Plan Bylaw No. 300 (3rd as amended and Adopt)

This item was originally scheduled for the August 22, 2023 Regular Council meeting which was postponed due to the McDougall Creek Wildfire. This item was before Council for consideration.

Requires a majority of all members of Council (4)

It was moved and seconded

Resolution No. C315/23

THAT Council give third reading to “City of West Kelowna Official Community Plan Bylaw No. 0300, 2023”, as amended; and

THAT Council adopt “City of of West Kelowna Official Community Plan Bylaw No. 0300, 2023”.

CARRIED; Councillor de Jong opposed

8.4 RECREATION AND CULTURE

8.5 ENGINEERING / PUBLIC WORKS / PARKS

8.5.1 Bartley Road Rehabilitation Tender Award

This item was originally scheduled for the August 22, 2023 Regular Council meeting which was postponed due to the McDougall Creek Wildfire. This item was before Council for consideration.

It was moved and seconded

Resolution No. C316/23

THAT the contract for the Bartley Road paving and civil construction be awarded to Mackinley Clark Paving Ltd in the amount of \$413,829.00 (excluding GST); and

THAT the Mayor and Corporate Officer be authorized to execute the contract on behalf of the City of West Kelowna.

CARRIED UNANIMOUSLY

8.6 CORPORATE INITIATIVES

8.7 FIRE RESCUE SERVICES

9. CORRESPONDENCE AND INFORMATION ITEMS

10. NOTICE OF MOTION

11. ADJOURNMENT OF THE REGULAR MEETING

The meeting adjourned at 3:49 p.m.

MAYOR

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER

From: noreply@esolutionsgroup.ca
To: [Delegation Requests](#)
Subject: New Response Completed for Request to Appear As a Delegation
Date: June 20, 2023 3:37:20 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe. **The City will never ask for personal or account information or account password through email.** If you feel this email is malicious or a scam, please forward to westkelowna@phishforward.beauchersecurity.com.

Hello, Please note the following response to Request to Appear As a Delegation has been submitted at Tuesday June 20th 2023 3:32 PM with reference number 2023-06-20-002.

- **Request to appear as a delegation on:**
9/19/2023
- **Name of person making the presentation:**
Allyson Graf
- **Name of the group or organization that the person is representing:**
YMCA Southern Interior BC
- **Daytime phone number:**
12503176625
- **Email address:**
allyson.graf@ymcasibc.ca
- **Presentation title:**
YMCA Southern Interior BC
- **What is your request to Council?**
We would like to provide an overview of the YMCA, the work we are doing in the community of West Kelowna and how our work aligns with municipal priorities.
- **Presentation points:**
Overview of the YMCA - our Mission, Vision and Values
Programs and services offered to serve Children, Youth and Families
Experience serving communities in the Interior
Unique expertise and competencies of the YMCA

Partnerships and Collaboration

[This is an automated email notification -- please do not respond]



OUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

Subject: **Council Meeting Schedule 2024**

Report Prepared by: Corinne Boback, Legislative Services Manager / Corporate Officer

RECOMMENDATION to Consider and Resolve:

THAT Council approve the 2024 City of West Kelowna Council Meeting Schedule.

STRATEGIC AREA(S) OF FOCUS

Foster Safety and Well-Being – We will pursue through direct action, advocacy, and collaboration with local and regional service providers, investments in community health, needs-based housing, emergency preparedness, policing, and other services that foster safety and well-being in West Kelowna.

BACKGROUND

Under the *Community Charter*, s.127 we must:

- (a) Make available to the public, a schedule of the date, time, and place of Regular Council Meetings; and
- (b) Give notice of the availability of the schedule in accordance with s. 94(2) of the Community Charter.

The notice will be published in the West K newspaper, posted on our notice board and posted to our website once approved by Council.

Council's Procedure Bylaw also requires that when a Council meeting falls on a statutory holiday or occurs during a week in which SILGA, FCM or UBCM annual conferences are held, the meeting will be held on an alternate Tuesday. For the 2024 year these conferences do not conflict with any proposed meeting dates, and conference dates are as follows:

- SILGA Convention – April 30 – May 3, 2024, in Kamloops;
- FCM Convention – June 6-9, 2024, in Calgary; and
- UBCM Convention – September 16-20, 2024, in Vancouver.

Alternate Recommendation to Consider and Resolve:

THAT Council approve the 2024 City of West Kelowna Council Meeting Schedule with amendments.

REVIEWED BY

Trevor Seibel, Deputy CAO

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☐ No ☒

Attachments:

- a. Draft – 2024 Council Meeting Schedule



Council Meetings 2024 Schedule

Council Chambers, 2760 Cameron Road, West Kelowna, BC


 Regular Council Meetings 1:30 p.m.

 Regular Council Meetings 6:00 p.m.

 Statutory Holiday

 Tentative Budget Meeting

 Tentative Year-end Closure

 Training/Conference SILGA: April 30- May 3 (Kamloops) FCM: June 6-9 (Calgary) UBCM: September 16-20 (Vancouver)

Note: Committee of the Whole Meetings are scheduled on an "As Required" basis and customarily take place on the third Tuesday of the month

JANUARY						
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ACTING MAYOR SCHEDULE - 2024

Councillor Da Silva	2023-NOV-01	to	2023-DEC-31
Councillor de Jong	2024-JAN-01	to	2024-FEB-28
Councillor Friesen	2024-MAR-01	to	2024-APR-30
Councillor Johnston	2024-MAY-01	to	2024-JUN-30
Councillor Millsap	2024-JUL-01	to	2024-AUG-31
Councillor Zanon	2024-SEP-01	to	2024-OCT-31



COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

Subject: **Good Neighbour Bylaw Amendments**

Report Prepared by: Mike Cain, Bylaw Services Manager

RECOMMENDATION to Consider and Resolve:

THAT Council adopt “City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023”; and

THAT Council adopt “City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023”; and

THAT Council adopt “Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023”.

STRATEGIC AREA(S) OF FOCUS

Foster Safety and Well-Being – We will pursue through direct action, advocacy, and collaboration with local and regional service providers, investments in community health, needs-based housing, emergency preparedness, policing, and other services that foster safety and well-being in West Kelowna.

BACKGROUND

- A) On July 25, 2023, Council adopted amendments to the Good Neighbour Bylaw No. 0151, 2018, and Schedule 4 in both the Ticket Information Utilization Bylaw No. 0095, and the Bylaw Enforcement Notice Bylaw No. 0093.

During the consolidation process, two errors were discovered in the ticketing bylaws as follows:

1. The Bylaw Enforcement Notice Bylaw Schedule 4, Section 3.1 the indicated penalty was inadvertently changed from \$500.00 to \$1,000.00. As Provincial legislation restricts the maximum penalty to \$500.00 for this Bylaw, the indicated penalty needs to revert to \$500.00; and

2. A new section in the Good Neighbour Bylaw, Section 8.7, to address nuisance dust was adopted. A corresponding penalty was not inserted into the two ticketing bylaws. Therefore, Staff is recommending an indicated penalty of \$500.00 for the Bylaw Enforcement Notice Bylaw, and for incidents that warrant escalated enforcement an indicated penalty of \$1,000.00 for the Ticket Information Utilization Bylaw.

- B) Staff is recommending amending the Good Neighbour Bylaw No. 0151, Section 8.3 (b) BOULEVARD AND LANEWAY MAINTENANCE, as the current language creates the condition where property owners are only required to remove weeds from areas that are grassed. This language creates the situation where boulevard areas that are dirt or gravel are exempt from the requirement to be weed free.

Current language:

“Every owner or occupier of real property shall maintain the sidewalk, boulevard, and land adjacent to their real property and in particular shall:

- (a) remove accumulations of filth, leaves, rubbish, discarded material, hazardous objects and materials which obstruct a drainage facility;
- (b) in keeping with the reasonable standard of maintenance in the area, keep grassed areas trimmed and free of all weeds;**
- (c) keep in good repair and up to City standards, all driveway crossings;
- (d) trim and maintain all plantings;
- (e) remove all filth, leaves, rubbish, discarded materials, hazardous objects and materials from all boulevards and sidewalks;
- (f) maintain a clear and unobstructed area, including snow and ice, having a radius of 1 (one) meter around a fire hydrant, as well as a clear 1 (one) meter width from the front of the hydrant to the nearest roadway.”

Recommended new language:

“Every owner or occupier of real property shall maintain the sidewalk, boulevard, and land adjacent to their real property and in particular shall:

- (a) remove accumulations of filth, leaves, rubbish, discarded material, hazardous objects and materials which obstruct a drainage facility;
- (b) in keeping with the reasonable standard of maintenance in the area, keep free of all weeds, and grassed areas trimmed.**
- (c) keep in good repair and up to City standards, all driveway crossings;
- (d) trim and maintain all plantings;
- (e) remove all filth, leaves, rubbish, discarded materials, hazardous objects and materials from all boulevards and sidewalks;
- (f) maintain a clear and unobstructed area, including snow and ice, having a radius of 1 (one) meter around a fire hydrant, as well as a clear 1 (one) meter width from the front of the hydrant to the nearest roadway.”

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
September 12, 2023	<p>THAT Council give first, second, and third reading to the “City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023”; and</p> <p>THAT Council give first, second, and third reading to the “City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023”; and</p> <p>THAT Council give first, second, and third reading to the “Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023”.</p> <p><u>CARRIED UNANIMOUSLY</u></p>	C131/23

Alternate Recommendation to Consider and Resolve:

That Council postpone the adoption of “City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023”; and

That Council postpone the adoption of “City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023”; and

That Council postpone the adoption of “Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023”.

Should Council wish to postpone adoption, it is requested Council provide direction on any further information that is required before rescheduling the bylaw for Council’s consideration.

REVIEWED BY

Brad Savoury, Director of Legal Services and Risk Management

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☐ No ☒

Attachments:

- 1) City of West Kelowna Good Neighbour Amendment Bylaw No. 0151.04, 2023
- 2) City of West Kelowna Ticket Information Utilization Amendment Bylaw No. 0095.51, 2023
- 3) Bylaw Enforcement Notice Bylaw Amendment Bylaw No. 0093.53, 2023

CITY OF WEST KELOWNA BYLAW NO. 0151.04

A BYLAW TO AMEND THE GOOD NEIGHBOUR BYLAW

WHEREAS the Council of the City of West Kelowna desires to amend the Good Neighbour Bylaw.

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna in open meeting assembled, hereby enacts as follows:

1. **Title**

This Bylaw may be cited as the "CITY OF WEST KELOWNA GOOD NEIGHBOUR AMENDMENT BYLAW NO. 0151.04, 2023."

2. **Amendments**

The "CITY OF WEST KELOWNA GOOD NEIGHBOUR BYLAW NO. 0151, 2018" is hereby amended as follows:

2.1 By deleting Section 8.3 Boulevard and Laneway Maintenance that reads:

"Every owner or occupier of real property shall maintain the sidewalk, boulevard, and land adjacent to their real property and in particular shall:

- (a) remove accumulations of filth, leaves, rubbish, discarded material, hazardous objects and materials which obstruct a drainage facility;
- (b) in keeping with the reasonable standard of maintenance in the area, keep grassed areas trimmed and free of all weeds;
- (c) keep in good repair and up to City standards, all driveway crossings;
- (d) trim and maintain all plantings;
- (e) remove all filth, leaves, rubbish, discarded materials, hazardous objects and materials from all boulevards and sidewalks;
- (f) maintain a clear and unobstructed area, including snow and ice, having a radius of 1 (one) meter around a fire hydrant, as well as a clear 1 (one) meter width from the front of the hydrant to the nearest roadway."

and replacing it with:

"Every owner or occupier of real property shall maintain the sidewalk, boulevard, and land adjacent to their real property and in particular shall:

- (a) remove accumulations of filth, leaves, rubbish, discarded material, hazardous objects and materials which obstruct a drainage facility;
- (b) in keeping with the reasonable standard of maintenance in the area, keep free of all weeds, and grassed areas trimmed;
- (c) keep in good repair and up to City standards, all driveway crossings;
- (d) trim and maintain all plantings;
- (e) remove all filth, leaves, rubbish, discarded materials, hazardous objects and materials from all boulevards and sidewalks;

- (f) maintain a clear and unobstructed area, including snow and ice, having a radius of 1 (one) meter around a fire hydrant, as well as a clear 1 (one) meter width from the front of the hydrant to the nearest roadway.”

READ A FIRST, SECOND AND THIRD TIME THIS 12TH DAY OF SEPTEMBER, 2023
ADOPTED THIS DAY OF

MAYOR

CORPORATE OFFICER

CITY OF WEST KELOWNA

BYLAW NO. 0095.51

A BYLAW TO AMEND THE TICKET INFORMATION UTILIZATION BYLAW

WHEREAS the Council of the City of West Kelowna desires to amend the City of West Kelowna Ticket Information Utilization Bylaw No. 0095;

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as the “CITY OF WEST KELOWNA TICKET INFORMATION UTILIZATION AMENDMENT BYLAW NO. 0095.51, 2023.”

2. Amendments

“City of West Kelowna Ticket Information Utilization Bylaw No. 0095”, Schedule 4, Ticket Offences for City of West Kelowna Good Neighbour Bylaw No. 0151, 208 is hereby amended by adding to the table, in its appropriate location the following:

“Fail to prevent emission of dust	8.7(a,b)	1000.00”
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READ A FIRST, SECOND AND THIRD TIME THIS 12TH DAY OF SEPTEMBER, 2023
ADOPTED THIS DAY OF

MAYOR

CORPORATE OFFICER

CITY OF WEST KELOWNA

BYLAW NO. 0093.53

A BYLAW TO AMEND THE BYLAW ENFORCEMENT NOTICE BYLAW

WHEREAS the Council of the City of West Kelowna desires to amend the City of West Kelowna Bylaw Enforcement Notice Bylaw No. 0093;

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as the "BYLAW ENFORCEMENT NOTICE BYLAW AMENDMENT BYLAW NO. 0093.53, 2023."

2. Amendments

"Bylaw Enforcement Notice Bylaw No. 0093" is hereby amended as follows:

- 2.1 a) By deleting the following in Schedule 4, Ticket offences for City of West Kelowna Good Neighbour Bylaw No. 0151, 2018 which reads:

"Obstruct an Officer	3.1	\$1000.00"
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and replace it with:

"Obstruct an Officer	3.1	\$500.00"
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- 2.1 b) By adding in Schedule 4, Ticket offences for City of West Kelowna Good Neighbour Bylaw No. 0151, 2018 in its appropriate location the following:

"Fail to prevent emission of dust	8.7(a,b)	500.00"
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READ A FIRST, SECOND AND THIRD TIME THIS 12TH DAY OF SEPTEMBER, 2023
ADOPTED THIS DAY OF

MAYOR

CORPORATE OFFICER



COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

Subject: **State of Local Emergency**

Report Prepared by: Corinne Boback, Legislative Services Manager / Corporate Officer

RECOMMENDATION to Consider and Resolve:

THAT Council endorse the extension of the State of Local Emergency for the McDougall Creek wildfire authorized by Mayor Gord Milsom extended on September 14, 2023, for a further week set to expire on September 21, 2023; and

THAT Council authorize a further week extension for the State of Local Emergency for the McDougall wildfire until September 28, 2023.

BACKGROUND

On August 17, 2023, the McDougall wildfire that was burning north of West Kelowna had grown exponentially by early morning and was anticipated to create an immediate threat to our community by early afternoon of that day. In order to initiate alerts and begin considering evacuation orders the City is required to declare a state of emergency. Once this is undertaken the Regional District of Central Okanagan (RDCO) then has the authority under bylaw to initiate actions through the Emergency Operations Center (EOC) to support any operations necessary to protect the community and respond to the wildfire.

Under the Emergency Program Act, s. 12, the head of a local authority may declare a State of Local Emergency relating to all or part of the jurisdictional area. As the threat was imminent, per the Act, the declaration must be made by bylaw, resolution, or order. Therefore, this authority was utilized to allow Mayor Milsom to make the declaration.

To maintain the orders and continue working towards recovery it is expected that the State of Local Emergency will be needed to extend a further week at this time.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
September 12, 2023	THAT Council endorse the State of Local Emergency declared on August 17, 2023, for one week by Mayor Gord Milsom in response to the wildfire imminent threat to West Kelowna; and THAT Council endorse the extension of the State of Local Emergency authorized by Mayor Gord Milsom extended on August 23, 2023, for a further week set to expire on August 31, 2023; and THAT Council consider and authorize that a further week extension for a State of Local Emergency until September 7, 2023, be approved.	C312/23
August 29, 2023	THAT Council endorse the State of Local Emergency declared on August 17, 2023, for one week by Mayor Gord Milsom in response to the wildfire imminent threat to West Kelowna; and THAT Council endorse the extension of the State of Local Emergency authorized by Mayor Gord Milsom extended on August 23, 2023, for a further week set to expire on August 31, 2023; and THAT Council consider and authorize that a further week extension for a State of Local Emergency until September 7, 2023, be approved.	C299/23

REVIEWED BY

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☐ No ☒



DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

File No: DP 22-26

Subject: **DP 22-26; Development Permit with Variances; 3401 Sundance Drive**

Report Prepared By: Jayden Riley, Planner III

RECOMMENDATION to Consider and Resolve:

THAT Council deny the Development Permit with Variances located at 3401 Sundance Drive (File: DP 22-26) and direct staff to close the file.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

On May 16, 2023, Council postponed consideration of the proposed development permit with variances and directed the applicant to revise the following elements of their proposal:

1. Include a turnaround and loading space on the Ensign Lane extension;
2. Secure legal access for units 52-56 (approx.) on the Ensign Lane extension;
3. Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
4. Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
5. Clearly provide a location for snow deposition on the Ensign Lane extension.

The applicant has since revised their proposal, addressing some of the items listed above, while also introducing additional variances. The May 16 report is also attached to the report (Attachment 1). Given the revisions do not address all of the previously identified items, a recommendation of denial has been included. It is noted, that an alternate

recommendation of postpone has been included to ensure the previously identified items are completed.

PROPERTY DETAILS			
Address	3401 Sundance Drive		
PID	028-172-001		
Folio	3641391.497		
Lot Size (m²)	1.29 ha (12,990 m ²)		
Owner	Gatehouse Developments Inc.	Agent	Carlo DiStefano, DiStefano Jaud Architecture
Current Zoning	R4 (Medium Density Multiple Family)	Proposed Zoning	N/A
Current OCP	Low Density Multiple Family	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	57-unit townhouse development (previously 58 units)
Development Permit Areas		Hillside, Sensitive Terrestrial Ecosystem, Form and Character	
Hazards		Hillside	
Agricultural Land Reserve		No	

ADJACENT ZONING & LAND USES			
North	^	P1 – Parks and Open Space	
East	>	R1 – Single Detached Residential	
West	<	R4 – Medium Density Residential	
South	v	R1 – Single Detached Residential	

PROPERTY MAP



DISCUSSION

Proposal Summary

The applicant is seeking a Form and Character, Hillside, and Sensitive Terrestrial Development Permit for a 57-unit townhouse development comprised of 20 units fronting Sundance Drive and 37 units fronting Ensign Quay Lane and the Ensign Lane extension (Figure 1, Attachment 3). The proposal has been modified to address some of the items, as directed by Council, when the proposal was first considered on May 16, 2023. It is noted that this has caused some minor revisions to the proposal, specifically related to the units fronting the Ensign Lane extension. All revision and a list of remaining and modified variances are outlined in the following sections of this report.



Figure 1: Site plan for revised 57-unit townhome development.

Revisions to Proposal to Address Council Direction from May 16, 2023

1. *Include a turnaround and loading space on the Ensign Lane extension.*

- The proposal now includes an additional turnaround for passenger vehicles near the terminus of the Ensign Lane extension and loading space (Figure 2).

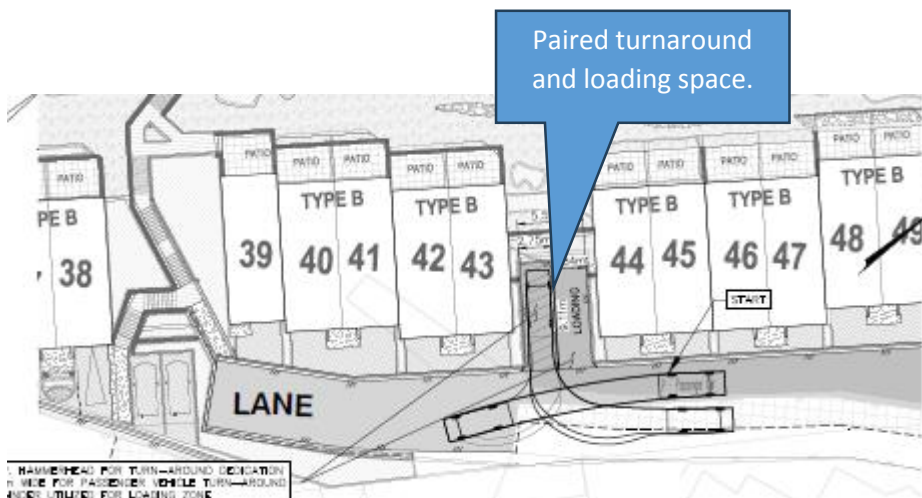


Figure 2: Revised turnaround with loading space

- The loading space dimensions will however require a variance to reduce the minimum dimensions (width) of a loading space from 3.0 m x 9.0 m to 2.8 m x 9.0 m – see variance No. 6.

2. Secure legal access to units 50-56 on the Ensign Lane extension.

- The applicant has revised their design to circumvent the private triangular piece of property belonging to the adjacent property owner (Figure 3). This has created a second egress to Ensign Lane, north-east of the triangular piece of land.
- The modification to the Ensign Lane extension access (units 39-57) now occurs over the subject property and private access easement only.
- Staff have concerns that this access poses practical connection due to grading changes that will need to be addressed as part of detailed design. These grading challenges may result in disturbance to lands outside of the applicant's property.

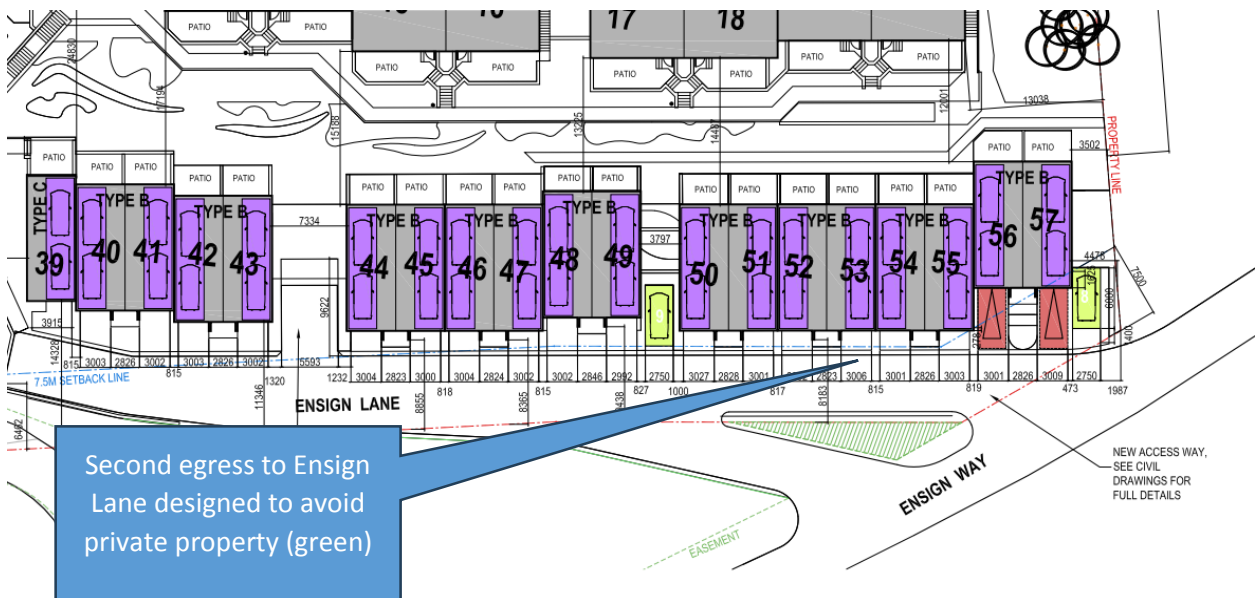


Figure 3: New laneway access to circumvent trespass on adjacent private property.

3. Reallocate the number of visitor spaces for each laneway to be proportional to the number of units.

- The applicant did not make any revisions to the number of visitor parking spaces.
- While the proposal does contain an adequate number of visitor spaces overall, in accordance with S.4.6 of Zoning Bylaw No. 0265, only two visitor parking spaces are provided on the Ensign Lane extension for 18 units (Figure 4). A proportional amount would equate to a minimum of four spaces.
- Staff have concerns related to the impacts of site circulation due to the limited visitor parking provided on the laneway proportional to the units and the absence of any on-street/lane parking and shortened driveways adjacent to the lane. Visitors to the lower laneway units would be required to drive to the site via Shannon Lake Road > Shannon Way > Sundance Drive to access parking on

Sundance Drive (higher proportion of spaces), then walk down a series of stairs to access the lower units. This is inconsistent with the City's Development Permit Guidelines which includes:

- S.4.3.4.34: Visitor parking should be easily identifiable and located close to site entrances, or on large sites, conveniently located throughout the site.
- Finally, this indirect route to the site is prohibitive to those with mobility challenges.



Figure 4: Visitor parking spaces not proportional to number of units and prohibitive to those with mobility challenges.

4. *Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus.*

- The applicant has revised their proposal to widen the lane at the terminus of the Ensign Lane extension adjacent to Unit 39 to accommodate turning movements for a 5.6 m passenger vehicle (Figure 5). Staff note that a crew cab Ford F150 with a 5.5 ft (short) box is 5.8 m in length.

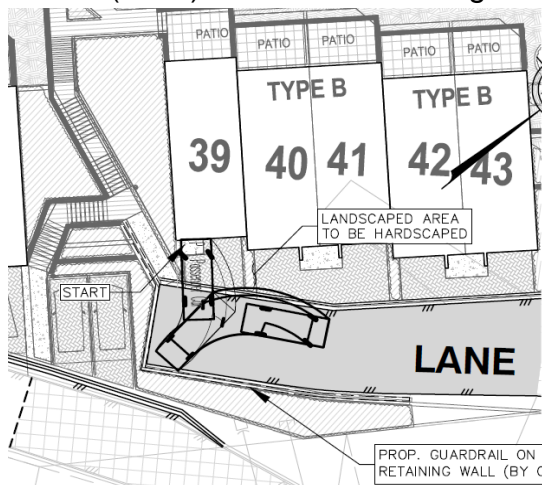


Figure 5: Turning movement for access to Unit 39

- Staff have noted that the landscape plan proposes a 0.4 m landscape wall and shrub bed located within the path of the turning radius. The applicant has confirmed they would hardscape this area to ensure the turning movements would be accommodated. This would be noted on the permit drawings and confirmed at time of building permit.

- The drawings provided by the applicant show a guardrail proposed on top of the retaining wall south of unit 39 (Figure 5), to be designed by others at time of building permit and in conjunction with the 3.5 m retaining wall.

are unable to meet the hillside standard width due to existing site constraints (retaining wall and house - Figure 6a & 6b). These sections are considered legal non-conforming, however do present circulation challenges given the significant increase in traffic from the units.

- Some of the laneway widening, driveway, loading and turnaround construction is proposed within the private easement boundary. Improvements within the private easement would require approval from neighbours that are party to the easement agreement; therefore, this work may not be completed as proposed. Staff have included an alternate motion that requires confirmation of the approval of easement holders prior to issuance of the development permit.



Figure 6b: Ensign Quay Lane access to units 21-38 (18 units) – ~5.6 m width is constrained by existing retaining wall and single family residence.

Summary of Variances (Zoning Bylaw No. 0265)

The following is a list of the variances proposed by this development. Only those that are new to the proposal or have been modified since the May 16 report (Attachment 1) will be discussed in the following sections of this report.

1. Reduction to the minimum required front parcel boundary (Sundance Drive) setback from 6.0 m to 4.0 m (varies);
2. Reduction to the minimum required interior parcel boundary setback from 4.5 m to 3.5 m **(NEW)**;
3. Reduction to the minimum required rear parcel boundary setback from 7.5 m to 5.97 m (varies) **(NEW)**;
4. Reduction to the minimum required distance from required visitor parking spaces (7) to the front parcel boundary (Sundance Drive) from 3.0 m from 0.0 m (varies);
5. Reduction to the minimum required number of loading spaces from 4 to 2 spaces **(NEW)**;
6. Reduction to the minimum dimensions of a (1) loading space from 3.0 m x 9.0 m to 2.8 m x 9.0 m **(NEW)**;

7. Increase the maximum height of three retaining walls from 2.5 m to 3.5 m **(NEW)**; and
8. Increase the width of driveway crossings at the property line from 7.0 m up to 9.4 m (varies).

Additional or Modified Variances

2. Reduction to the minimum required interior parcel boundary setback distance from 4.5 m to 3.5 m S.10.11.5(g)iii – (Figure 7).

- The addition of the turnaround and loading spaces has required the re-siting of the units on the Ensign Lane extension, including unit 57 to be situated within the minimum required interior parcel boundary setback distance from 4.5 m to 3.5 m at its closest distance.
- Staff are supportive of this variance as impacts to the adjacent property is anticipated to be negligible.



Figure 7: Variance to the interior parcel boundary setback

3. Reduction to the minimum required rear parcel boundary setback distance from 7.5 m to 5.97 m (varies) (S.10.11.5(g)ii) (Figure 8).

- This variance was identified following the May 16 Council meeting after a re-interpretation of the rear parcel boundary and its application to public lanes as opposed to private lanes. Due to this laneway being private, this property line is considered as a rear boundary and a 7.5 m setback is required as opposed to 6.0 m for a double fronting property. The variance affects proposed units 21-27 and 57.
- Staff are supportive of this variance given that the proposal would otherwise meet a 6.0 m front setback distance from the parcel boundary, which is determined to be adequate separation.



Figure 8: Variance to the rear parcel boundary setback

5. Reduction to the minimum number of loading spaces from four to two spaces (S.4.10.1(a)) (Figure 9).

- This variance has been reduced from the original submission with the addition of a loading space on the Ensign Lane extension (Figure 9).
- Each laneway now includes one loading space.
- Loading can otherwise be accommodated on the Sundance Drive units via driveway or on-street parking.
- Loading space No. 1 will require approval of the adjacent property owner due to the installation of a retaining wall within the easement area.

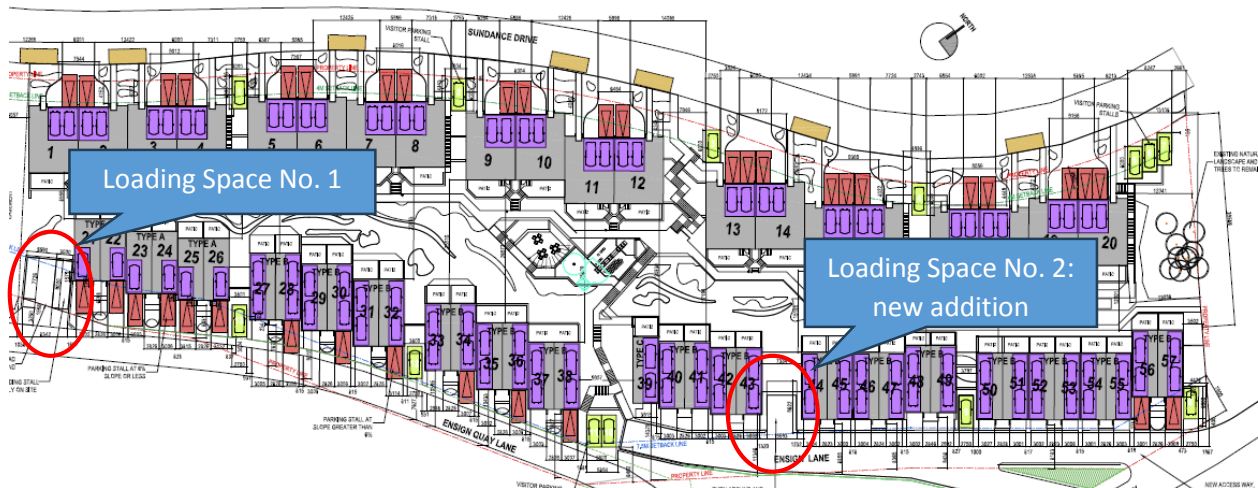


Figure 9: Variance to the number of loading spaces.

6. To reduce the minimum dimensions of a loading space from 3.0 x 9.0 m to 2.84 m x 9.0 m (S.4.10.2(a)) (Figure 10).

- This variance is required to pair the loading space with a turnaround on the Ensign Lane extension and not impact the unit count.
- The width of the loading space is slightly less than standard, by 0.16 m.
- Staff do not have concerns related to this variance.

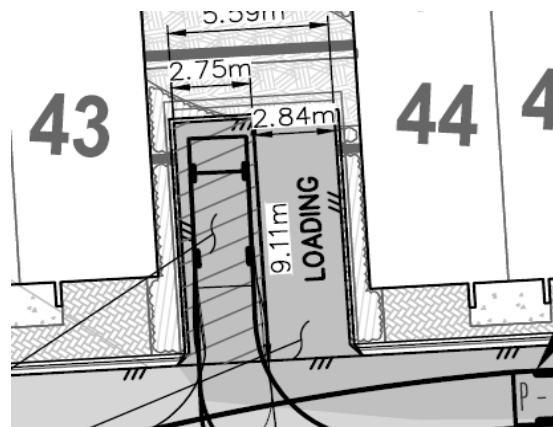


Figure 10: Variance to loading space dimensions.

7. To increase the maximum height for three individual retaining walls from 2.5 m to 3.5 m (S.3.14.1) (Figure 11).

- This variance has been modified to include an additional 3.5 m retaining wall to accommodate the revised laneway access design and visitor parking space adjacent to Unit 57.

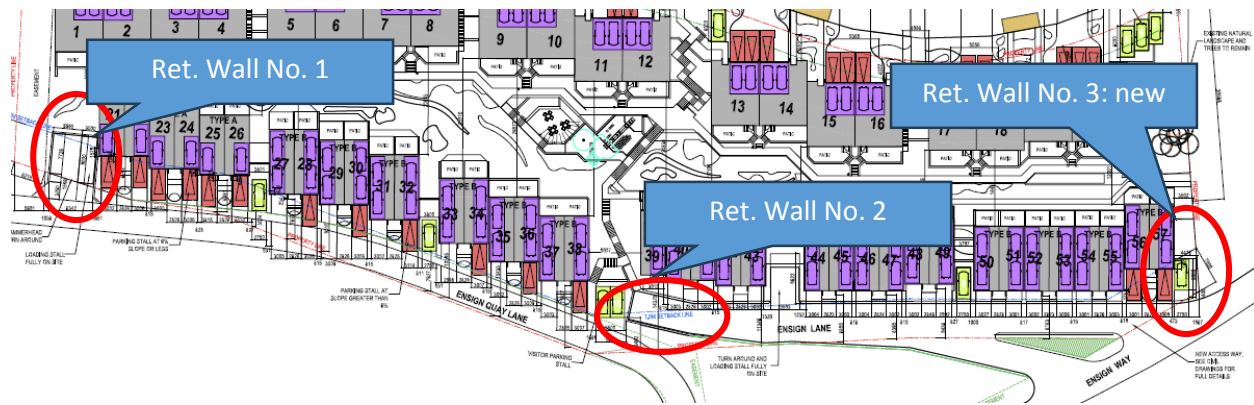


Figure 11: Variance to retaining wall height x3.

Public Notification

In accordance with the *Local Government Act*, staff have mailed 92 notices to residents and tenants located within 100 m of the subject property to provide an opportunity to comment on the proposal. Any submissions received will be attached to this report prior to its publication.

Should Council decide to postpone consideration of the proposal, no further notification will be required for subsequent iterations of the proposal subject to there being no additional variances proposed or a reduction to those currently included.

CONCLUSION

Staff recommend that Council deny the application based on the following rationale:

- The application has not been revised to align with the previous direction provided by Council via the resolution from May 16, 2023.
- The number of units proposed, particularly on the Ensign Lane extension, have caused numerous variances to be requested and are anticipated to cause significant impacts to new and existing residents from a lack of visitor parking and designated snow storage areas, as well as safety challenges related to pedestrian and vehicle site circulation.
- The proposal requires approval from adjacent easement holders; feedback from some neighbouring residents suggests approvals will not be provided.
- The application is not consistent with the City's Development Permit Guidelines, particularly around access to visitor parking, which should be easily identifiable and located close to site entrances, or on large sites, conveniently located throughout the site.
- Concerns related to visitor pedestrian access for those with mobility challenges.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
May 16, 2023	<p>THAT Council postpone consideration of the proposed multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) and direct the applicant to revise the following elements of their proposal:</p> <ul style="list-style-type: none">• Include a turnaround and loading space on Ensign Lane;• Secure legal access for units 52-56 (approx.) on the Ensign Lane extension;• Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;• Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and• Clearly provide a location for snow deposition on Ensign Lane.	C213/23

Alternate Recommendation to Consider and Resolve:

1. Postpone consideration of Development Permit (file DP 22-26)

THAT Council postpone consideration of the proposed development permit (DP 22-26) and direct the applicant to revise the proposal to include designated snow storage locations, proportional visitor parking on the Ensign Lane extension, and ensure works within the easement or on neighbouring properties are addressed prior to issuance.

Should Council resolve to postpone consideration, further direction to staff is requested.

2. Authorize Approval of Development Permit with Variances (file DP 22-26), with issuance subject to agreement from easement holders

THAT Council authorize approval of a development permit with variances (DP 22-26) with issuance of the permit subject to approval by affected easement or land holders for any works proposed within the access easement or neighbouring properties.

REVIEWED BY

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. May 16, 2023, Council Report (DP 22-26)
2. May 16, 2023, Late Item Submissions
3. Architectural Submission (Site Plan, Elevations, Floor Plan, Renderings)
4. Landscape Plan
5. Draft Development Permit DP 22-26
6. Delegation Request – Ryan Jones
7. Submission as of September 21, 2023



DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: May 16, 2023

From: Paul Gipps, CAO

File No: DP 22-26

Subject: **DP 22-26; Development Permit with Variances; 3401 Sundance Drive**

Report Prepared By: Jayden Riley, Planner III

RECOMMENDATION to Consider and Resolve:

THAT Council postpone consideration of the proposed multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) and direct the applicant to revise the following elements of their proposal:

- Include a turnaround and loading space on Ensign Lane;
- Secure legal access for units 53-56 (approx.) on Ensign Lane;
- Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
- Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
- Clearly provide a location for snow deposition on Ensign Lane.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

The subject property is currently vacant, located in the Shannon Lake neighbourhood between Ensign Quay Lane and Ensign Lane to the south-east and Sundance Drive to the north-west. The property includes steep hillsides, sloping downward from Sundance Drive toward the private laneways, which are currently shared by nine existing residences and the subject property via an access easement.

PROPERTY DETAILS			
Address	3401 Sundance Drive		
PID	028-172-001		
Folio	3641391.497		
Lot Size (m²)	1.29 ha (12,990 m ²)		
Owner	Gatehouse Developments Inc.	Agent	Carlo DiStefano, DiStefano Jaud Architecture
Current Zoning	R4 (Medium Density Multiple Family)	Proposed Zoning	N/A
Current OCP	Low Density Multiple Family	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	58-unit townhouse development
Development Permit Areas	Hillside, Sensitive Terrestrial Ecosystem, Form and Character		
Hazards	Hillside		
Agricultural Land Reserve	No		

ADJACENT ZONING & LAND USES		
North	^	P1 – Parks and Open Space
East	>	R1 – Single Detached Residential
West	<	R4 – Medium Density Residential
South	v	R1 – Single Detached Residential

NEIGHBOURHOOD MAP



PROPERTY MAP



DISCUSSION

Legislative Requirements

Council has the authority under S.490 of the *Local Government Act* to issue a Development Permit. Section 498 of the *Local Government Act* gives Council the authority to issue a Development Variance Permit that varies, in respect of the land covered in the permit, the provision of the Zoning Bylaw.

Proposal

The applicant is seeking a Form and Character, Hillside, and Sensitive Terrestrial Development Permit for a 58-unit townhouse development comprised of 20 units fronting Sundance Drive and 38 units fronting Ensign Quay Lane and Ensign Lane (Figure 1, Attachment 2).

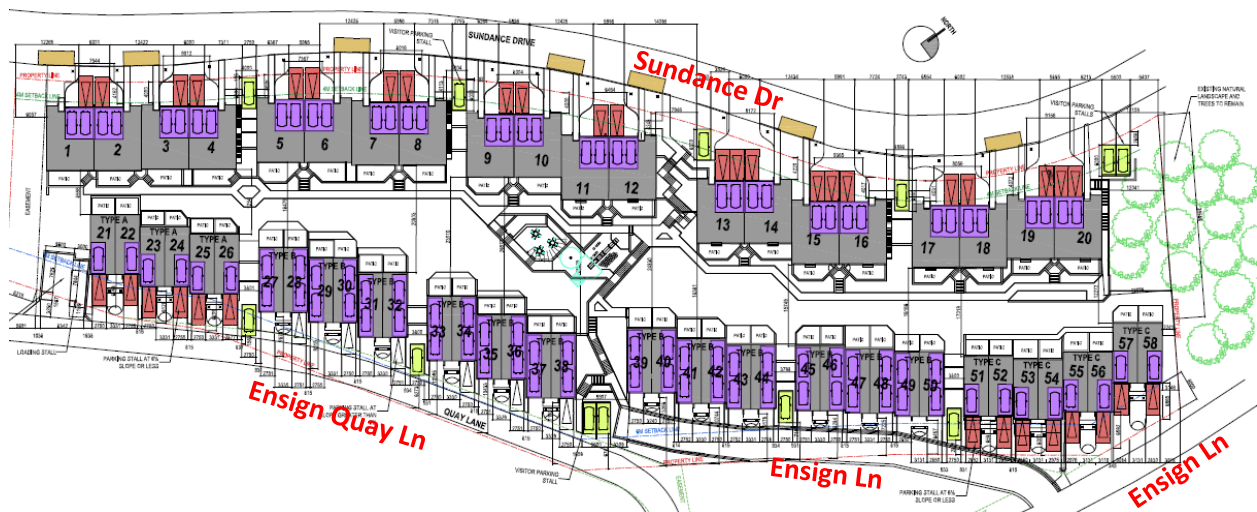


Figure 1: Site Plan

Due to topographical conditions combined with the proposed density, this application contains several variances to accommodate the proposed number of units related to siting, off-street parking, and site circulation. The following variances (7) are included in this proposal:

1. Reduction to the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies);
2. Reduction to the minimum required setback of loading and visitor parking spaces from the front or exterior parcel boundary from 3.0 m from 0.0 m (varies);
3. Reduction to the minimum required number of loading spaces from 4 to 1 space(s);
4. Reduction to the minimum dimensions of a loading space from 3.0 m x 9.0 m to the dimensions noted on site plan;
5. Increase the maximum height of two retaining walls from 2.5 m to 3.5 m;
6. Increase the width of driveway crossings at the property line from 7.0 m to 9.4 m (varies); and
7. Forgo the requirement to install a turnaround at or near the terminus of a private hillside lane.

Site Specific Considerations

The subject property is located upland from existing residences on Ensign Lane and Ensign Quay Lane. Both laneways include a series of private easements for access between the subject and adjacent properties, as well as Statutory Right of Ways for public utilities and emergency access. Subject to Council approval, the laneways would each be shared between the (9) existing residents and the proposed 38 townhouse units by a six-metre-wide drive aisle that does not permit on-street parking. Due to the topography of the site, retaining walls with height variances are required to accommodate a turnaround and loading space on the segment of Ensign Quay Lane and an extension of the physical laneway on Ensign Lane to access proposed units 39-44 (Attachment 2, Figure 1).

Bylaw and Policy Review

Official Community Plan Bylaw No. 0100

Development Permit Areas

The proposed development is subject to three of the City's DPAs: 1) Multiple Family and Intensive Residential, 2) Hillside, and 3) Sensitive Terrestrial Ecosystem.

1. **Multiple Family and Intensive Residential DPA** guidelines ensure that residential development is well designed and appropriately integrated into the community through use of good urban design principles. This includes strong design, amenity space contributions, and architectural focal points. The proposal is found to be generally consistent with the form and character, and building materials guidelines of this DPA; however, the proposal is less consistent with the guidelines in terms of adjacent use considerations, and potential hazards for

vehicle and pedestrian circulation due to disproportionate allocation of visitor parking. More detail is provided in the following sections of this report.

2. The **Hillside DPA** guidelines are applied to address steep slopes to ensure public safety and the protection of environmentally sensitive areas. This includes addressing rockfall hazard mitigation, location of roads and laneways, and setbacks to disturbance zones. The applicant has submitted a geotechnical report that has identified existing site conditions and provides recommendations related to site preparation and foundation design, site grading, rock slopes, drainage, pavement design and utility installation, and has confirmed the development, as proposed, is safe for the intended use. The recommendations of the geotechnical report and memo are included in the attached permit (Attachment 1). The proposal is found to be in general accordance with the DPA guidelines; however, the proposed laneway extension (Ensign Lane) it is not due to the adjacent steep slope, creating a safety hazard. More information is provided in the following sections of the report.

3. The **Sensitive Terrestrial Ecosystem DPA** guidelines are applied to all land as having environmental values. These guidelines ensure that development considers relevant provincial legislation, that site design is consistent with the environmental reporting, and to minimize disturbance to highly sensitive environmental areas. As part of this application, an Environmental Assessment was submitted, noting the area to contain Moderate (ESA 2) and Low (ESA 3) Environmentally Sensitive Areas. Most of the disturbance is proposed within the ESA 3 area. A portion of the ESA 2 area to the north-east is being preserved. Recommendations of the environmental report would be included as part of the permit conditions.

Form and Character Review

The proposal contains a total of 58 townhome units within 11 buildings; this includes 20 units on Sundance Drive (units 1-20), 18 units on Ensign Quay Lane (units 21-38), and 20 units on Ensign Lane (units 39-58). Three colour variations (i.e., dark, medium, light) are proposed to be applied to each grouping of buildings.

Building materials for all buildings include Hardie lap siding, brick – pacific art stone, Hardie plank, Hardie board trim lintel, painted fascia, dual black asphalt shingle roofing, and aluminum railing with glass panels and pickets – full architectural drawings are provided in Attachment 2. A list of additional features unique to each unit type is include below:

- Sundance Drive (Units 1-20):
 - Three storeys, three bedrooms, and double (parallel) garages.
 - Each unit contains an upper (entry) floor with garage, living area and master bedroom, mid-level floor with two bedrooms, living space and balcony, and a lower-level basement with patio.
 - Each unit is accessed by a paired driveway.



Figure 2: Street elevation (Sundance Dr., Units 1-20)



Figure 3: Rear elevation (Sundance Dr., Units 1-20)



Figure 4: Rendering of Sundance Dr. units with colour variation between two buildings, Units 1-20

- Ensign Quay Lane (Units 21-38)
 - Contains both “Type A” and “Type B” units.
 - Type A units are contained in one six-unit building (units 21-26) and include single garages with one required tandem parking space on the driveway in front of each (6) garage.
 - Type B units (units 27-38) are contained in two six-unit buildings and include double (tandem) garages with no driveway parking.
 - Each unit contains a lower (entry)-level with garage and flex room, mid-level living space with rear balcony, and three bedrooms at the top level.



Figure 5: Front / lane elevation (units 21-38 and 39-58)



Figure 6: Rear elevation (units 21-38 and 39-58)



Figure 7: Rendering of lane units with colour variation (units 21-38 and 39-58)

- Ensign Lane (Units 39-58)
 - Contains both “Type B” and “Type C” units.
 - Type B units (units 39-50) are contained in two six-unit buildings and include double (tandem) garages with no driveway.
 - Type C units are contained in one eight-unit building (units 51-58) and include single garages with one required tandem parking space on the driveway in front of each (8) garage.
 - Identical to the units on Ensign Quay Lane, each unit contains an entry (lower)-level with garage and flex room, mid-level living space with rear balcony, and three bedrooms at the top level.



Figure 8: Laneway units (combined) streetscape with colour variation (units 21-58)

Landscaping / Amenity Space

A landscape plan was submitted with the application that proposes a common amenity space between the upper (Sundance Dr.) and lower (laneway) units accessible to all owners, providing connectivity through the property (Attachment 3, Figure 9).

The proposed landscaping consists of a mixture of concrete pavers, decorative rocks, dry meadow slope retention hydro-seed, coarse boulder slope retention, composite wood decking, xeriscape shrub beds, as well as a mixture of native trees, shrubs, dwarf conifers, grasses, perennials, and ground covers. The interior amenity area also includes a series of landscape walls and stairs leading to a main boardwalk with handrails, as well as a seating area with trellis and bar tables and a playground area (Figure 9). See Attachment 3 for full details.

The proposed landscaping estimate is \$320,064.72. Subject to Council approval, and in accordance with the City's Development Applications Procedures Bylaw No. 0260, 125% (\$400,080.90) of this amount would be required as a condition of permit issuance – see Alternate Motion and Attachment 1.



Figure 9: Landscape plan / site plan, rendering, and conceptual drawing.



Figure 10: Playground area (proposed for centre of subject property), rendering and sample image.

Zoning Bylaw No. 0265

The subject property is zoned Medium Density Multiple Residential (R4). The proposed development is generally consistent with the siting regulations of the R4 zone except for the front parcel boundary setback (Sundance Drive), but otherwise meets maximum coverage (50%) and height (12.0 m up to max. three storeys). However, the proposal does contain seven variances, with the majority related to the Bylaw's off-street parking regulations. An overview of the proposed variances is provided below.

Proposed Zoning Bylaw Variances:

1. S.10.11.4(g)(i) – Reduce the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies), in accordance with the site plan (Attachment 2, Figure 1 and 11). This variance affects all five buildings located on Sundance Drive. Recent amendments to the Bylaw allowing for setback measurements to be taken from the back of a curb or sidewalk were not added to the R4 Zone.

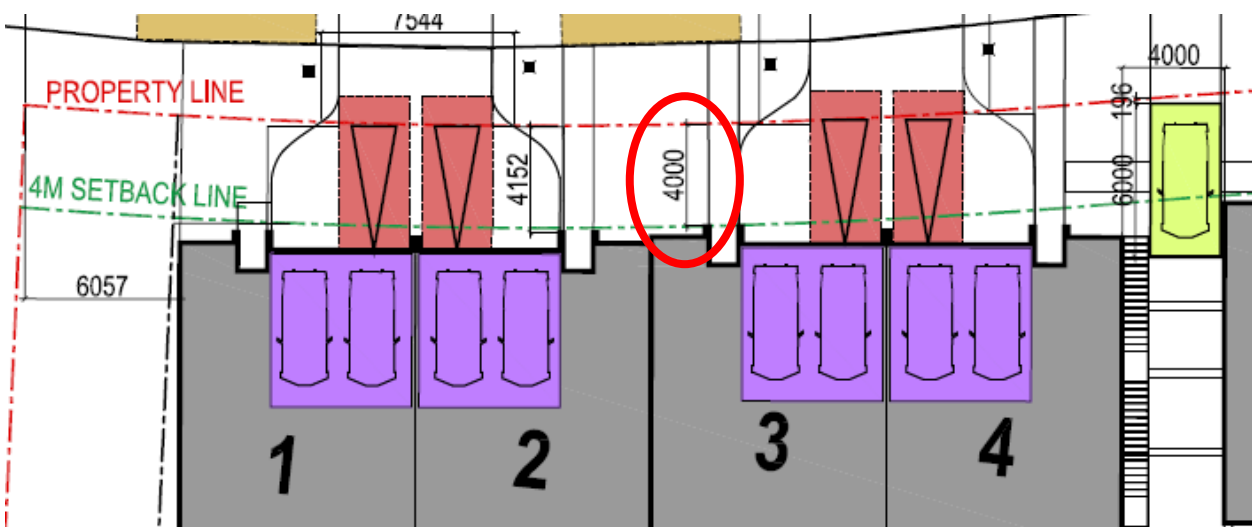


Figure 11: Setback variance for Sundance Units (1-20) – example of Building 1, units 1-4.

Since the Sundance Drive Right of Way will accommodate 3-4 metres of driveway from the curb to the property line, plus the additional 4 m setback (minimum) from

the property line to the buildings, vehicle overhang into the physical roadway is not anticipated. Staff support this variance.

2. S.4.3.2(b) – Reduce the minimum required distance between the required parking, loading and visitor parking spaces to the front or exterior parcel boundary from 3.0 m from 0.0 m (varies), in accordance with the site plan (Attachment 2, Figure 12).



Figure 12: Site plan, parking setback variances with insert.

This proposed variance affects nine of 12 visitor spaces, one (and sole) loading space, and 13 required townhouse parking spaces proposed in front of the garages for the Type A and C units located on the laneways (i.e., units 21-26, 51-56, and 58).

Staff have concerns about the potential impact of these variances, specifically in regard to the laneway and site circulation.

The intent of this bylaw regulation is to provide a buffer between more intensive multiple-family surface parking areas and public or private roadways, typically via landscaping. In this circumstance, the regulation is applied due to required parking being proposed in tandem with, and in front, of the single garages adjacent to the laneway. While this particular application of the parking regulation was not specifically considered with its original intent, it does reflect overall site design

challenges to accommodate the overall density and does have the potential to contribute toward site circulation challenges and periodic obstruction of the laneway.

3. S.4.10.1(a) – Reduce the minimum required number of loading spaces from four to one space(s), subject to a variance in the dimensions of the loading space (see Variance No. 4, Figure 13).

The number of loading spaces is proposed to be reduced from four to one space(s) to maximize the number of units on the site. The proposal includes a single loading space near the terminus of Ensign Quay Lane, adjacent to a proposed emergency vehicle turnaround. No loading space is proposed on the segment of Ensign Lane, and no loading spaces are proposed from Sundance Drive.



Figure 13: Site plan with loading space

Staff have concerns with a variance to reduce the required number of loading spaces, due to potential circulation and safety impacts to Ensign Lane. Ideally, the proposal would include a loading space on each segment of laneway for each grouping of townhome buildings (for example, units 21-38 and 39-58). In this case, only units 21-38 are served by a loading space. Without a loading space on Ensign Lane, it is anticipated that moving vehicles would temporarily block the laneway and would need to reverse in or out. Staff do not support this variance and recommend that a loading space is provided on Ensign Lane.

The lack of a loading space on Sundance Drive is not anticipated to create significant impacts due to the additional driveway depth between the property line and the curb and availability of on-street parking.

4. S.4.10.2(a) – Reduce the minimum required dimensions of a loading space from 3.0 m x 9.0 m to 7.8 m x 3.0 m (Attachment 2, Figure 14).

Related to Variance No. 3 above, the single loading space proposed on Ensign Quay Lane cannot be adequately accommodated on the subject property (Figure 14).

The construction of the loading space must also occur within the easement boundary contained on the adjacent property. The access easement permits maintenance of the physical laneway. The easement boundary also widens at this point of the laneway, so vehicles using the loading space are not anticipated to obstruct the pass or repass of vehicles. Staff support the proposed variance.

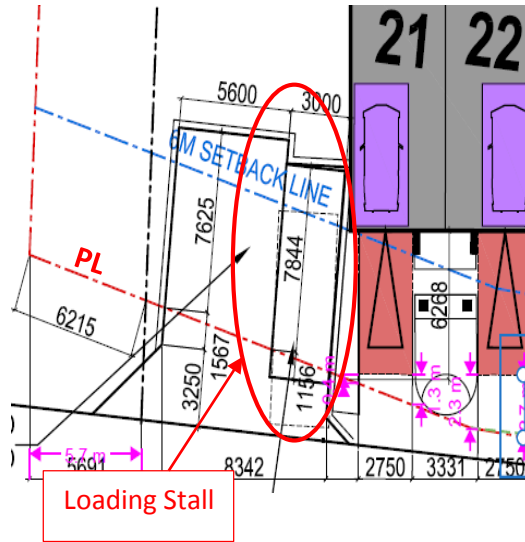


Figure 14: Loading space dimensions.

5. S.3.14.1 – Increase the maximum height of two retaining walls from 2.5 m to 3.5 m (Figures 15-18).

The proposal includes two over-height retaining walls up to a maximum 3.5 m. The first retaining wall (Figure 15) is proposed to accommodate the emergency turnaround at the terminus of Ensign Quay Lane. Staff are supportive of this variance, given the challenging topography of the site, the critical purpose of the turnaround, and the limited impacts to adjacent properties.

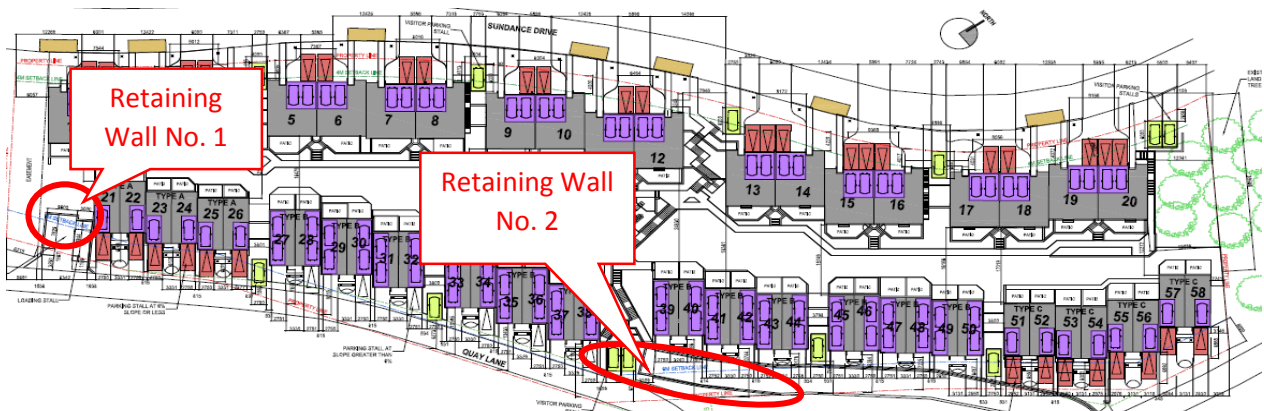


Figure 15: Over-height retaining walls (2)

The second over-height retaining wall (Figure 15-18) is proposed to extend the existing physical laneway on Ensign Lane to accommodate vehicle access/egress to units 39-44, as the existing site topography is steep at this location. The wall is

proposed to be located adjacent to the easement boundary but not encroach within it or the adjacent property.

Staff have do not have concerns with the height of the wall, but do have concerns due to its location and the proximity of the extended portion of laneway to the units near its terminus as it relates to on-site circulation and safety. Proposed units 39 and 40 are likely to encounter challenges with egress from their tandem garages given the minimal space available for turning. Where turning is possible, the wall results in an immediate 3.5 m drop in elevation to surrounding steep slopes and visitor parking spaces. Winter conditions and snow storage at the terminus of the lane is also likely to further exacerbate this hazard.

Staff are recommending that Council direct the applicant to revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus.

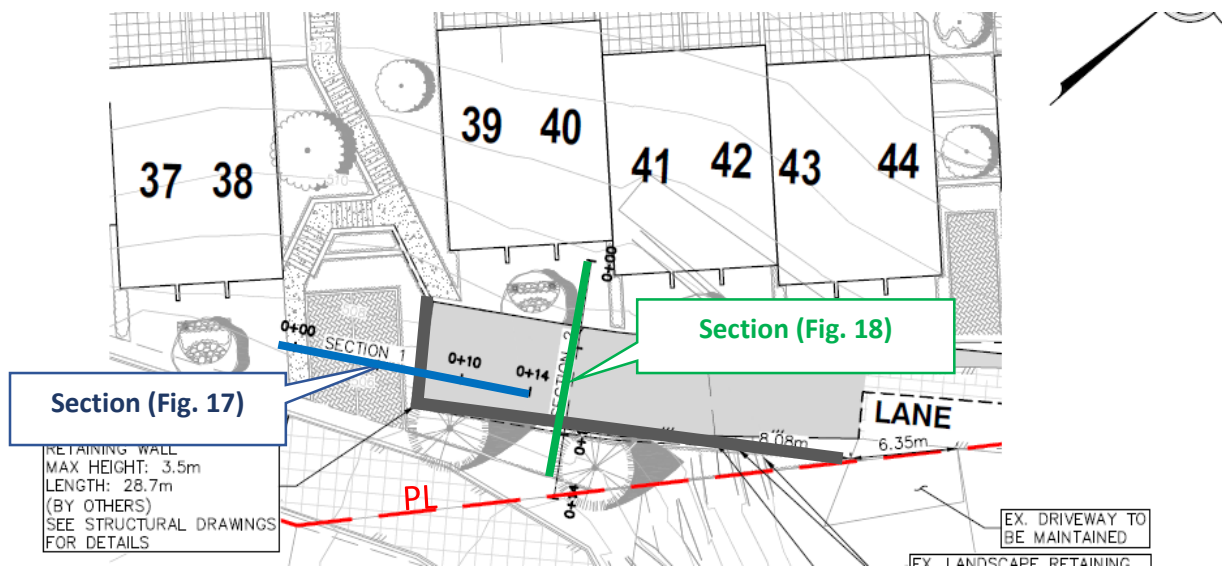


Figure 16: Over-height retaining wall No. 2.

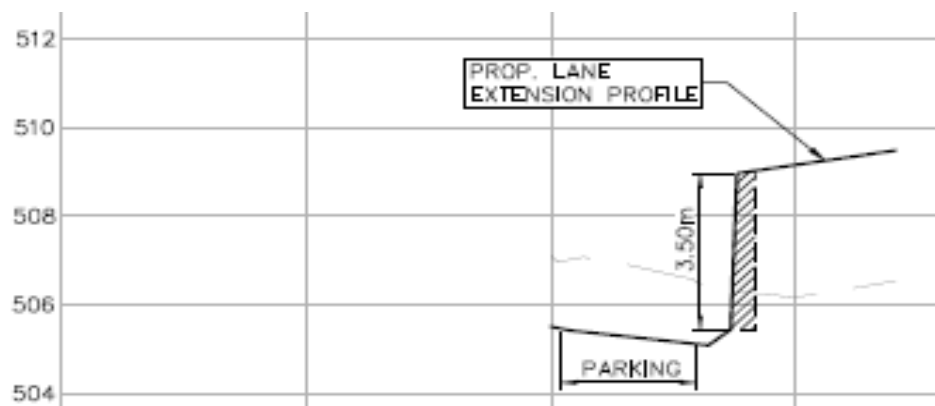


Figure 17: Over-height retaining wall No. 2, section drawing (blue).

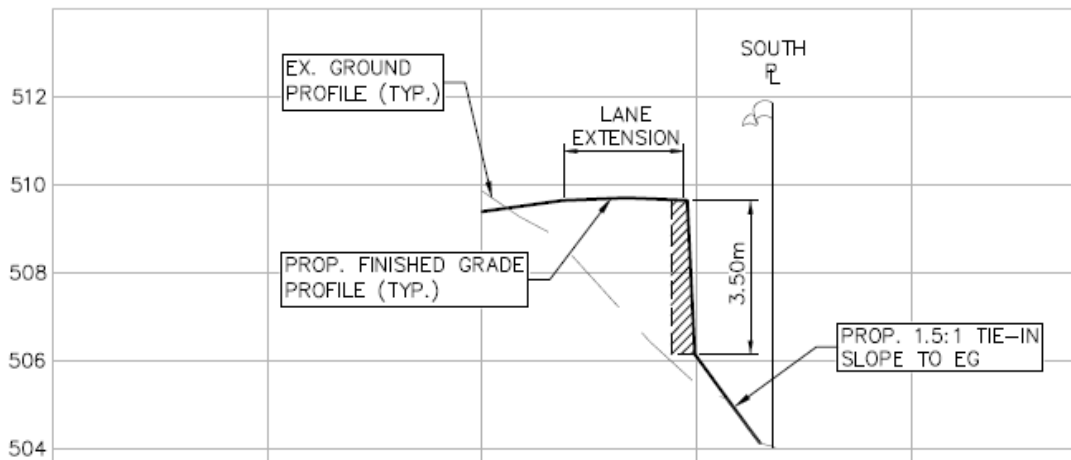


Figure 18: Over-height retaining wall No. 2, section drawing (green).

6. S.4.4.3(ii)(b) – Increase the width of driveway crossings at the property line (Sundance Drive) from a maximum 7.0 m to 9.4 m (varies), in accordance with the site plan (Attachment 2, Figure 19).

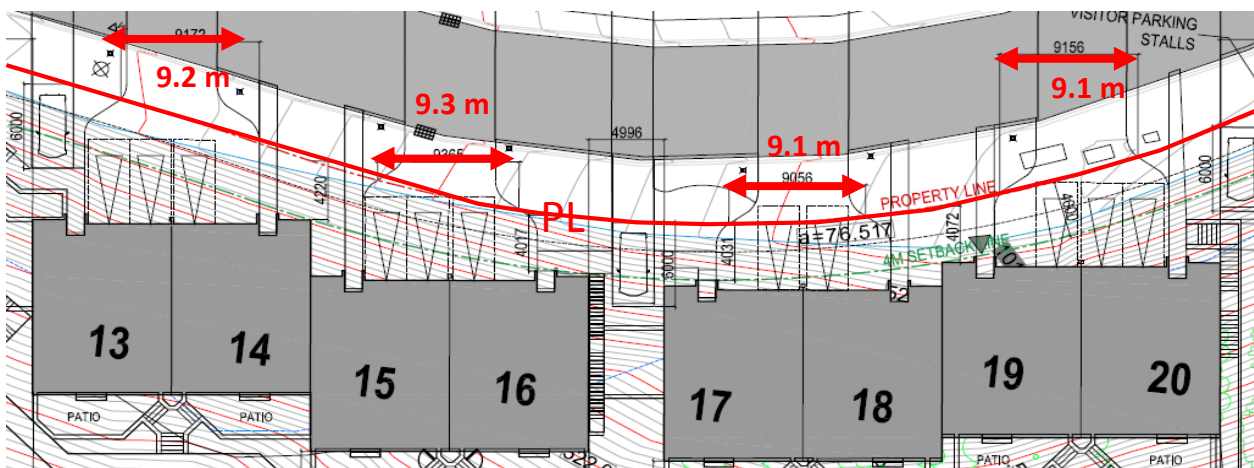


Figure 19: variance to the width of driveway crossings (4 of 8 crossing over 7.0 m shown here).

A variance to the maximum width of driveway crossings is to accommodate double driveways for the units on Sundance Drive. This variance applies to eight of 10 paired driveway crossings. Staff do not anticipate impacts from the paired driveways and support the variance.

Works and Services Bylaw No. 0249 Variance

7. S.8.2.2: To forgo the requirement to install a turnaround at or near the terminus of private hillside lane (Figure 20).

The City's Works and Services Bylaw requires private hillside lanes to include a turnaround at or near their terminus. This is for the purpose of improving vehicular

circulation, particularly with emergency or service vehicles (snow clearing, delivery, waste management). In the absence of a turnaround, it is anticipated that larger vehicles would need to turnaround on private driveways or reverse in or out of the laneway. This variance applies to Ensign Lane only, as a turnaround is proposed on Ensign Quay Lane.

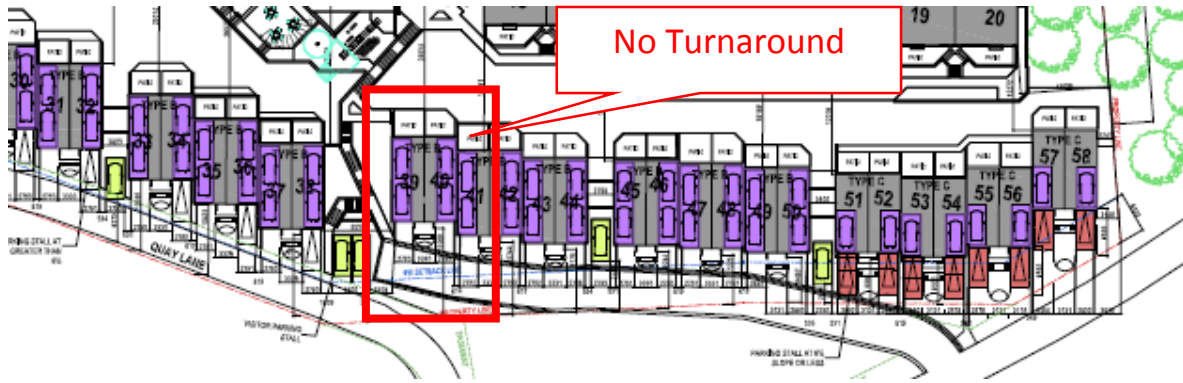


Figure 20: Variance to forgo turnaround on Ensign Lane

Although the City's Fire Department has confirmed they do not oppose the variance, the City's Engineering Department has stated that the turnaround is important for vehicle circulation and safety. In the absence of a turnaround, service and emergency vehicles would potentially be obstructed or obstruct the laneway. Passenger vehicles are also anticipated to have challenges turning around, given that the laneway contains only two visitor spaces and short driveway apron depths for units 39-50. Also, units 51-58 include only single garages; while consistent with the Zoning Bylaw regulations for off-street parking, including required parking in front of the garages adjacent to the laneway may exacerbate anticipated access and circulation challenges.

Technical Review

Servicing

A Functional Servicing Report (FSR) has been provided that concludes the property is well suited for this development and can be developed for the intended use. The property is noted to be connected to community water, storm, and sanitary sewer services. Access is accommodated by the public road, Sundance Drive, and two private laneways – one of which requires a variance to forgo a turnaround (see variance No. 7, Figure 20).

Construction Management Plan

A Construction Management Plan (CMP) was submitted with the application (Attachment 4). The CMP outlines the phasing of the proposed development and steps to mitigate impacts to adjacent properties. Primary site access is proposed from Sundance Drive, with the lanes to the south used only in case of emergency and as needed to conduct construction activities otherwise not feasible from Sundance Drive. Construction is proposed to occur in five phases in the sequence noted in Figure 21.



Figure 21: Construction phasing (Construction Management Plan)

Work is anticipated to occur on the laneways to extend and tie-in utilities to existing city infrastructure. A variety of mitigation measures are noted in the CMP, including those that address erosion and sediment, dust, traffic, trade parking, and noise. The CMP is also intended to be attached as a schedule to the permit to ensure construction follows the noted phasing and mitigation measures.

Snow-Clearing

Ensign Lane and Ensign Quay Lane are both private lanes. Snow-clearing and maintenance of the physical laneways are managed by the subject property and those existing residences/properties located to the south-east of the laneway. Snow storage locations for the site have not been confirmed and have the potential to cause circulation challenges, specifically on Ensign Lane due to the retaining wall and lack of turnaround or loading space. Based on this, it is recommended that additional consideration be put into snow storage.

Garbage / Recycling Collection

Due to the proposed variances to the turnaround requirement on Ensign Lane, staff have been in contact with the RDCO to confirm serviceability related to garbage and recycling collection. The RDCO confirmed that due to the size of the large collection vehicles, narrow width of the laneway, and lack of turnaround, the townhouse units (21-58) are not serviceable. In providing this information to the applicant, they have stated that private

garbage/recycling collection has been confirmed possible through the use of smaller trucks for ease of access in the area.

Referrals

The application was referred to multiple external agencies and internal departments. Staff have been working with the applicant to reduce the number of variances as much as possible, with the priority being the mitigation of potential impacts to existing and proposed residences along the laneways and related circulation and safety. Detailed review comments were provided to the applicant for their consideration in advance of their request to move the application forward to Council as presented.

Public Notification

In accordance with the *Local Government Act*, a notice has been mailed to residents and tenants within 100 m of the subject property advising them how to make a submission for Council's consideration, as it relates to the proposed variances. Notice of Application signage has also been installed on the subject property in accordance with the City's Application Procedures Bylaw No. 0260. At the time of writing this report, one submission has been received.

During the notification period, staff were advised of a small area of land that would require an access easement to support access for approximately four units (53-56) proposed on Ensign Lane. This issue has been included in the recommended motion to be addressed prior to the application returning to Council for consideration. Alternatively, should Council otherwise be supportive of the variances and the proposal as presented, registration of an access easement will be included as a condition of permit issuance – see Alternate Motion.

CONCLUSION

While staff are generally supportive of the additional housing units, form and character, building materials and many of the variances, individually, there are concerns with the collective impacts of the variances related to off-street parking and circulation. When considered collectively, their anticipated impact creates a situation that may compromise the safety and circulation of site traffic and visitors to the site, as well as service and emergency vehicles, particularly on Ensign Lane.

Staff recommend that Council postpone consideration of this application and direct the applicant to revise the following elements of their proposal without resulting in further variances:

- Include a turnaround and loading space on Ensign Lane;
- Secure legal access for units 53-56 (approx.) on Ensign Lane;

- Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
- Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
- Clearly provide a location for snow deposition on Ensign Lane.

Alternate Recommendation to Consider and Resolve:

1. Authorize Issuance of Development Permit (DP 22-26)

THAT Council authorize the issuance of a multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) to accommodate a 58-unit townhome development, in accordance with the attached permit (Attachment 1), subject to:

- submission of a landscape security in the amount of \$400,080.90; and
- registration of an easement for legal access to units 53-56 (approx.) proposed on Ensign Lane.

2. Deny Application

THAT Council deny Development Permit (DP 22-26).

Council may wish to deny the application and have the applicant redesign the proposal in accordance with the City's Official Community Plan Development Permit guidelines. If the proposal was revised, the Development Permit would require further consideration by Council.

REVIEWED BY

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Draft Development Permit (DP 22-26)
2. Architectural Submission (Site Plan, Elevations, Floor Plan, Renderings)
3. Landscape Plan
4. Construction Management Plan
5. Submission
6. Delegation Request – Ryan Jones



CITY OF WEST KELOWNA
DEVELOPMENT PERMIT WITH VARIANCES
DP 22-26

To: Gatehouse Developments (Providence) Inc., BC1304810
 c/o Mike Sheppard
 PO Box 2585 STN Sardis Main
 Sardis, BC, VR2 1A8

CC: Distefano Jaud Architecture
 c/o Carlo Distefano
 3-1331 Ellis Street
 Kelowna, BC, V1Y 1Z9

1. This Permit is issued subject to compliance with all of the Bylaws of the City of West Kelowna applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Permit applies to and only to those lands within the City of West Kelowna described below, and any and all buildings, structures and other developments thereon:

Lot 37 District Lot 2044 ODYD Plan KAP90501
(3401 Sundance Drive)

3. This **Multiple Family and Intensive Residential (DPA 3), Hillside (DPA 4), and Sensitive Terrestrial Ecosystem (DPA 5) Development Permit Area** allows the construction of 58 townhouses contained in 11 buildings, subject to the following conditions and related Schedules:
 - The siting and phasing of buildings are to be in accordance the **Site Plan Drawings A1.0-A1.2**, prepared by DiStefano Jaud Architecture, dated February 24, attached as Schedule "A";
 - The exterior design and finish of buildings are to be in accordance with the **Architectural submission** (elevations, material and colour schedule, renderings) prepared by DiStefano Jaud Architecture, dated February 24, attached as Schedule "A";
 - The dimensions and siting of retaining walls and servicing to be constructed on the land shall be in accordance with **Civil Engineering Drawings** (site servicing and grading plans, pre- and post-contours, hammerhead cross sections, lane extension cross sections, stormwater management, erosion and sediment control plan, turning movement plan), package prepared by Aplin & Martin Consultants Ltd., prepared April 18, 2023, Rev. 2, "Issued for Development Permit", Project No. 20-3099, attached as Schedule "B";
 - Landscaping to be provided on the land in general accordance with **Landscape Drawings**, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-L01C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, dated April 18, 2023, attached as Schedule "C";

- Construction activities to be in accordance with the **Environmental Assessment Reports**, prepared by Makonis Consulting Ltd., dated June 2021, attached as Schedule “E”, including but not limited to:
 - i. Bird nest surveys must be done in advance of clearing of trees between March 15 and August 15, and in accordance with the *Migratory Birds Act* and any other provincial and federal regulations;
 - ii. Spoil piles left for longer than two weeks are to be tarped;
 - iii. Spill-containment kits will be on site and on each equipment/machine; and
 - iv. Work site will be maintained in clean state. Oil containers, cans, grease tubes, rags, etc. and any other materials packaging will be removed from the work area to an approved disposal location immediately on completion of the servicing job.
- All construction activities and associated mitigation measures to be conducted on the land be in general accordance with the **Geotechnical Report**, dated February 8, 2021, and Geotechnical Memo, dated July 8, 2021, prepared by Interior Testing Services Ltd, attached as Schedule “F”, and the **Construction Management Plan**, prepared by Gatehouse Developments Inc., dated September 2, 2020, attached as Schedule “G”, including but not limited to:
 - i. Rock slopes to be finished no steeper than 0.5(H):1(V) and scaled to remove loose particles at the rock face.
 - ii. 3 m clearing of overburden above the top of the rock cuts with chain link fencing installed above the rock cut.
 - iii. With rock slopes less than 2.5 m in height, a catchment buffer is not required, however where rock cuts are 2.5 m to 6 m in height, provide a 1.5 m wide catchment buffer area.
 - iv. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - v. Finished grades should be sloped away from the building in order to minimize infiltration of water into the backfill zone. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - vi. ITSL should be called to review the stripped subgrade prior to placing new grading fills, as well as during fill placement to carry out periodic field density testing.
- The following **variances to Zoning Bylaw No. 0265** are included as part of this Development Permit, in accordance with the Site Plan attached as Schedule “A”, and Civil Package, attached as Schedule “B”:
 - i. S.10.11.4(g)(i) – Reduce the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies);
 - ii. S.4.3.2(b) – Reduce minimum required distance between the parking and loading spaces from the front or exterior parcel boundary from 3.0 m from 0.0 m (varies);
 - iii. S.4.10.1(a) – Reduce the minimum required number of loading spaces from four to one space(s);

- iv. S.4.10.2(a) – Reduce the minimum required dimensions of a loading space from 3.0 m x 9.0 m; and
 - v. S.3.14.1 – Increase the maximum height of two retaining walls from 2.5 m to 3.5 m.
 - vi. S.4.4.3(ii)(b) – increase the width of driveway crossing at the property line from a maximum 7.0 m to 9.4 m (varies).
- The following **variances to the Works and Services Bylaw No.0249** are included as part of this Development Permit, in accordance with the Site Plan attached as Schedule “A”:
 - i. S.8.2.2: Forgo the requirement to install a turnaround at or near the terminus of private hillside lane.
4. As a condition of the issuance of this Permit, the property owner shall deposit 125% of the cost estimate for the **Landscaping (\$400,080.90)** as performance security to ensure installation of the required landscape plantings, in accordance with the City’s Development Application Procedures Bylaw No. 0260 and Schedule “D”.
 5. Should any interest be earned upon the security, it shall accrue to the Permittee and be paid to the Permittee if the security is returned. The condition of the posting of the security is that should the Permittee fail to carry out the development hereby authorized, according to the terms and conditions of the Permit within the time provided, the City of West Kelowna may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Permittee, or should the Permittee carry out the development permitted by this Permit within the time set out below, the security shall be returned to the Permittee. There is filed accordingly:
 6. The land described herein shall be developed strictly in accordance with the terms and conditions of this Permit and any plans and specifications attached to this Permit, which shall form a part hereof. Should any changes be required to this permit, please ensure that you obtain written approval from City of West Kelowna prior to making any changes.
 7. If this Development Permit has not been issued within one year from approval, Development Permit DP 22-26 shall be deemed to have been refused and the file will be closed.
 8. **This Permit is not a Building Permit.**
 9. **This is not a Municipal Highways Permit.**
 10. All archaeological sites in B.C. are protected under the Heritage Conservation Act. This applies to whether sites are located on public or private land and whether the site is known or unknown. If you think you have uncovered an archaeological site during a building project or renovation, please do not disturb the site further and call B.C.’s Archaeology Branch immediately at (250) 953-3334. Branch archaeologists will review your project plans and make recommendations to manage site impacts and secure the required permitting.
 11. Subject to the terms of the permit, where the holder of a permit issued under the *Local Government Act* does not substantially commence any construction with respect to which

the permit was issued within one year after the date it is issued, the permit lapses.

AUTHORIZING RESOLUTION NO. CXXX/XX PASSED BY THE MUNICIPAL COUNCIL ON _____, 2023.

Signed on _____

Corporate Officer

I hereby confirm that I have read and agree with the conditions of Development Permit DP 22-26 with Variances and will ensure that copies of the Permit will be provided to onsite personnel at time of construction.

Signed on _____

Property Owner or Agent

ISSUED on _____

Schedules:

- A. Architectural submission by DiStefano Jaud Architecture, Drawing No. A1.0-1.2, A2.1-2.7, A3.1-3.7, dated February 24, 2023.
- B. Civil drawings, prepared by Aplin & Martin Consultants Ltd., Drawing Package prepared April 18, 2023, Rev. 2, Issued for Development Permit, Project No. 20-3099.
- C. Landscape Plan, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-Lo1C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, dated April 18, 2023.
- D. Landscape Estimate, prepared van der Zal + Associates, dated May 5, 2023, Project File Reference 2021-128.
- E. Environmental Reports, prepared by Makonis Consulting Ltd., dated June 2021.
- F. Geotechnical report, dated February 8, 2021, and Geotechnical Memo dated, July 8, 2021, prepared by Interior Testing Services Ltd.
- G. Construction management plan, prepared by Kyle Weiher, Senior Project Manager, Gatehouse Construction Inc., no date.

Attachment 2

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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WWW.DISTEFANOJAUD.COM

PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
FULL SITE PLAN
PROJECT DETAILS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A1.0

SCALE
1:300



SITE INFORMATION

PARCEL ZONING	R4
PARCEL AREA	13,043m2
TOTAL FOOTPRINT AREA OF HOMES	4,534m2
LOT COVERAGE	35%

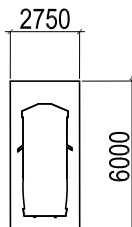
AMENITY SPACE

FOR MORE THAN 1 BEDROOM UNIT	25.0m2 (269.1 ft2)
TOTAL NUMBER OF UNITS	58
REQUIRED AMENITY AREA	25 X 60 = 1,500m2 OF AMENITY SPACE
AMENITY AREA PROVIDED	4,012 m2 for central court area and patios

LOADING	
REQUIRED	PROVIDED
1 PER 15 UNITS TOTAL UNITS = 58 REQUIRED = 3.9 = 4	PROVIDE 1 DEDICATED LOADING ZONE ON QUAY LANE ON SUNDANCE 8 HOMES HAVE ADEQUATE DRIVEWAY AREA TO ALLOW FOR LOADING ON QUAY LANE 3 HOMES HAVE ADEQUATE DRIVEWAY AREA TO ALLOW FOR LOADING

TANDEM UNIT TYPE A: SHORT UNIT WITH ONE DEDICATED STALL IN GARAGE	TANDEM UNIT TYPE B: DEEP UNIT WITH 2 CAR TANDEM GARAGE	TANDEM UNIT TYPE C: SHORT UNIT WITH ONE DEDICATED STALL IN GARAGE. SUBSTANTIAL GRADE CHANGE RESULTS IN PATIO ONLY ACCESSIBLE AT 3RD LEVEL. FRONT DECK ALSO EXTENDED AT 2ND LEVEL ON FRONT OF UNIT.
--	---	---

SUNDANCE DRIVE UNITS - SIDE BY SIDE		QUAY LANE UNITS - TANDEM		
TYPICAL UNIT AREA = 112.4m2		TYPICAL UNIT AREA TYPE A = 59.92m2	TYPICAL UNIT AREA TYPE B = 60.29m2	TYPICAL UNIT AREA TYPE C = 59.92m2
TOTAL NUMBER OF UNITS = 20		TOTAL NUMBER OF UNITS TYPE A = 6	TOTAL NUMBER OF UNITS TYPE B = 24	TOTAL NUMBER OF UNITS TYPE C = 8
TOTAL AREA FOR ALL UNIT 1 TO 20 = 2,248 m2		TOTAL AREA FOR ALL UNITS = 360 m2	TOTAL AREA FOR ALL UNITS = 1,447 m2	TOTAL AREA FOR ALL UNITS = 479 m2

UNIT PARKING				ALL REQUIRED PARKING STALL SIZES SHOWN ARE DESIGNED AS PER BYLAW STALL SIZE REQUIREMENTS					
REQUIRED		PROVIDED SUNDANCE				REQUIRED		PROVIDED QUAY	
PER UNIT = 2 TOTAL UNITS = 20 REQUIRED = 40		GARAGE PARKING = 40				PER UNIT = 2 TOTAL UNITS = 38 REQUIRED = 76	GARAGE PARKING = 62		
		DRIVEWAY STALLS = 23					DRIVEWAY STALLS = 14		
		TOTAL = 63					TOTAL = 76		
VISITOR PARKING									
DEVELOPMENT VISITOR PARKING REQUIREMENT = 10% OF REQ. TOTAL REQUIRED = 40 + 76 = 116 x 10% = 11.6 OR 12				TOTAL REQUIRED PARKING = 40+76 = 116 x 10% = 11.6 OR 12 PROVIDED 12					
ALSO AVAILABLE IS STREET PARKING ALONG SUNDANCE ROAD				STALLS AVAILABLE = 7					

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PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

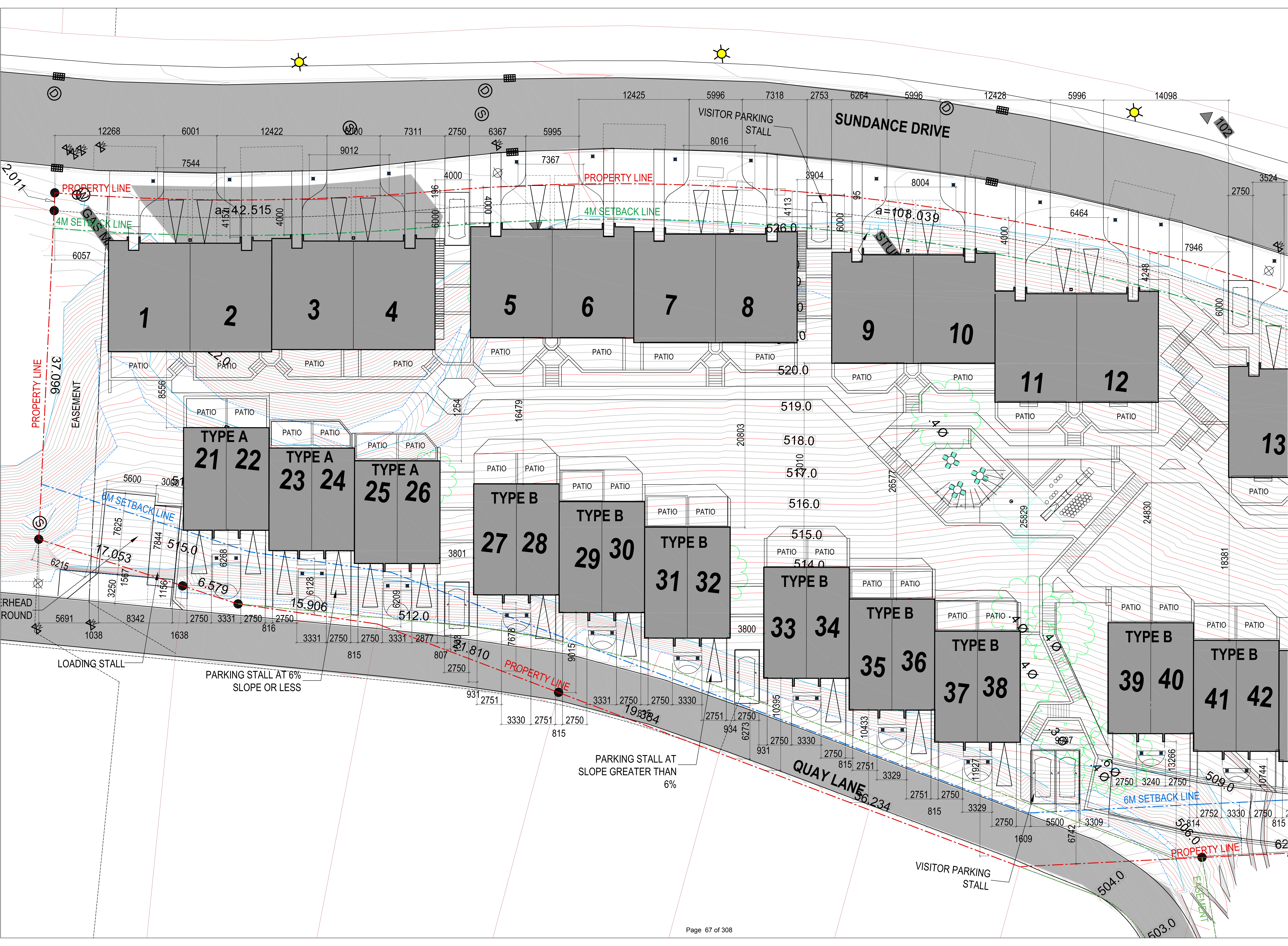
PROJECT LOCATION
3401 SUNDANCE DRIVE

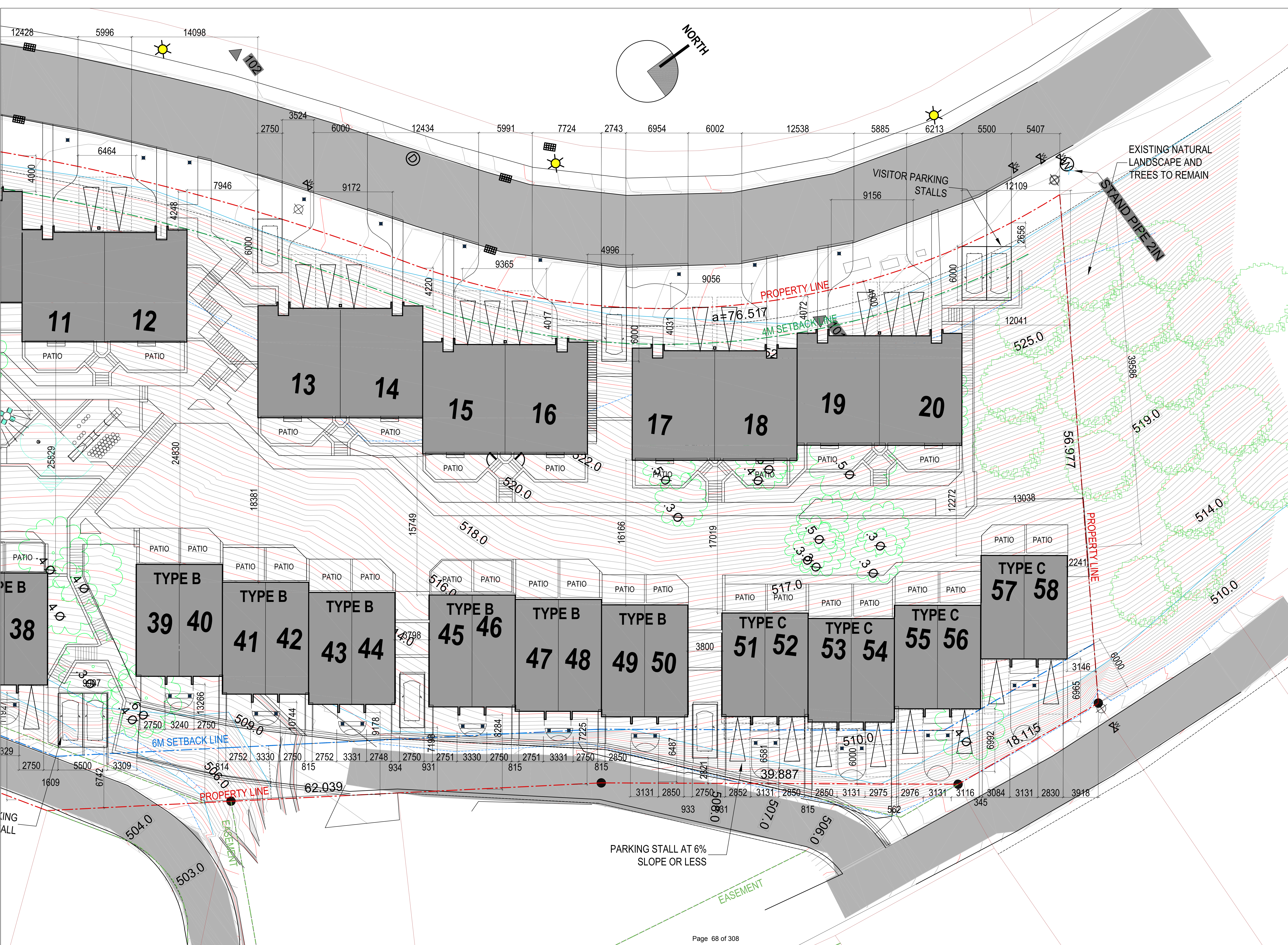
SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A1.1

SCALE
1:175





ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A1.2

SCALE
1:175

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

PROFESSIONAL SEAL(s)
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UNIT AREAS:

MAIN FLOOR (ENTRY LEVEL) = 1,210 sq.ft.
MID-LOWER LEVEL = 738 sq.ft.
BOTTOM LEVEL = 738 sq.ft.

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED IN 2 GROUPS OF 2 FOR A TOTAL OF 4 UNITS AS PER THE SITE PLAN.
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
ENTRY LEVEL PLAN
SUNDANCE ROAD

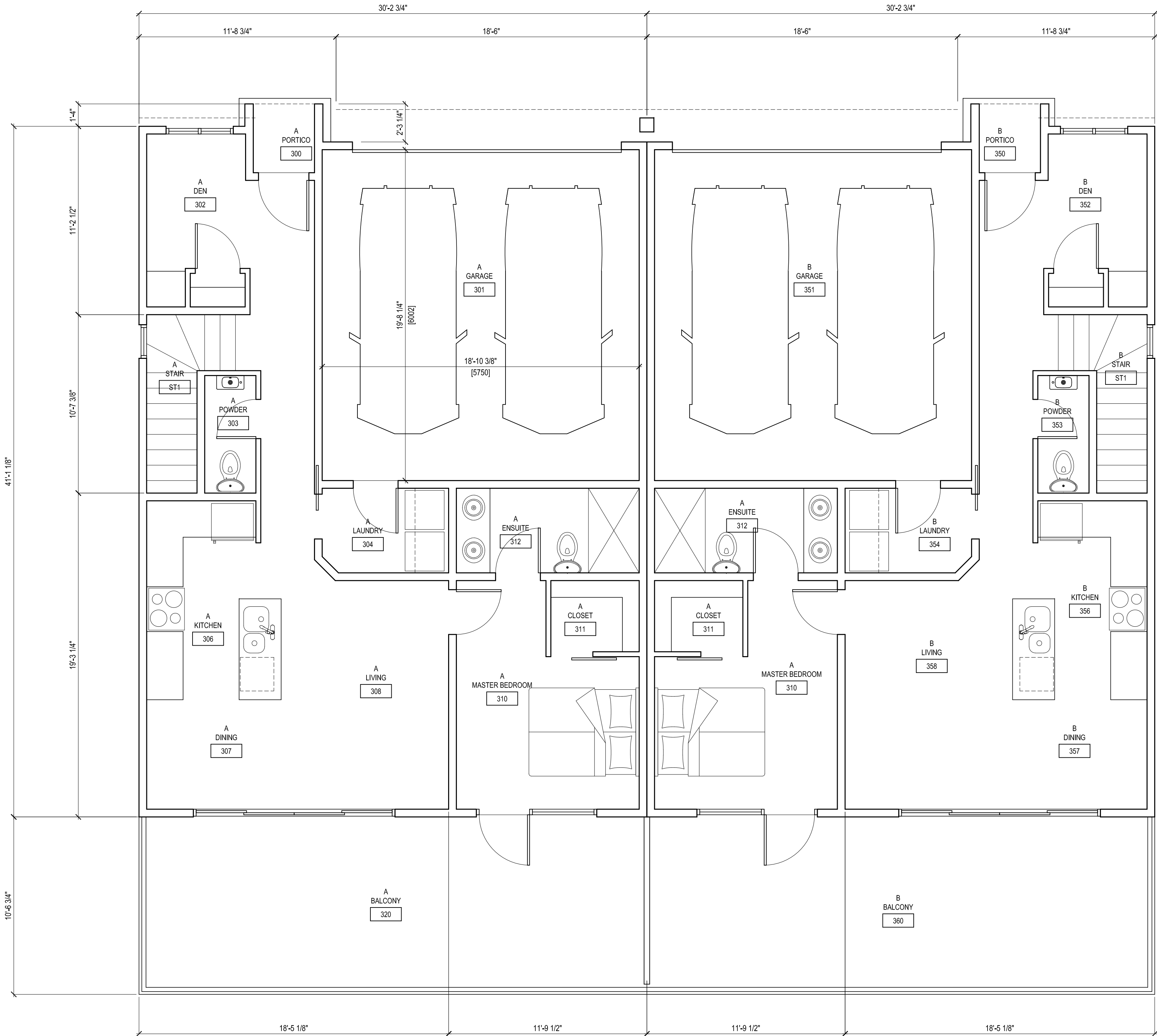
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.1

SCALE
1/4"=1'-0"

SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE



1
A2.1
ENTRY LEVEL
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

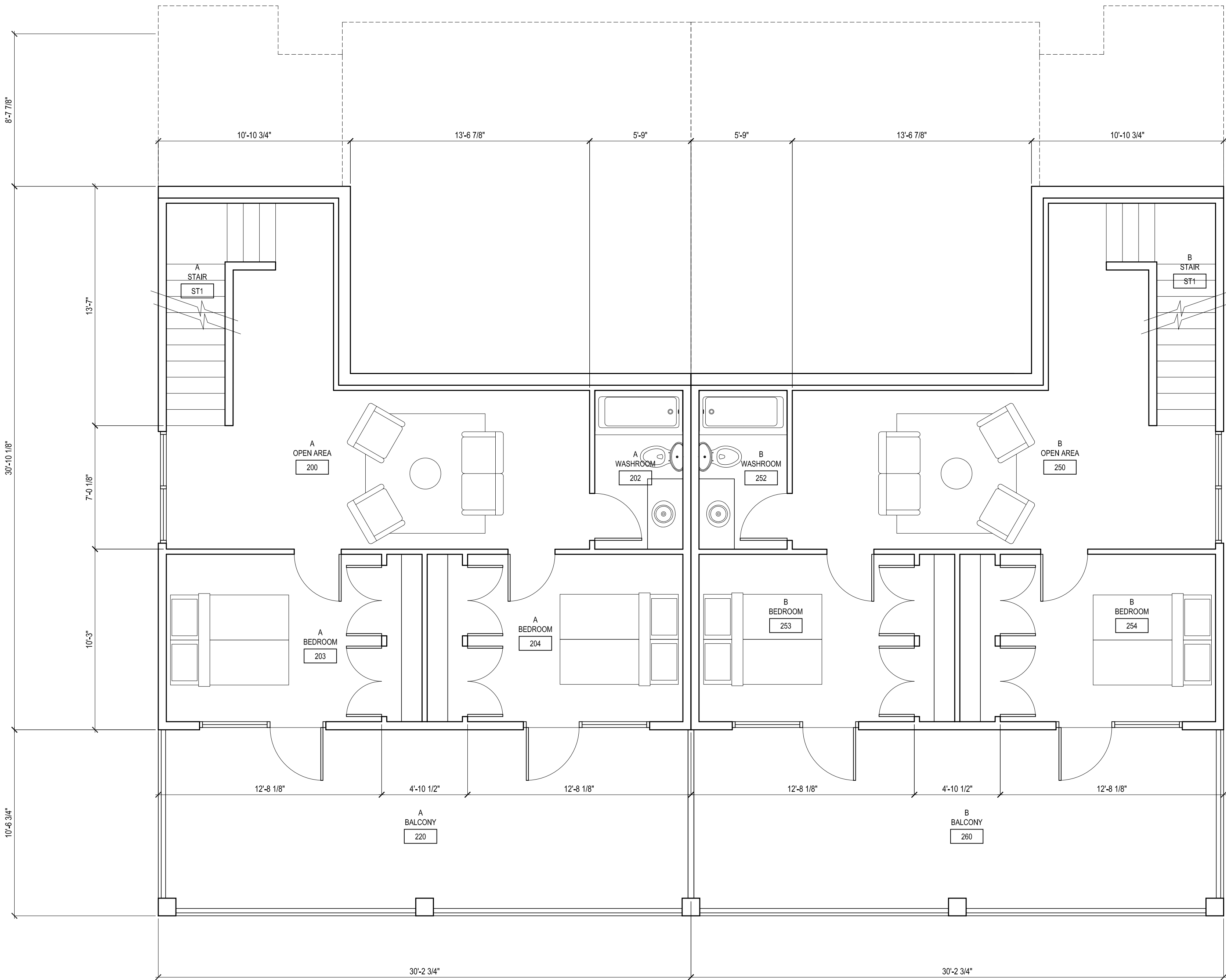
SHEET TITLE
SIDE BY SIDE UNITS
LOWER MID-LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.2

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.2
MID-LOWER LEVEL
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 11, 2023

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

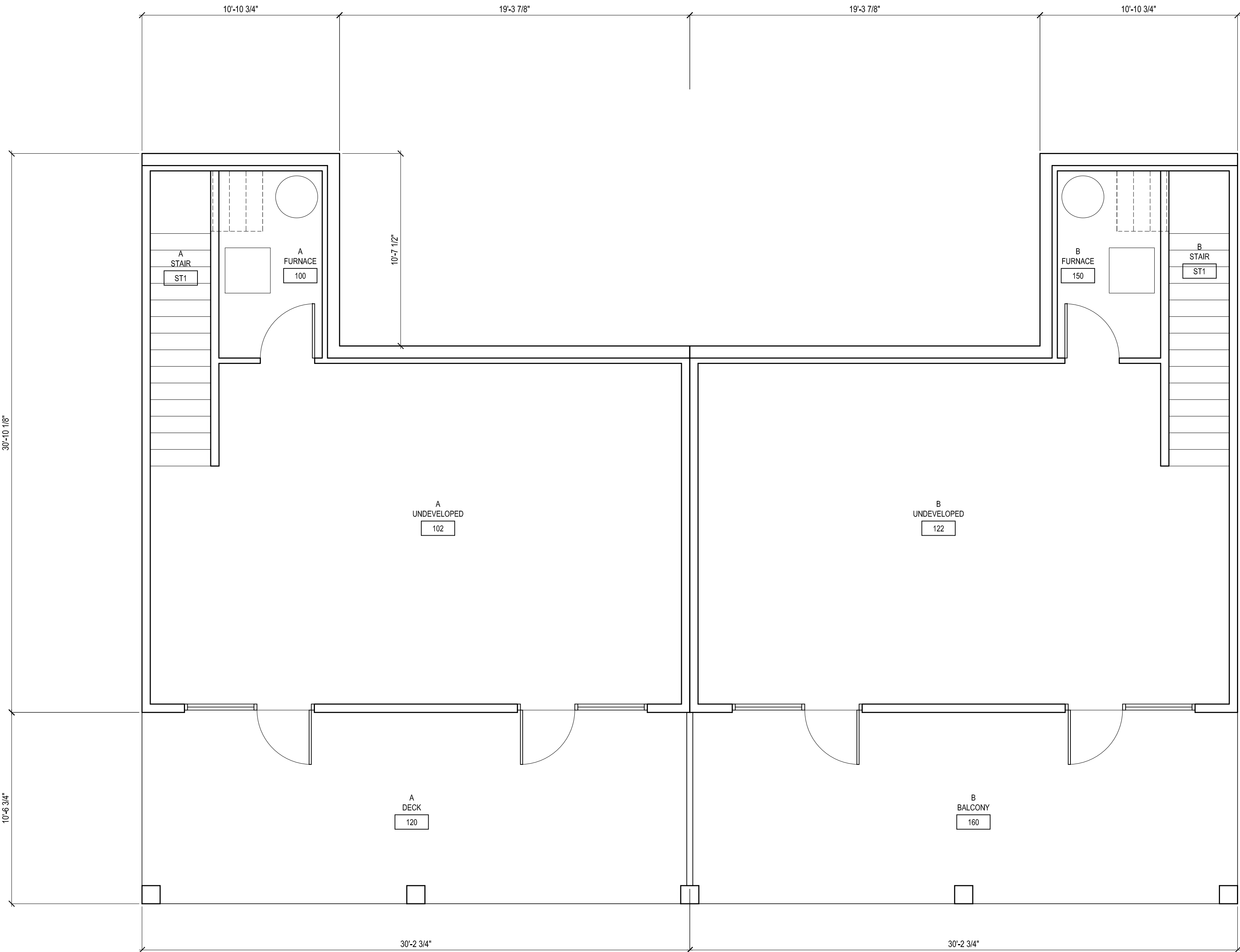
SHEET TITLE
SIDE BY SIDE UNITS
BOTTOM LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.3

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.3 LOW LEVEL
1/4"=1'-0"

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PROJECT
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PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

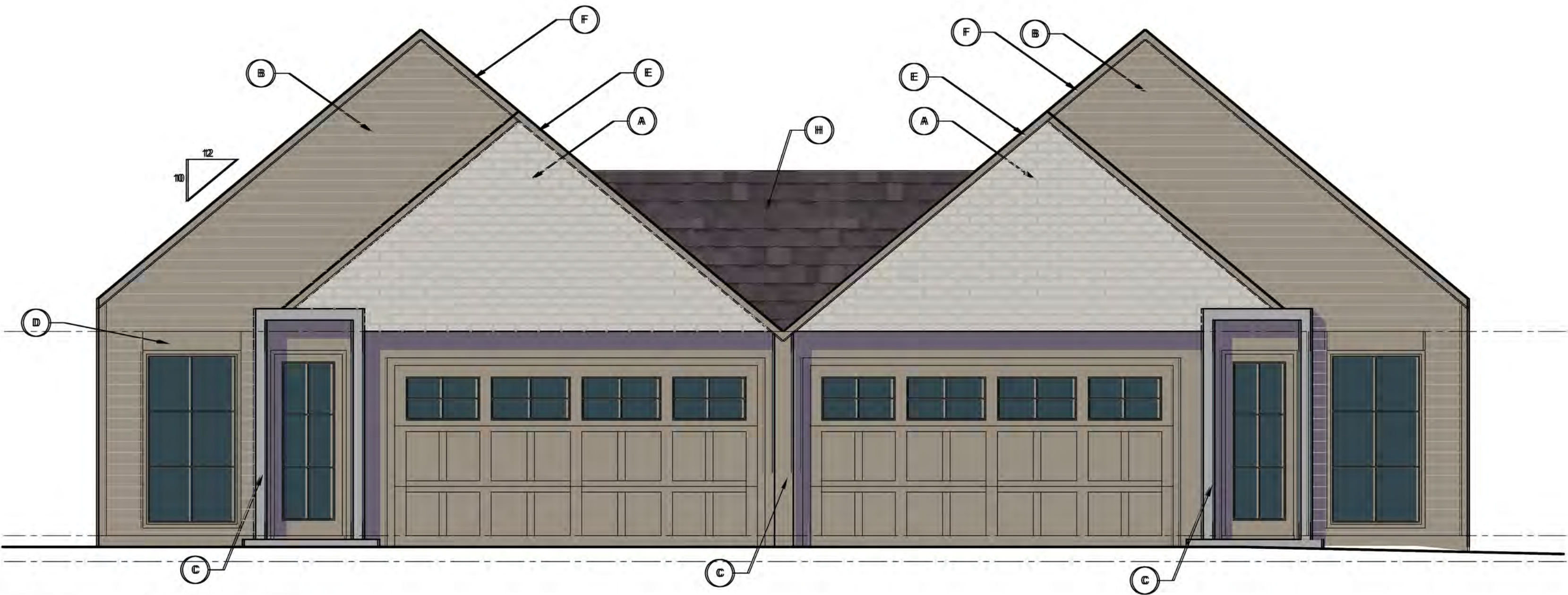
SHEET TITLE
SIDE BY SIDE UNITS
ELEVATIONS

DATE
FEBRUARY 24, 2023

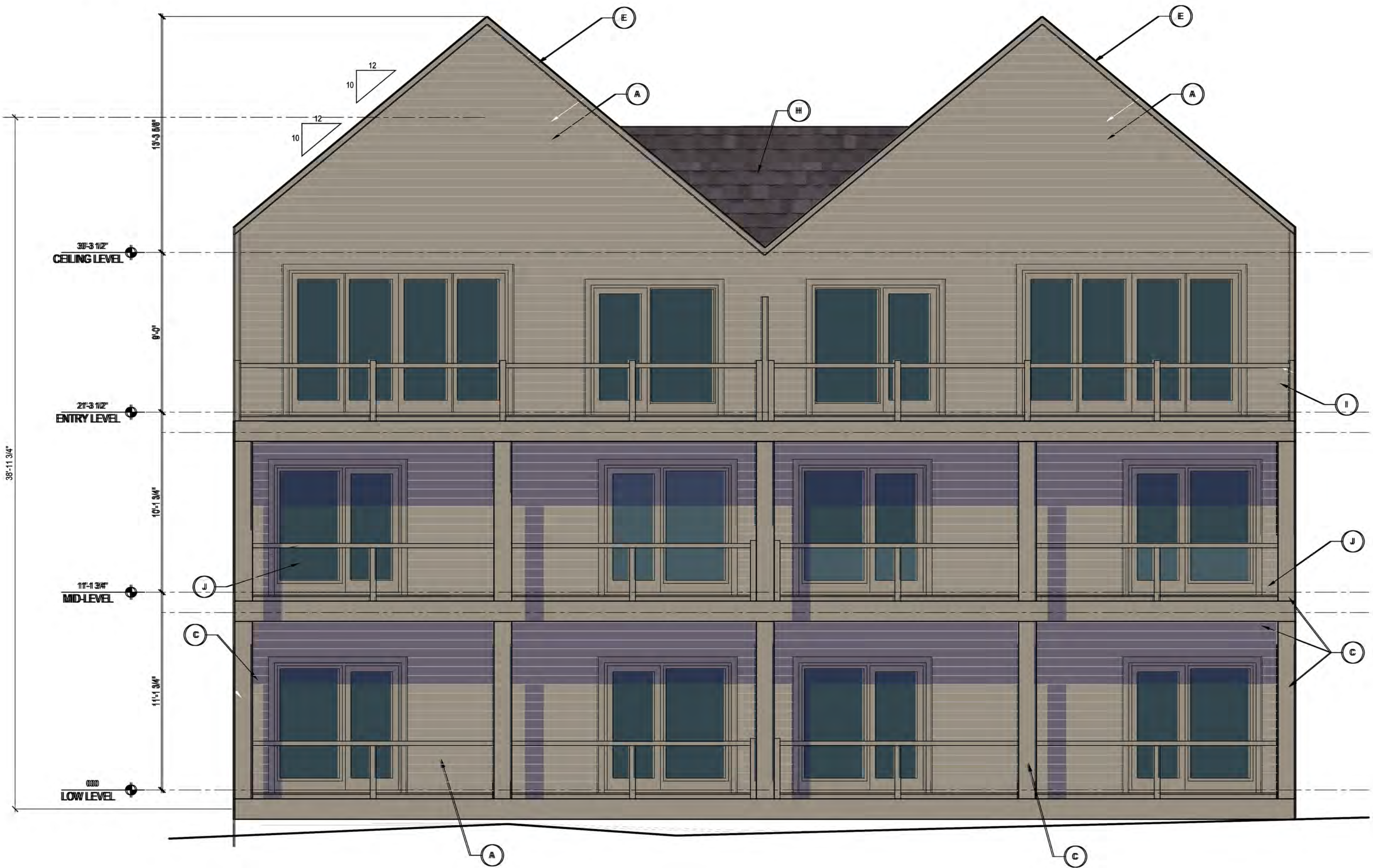
SHEET NUMBER

A2.4

SCALE
1/4"=1'-0"



1
A2.4
STREET ELEVATION - COMBINED
1/4"=1'-0"



2
A2.4
REAR ELEVATION - DOUBLE UNIT
1/4"=1'-0"

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION		
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS		GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUP OF UNITS USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE IN COLOUR TONES FROM LIGHT TO MID AND THEN TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS. THE COLOURS SHOWN IN THIS ELEVATION REPRESENT THE MID-TONE COLOUR OPTION.

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
ELEVATIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.5

SCALE
1/4"=1'-0"



GENERAL NOTES:

- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
- REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
- DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAP ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION	TYPE	DESCRIPTION
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS	K	GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
STREET SCAPE
ELEVATIONS

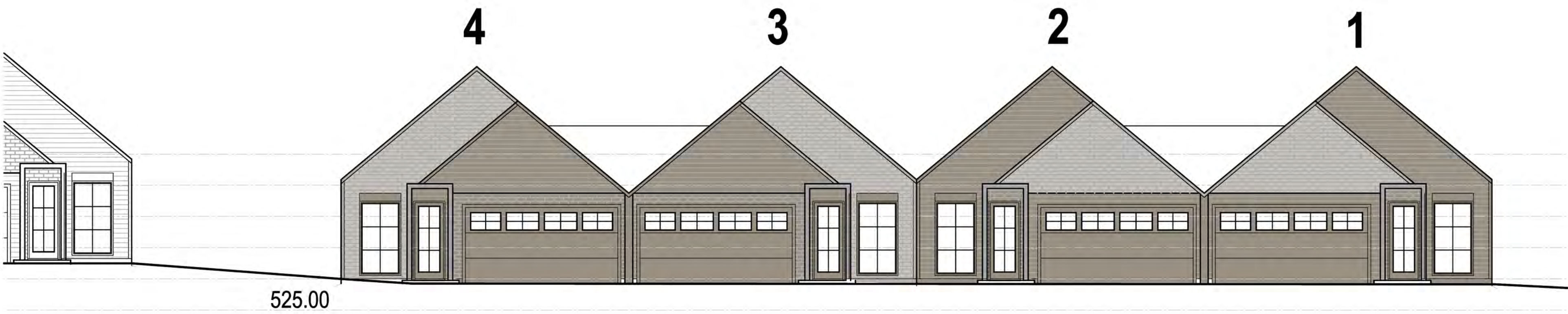
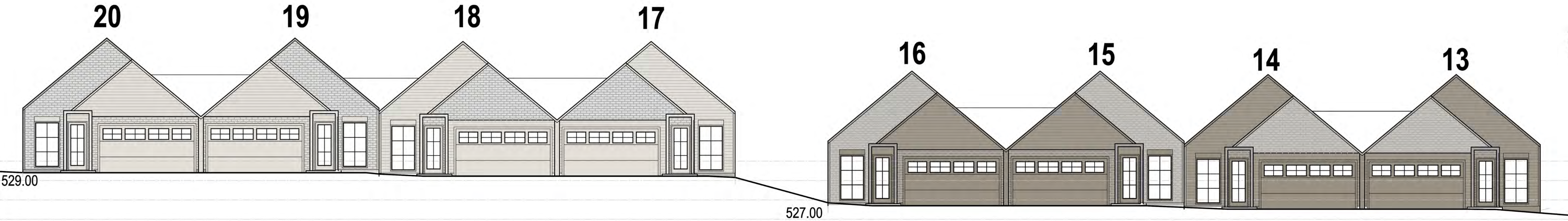
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.6

SCALE
1/8"=1'-0"

1
A2.6
STREET SCAPE
1/8" = 1'-0"



ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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FINAL RENDERINGS BEING COMPLETED AND WILL BE SUBMITTED AS SOON AS DONE. MINOR CHANGES ONLY TO OCCUR TO IMAGES

PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPINGS OF LIGHT TONE UNITS AND MID TONE UNITS IN DISTANCE



PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPING OF DARK TONE UNITS

DiStefano Jaud Architecture

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
RENDERED PERSPECTIVES

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A2.7

SCALE
N.T.S.

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	MARCH 17, 2023

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT NUMBER
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PROJECT LOCATION
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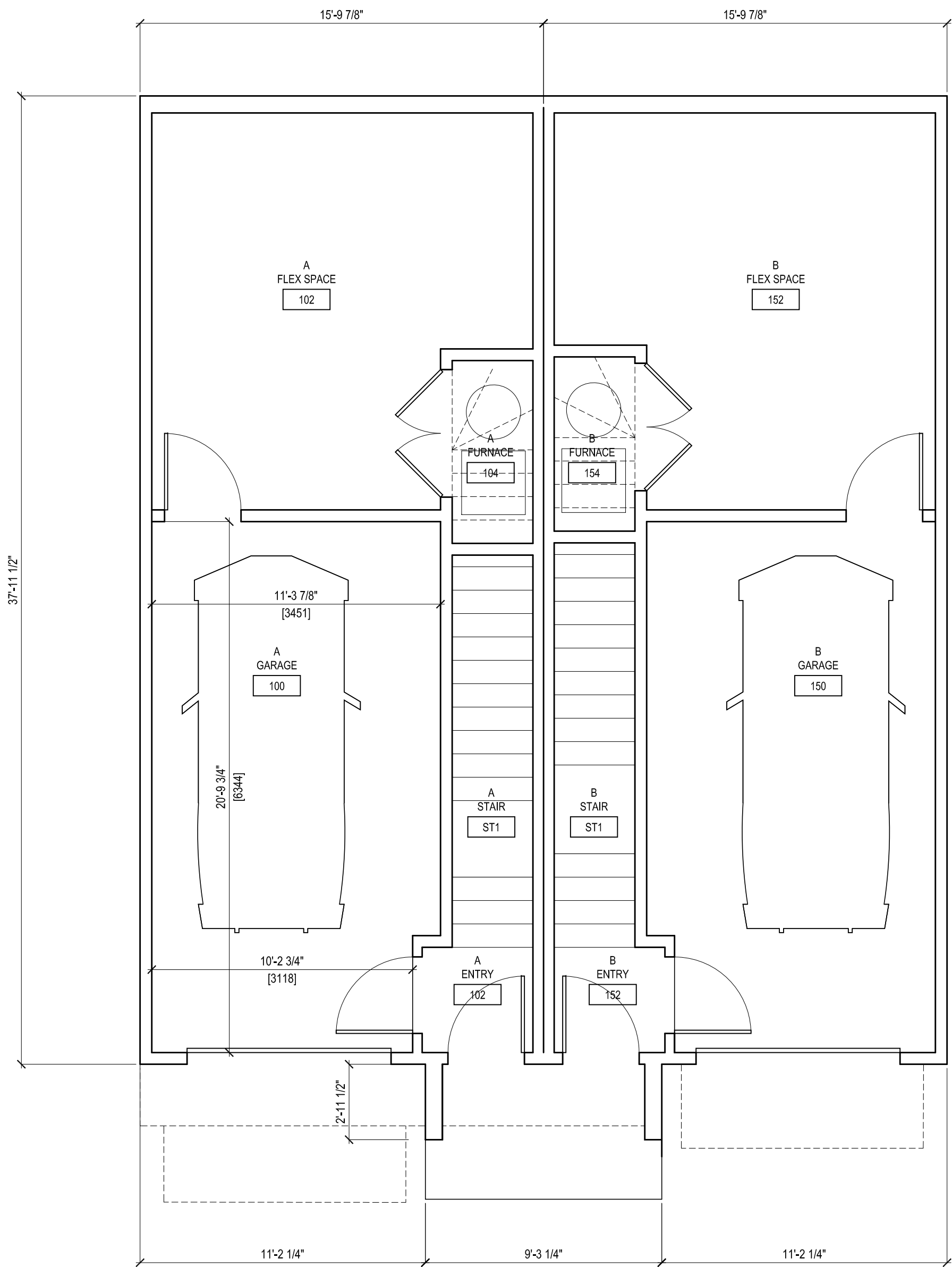
SHEET TITLE
TANDEM UNITS - TYPE A
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A3.1

SCALE
1/4" = 1'-0"

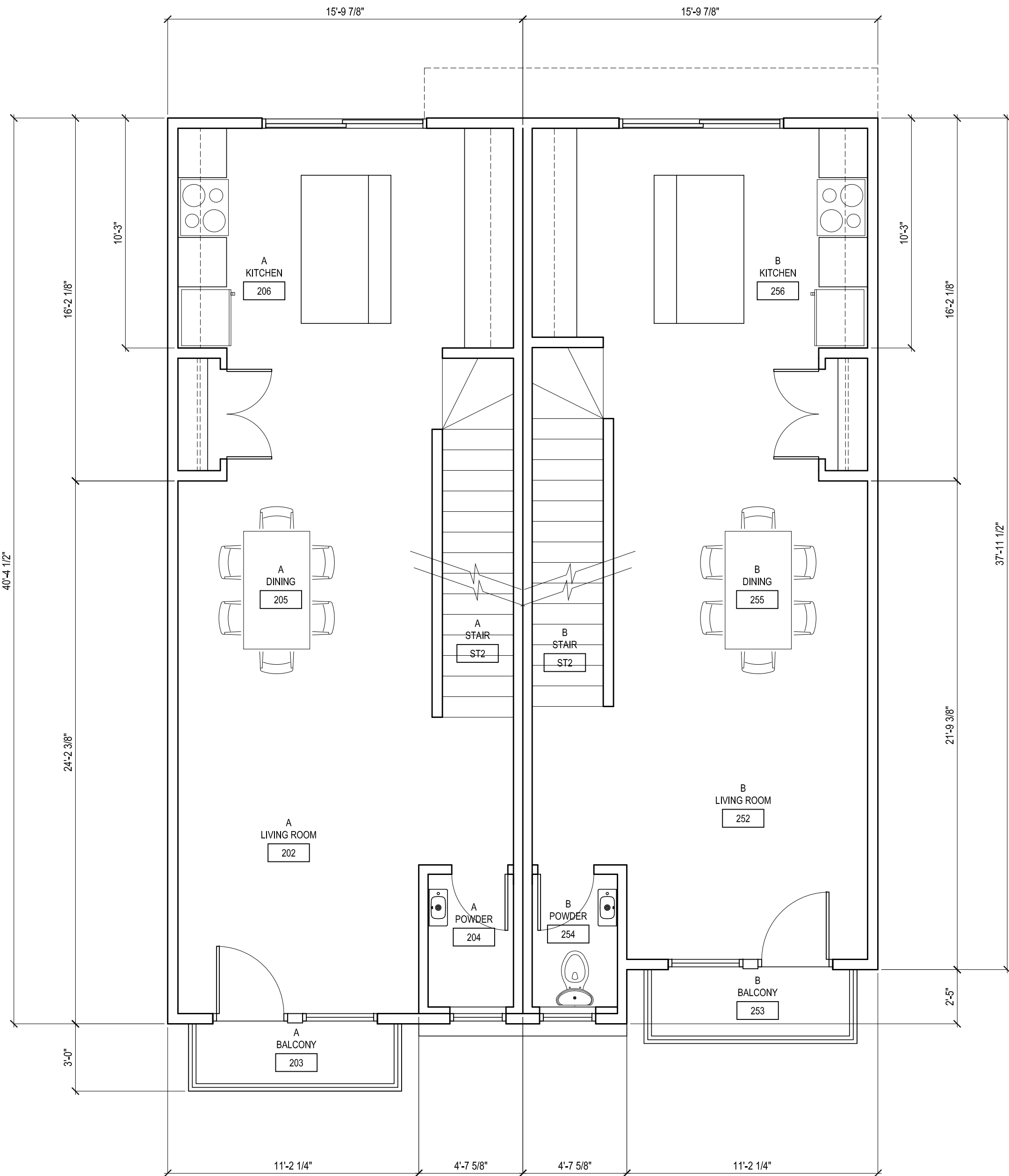


1 ENTRY LEVEL - TYPE A
A3.1 1/4" = 1'-0"

UNIT AREAS TYPE A:

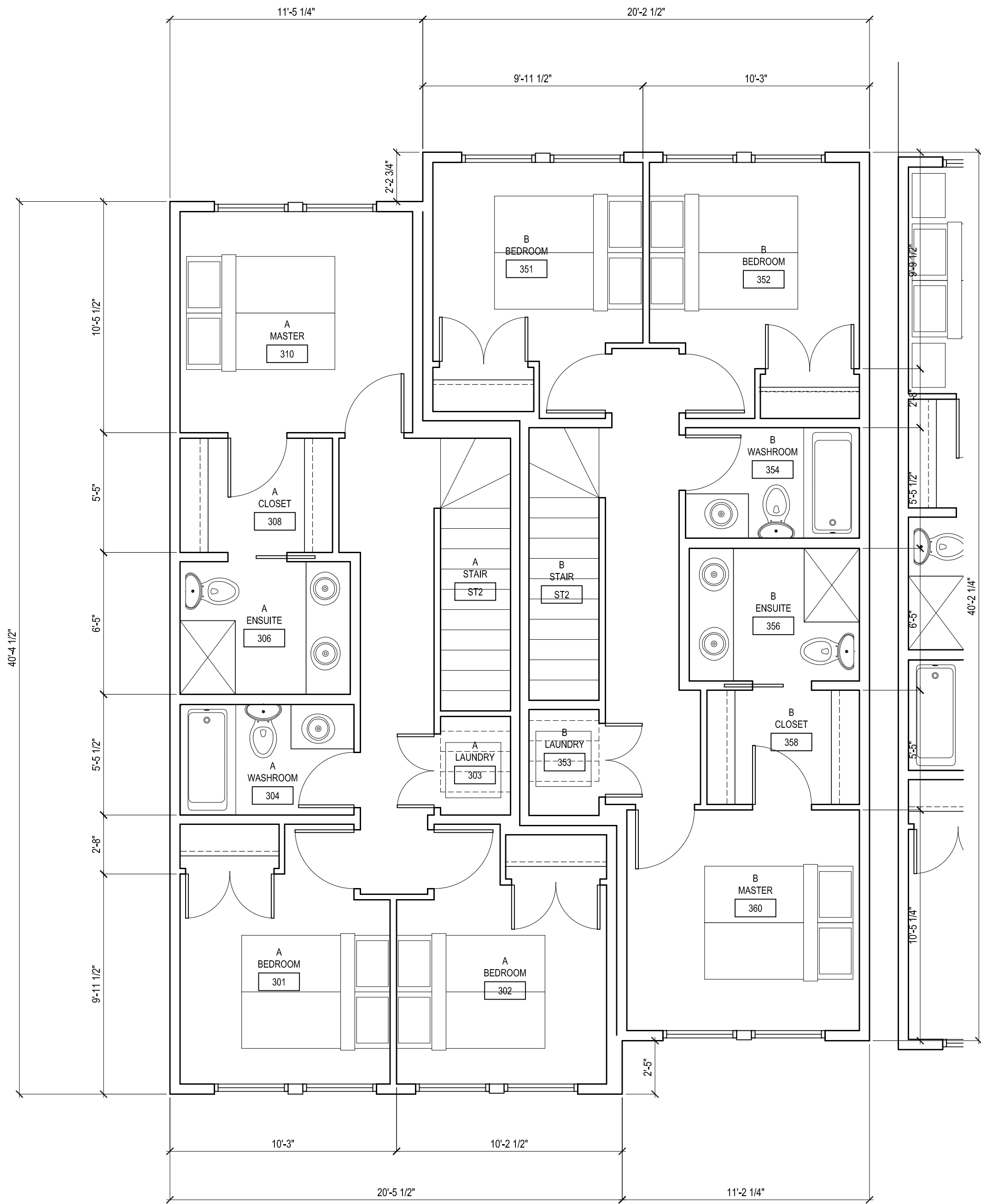
SIDE A
ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 668 sq.ft.
TOP FLOOR = 679 sq.ft.

SIDE B
ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 645 sq.ft.
TOP FLOOR = 678 sq.ft.



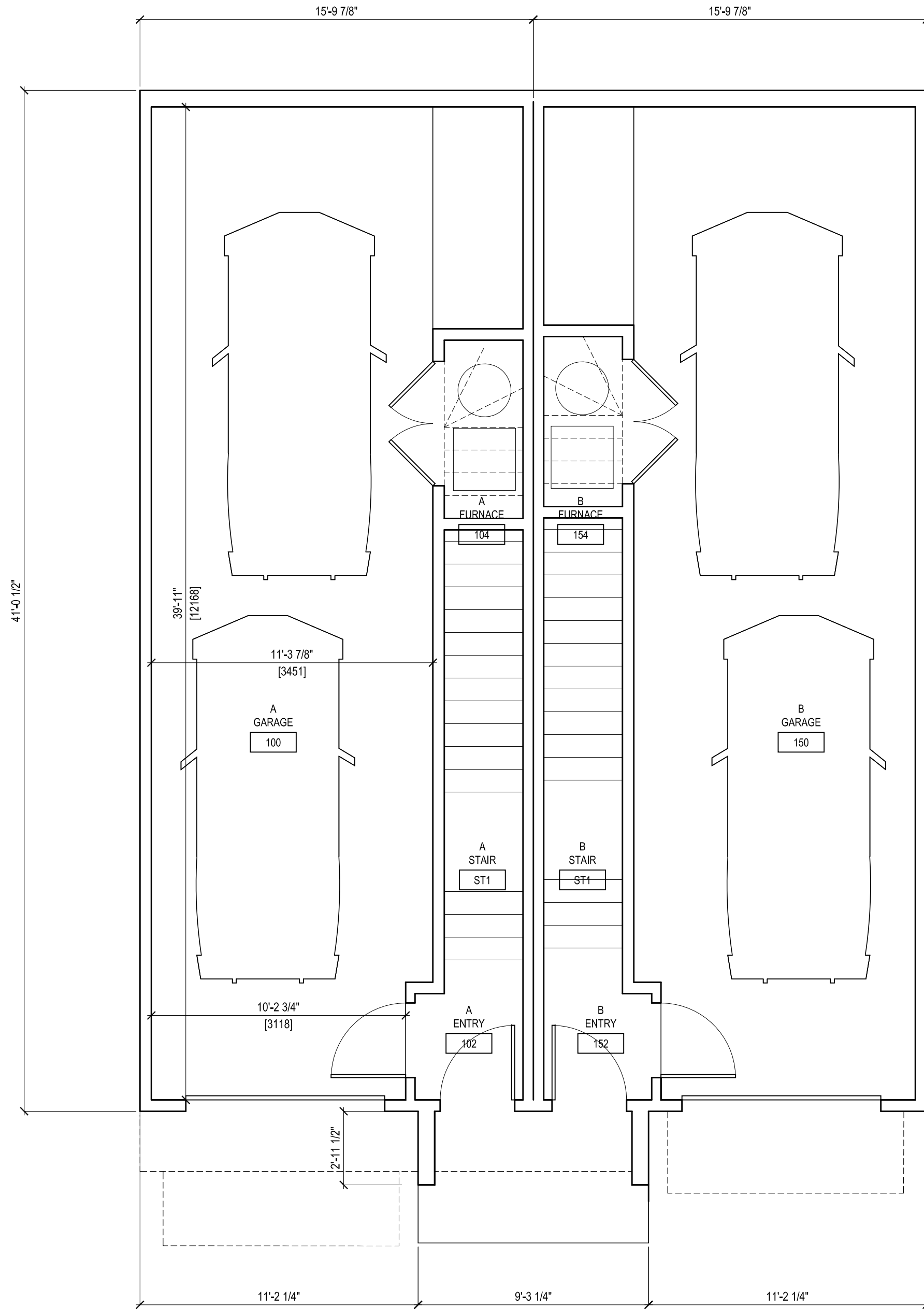
2 MID LEVEL - TYPE A
A3.1 1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE A - SHORTENED PLAN
QUAY LANE FRONTAGE



1 TOP LEVEL - TYPE A
A3.2 1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE A - SHORTENED PLAN
QUAY LANE FRONTAGE



2 ENTRY LEVEL - TYPE B
A3.2 1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE B - ELONGATED PLAN
QUAY LANE FRONTAGE

UNIT AREAS TYPE B:

SIDE A
ENTRY LEVEL (GARAGE) 649 sq.ft.
MID LEVEL = 686 sq.ft.
TOP FLOOR = 683 sq.ft.

SIDE B
ENTRY LEVEL (GARAGE) 649 sq.ft.
MID LEVEL = 663 sq.ft.
TOP FLOOR = 711 sq.ft.

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	MARCH 17, 2023

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS, TYPE A AND B
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A3.2

SCALE
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT
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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

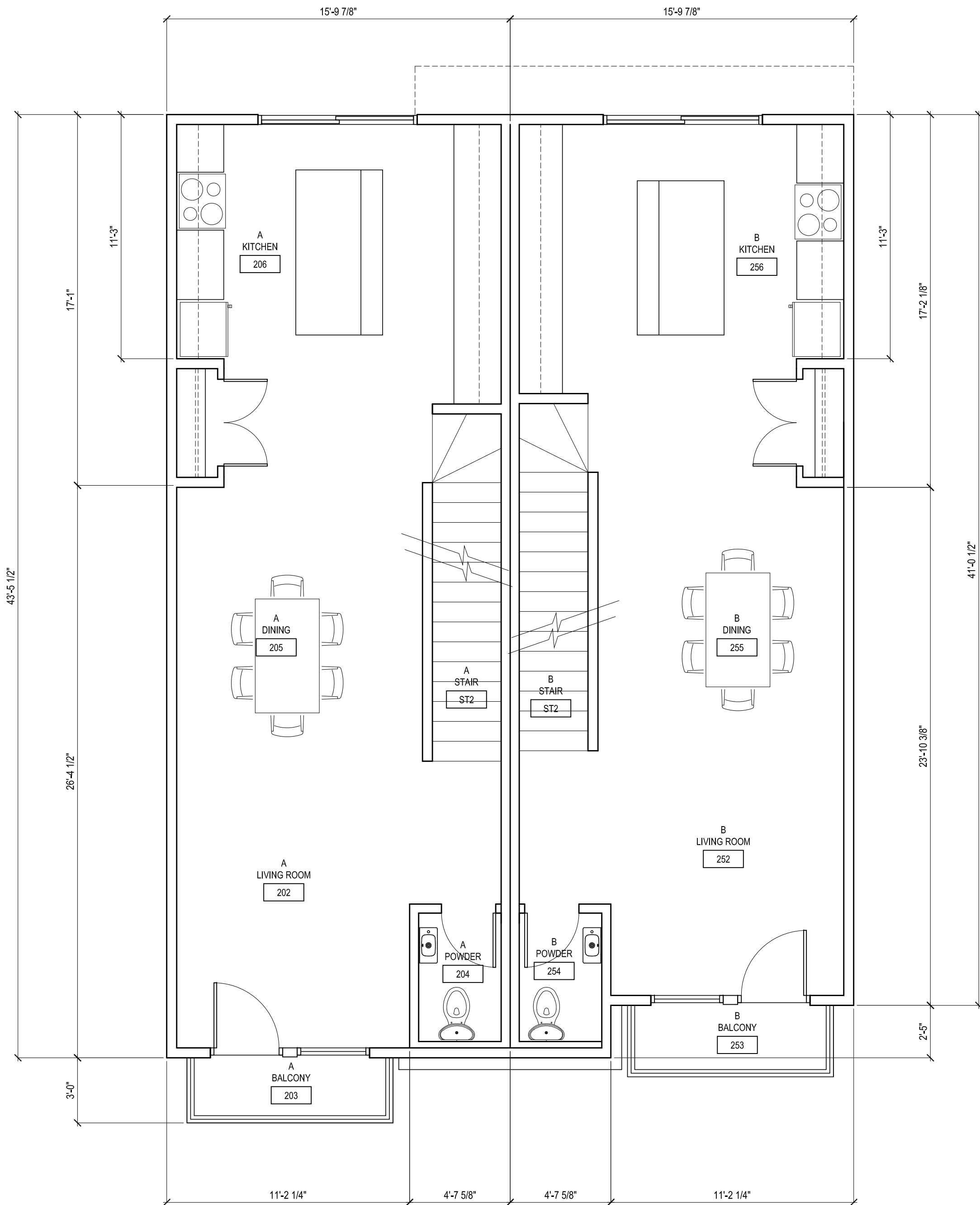
SHEET TITLE
TANDEM UNITS - TYPE B
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

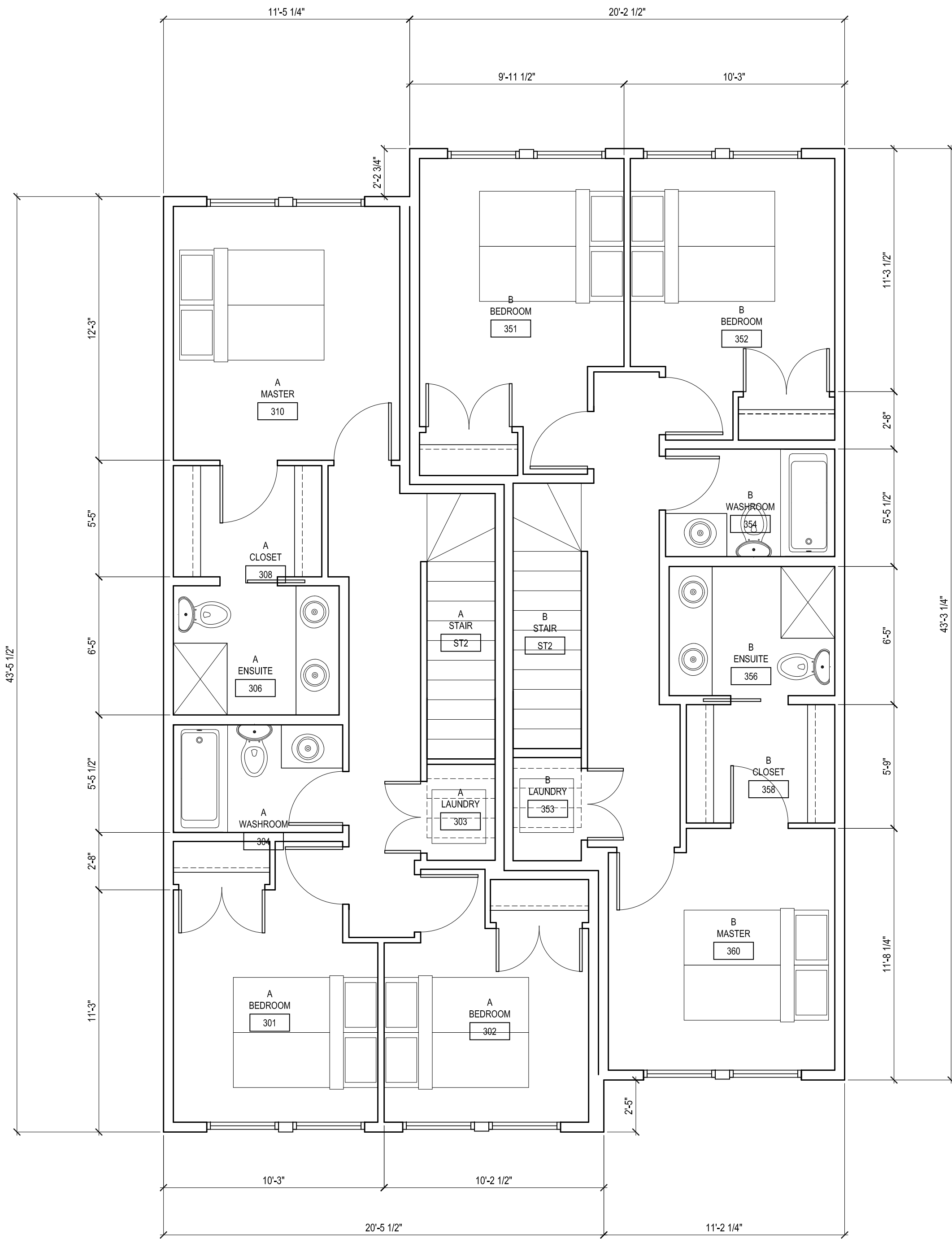
SHEET NUMBER

A3.3

SCALE
1/4"=1'-0"



1
A3.3
MID LEVEL - TYPE B
1/4" = 1'-0"



2
A3.3
TOP LEVEL - TYPE B
1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE C - SHORTENED PLAN,
EXTENDED FRONT DECK
QUAY LANE FRONTAGE

UNIT AREAS TYPE C:

SIDE C
ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 668 sq.ft.
TOP FLOOR = 679 sq.ft.

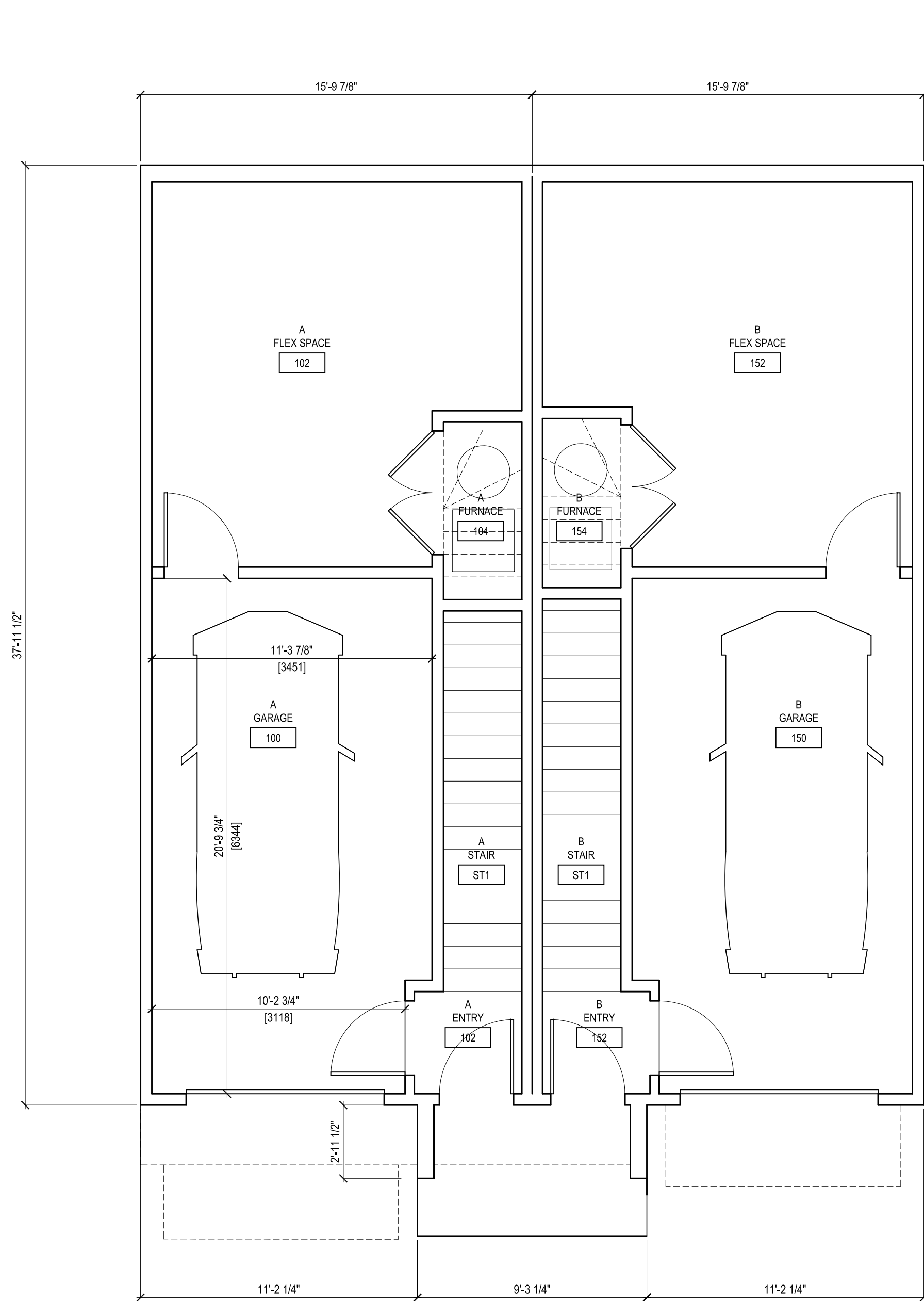
GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING, FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

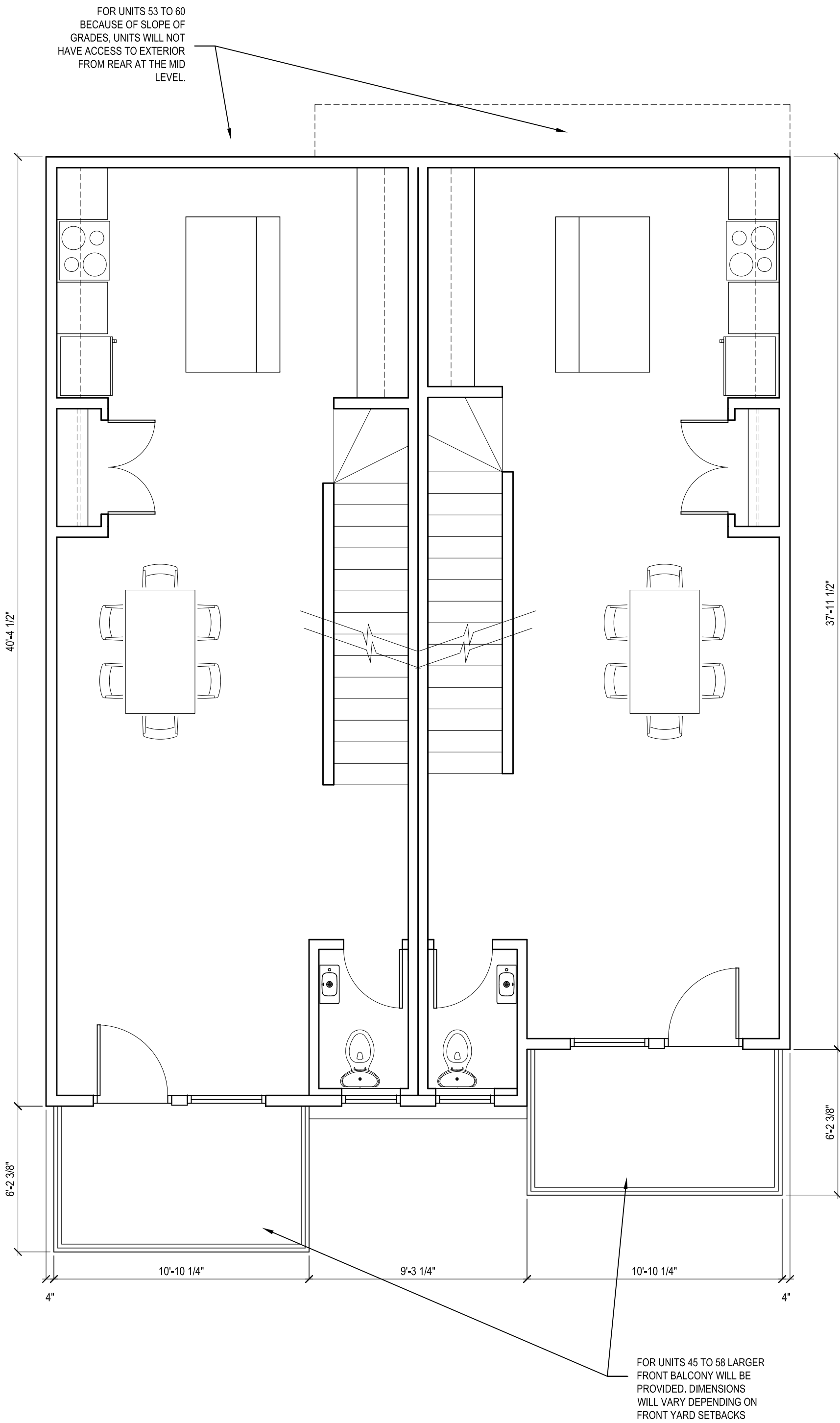
ISSUE	NO.	DATE
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DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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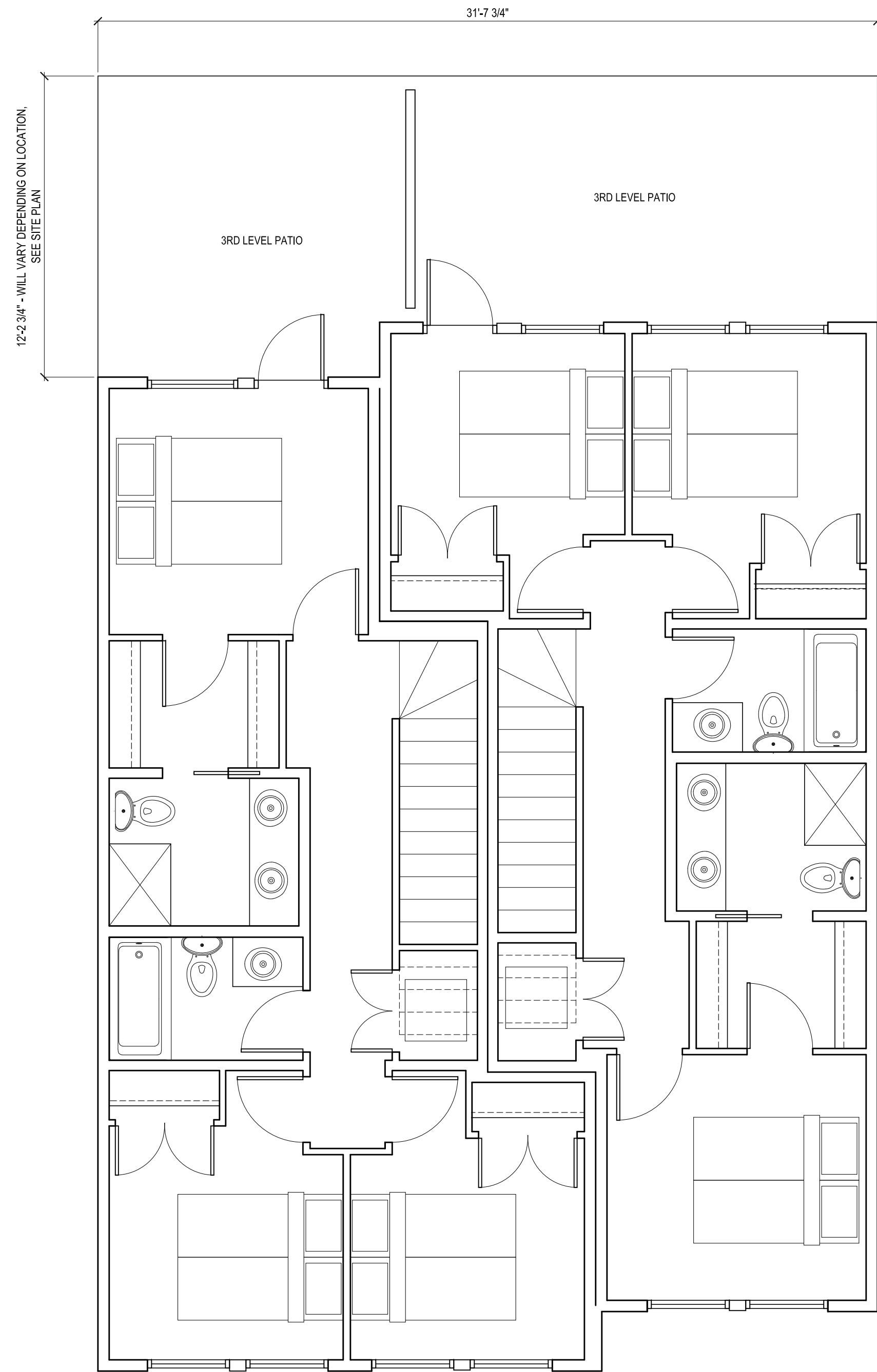
TANDEM UNITS 51 TO 58



1 ENTRY LEVEL - TYPE C
1/4" = 1'-0"



2 MID LEVEL - TYPE C
1/4" = 1'-0"



3 TOP LEVEL - TYPE C
1/4" = 1'-0"

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

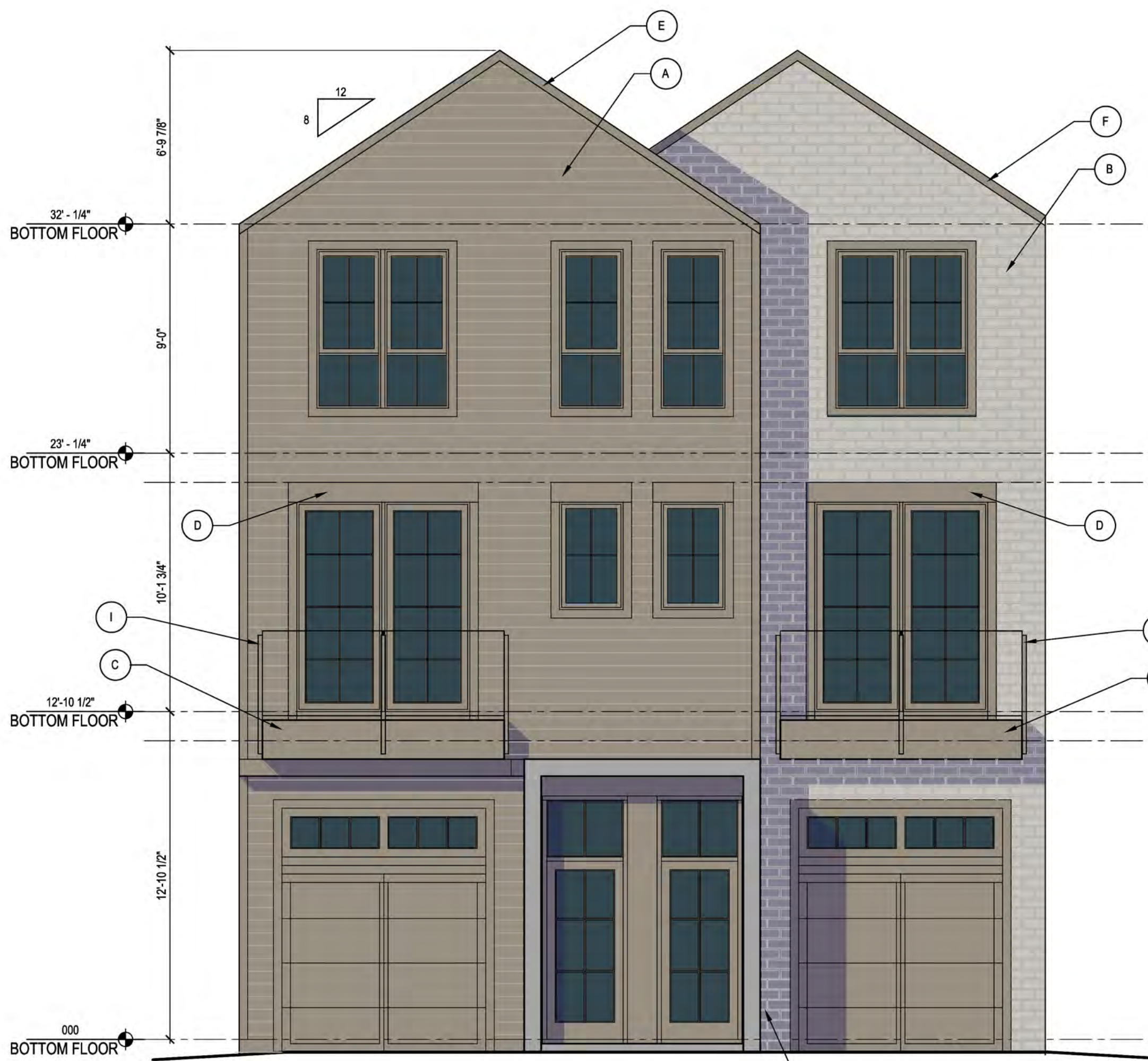
SHEET TITLE
TANDEM UNITS - TYPE C
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

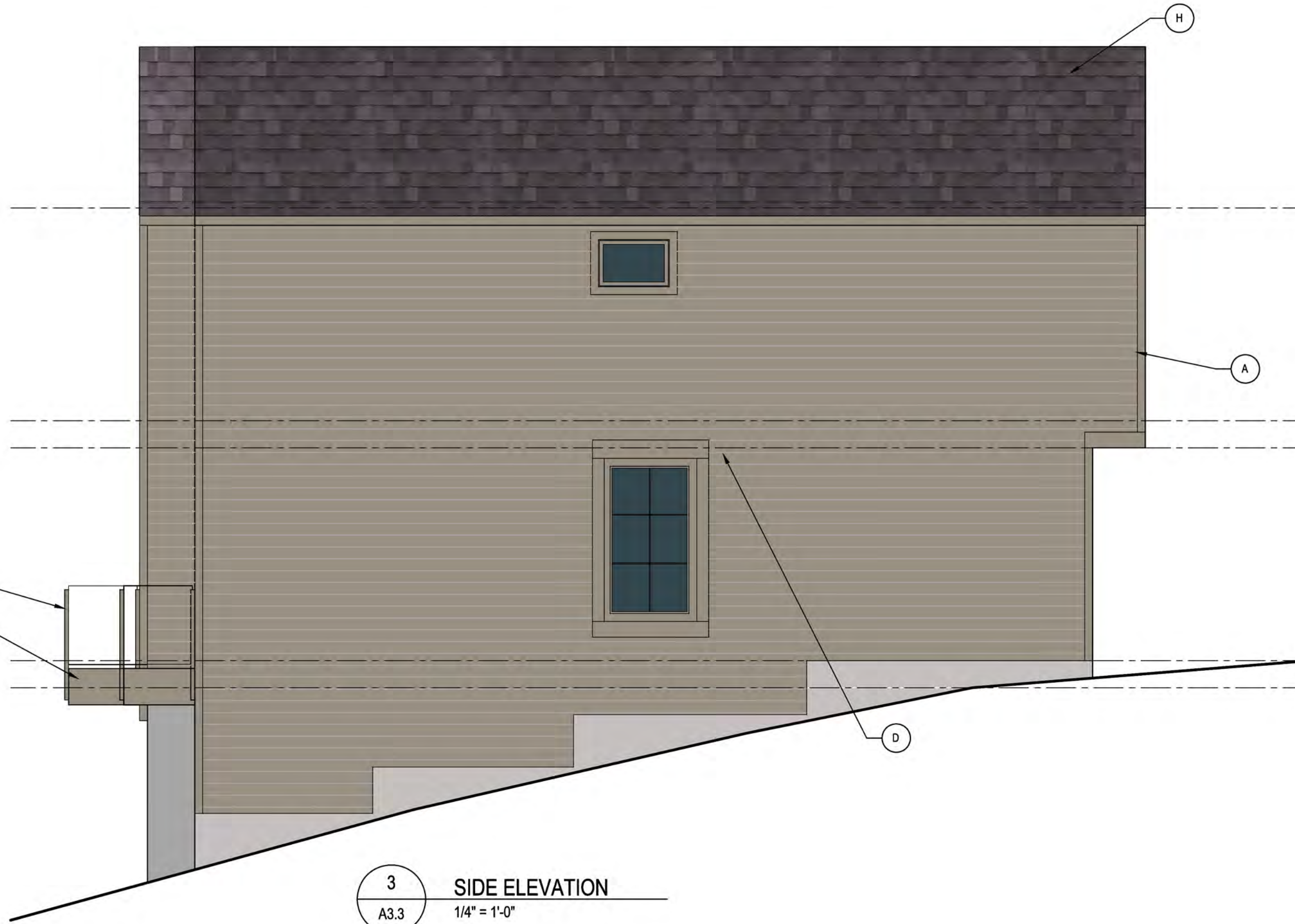
SHEET NUMBER

A3.4

SCALE
1/4"=1'-0"



1 STREET ELEVATION
A3.3 1/4" = 1'-0"



3 SIDE ELEVATION
A3.3 1/4" = 1'-0"



2 REAR ELEVATION
A3.3 1/4" = 1'-0"



4 SIDE ELEVATION
A3.3 1/4" = 1'-0"

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS
C	HARDIE PLANK CLAD ELEMENT COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS
F	PAINTED FASCIA COLOUR: 3 VARIATIONS
H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
	GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

GENERAL NOTES:

- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
- REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
- DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAPE ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
**TANDEM UNITS - TYPE C
TYPICAL ELEVATIONS
QUAY LANE**

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A3.5

SCALE
1/4"=1'-0"



ISSUE	NO.	DATE
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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS - TYPE C
STREETSCAPE ELEVATION
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.6

SCALE
1/8"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PERSPECTIVE STREET VIEW OF
SUNDANCE DRIVE SIDE-BY-SIDE
UNITS SHOWING GROUPINGS OF
LIGHT TONE UNITS AND MID
TONE UNITS IN DISTANCE

FINAL RENDERINGS BEING
COMPLETED AND WILL BE
SUBMITTED AS SOON AS DONE.
MINOR CHANGES ONLY TO
OCCUR TO IMAGES

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PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS
RENDERED VIEW
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.7

SCALE
N.T.S.

GATEHOUSE SUNDANCE DEVELOPMENT

Issued for Development Permit

Contact Information

VDZ+A

Project Landscape Architecture

Kelowna Studio
302 - 1150 Sunset Drive
Kelowna, British Columbia, V1Y 9Z5

Fort Langley Studio
102 - 9181 Church Street
Fort Langley, British Columbia, V1M 2R8

Mount Pleasant Studio
102-355 Kingsway
Vancouver, British Columbia, V5T 3J7

Primary project contact:
Kim McNamee
Landscape Architect
kim@vdz.ca
778 298 9738

Alternate contacts (incase away):
Stephen Heller
Associate Landscape Architect
stephen@vdz.ca
604 546 0925

Key Project Contacts

GATEHOUSE

Project Owner

PO BOX 2585 Stn Sardis
Chilliwack, BC V2R 1A8

Mike Shepherd
mike@madebygatehouse.com
604 819 2192

Distefano Jaud Architecture

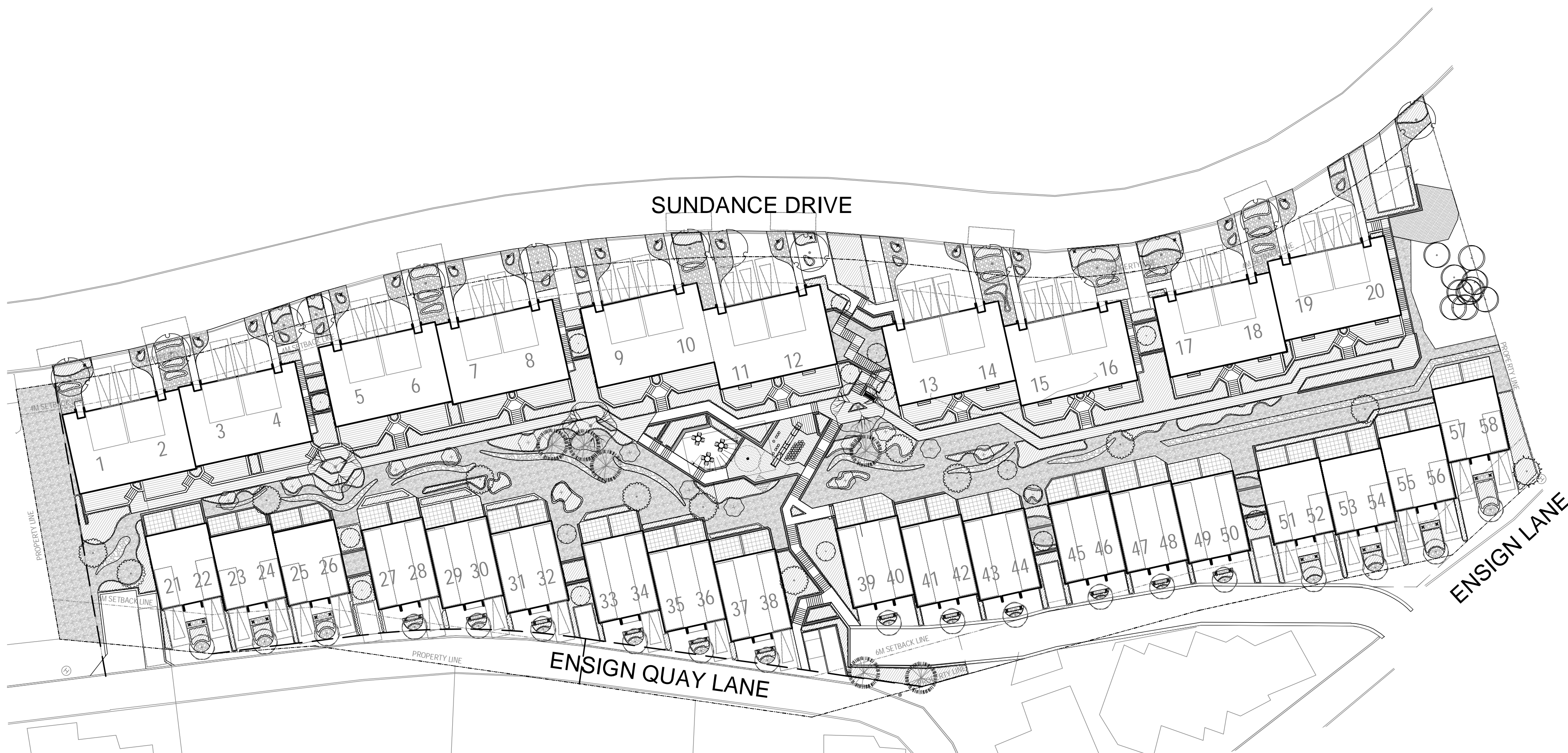
Project Architect

3, 1331 Ellis Street
Kelowna, BC V1Y 1Z9

Carlo Distefano
carlo@distefanojud.com
250 868 9278

Sheet List Table

Sheet Number	Sheet Title
L-00	COVER PAGE
L-01A	LANDSCAPE SITE PLAN WEST
L-01B	LANDSCAPE SITE PLAN CENTER
L-01C	LANDSCAPE SITE PLAN EAST
L-02	PLANT PALETTE
L-03A	FENCE AND GRADING PLAN WEST
L-03B	FENCE AND GRADING PLAN CENTER
L-03C	FENCE AND GRADING PLAN EAST
LS-01	SECTION A & B
LS-02	SECTION C & D
LS-03	SECTION E & F
LD-01	DETAILS
LD-02	DETAILS
LD-03	DETAILS
LD-04	DETAILS



1 SITE PLAN OVERVIEW
Scale 1:500



2 LOCATION MAP
NTS

VDZ+A

LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO
100-9181 Church St | 102-355 Kingsway
Fort Langley, BC | Vancouver, BC
V1M 2R8 | V5T 3J7
www.vdz.ca 604-882-0024

8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
No.	By:	Description	Date
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Project: PROVIDENCE / SUNDANCE			
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Drawn:	Stamp:		
AH ET			
Checked:			
SH			
Approved:	Original Sheet Size:		
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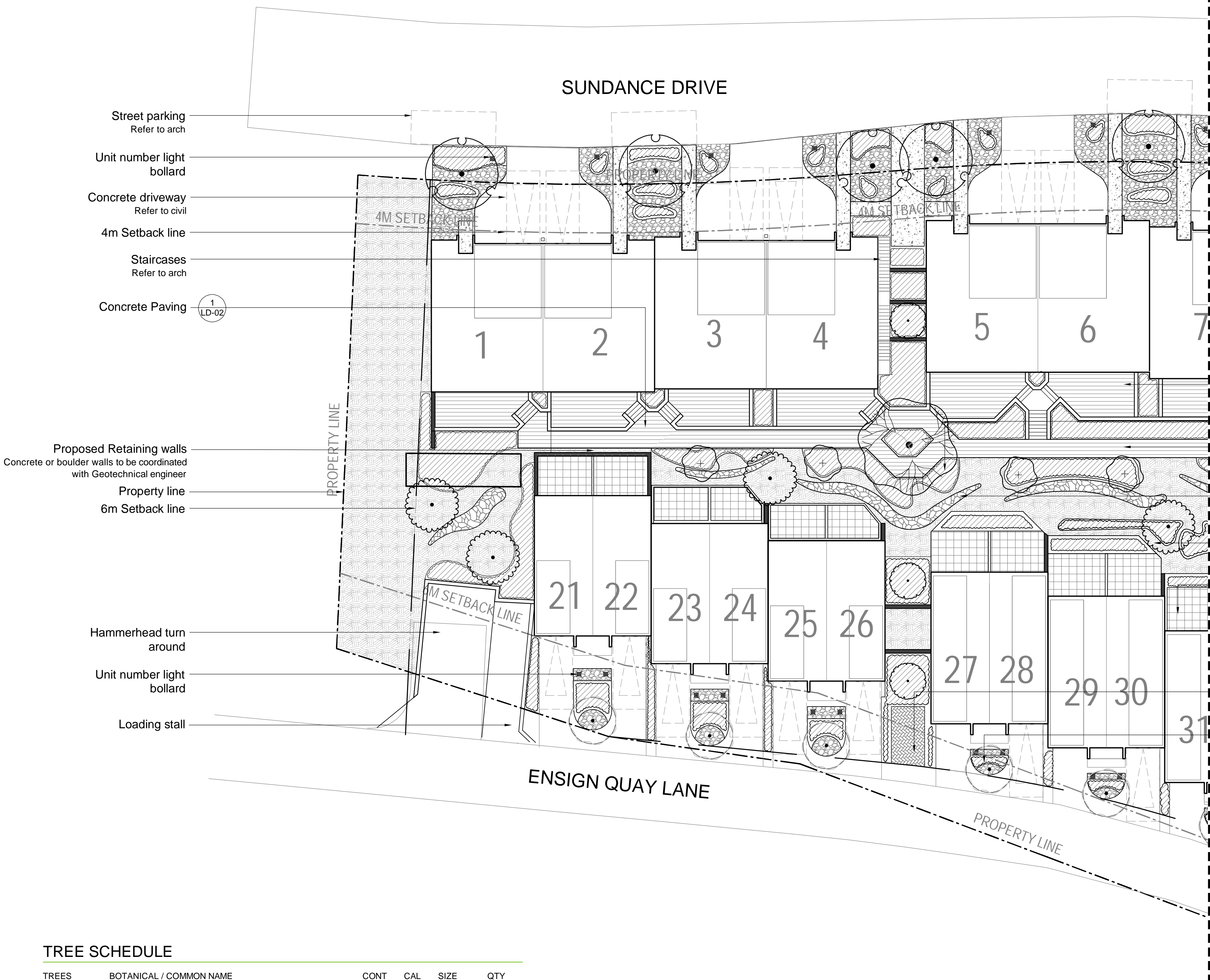
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COVER PAGE



VDZ Project #:
DP2022-16

Drawing #:
L-00

Z:\PROJECTS\DEVELOPMENT PERMIT\ACTIVEDP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWGS\SHEETS\L-01 SITE PLAN.DWG



REFER TO L-01B

MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 3" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

VDZ+A

LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO

100-9181 Church St | 102-355 Kingsway

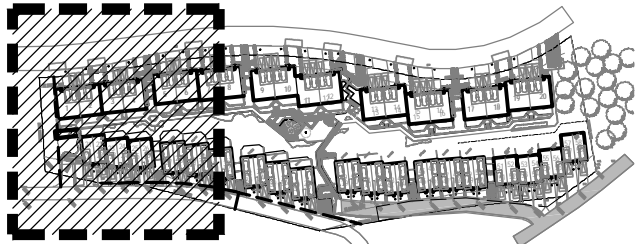
Fort Langley, BC | Vancouver, BC

VIM 2881 VST 317

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Key Map (NTS)



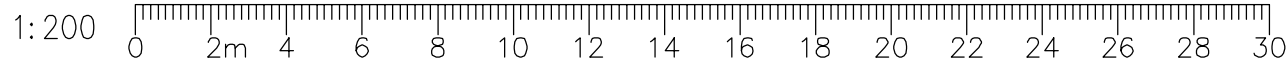
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TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	19
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3



MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 2" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

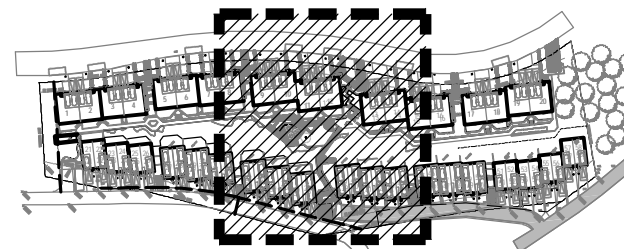
KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

TREE SCHEDULE

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	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

1:200 0 2m 4 6 8 10 12 14 16 18 20 22 24 26 28 30

Key Map (NTS)



No.	By:	Description	Date
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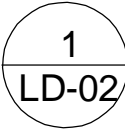
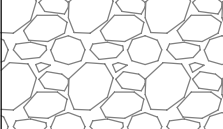
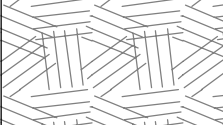
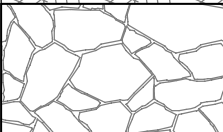


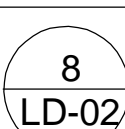
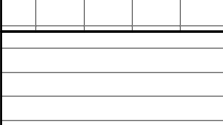
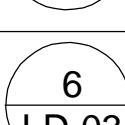
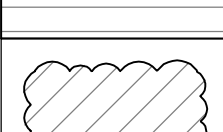
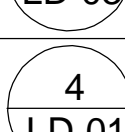
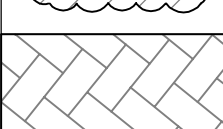
LANDSCAPE SITE PLAN CENTER




Drawing #:

DP2022-16

Drawing #:

L-01B

KEY	REF.	DESCRIPTION
		CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 3" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
		COARSE BOULDER SLOPE RETENTION
		HYDRAPRESSED SLABS Lower patios typ.
		COMPOSITE WOOD DECK Upper patios typ.
		XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers


KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL, REFER TO CIVIL AND STRUCTURAL PLAN
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

REVISIONS TABLE FOR DRAWINGS

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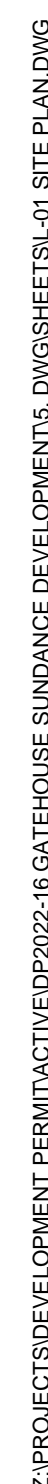
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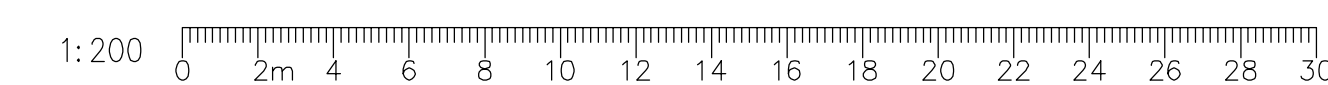


VDZ Project #:
DP2022-16

Drawing #: L-01C



TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum `Schmidt` / Rocky Mountain Glow Maple	B&B		6cm cal.	19
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos `Skyline` / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta `Contorta` / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

























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Tree Schedule

Trees	Botanical / Common Name	Cont	Cal	Size	Qty
	Acer grandidentatum `Schmidt` / Rocky Mountain Glow Maple	B&B		6cm cal.	19
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos `Skyline` / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta `Contorta` / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

Plant Schedule

SHRUBS	BOTANICAL / COMMON NAME	CONT	
	Arbutus unedo 'Compacta' / Dwarf Strawberry Tree	#3	
	Artemisia tridentata / Big Sagebrush	#2	
	Berberis thunbergii 'Concorde' / Concorde Barberry	#2	
	Caryopteris x clandonensis / Bluebeard	#2	
	Ericameria nauseosa / Rubber Rabbitbrush	#2	
	Potentilla fruticosa 'Tangerine' / Tangerine Potentilla	#2	
	Rhus glabra / Smooth Sumac	#2	
DWARF CONIFERS	BOTANICAL / COMMON NAME	CONT	
	Juniperus chinensis 'Torulosa' / Hollywood Juniper	#3	
	Juniperus sabina / Savin Juniper	#2	
	Pinus mugo 'Pumilio' / Mugo Pine	#2	
GRASSES	BOTANICAL / COMMON NAME	CONT	
	Chasmanthium latifolium / Wood Oats	#1	
	Elymus mollis / American Dunegrass	#1	
PERENNIALS	BOTANICAL / COMMON NAME	CONT	
	Balsamorhiza sagittata / Arrowleaf Balsamroot	#1	
	Centranthus ruber / Red Valerian	#1	
	Echinacea purpurea 'Firebird' / Firebird Coneflower	#1	
	Eryngium alpinum 'Blue Star' / Sea Holly	#1	
	Perovskia x 'Little Spire' / Russian Sage	#1	
	Sedum x 'Thunderhead' / Thunderhead Sedum	#1	
	Yucca glauca / Narrowleaf Yucca	#2	
GROUND COVERS	BOTANICAL / COMMON NAME	CONT	SPACING
	Arctostaphylos uva-ursi / Kinnikinnick	#1	300mm
	Juniperus horizontalis 'Andorra' / Andorra Juniper	#1	600mm
	Thymus pseudolanuginosus / Woolly Thyme	#1	300mm



Ericameria nauseosa



Balsamorhiza sagittata



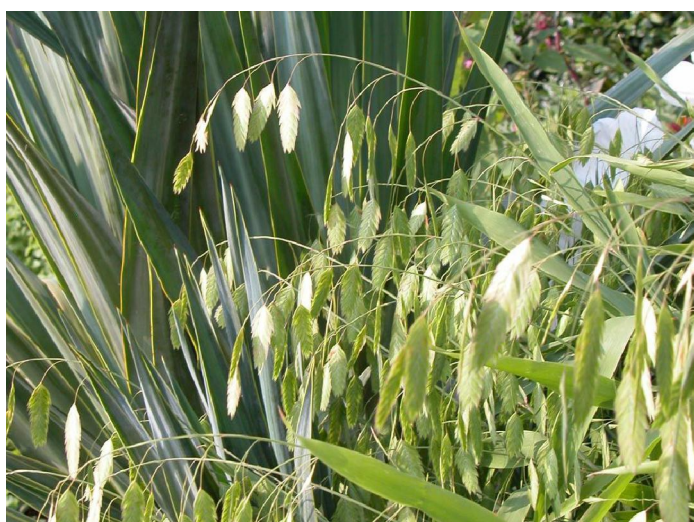
Potentilla fruticosa 'Tangerine'



Arbutus unedo 'nana'



Coreopsis 'Moonbeam'



Chasmanthium latifolium



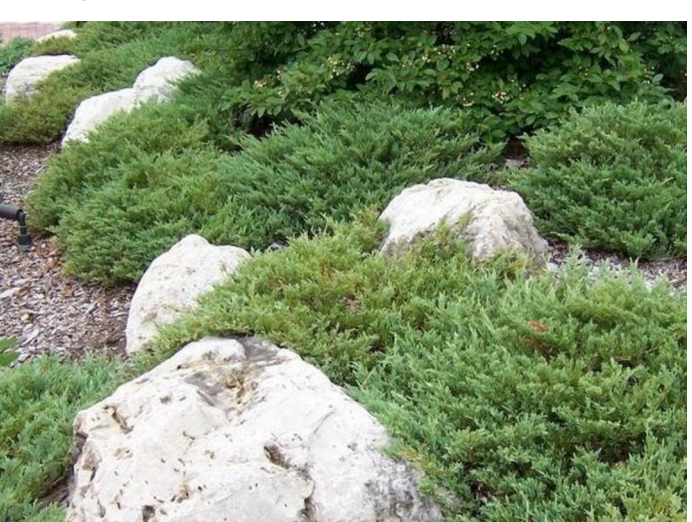
Elymus mollis



Centranthus Ruber



Eryngium Alpinum 'Blue star'



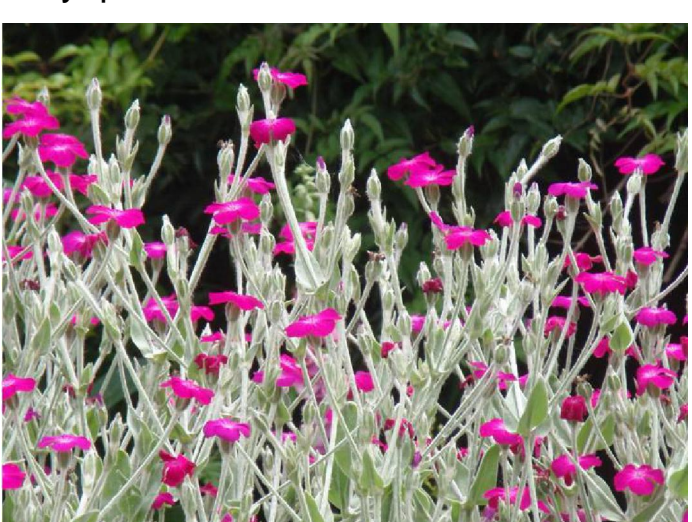
Juniperous Horizontalis 'Andorra'



Juniperous Sabina



Caryopteris x Clandonensis



Lychnis



Sedum x `Thunderhead`



Berberis Thunbergii 'Concorde'



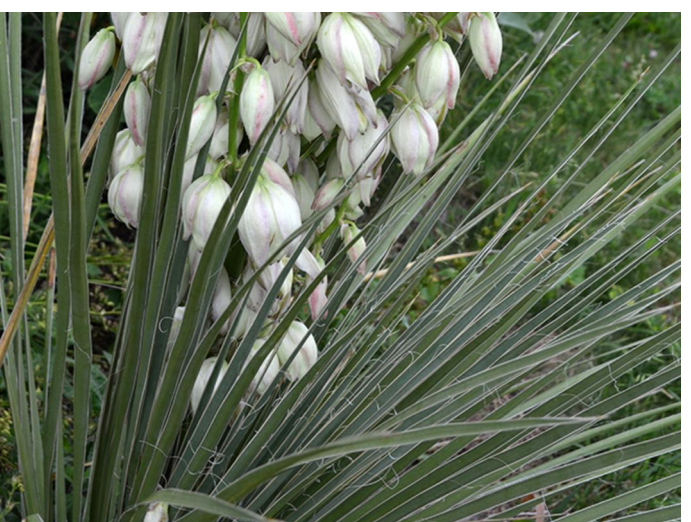
Artemisia Tridentata



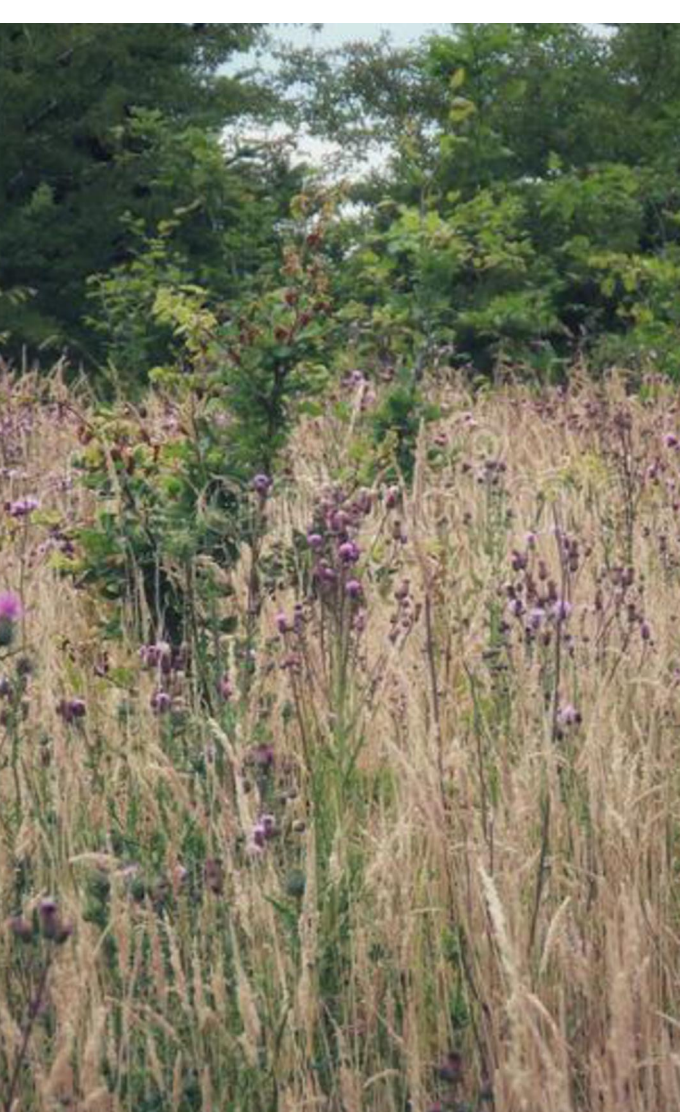
Thymus Pseudolanuginosus



Arctostaphylos Uva-Ursi



Yucca Glauca



Dry meadow seed mix

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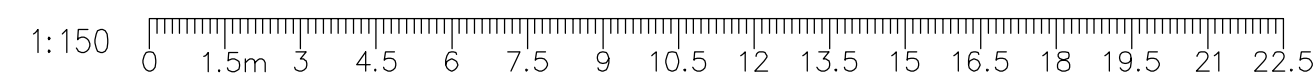
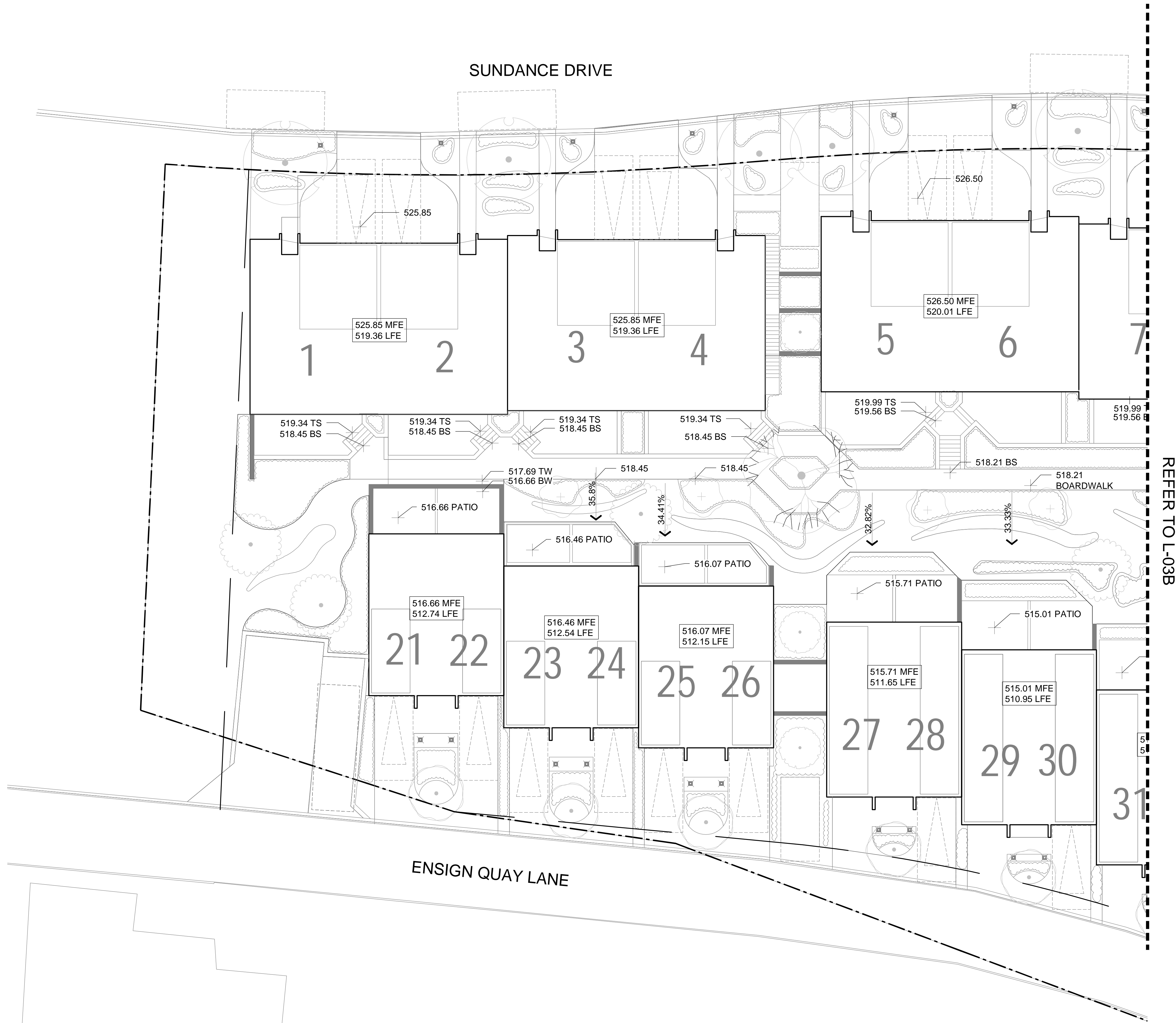
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4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
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Key Map (NTS)



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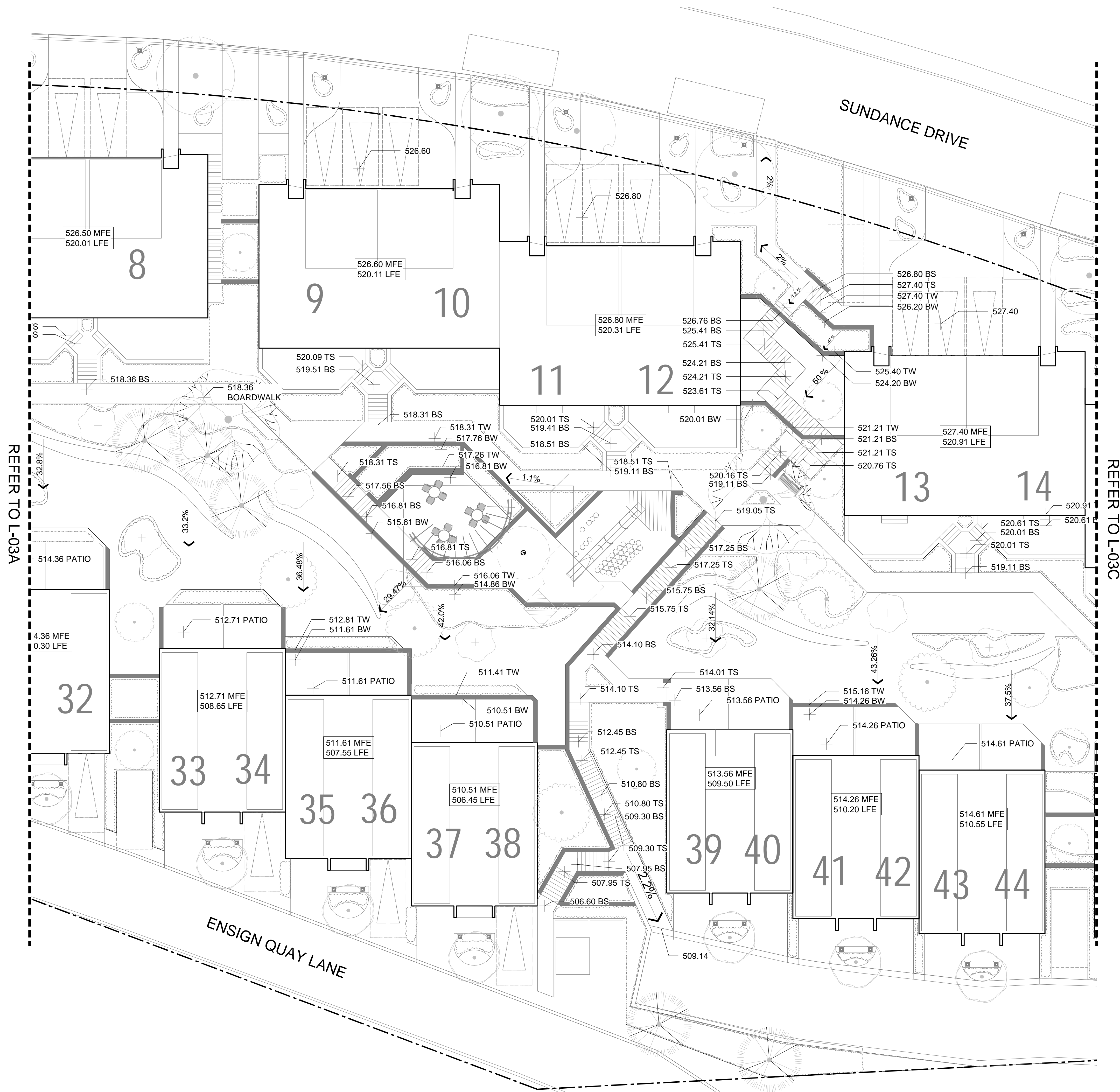
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DP2022-16

Drawing #:

L-03A

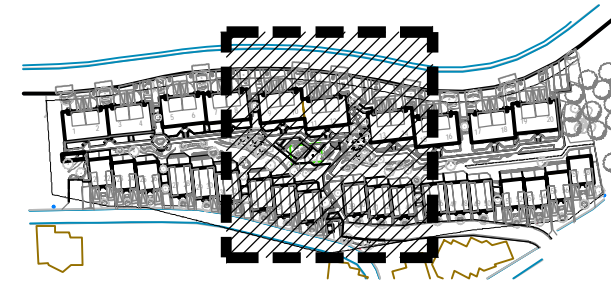
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REFER TO L-03A

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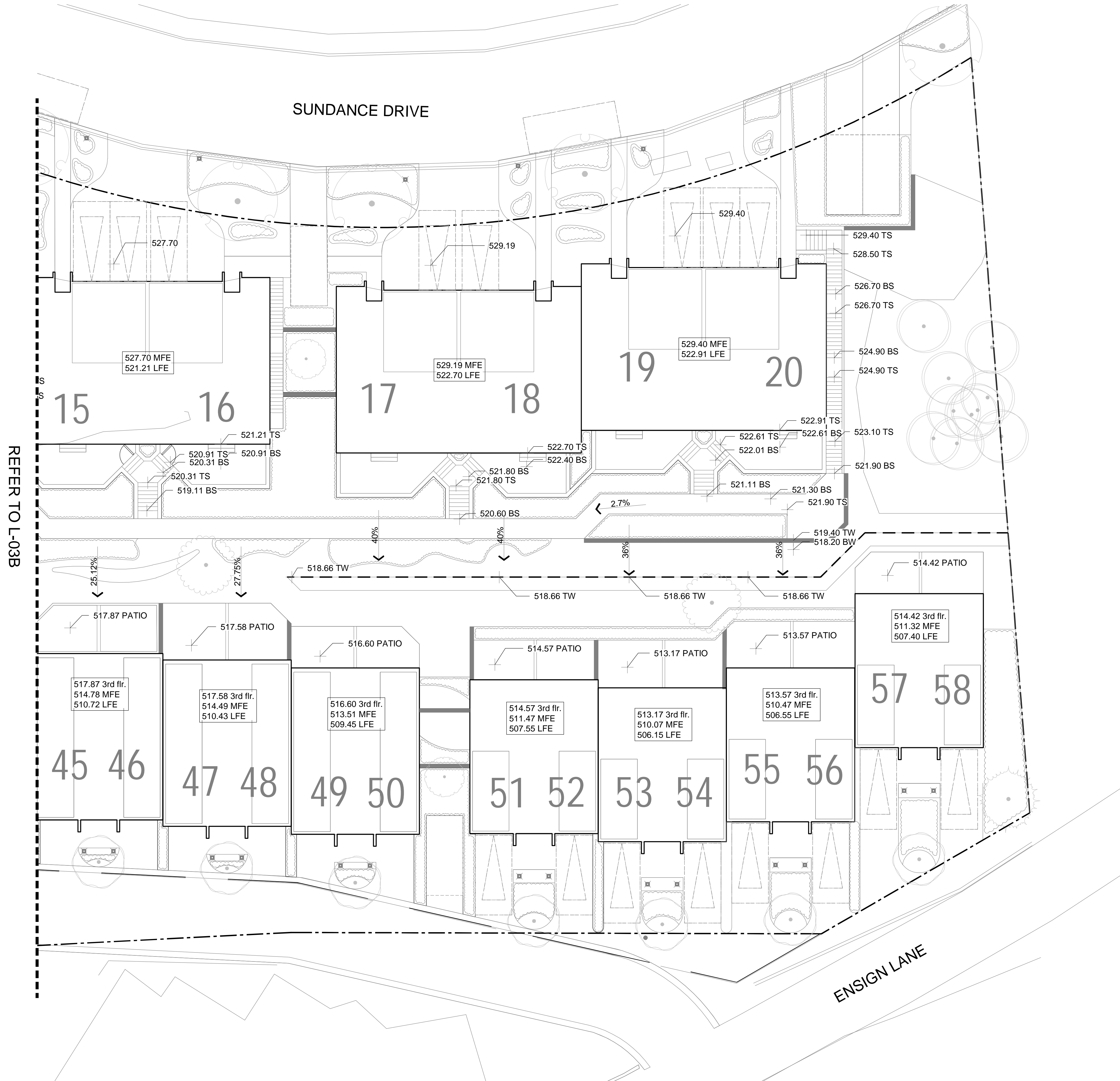
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VDZ Project #:
DP2022-16

Drawing #:
L-03B

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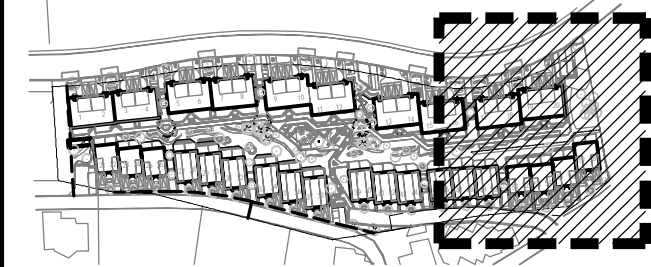


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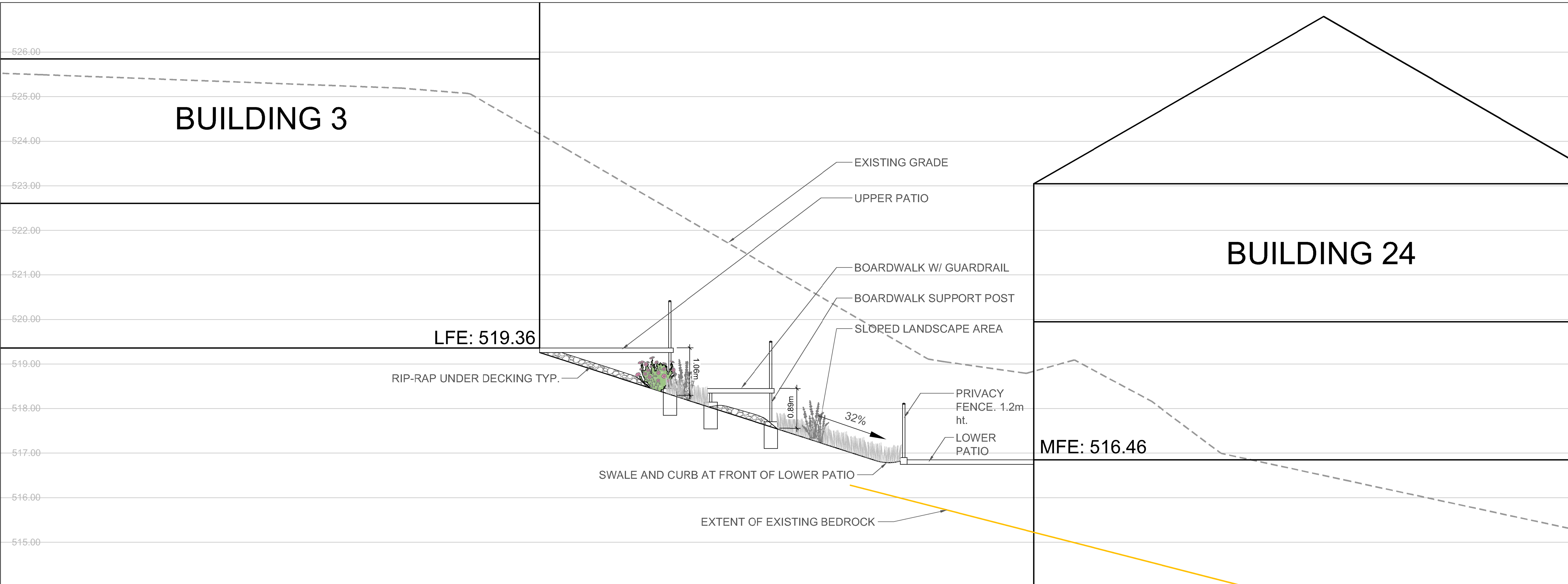
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Drawing Title:
FENCE AND GRADING PLAN EAST

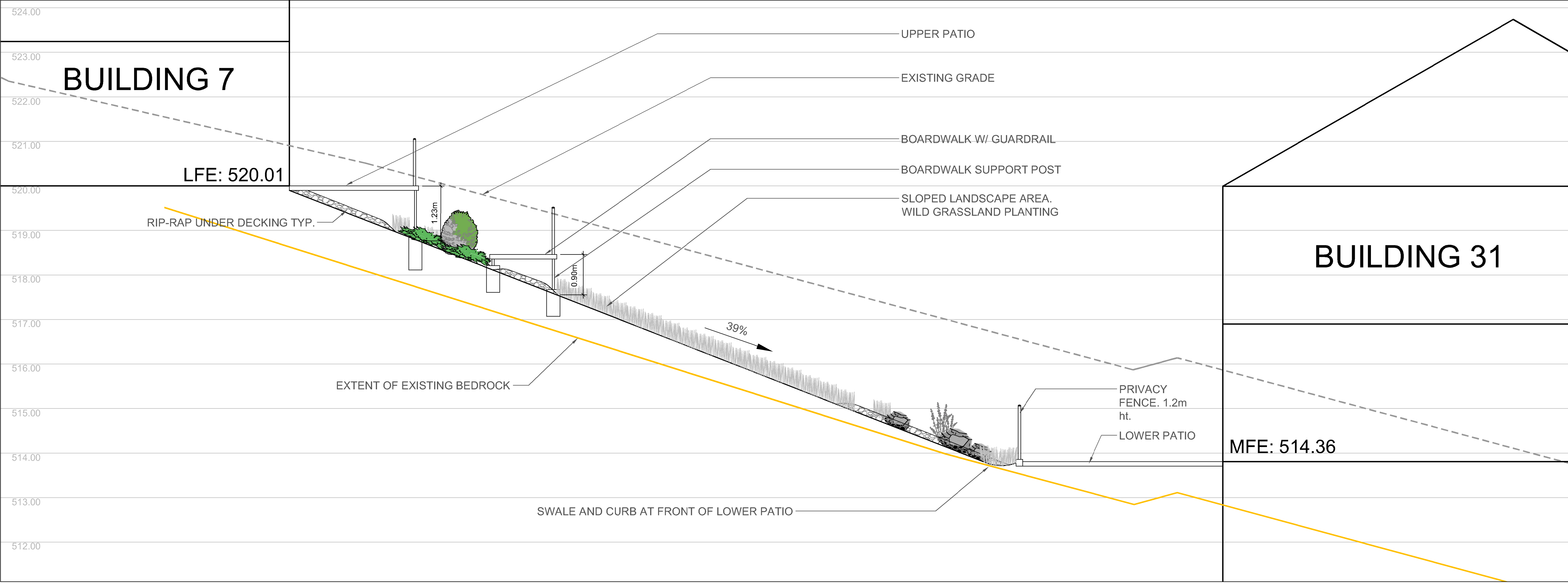


VDZ Project #:
DP2022-16

Drawing #:
L-03C



A SECTION THROUGH UNIT 3 AND 24
Scale 1:50



B SECTION THROUGH UNIT 7 AND 31
Scale 1:50

LEGEND

	EXISTING GRADE
	EXISTING BEDROCK

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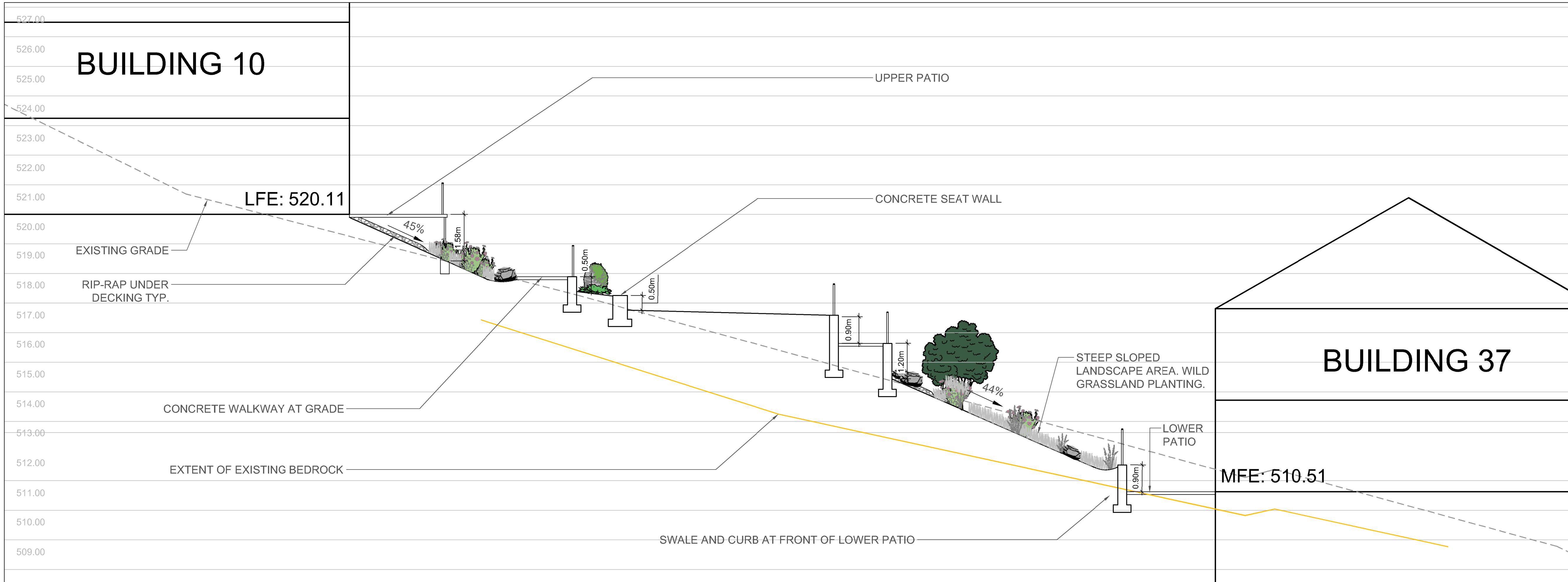
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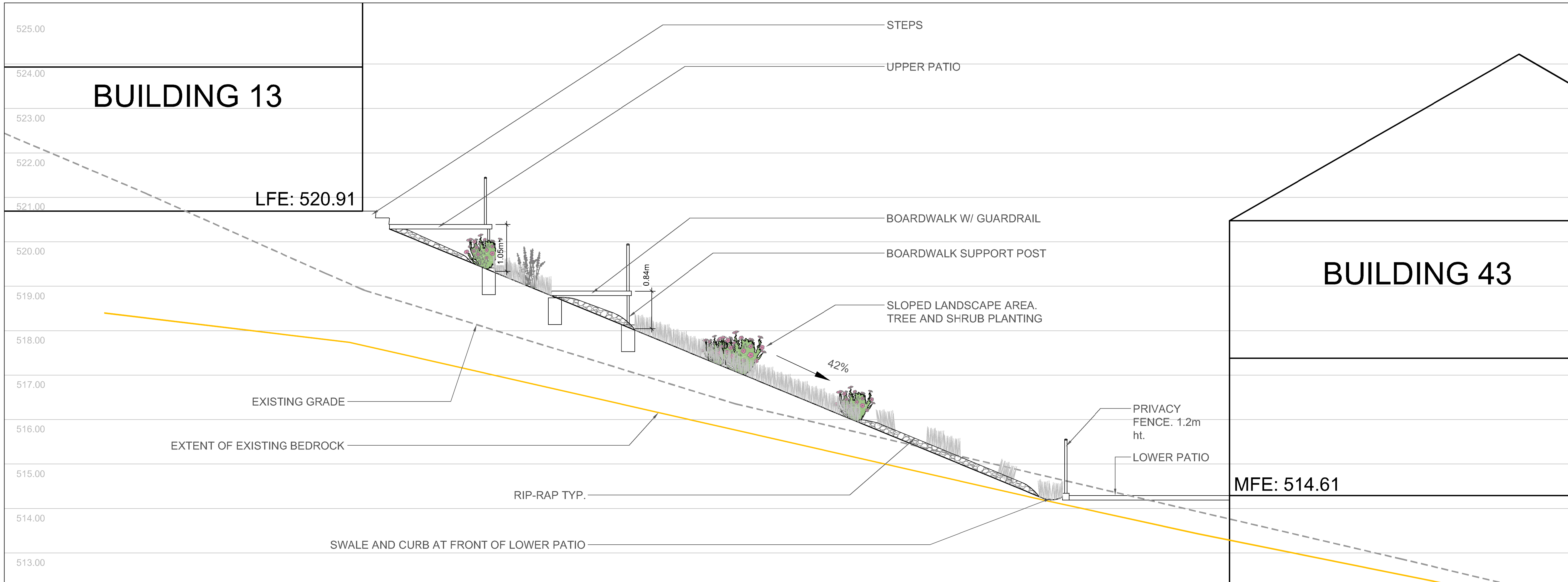
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C SECTION THROUGH UNIT 10 AND 37
Scale 1:75



D SECTION THROUGH UNIT 13 AND 43
Scale 1:50

LEGEND	
	EXISTING GRADE
	EXISTING BEDROCK

No.	By:	Description	Date
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LEGEND

---	EXISTING GRADE
---	EXISTING BEDROCK

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No.	By:	Description	Date

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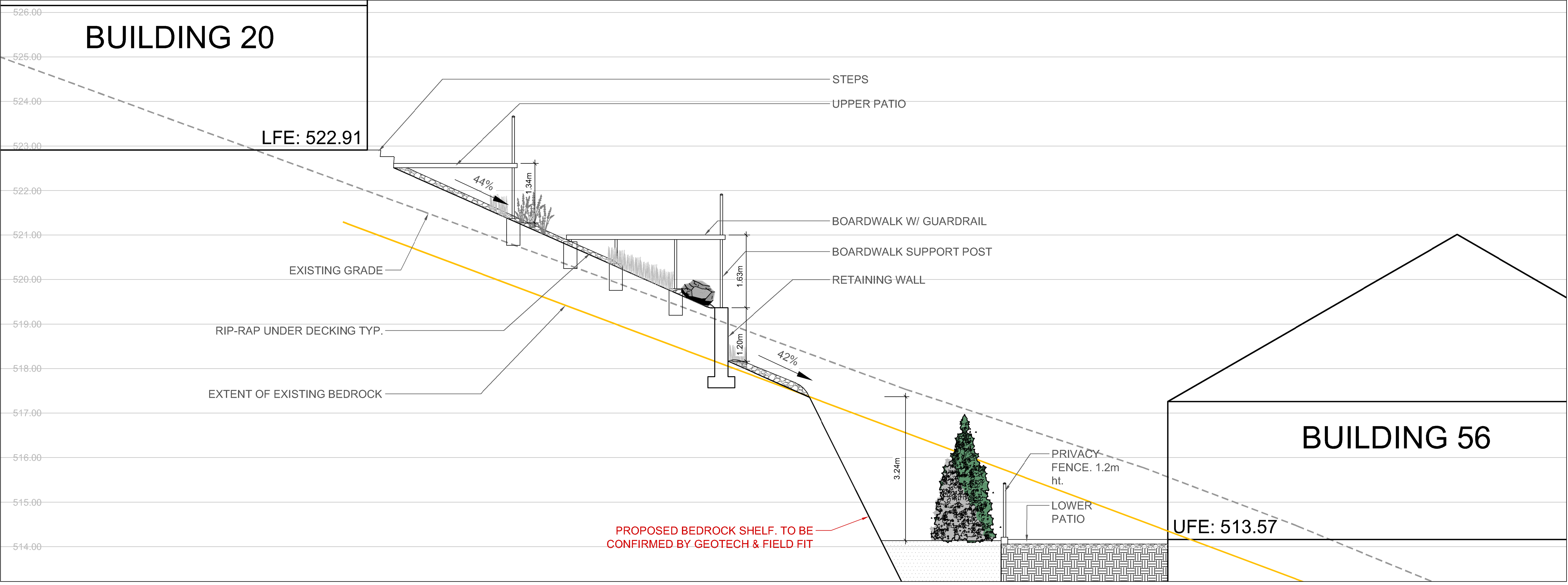
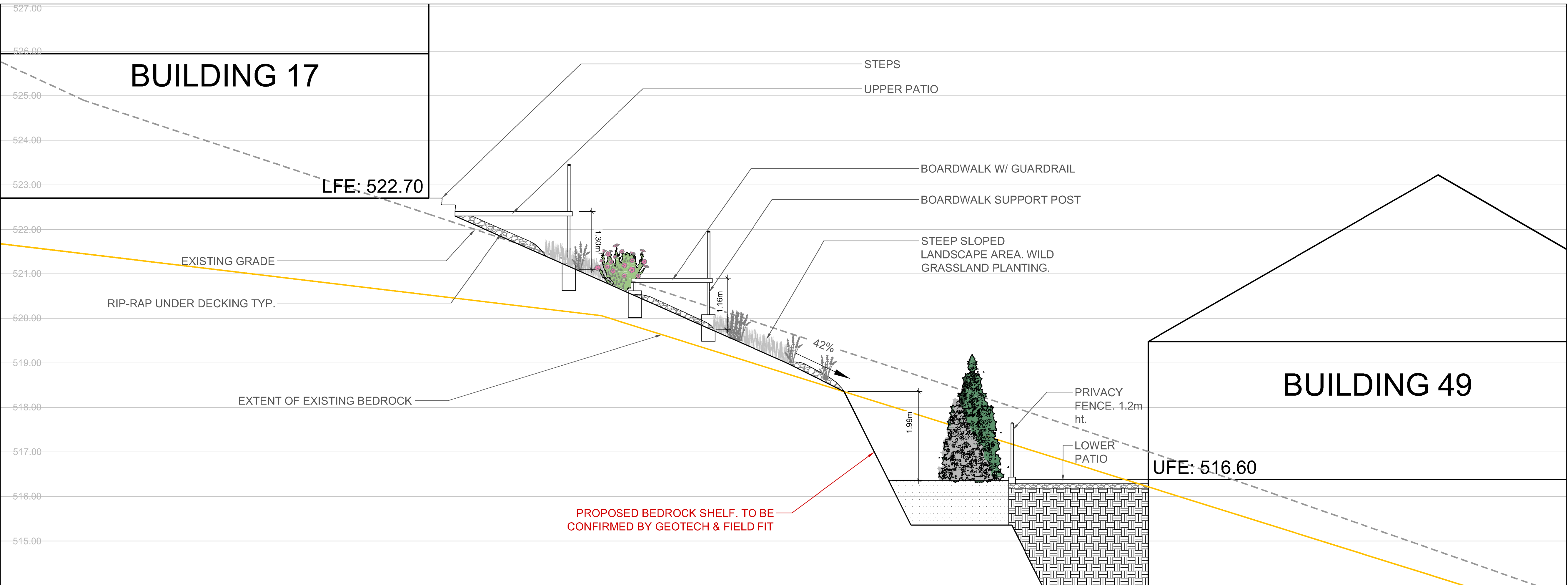
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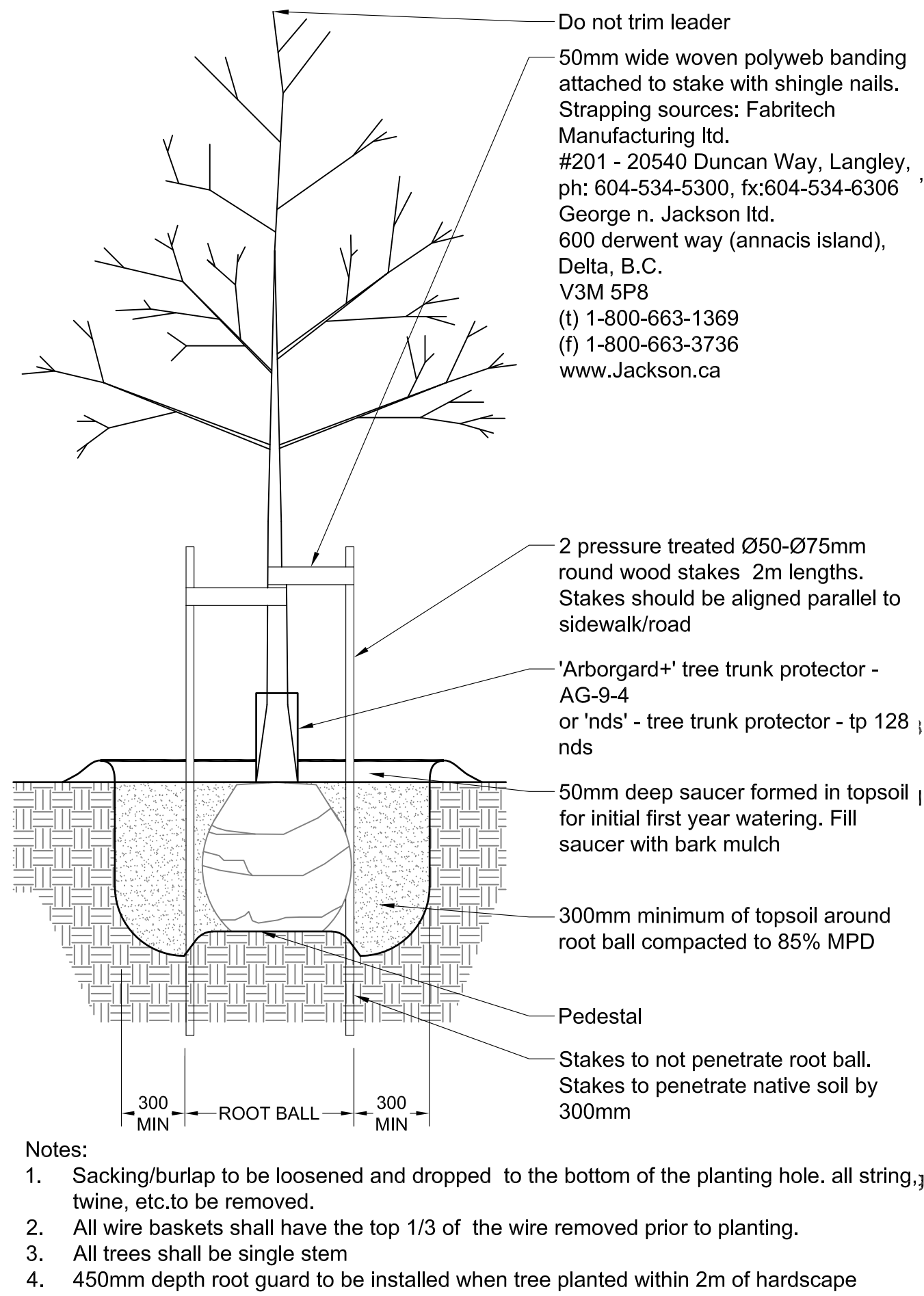
LANDSCAPE SECTIONS

VDZ Project #:
DP2022-16

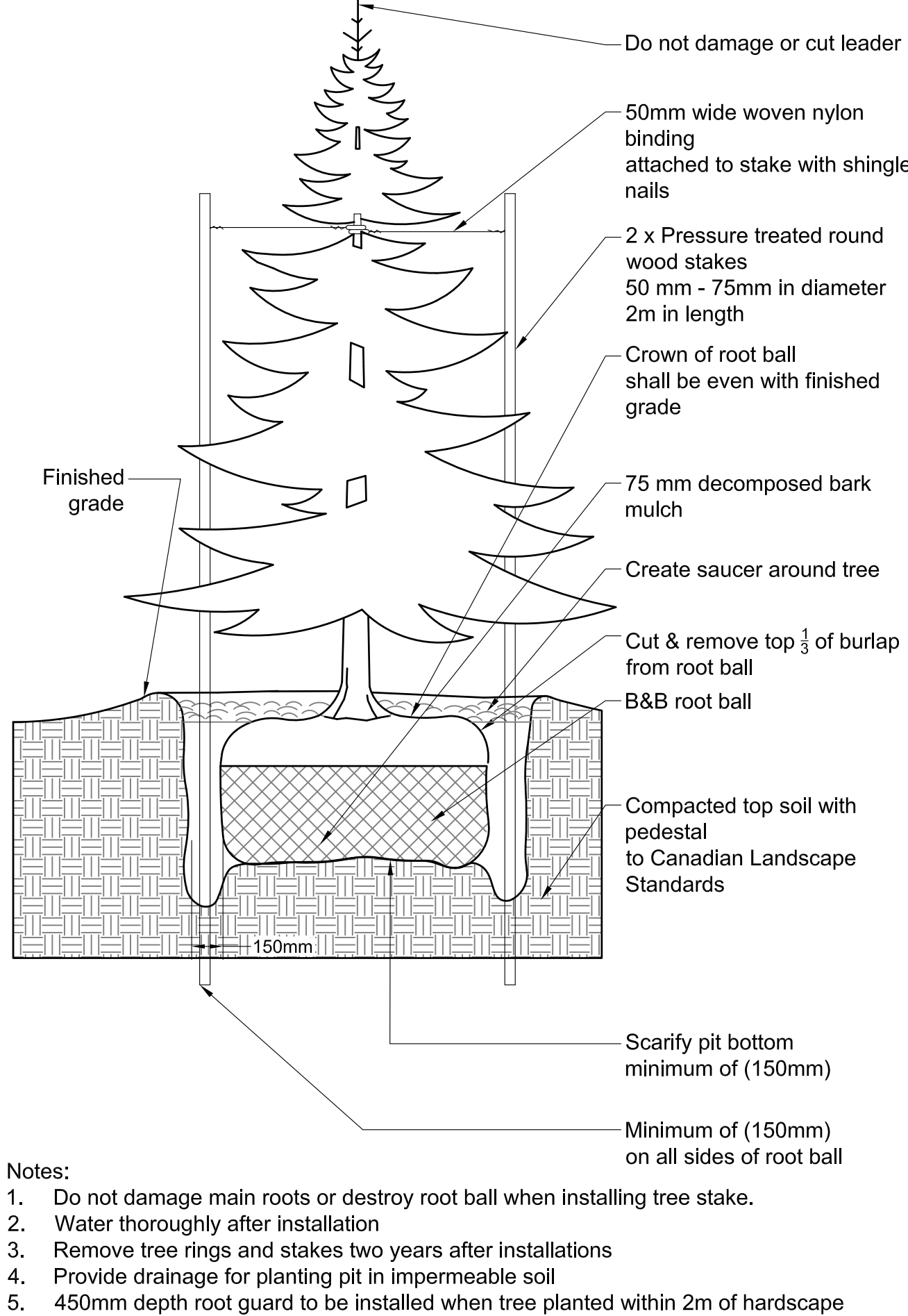
Drawing #:
LS-03



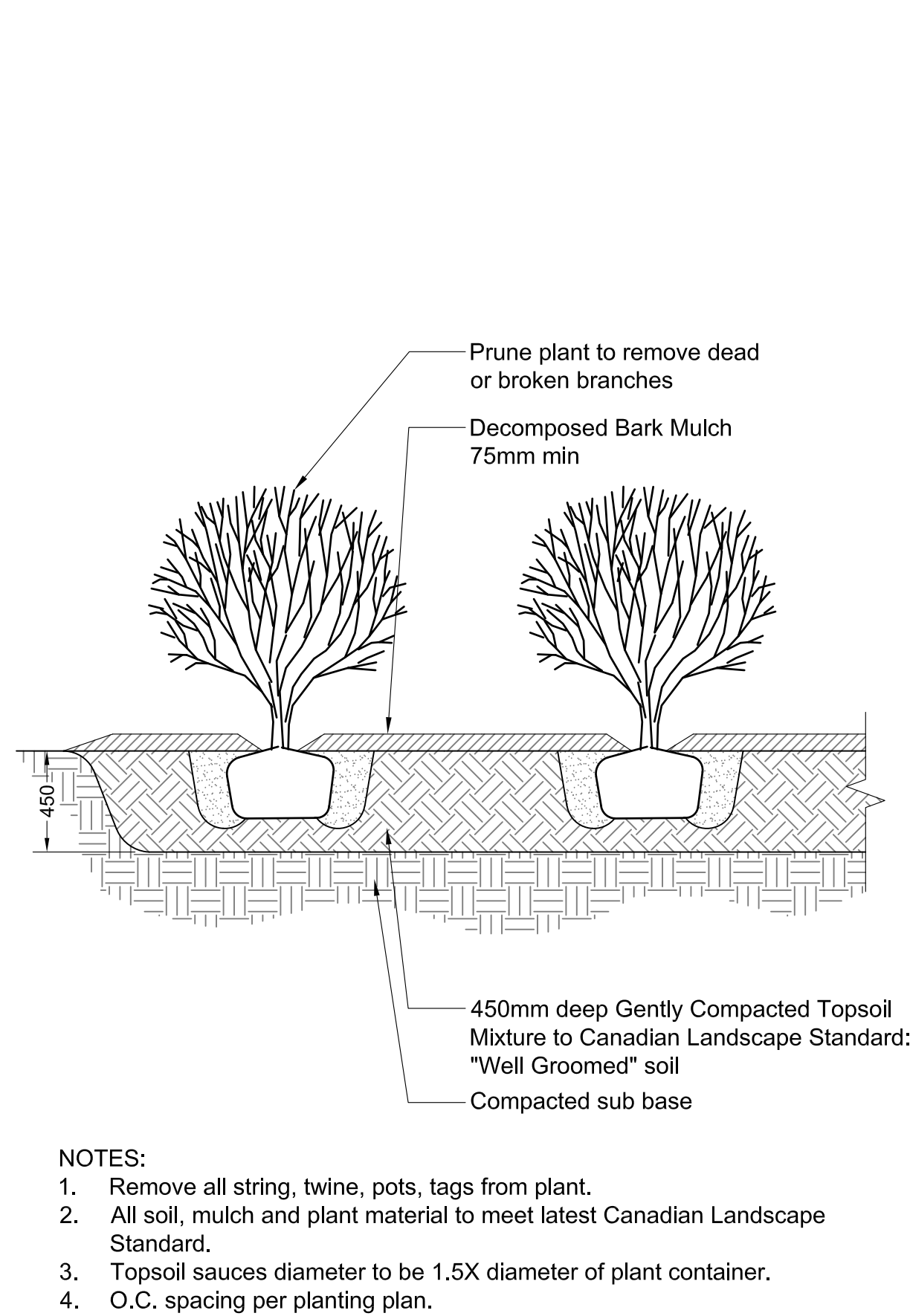
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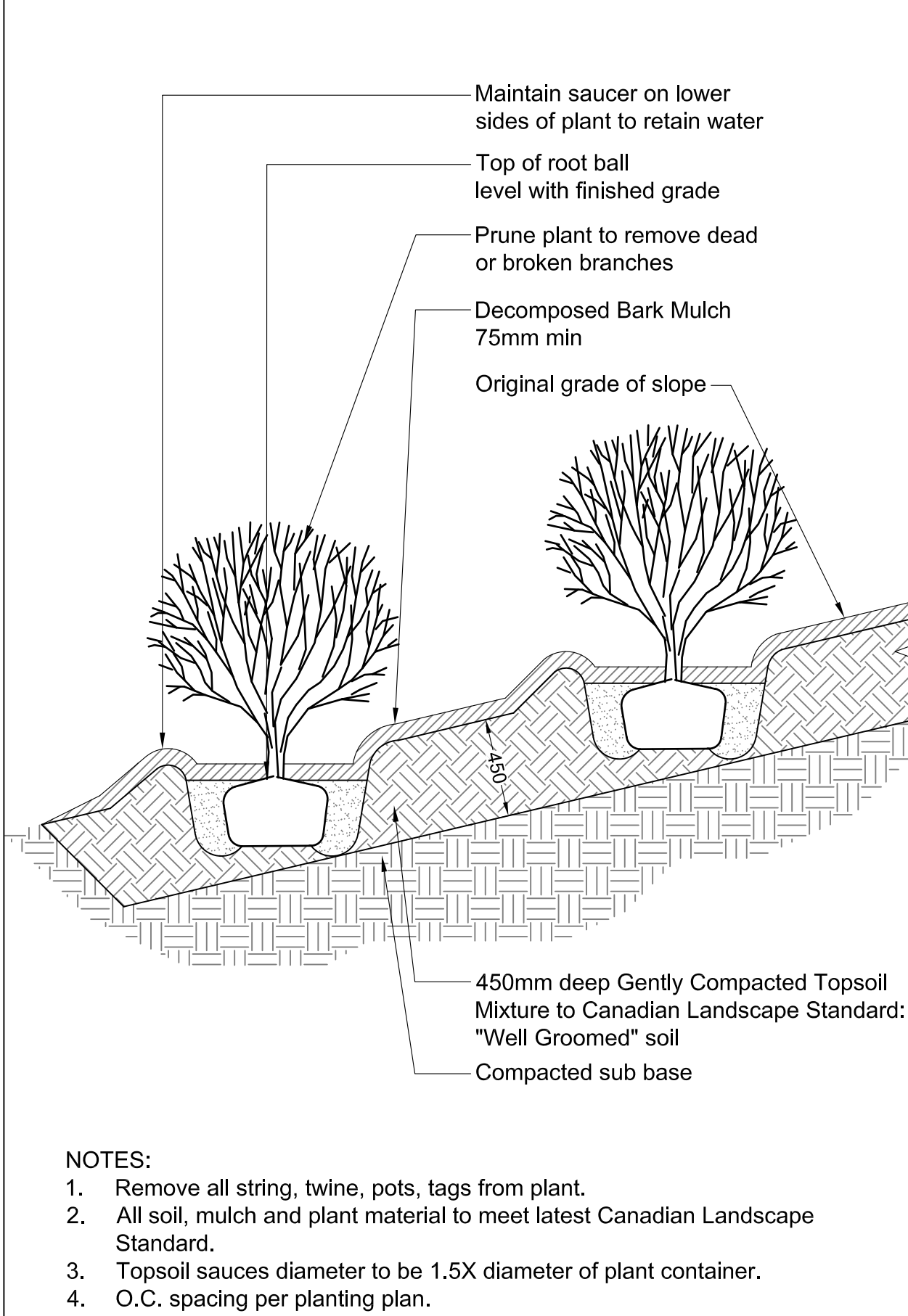
1 DECIDUOUS TREE PLANTING
Scale 1:25



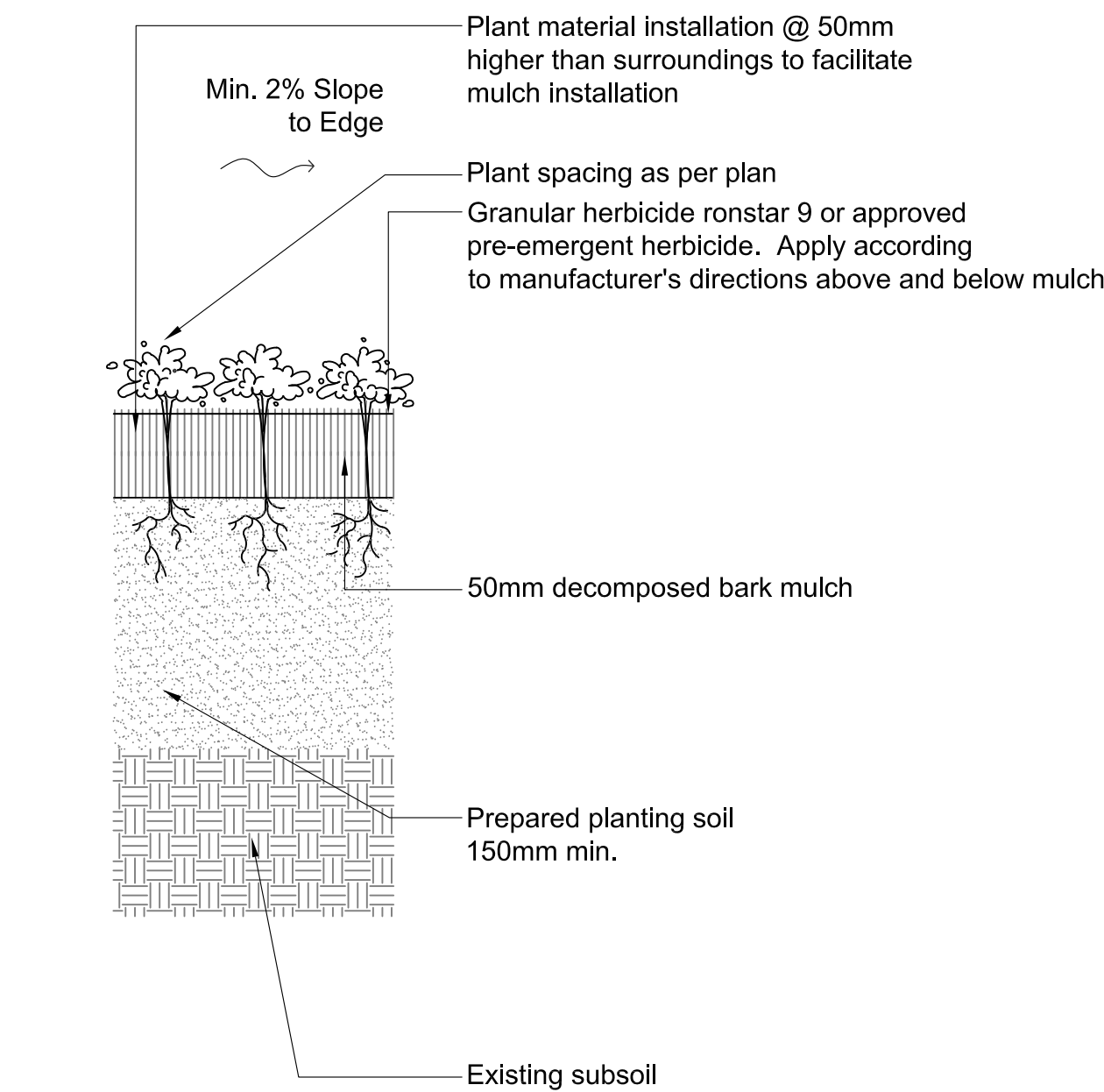
2 CONIFEROUS TREE PLANTING
Scale 1:25



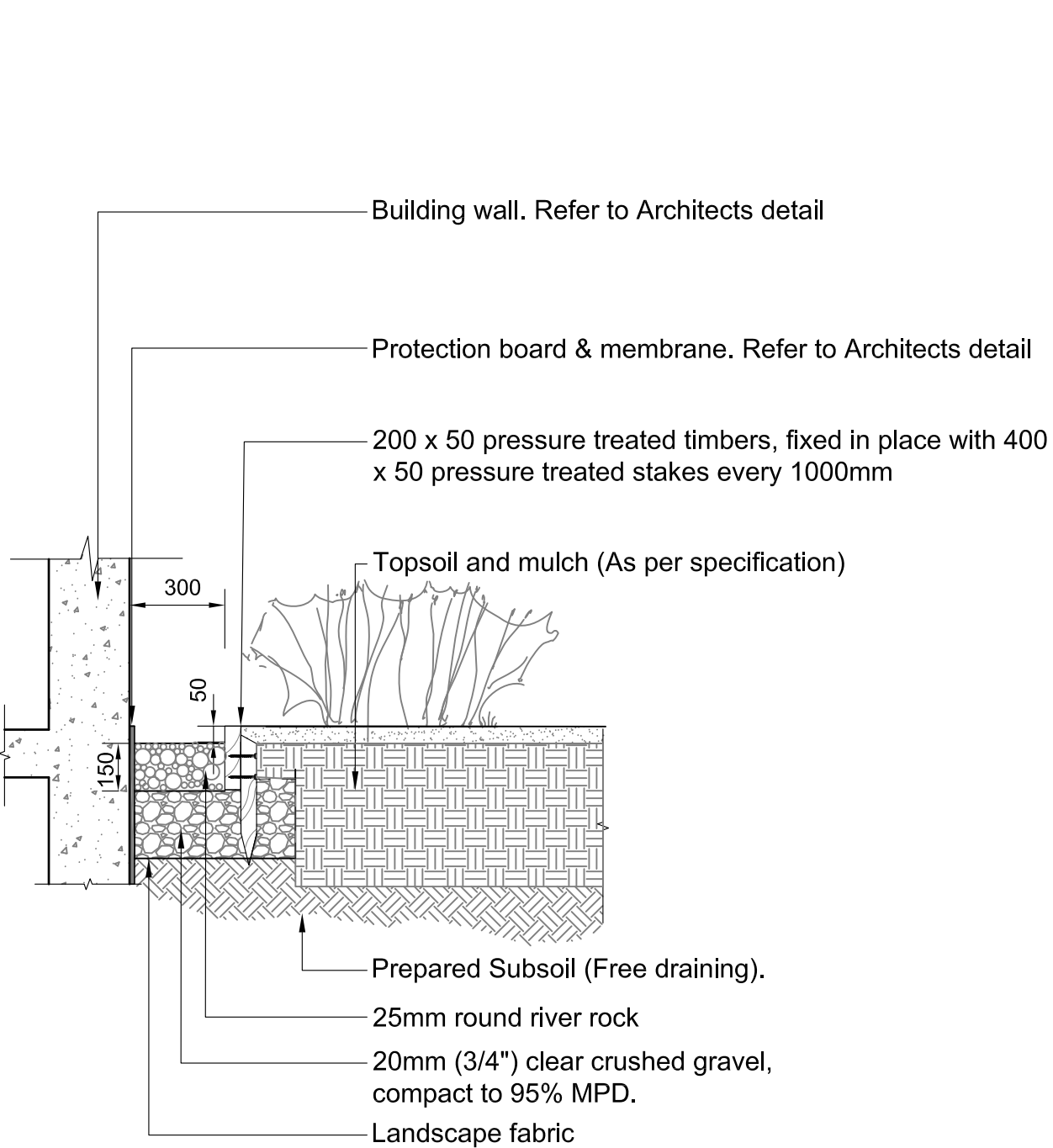
3 SHRUB PLANTING
Scale 1:25



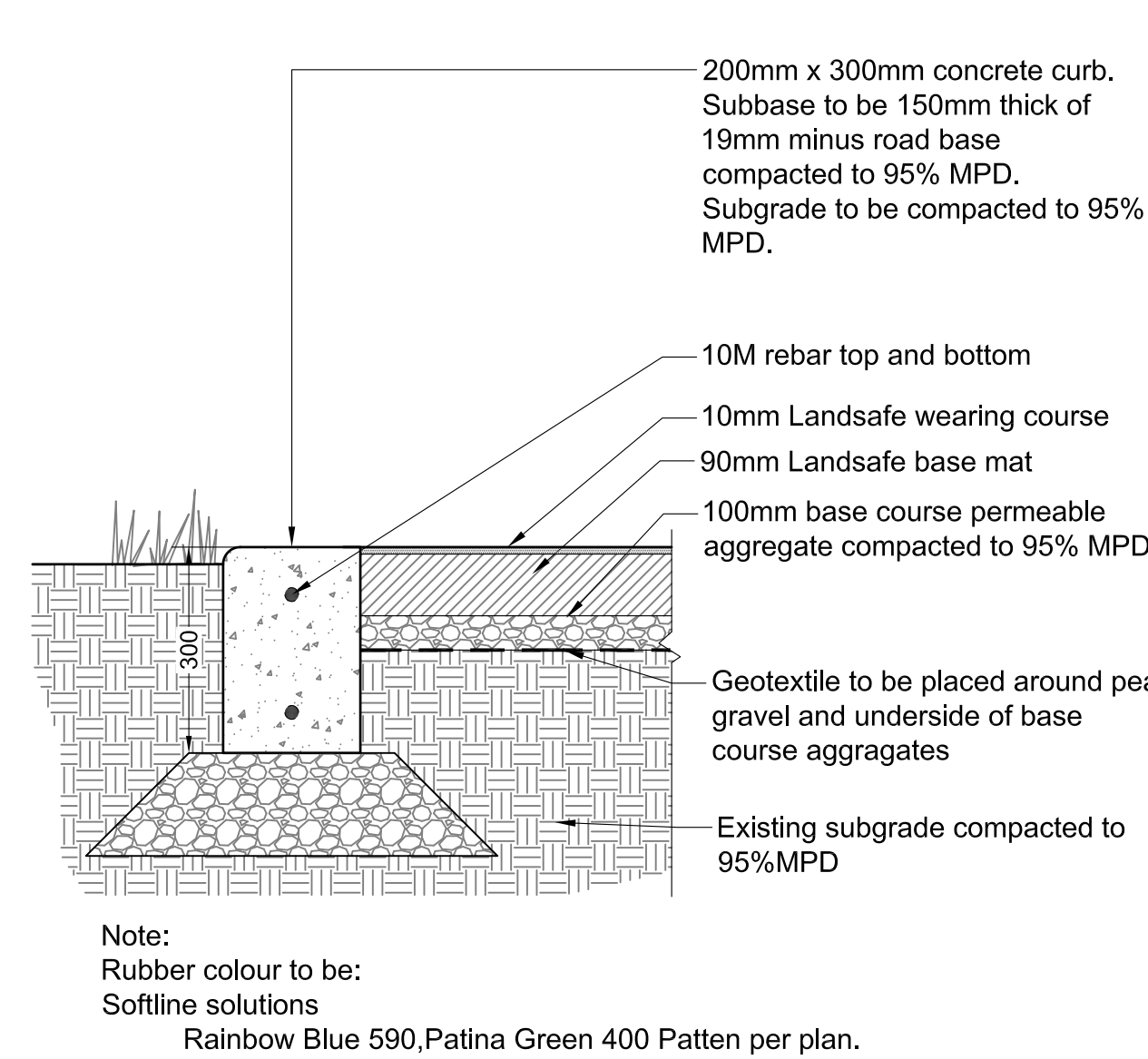
4 SHRUB PLANTING ON SLOPE
Scale 1:25



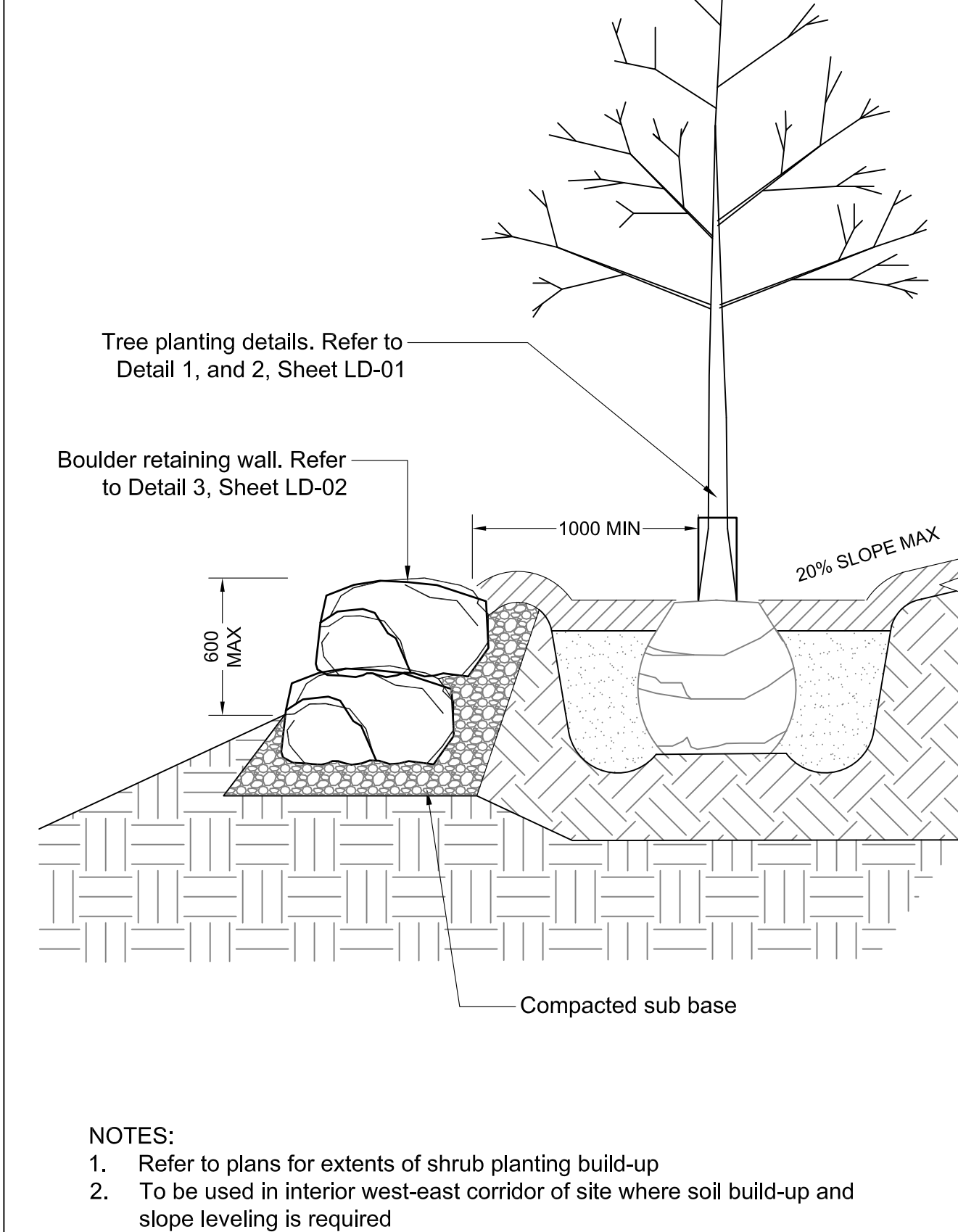
5 GROUND COVER PLANTING
Scale 1:10



6 DRIP STRIP
Scale 1:20



7 RUBBER SAFETY SURFACE
Scale 1:10



8 PLANTING BEDS ON SLOPE
Scale 1:25

No.	By:	Description	Date
8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
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2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18

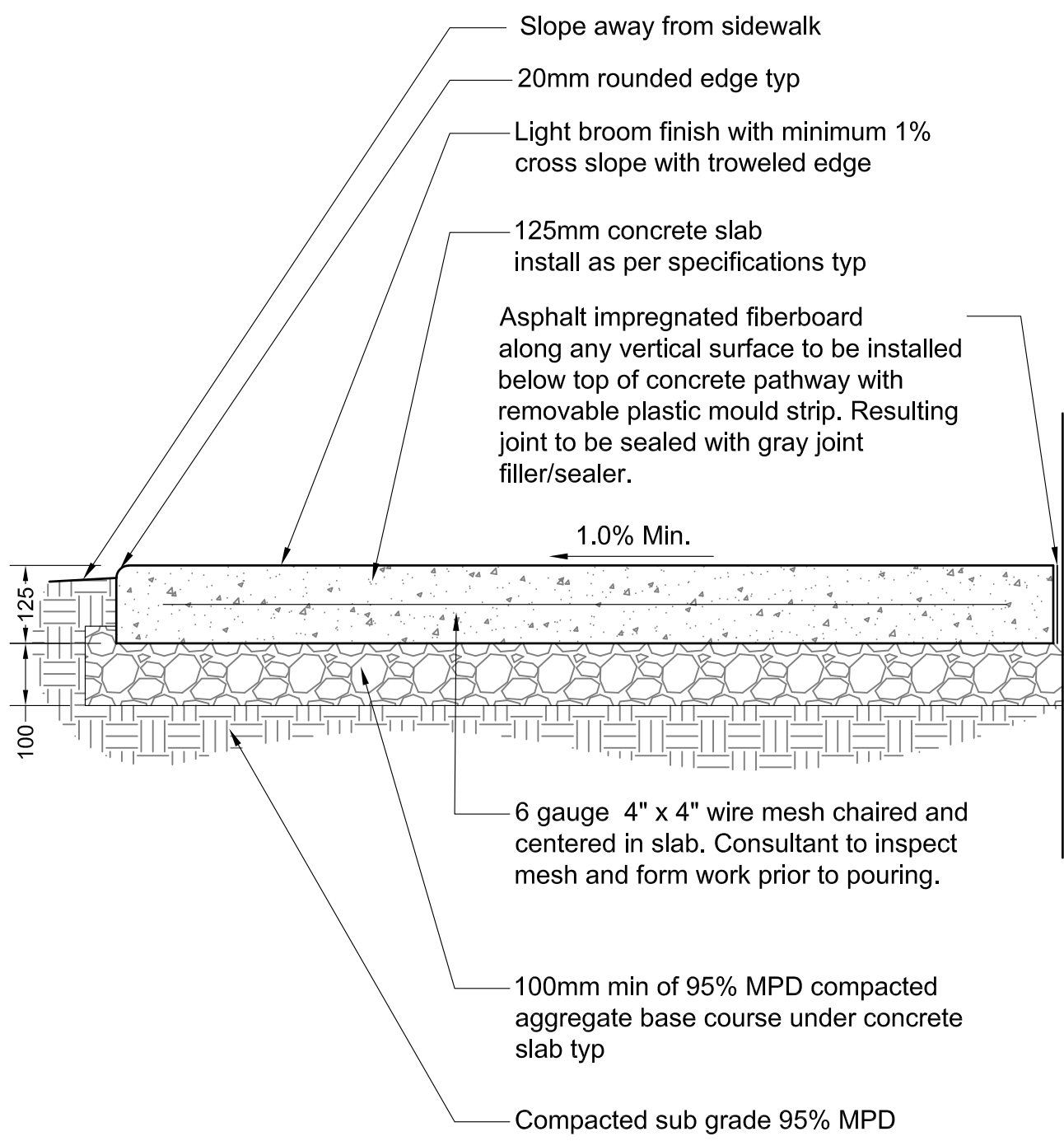
REVISIONS TABLE FOR DRAWINGS

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No.	By:	Description	Date
REVISIONS TABLE FOR SHEET			
Project: PROVIDENCE / SUNDANCE			
Location: 3401 Sundance Drive West Kelowna, B.C.			

Drawn: ET AH	Stamp:
Checked: SH	
Approved: APPROVED	Original Sheet Size: 24"x36"
Scale: AS SHOWN	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS TO DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

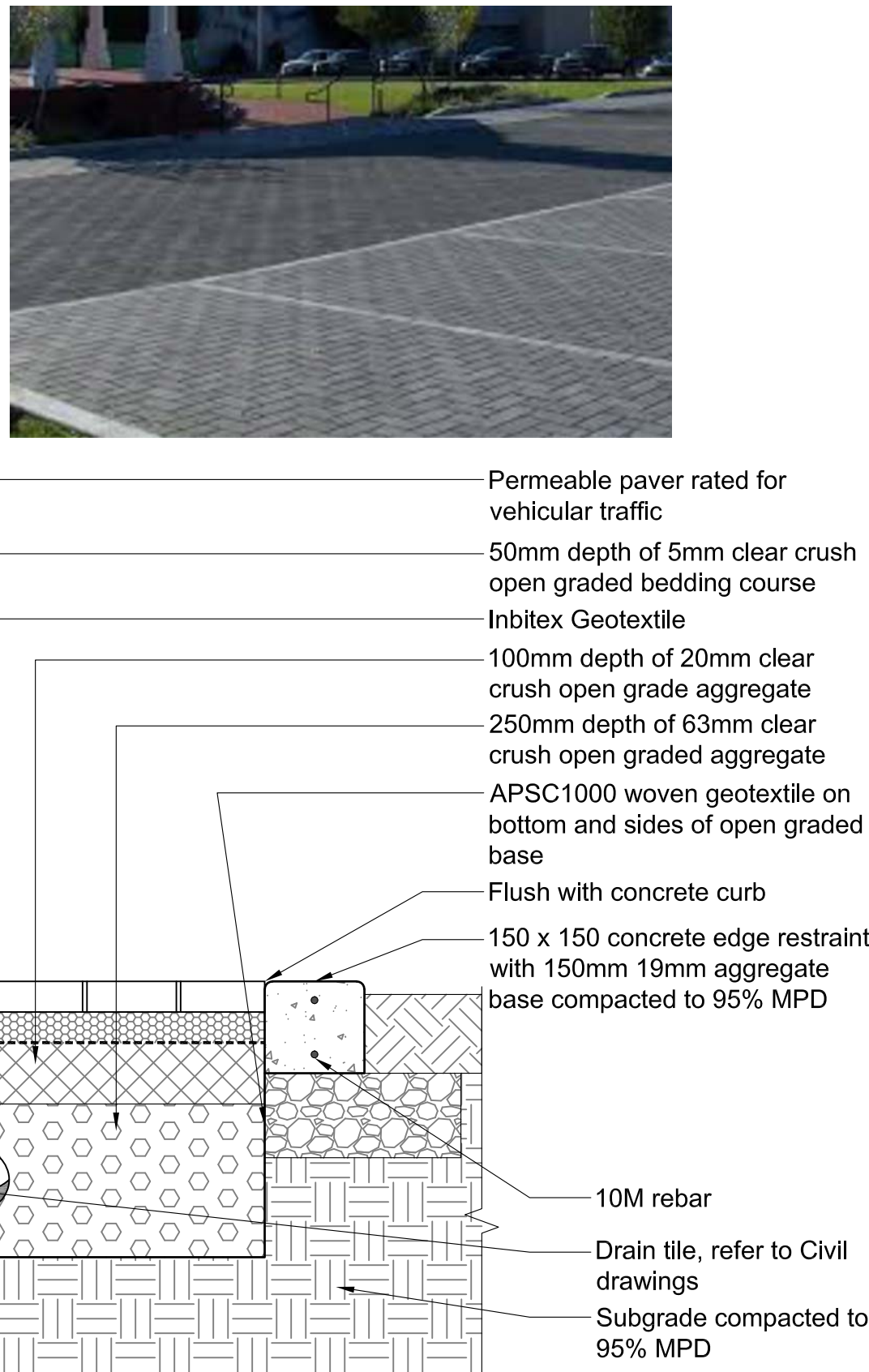
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- Notes:
- Contractor to provide expansion joints where concrete meets all vertical structures.
 - Horizontal scorelines at 1500mm o.c. both ways unless shown otherwise. Refer to plans.
 - Center scoreline on 150mm smooth finish or to match existing concrete pathway.
 - All components as specified or approved alternate.
 - Concrete to slope to drain.
 - Expansion joint every 3000mm o.c. both ways.
 - Contractor to contact Landscape Architect for scoreline approval prior to cutting.

1 CONCRETE PAVING

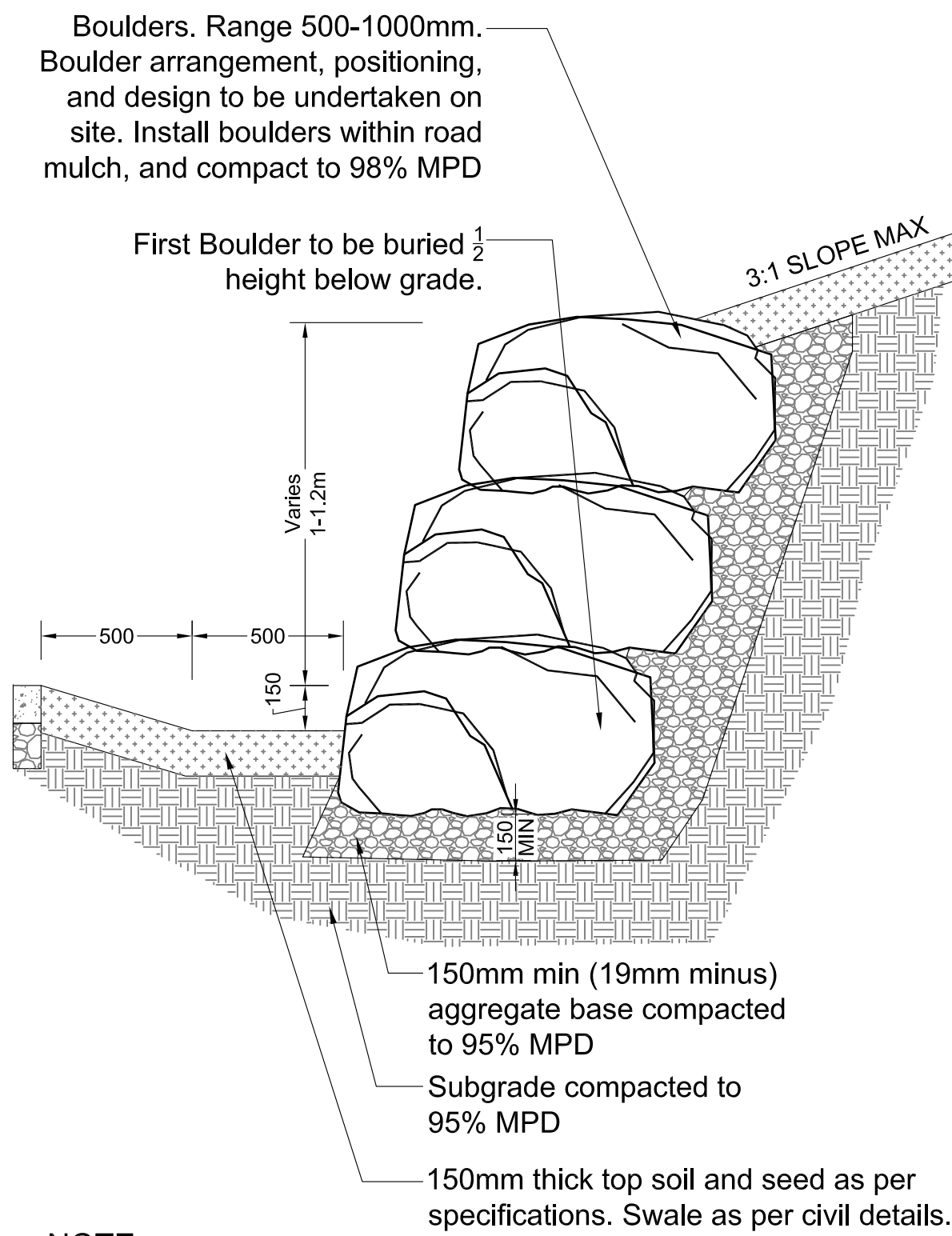
Scale 1:10



- Notes:
- Install all components as per manufacturer's specifications.
 - Permeable Mission Paver - by Basalite
 - Colour: Charcoal
 - Herringbone pattern.
 - Concrete curb control joint every 3m.

2 VEHICULAR PAVING

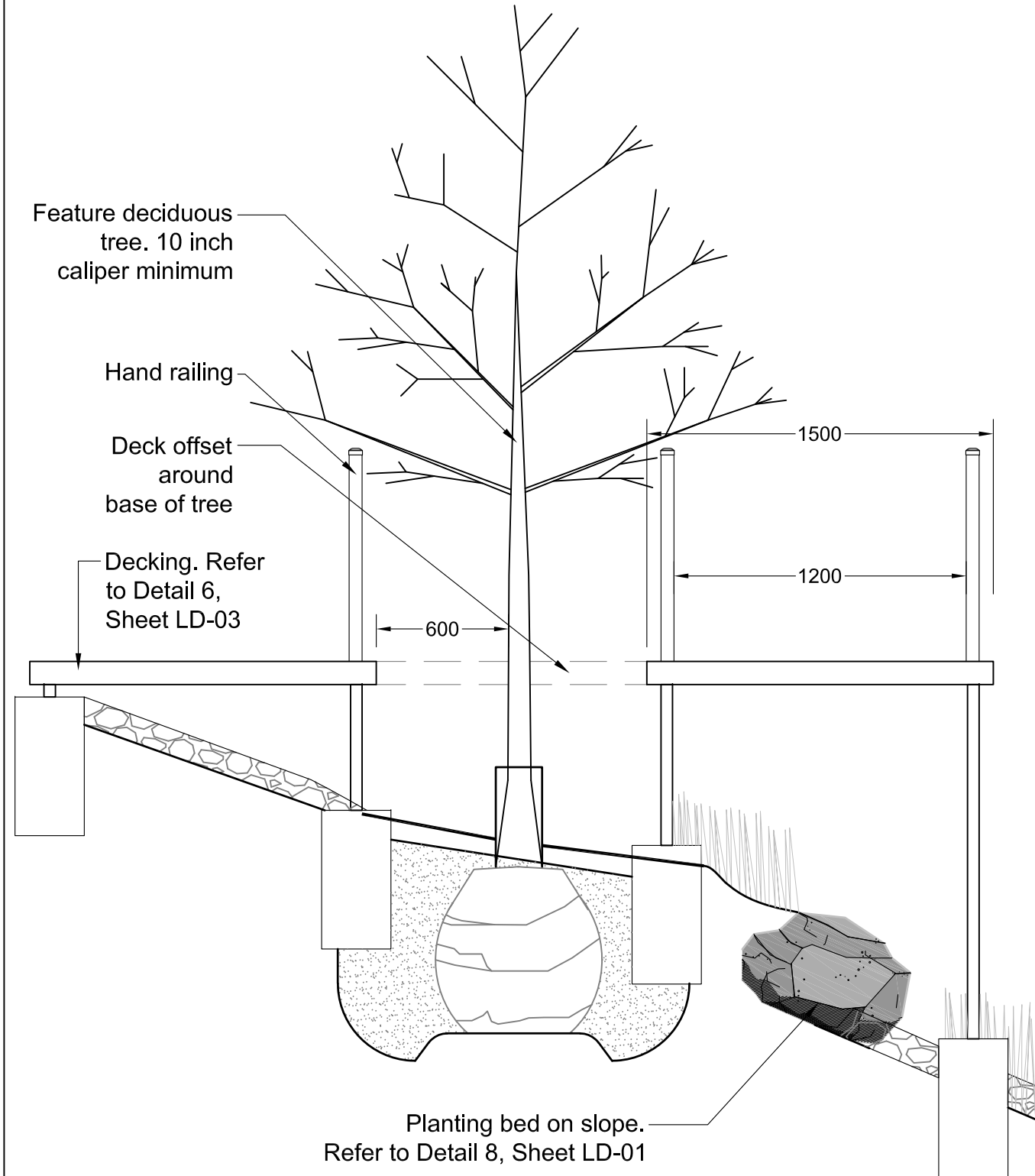
Scale 1:10



- NOTE:
- All walls over 1.2m in height will need to be Structurally Engineered.
 - All planters to be connected to Slab drainage system.

3 BOULDER RETAINING WALL

Scale 1:20



4 BOARDWALK TREE WELL

Scale 1:25



Supplier: TBD
Product: Hill climbing hand holds
Dimensions: 463 x 161 x 110 cm
Age Group: 4+
or approved equal

Supplier: KOMPAN
Product: EMBANKMENT SLIDE- PCM110121
Dimensions: 463 x 161 x 110 cm
Age Group: 4+
Play Capacity: 3
or approved equal

5 ADVENTURE CLIMBING HILL

NTS



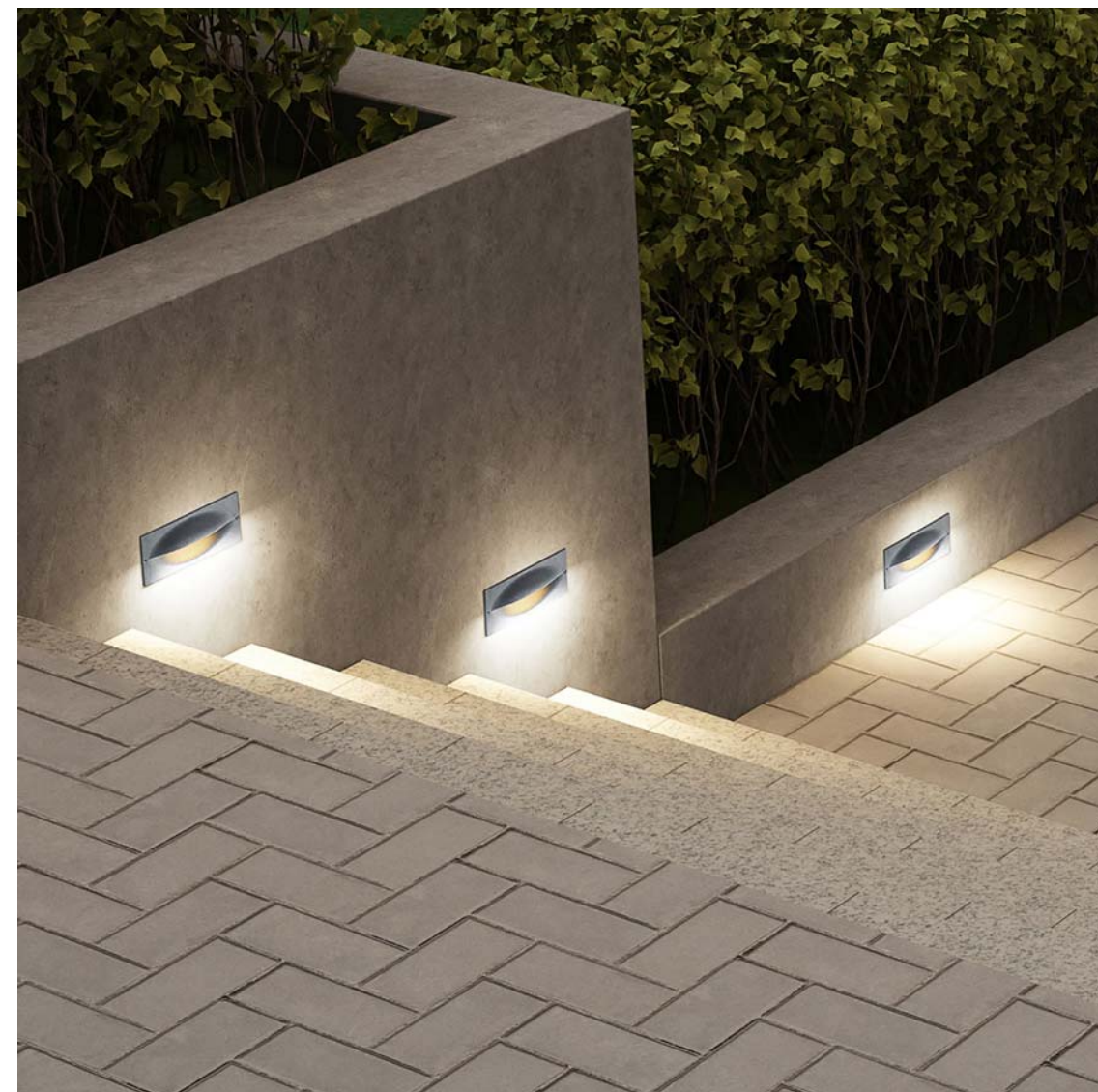
Lighting bollard

Finish: brushed stainless steel or oxidized steel
Mount: surface mounted to deck

Product and supplier: TBD

6 LIGHTING BOLLARD

NTS



Supplier: ERCO Lighting
Product: Lightmark - Recessed Wall/Step Light

Or Approved Alternative

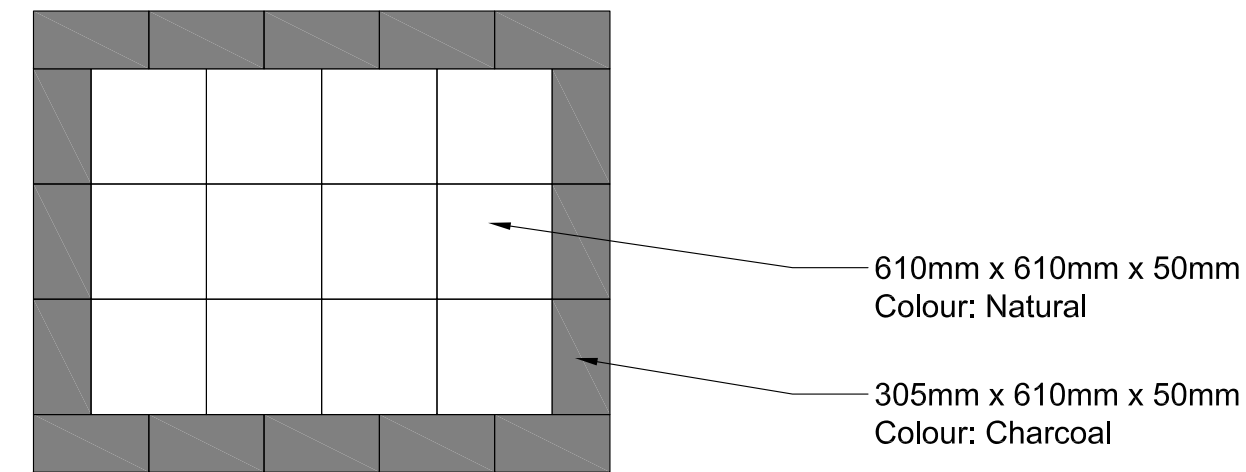
Finish: Silver
Mount: Surface Mount

Contact: 1-877-372-6584

- Notes:
- Lighting through common amenity areas

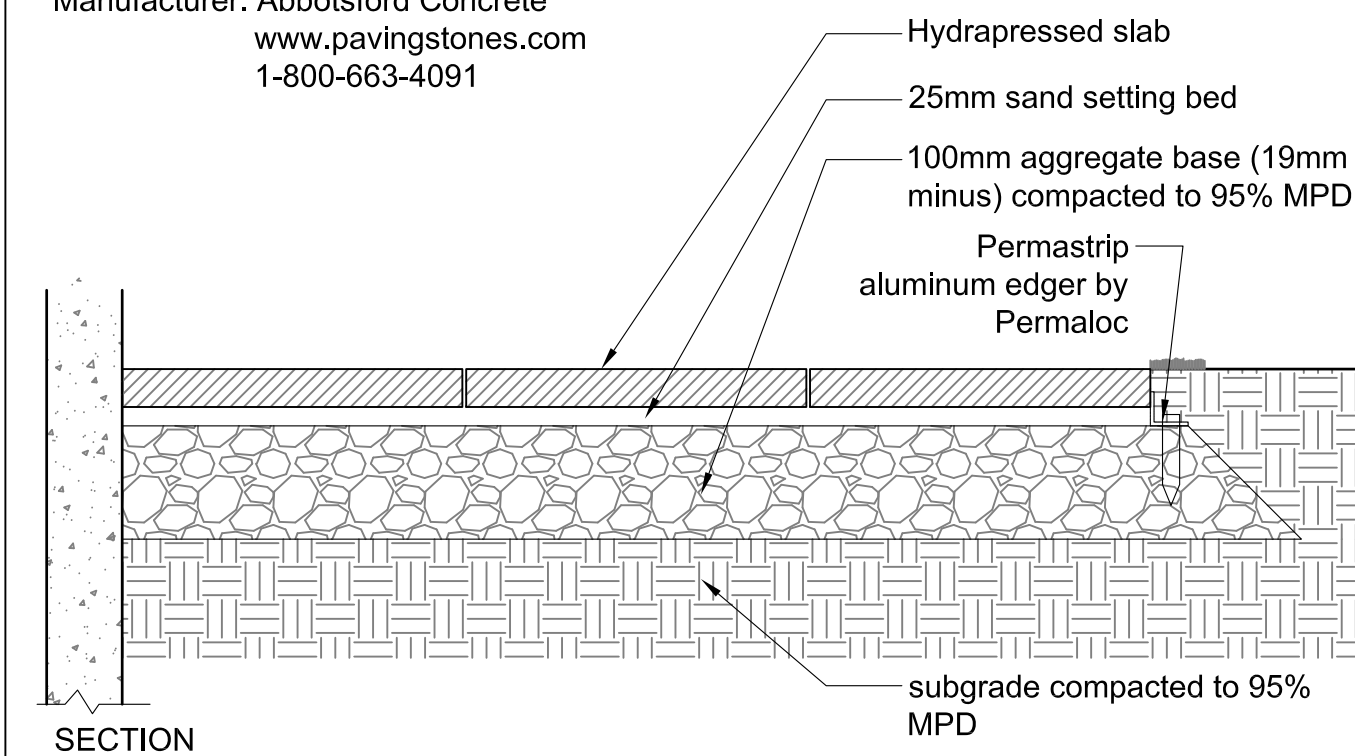
7 WALL / STEP LIGHT

NTS



PLAN
Scale: 1:40

Unit Paver Details:
Type: Cortez HydraPressed Slabs
Size: L610 X W610 X T50mm min.
Colours: Natural, Charcoal
Manufacturer: Abbotsford Concrete
www.pavingstones.com
1-800-663-4091



SECTION

8 HYDRAPRESSED SLABS

1:10

VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY
FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO
100-9181 Church St | 102-355 Kingsway
Fort Langley, BC | Vancouver, BC
V1M 2B8 | VST 317
www.vdz.ca | 604-882-0024

No.	By:	Description	Date
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REVISIONS TABLE FOR DRAWINGS
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No.	By:	Description	Date
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REVISIONS TABLE FOR SHEET

Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

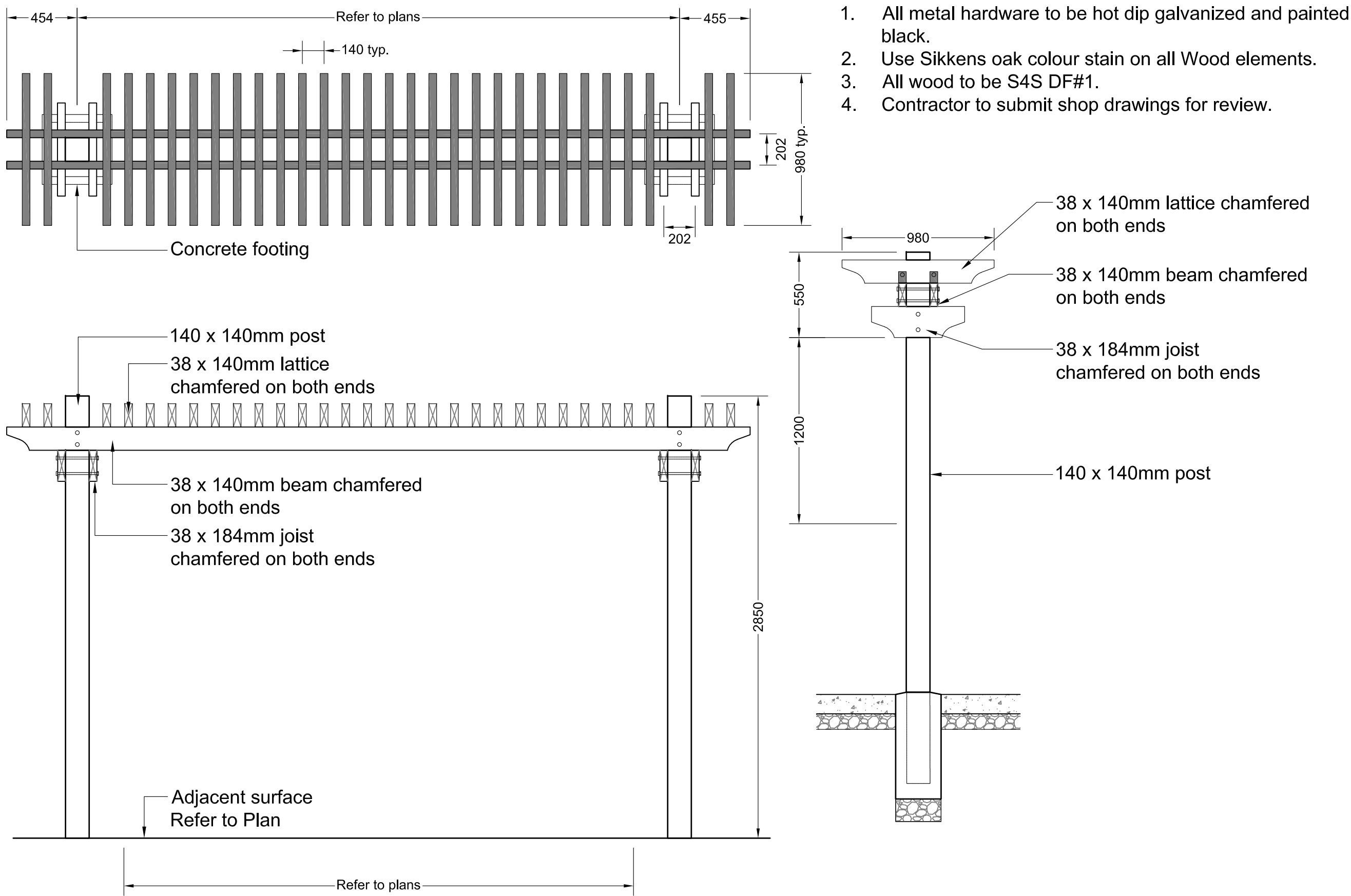
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Checked: SH	
Approved: APPROVED	Original Sheet Size: 24"x36"
Scale: AS SHOWN	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REWORKING/REPAIR/REWORK DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

Drawing Title:
DETAILS

VDZ Project #:
DP2022-16

Drawing #:
LD-02

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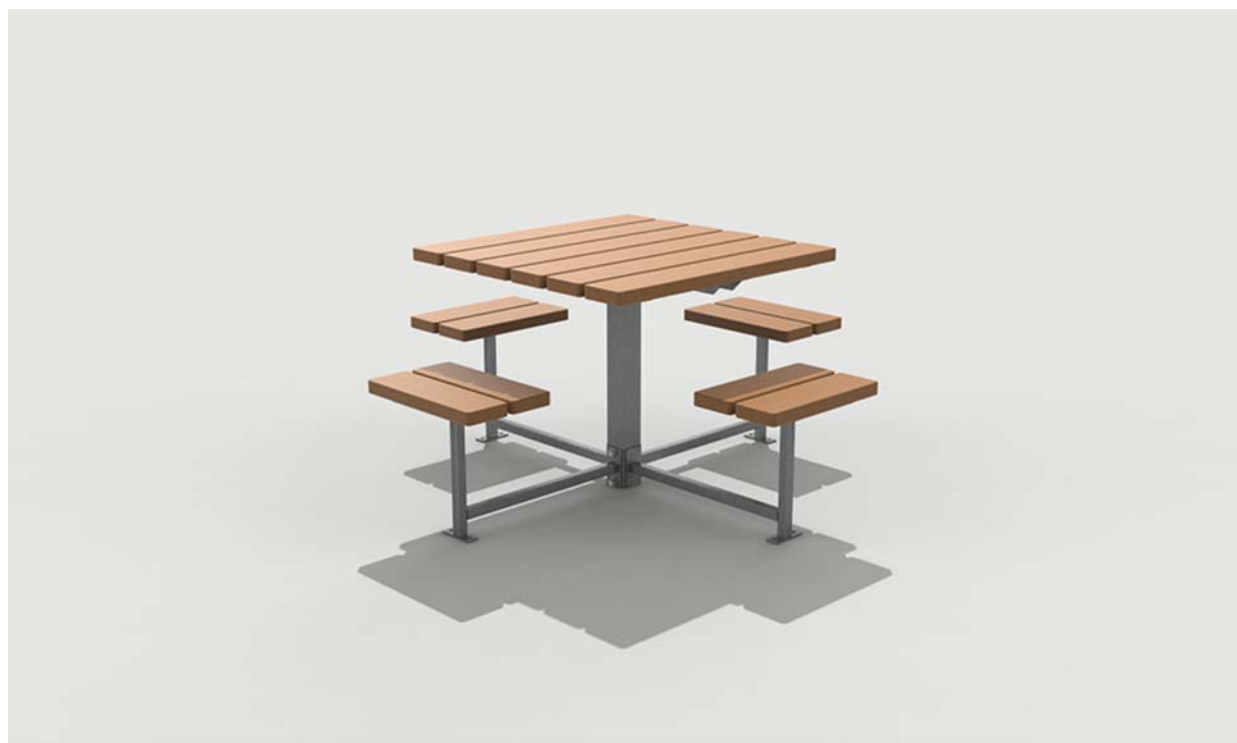


1 LINEAR TRELLIS

Scale 1:25

NOTES:

1. Install to manufacturers specifications.
2. Or approved substitute.



Picnic Table
Model Number: MLPT400-S-W
Size: 749mm x 1581mm x 445mm
Options: Surface Mount
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approved equal

3 PICNIC TABLE

N.T.S



Bar Counter

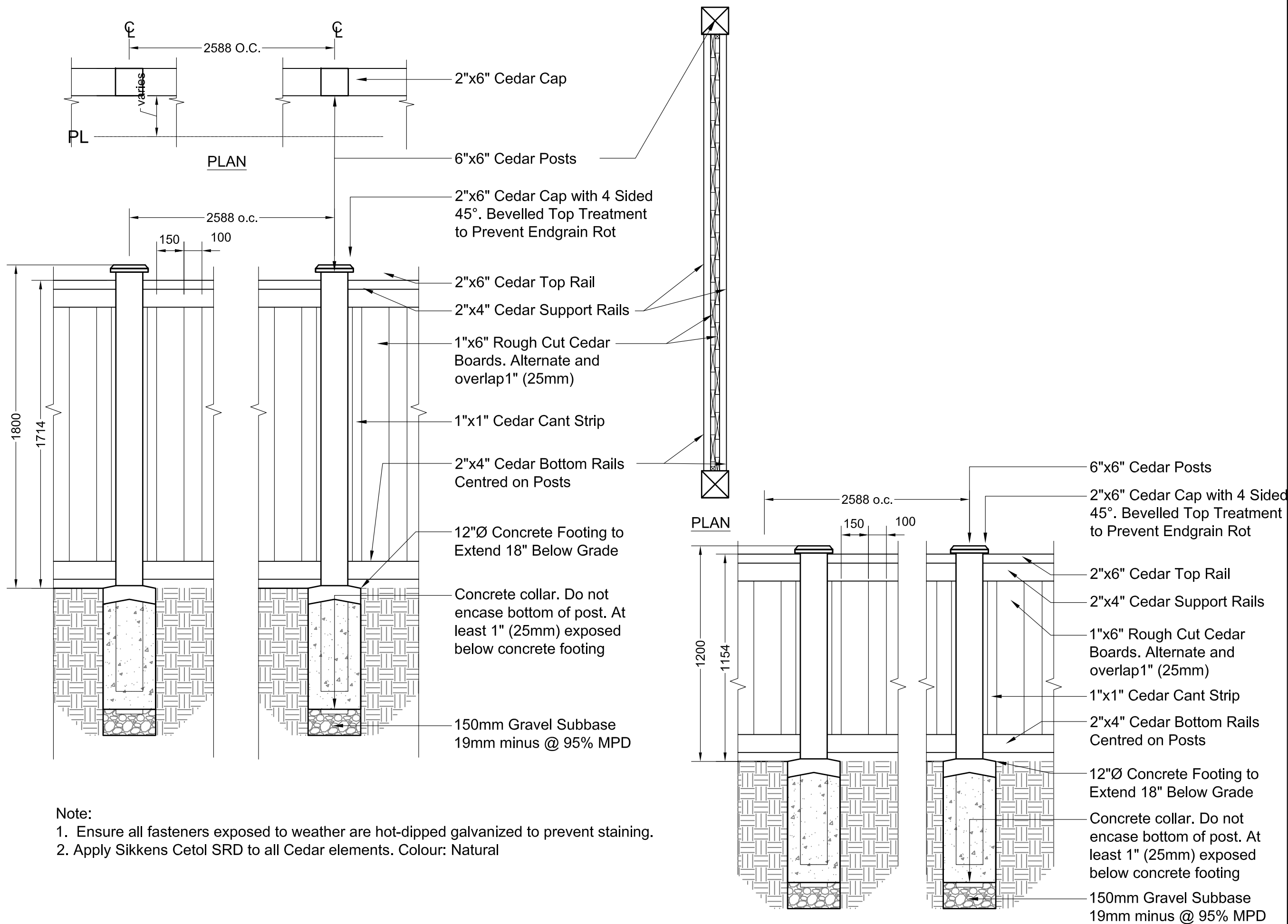
Material: Polished concrete top

Chairs

Details: Bar stool height
Material: Metal

4 BAR TABLE

N.T.S

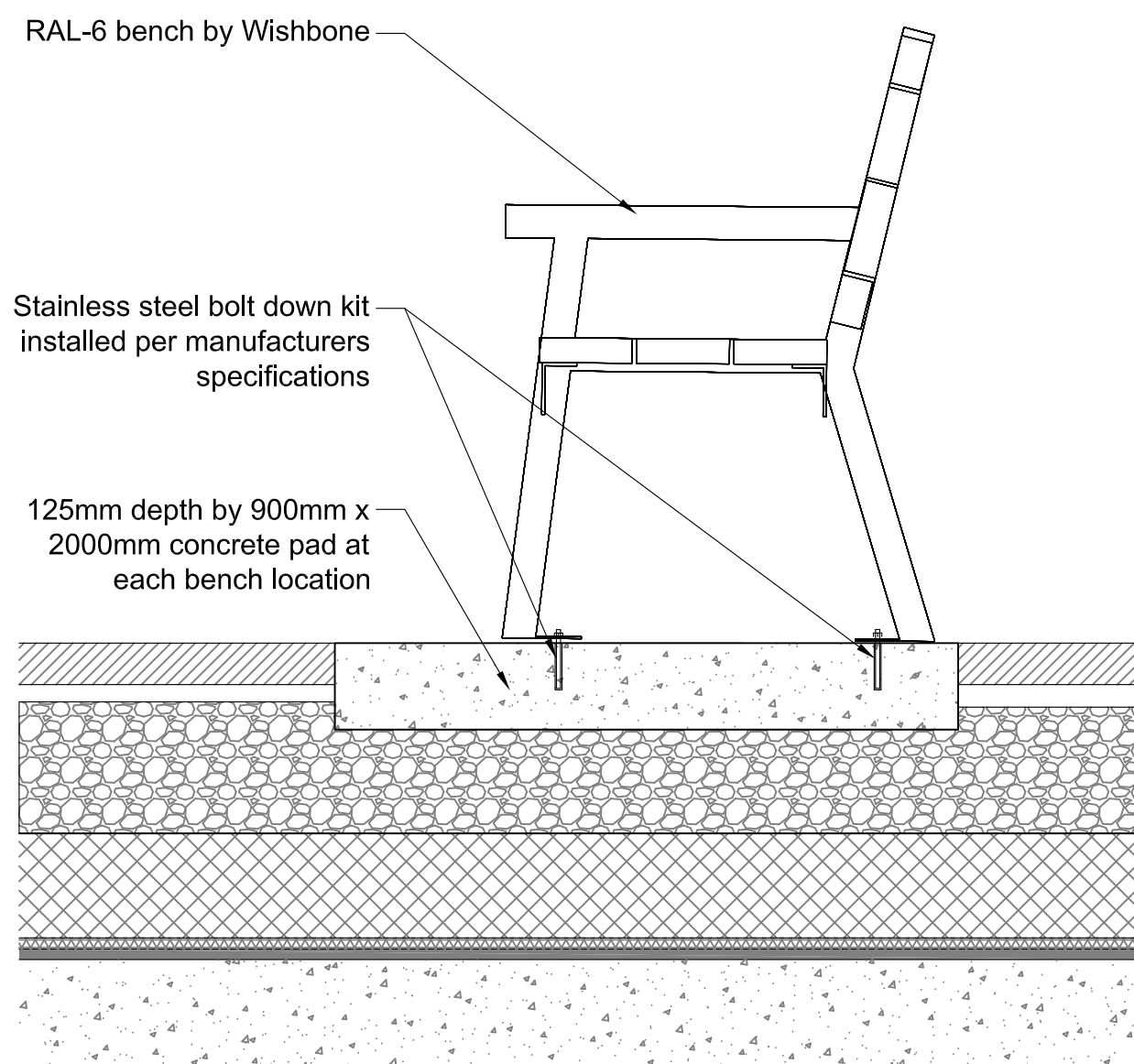


2 CEDAR FENCE

Scale 1:20

NOTES:

1. Install bench to manufacturers specifications.
2. Concrete pad to be included at all bench locations.
3. Size of concrete pad as noted on details unless shown otherwise on plans.
4. Or approved substitute.
5. Refer to concrete paving detail for additional information on concrete pad.



Bench
Model Number: MLB870-W
Size: 1765mm
Options: Center Arm
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approved equal



5 BENCH

N.T.S

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PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn: ET AH	Stamp:
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Scale: AS SHOWN	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS TO DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

Drawing Title:
DETAILS

VDZ Project #:
DP2022-16

Drawing #:
LD-03

GATEHOUSE

The City of West Kelowna
2760 Cameron Road
West Kelowna, BC, V1Z 2T6

Attention: Jayden Riley, Planner III

RE: Providence Townhomes – Construction Management Plan

Dear Mr. Riley

The following Construction Management Plan is being provided to City of West Kelowna Staff and Council to help address any concerns related to the project. The following Plan will outline the construction phasing and steps to mitigate impacts to adjacent properties.

Introduction

The Providence Townhouse project consists of 58 units on the hillside between Sundance Drive to the northwest and Ensign Quay Lane to the southeast, divided as follows: 20 Side by Side units fronting Sundance Drive and 38 Tandem units with access off of Ensign Quay Lane & Ensign Lane as illustrated below.



GATEHOUSE

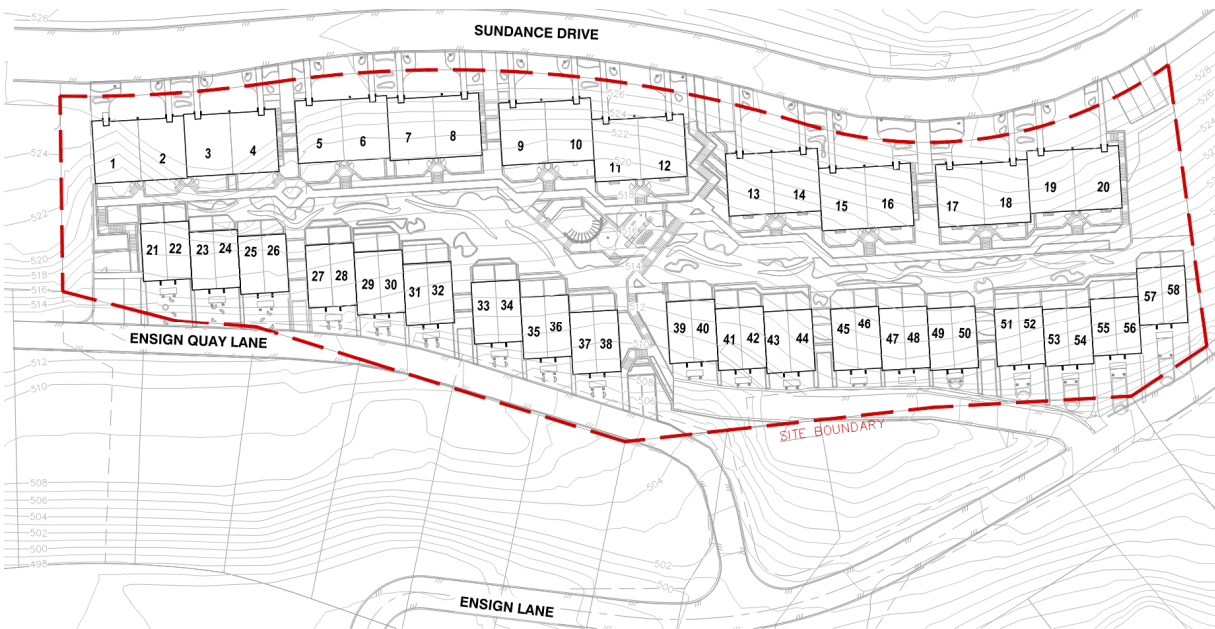
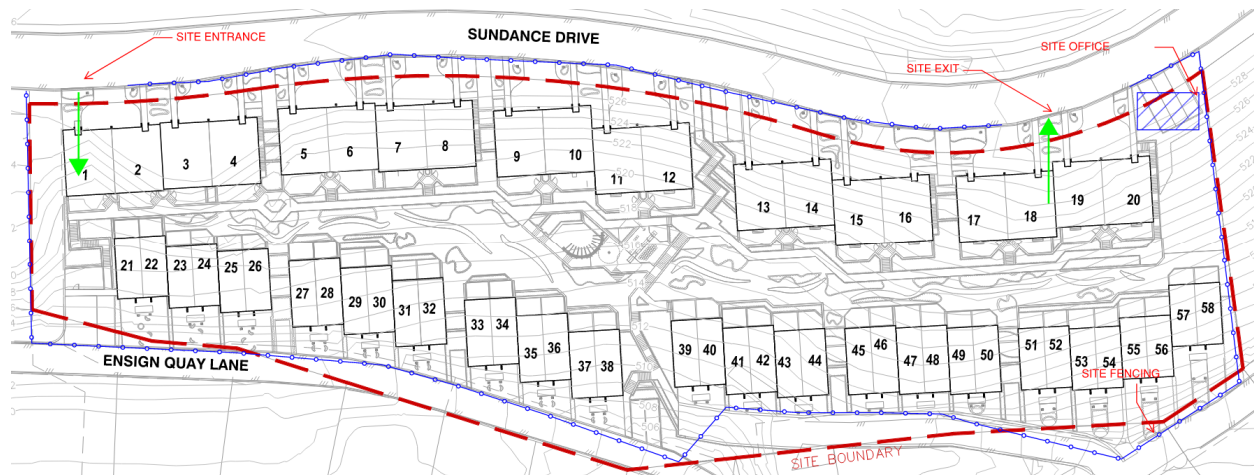


Figure 1: Site Location

Site Access

Gatehouse Developments is aware of the importance of mitigating the impacts of this development to the neighbouring properties. To ensure this is achieved, the primary site access will be from Sundance Drive to the Northwest of the site as per the figure below:



GATEHOUSE

Figure 2: Site Access

The lanes to the south of the site will only be used in case of emergency and as needed to safely complete construction activities otherwise not feasible from Sundance Drive. Anticipated work in the lanes will include underground pipe work, concrete placing, landscaping and minimal deliveries of construction materials. When work is required within the lanes, advanced notice will be given to adjacent properties and local access will be maintained via traffic control personnel.

Construction Sequencing

Phase 1: Gatehouse will subcontract a professional site servicing contractor experienced in hillside developments to undertake this phase. Work will include clearing trees and grubbing as well as removing organics from the site. It is anticipated during this phase that construction traffic will be increased as organic material will need to be hauled off site via dump truck to an approved dump site. At completion of this phase an interior construction road will be installed for the completion of the next phases of construction.

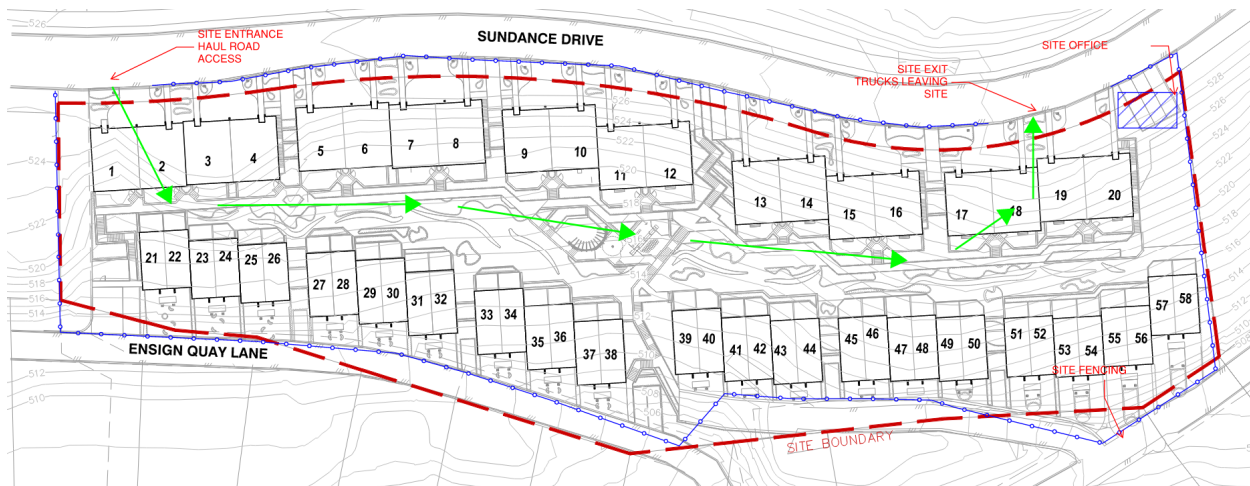


Figure 3: Construction Road & Site Access

Phase 2: The site servicing contractor will commence with excavation of the foundations for both the side by side and tandem units. Organic material within the building footprints will be hauled off site. Construction of the townhouse units is anticipated to be broken into 5 Phases as shown below:



GATEHOUSE

Figure

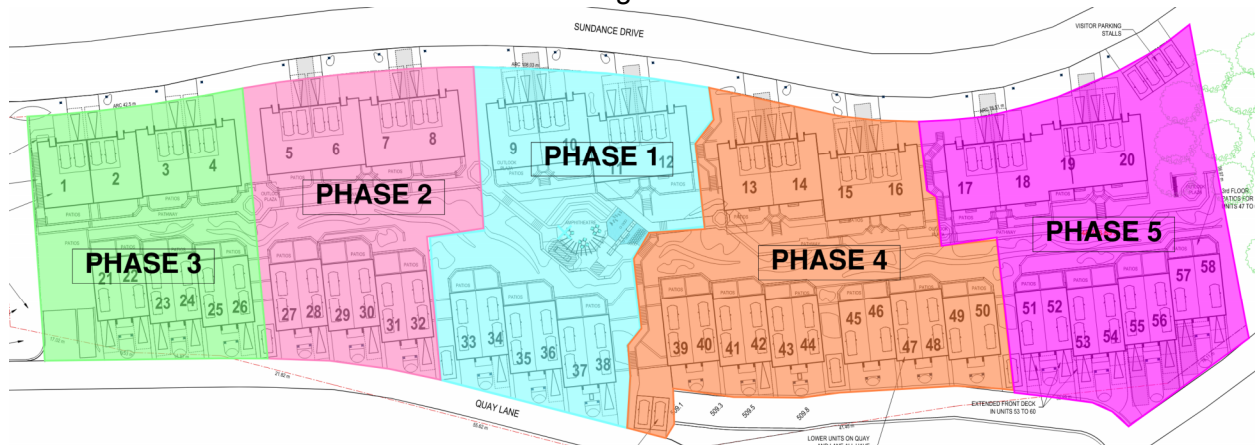


Figure 4: Construction Phasing

Phase 3: Construction of all residential units will be managed by Gatehouse Construction with work subcontracted out to local professional trades. Work will include concrete forming, framing, exterior finishes, interior finishes and landscaping.

Lane Work

Gatehouse Developments understands the need to minimize the impacts of this development on the neighbouring properties. While the majority of the work will be confined to the limits of the project site, some work is required with the lanes. Work will include utility tie-ins to existing City infrastructure on Ensign Quay Lane and the Ensign Lane. As previously noted a more comprehensive plan will be provided well in advance to the affected properties. A high-level conceptual plan is provided below for your information:



Figure 5: Lane Works

Impact Mitigation Strategies

Gatehouse Development is committed to ensuring that all of our developments minimize impacts to the surrounding areas. Below is a high-level table for outlining impact mitigation strategies. These will be further refined as we engage local professional contractors to complete construction of the Providence Townhouse Development

GATEHOUSE

Impact	Mitigation Strategy
Construction Noise	<ul style="list-style-type: none"> • Work only conducted during CoWK permitted hours
Storm Water Management	<ul style="list-style-type: none"> • Silt fence installed at perimeter of site • Gravel pads at entry/exit to site • Silt sacs on all existing manholes • Treatment system as per submitted ESC Plans
Dust	<ul style="list-style-type: none"> • Street sweeping as required • Water down services onsite to mitigate dust
Traffic Control	<ul style="list-style-type: none"> • Develop traffic management plans for specific work in lanes • Traffic management personnel (flaggers) to ensure local access to neighbouring properties
Fire	<ul style="list-style-type: none"> • Daily forest fire risk monitoring • Suppression equipment as required by risk level
Trade Parking	<ul style="list-style-type: none"> • No trade parking/staging in lanes
Wild Life	<ul style="list-style-type: none"> • Assessment and monitoring as required

We trust that this provides an overview of the anticipated construction phases and outlines the strategies to mitigate the impacts on the neighbouring properties. As construction is a fluid undertaking, this framework will be used to assess the site conditions throughout the project and changes may be necessary. Should any aspect of the work plan change in a way that would impact residents, our construction team will work with all stakeholders to ensure this development is completed to the satisfaction of all.

If City Staff or Council have any additional questions, please feel to contact us at your convenience.

Sincerely,

Kyle Weiher

GATEHOUSE

Senior Project Manager
Gatehouse Construction Inc.



From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attention City Clerk - DP 22-26
Date: May 10, 2023 8:29:53 AM

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From Sharon and Robert Jones

LOT 4 PLAN KAP78710
DISTRICT LOT 2044, OSOYOOS DIV. OF YALE LAND
DISTRICT PID: 026-380-749

After reviewing the delivered Notice for Development Permit with Variance (DP 22-26) with the members of our community, we believe the decision to move forward with the proposed development variances for the construction of 58 townhouses would be unsafe and problematic for a number of reasons.

Our primary concern is with safety. In the past several years, we have had very dry summers and with plans for these townhomes to be built so close together, along with the reduced boundary setbacks, any severe situation involving a fire would bring large-scale destruction to the homes and structures located nearby. In the event of an emergency, fire trucks could not properly tend to the buildings, as the lane has a sharp, blind corner with a narrow width under 17 feet and a sharp turning radius that a vehicle over 32 feet would not be able to navigate. Even if a small fire truck, ambulance or other emergency vehicle was able to manage their way up the lane, there is no turn around location, thus causing extended time concerns and even unnecessary deadly circumstances.

Another major concern is the likely degradation of the physical landscape on the hill side which we worry will cause some level of instability and even possibly water runoff concerns, erosion and potential flooding with landslides. In regard to maintenance and upkeep around the new properties during winter months, there should be a designated area on the property for snow removal and snow storage.

We believe the situation of the resulting reduced parking, loading and visitor parking for the new properties should be considered a non-starter, as there is already insufficient space for parked vehicles on Ensign Quay Lane and Ensign Lane, with a width of under 17 feet. With the reduced driveway sizes, it is simply not feasible to suggest an alternative for additional parking.

The city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there are serious concerns about what will happen with emptying garbage, recycle and compostable bins for all of the newly proposed townhomes.

Ensign Quay Lane has a width of less than 17 feet wide with no turnaround and with the 7 current homes, the proposal of an additional 20 homes, the expectation is a minimum of 60 additional vehicles using this laneway with no parking, no turnaround and only one shared egress and ingress entryway..

To possibly remedy many mentioned safety issues above access to the new development should be from Sundance Drive and townhomes should be reduced by a third in size. We recommend a council member physically visit our community on Ensign Quay Lane to see how apparent these concerns will become.

The mixture of juvenile and adult trees will be a loss to the neighbourhood, not only in regard to screening, shade and greenery but also with the loss of the roots would most likely destabilize the hillside. As a whole, this green area is a visually enjoyable aspect for the entire community and will have a negative impact with insensitive and obstruction overbuilding on greenery and openness.

In conclusion, it is not safe to add that number of dwellings to a blind drive, narrow single entrance/exit and non-turn around lane.

Thank you for your assistance with this matter,
Owners Lot 4

From: noreply@esolutionsgroup.ca
To: [Delegation Requests](#)
Subject: New Response Completed for Request to Appear As a Delegation
Date: May 8, 2023 3:37:35 PM

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Hello, Please note the following response to Request to Appear As a Delegation has been submitted at Monday May 8th 2023 3:36 PM with reference number 2023-05-08-004.

- **Request to appear as a delegation on:**
5/16/2023
- **Name of person making the presentation:**
Ryan Jones
- **Name of the group or organization that the person is representing:**
Gatehouse Developments
- **Daytime phone number:**
7789680230
- **Email address:**
rjones@stewartland.ca
- **Presentation title:**
The Providence Development
- **What is your request to Council?**
Support of the Development Permit and Variances.
- **Presentation points:**
TBD

[This is an automated email notification -- please do not respond]

SUBMISSIONS

Development Permit with Variances
(File No. DP 22-26 3401 Sundance Dr)

NO.	Date RECEIVED	TIME RECEIVED	RECEIVED FROM
Submissions included with Report to Council			
1.	May 10 th 2023	8:30 AM	Jones, Sharon & Robert
Submissions included with late agenda items to Council			
2.	May 12 th 2023	12:41 PM	Peterson, Jamie
3.	May 12 th 2023	6:31 PM	Fairman, Kaylee
4.	May 13 th 2023	8:26 AM	Hennig, Josh
5.	May 13 th 2023	4:11 PM	Getty, Richard & Sinclair, Tracy
6.	May 13 th 2023	5:06 PM	Smiley, Karen
7.	May 13 th 2023	7:20 PM	Parrott, Lyndsay
8.	May 13 th 2023	11:15 PM	Pan, Queena
9.	May 13 th 2023	11:19 PM	Ying, Mei
10.	May 14 th 2023	4:35 PM	Ruscheinsky, Todd & Koral, Jo-Anne
11.	May 14 th 2023	7:41 PM	Larratt, Ian & Carolyn
12.	May 14 th 2023	9:34 PM	Larratt, Bruce
13.	May 15 th 2023	7:18 PM	Manaog, Heather
14.	May 15 th 2023	7:57 AM	Tyabji, Alex
15.	May 15 th 2023	3:22 PM	Savan, Andreea

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: City Clerk - DP 22-26
Date: May 12, 2023 12:41:56 PM
Attachments: [Letter to City of West Kelowna re: DP22-26.pdf](#)

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Good afternoon,

Please find enclosed my letter opposing to the variance in regards to DP 22-26.

Yours truly,

Jamie Peterson

May 12, 2023

West Kelowna City Council
2760 Cameron Road
West Kelowna, BC V1Z 2T6

Attention: City Clerk

Re: DP22-26

Proposed Development Permit with Variance for egress/ingress from Ensign Quay Lane

I understand that there is a proposed Development Permit with Variance (DP 22-26) for the construction of 58 townhouses with egress from Ensign Quay Lane.

I have a number of concerns regarding this development, and specifically with the development using the Ensign Quay Lane.

My primary concern is with safety. At the end of Ensign Quay Lane is a daycare and already it is difficult to drop off and pick up my children from that daycare. The biggest issue being the sharp turn near the beginning of Ensign Quay Lane. That turn is blind and there is not enough room for 2 vehicles to travel along the Lane at the same time, unless both vehicles are narrow, and even then with 2 narrow vehicles, you are passing with less than a foot of space between them.

Currently, in the event of an emergency, fire trucks may not be able to properly tend to the buildings, as the lane has a sharp, blind corner with a narrow width under 17 feet and a sharp turning radius that a vehicle over 32 feet would not be able to navigate. Even if a small fire truck, ambulance or other emergency vehicle was able to manage their way up the lane, there is no turn around location, thus causing extended time concerns and even unnecessary deadly circumstances. If more vehicles were to use this Ensign Quay Lane, it would make it even more difficult for emergency vehicle.

Currently, if there is a delivery truck, or any vehicle that is slightly wider than an average vehicle, making deliveries to any of the homes along the Ensign Quay Lane, there is not room for a vehicle to pass the delivery truck. Instead, you have to pull to the side of the Ensign Quay Lane, which many of the home owners dug out to allow for additional parking (as their driveways are shallow and there are no other options for additional parking), when you can (as long as there is an open spot) to allow the delivery truck to finish its delivery, drive the rest of the way down the Ensign Quay Lane and use the daycare's private parking to make a 4 or 5 point turn in order to turn around and come back down the Lane, before you can pull out of a spot and proceed down the Lane.

With respect to the sharp corner at the beginning of the Ensign Quay Lane: it is treacherous in the winter. On numerous occasions last winter when I have been either picking up or dropping my children off at daycare, there have been vehicles coming down the Ensign Quay Lane towards Ensign Lane, as they are unable to see around the turn, they nearly turned into my vehicle as I am driving up the Ensign Quay Lane towards that turn. In those events, with the very

icy conditions, I had to back down the Ensign Quay Lane and into Ensign Lane while a vehicle comes down towards me, sometimes being unable to stop in a timely manner, forcing me to back down ever further, hoping I don't back into another vehicle traveling along Ensign Lane.

With the proposed development, you would have an additional 58 townhouses meaning at least 60 extra vehicles travelling along the Access Road, plus that many more delivery vehicles. Not to mention the work vehicles that would need to use the Access Road to get to the development. That much traffic would make the already unsafe road an absolute disaster and would be a very negligent act for the City to allow the Ensign Quay Lane to be used in that manner, unless the City makes alterations to the Lane to make it a safe option prior to any work being done on the development.

Even the city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there are serious concerns about what will happen with emptying garbage, recycle and compostable bins for all of the newly proposed townhomes.

Ensign Quay Lane has a width of less than 17 feet wide with no turnaround and with the 7 current homes, the proposal of an additional 20 homes, the expectation is a minimum of 60 additional vehicles using this laneway with no parking, no turnaround and only one shared egress and ingress entryway.


To possibly remedy many mentioned safety issues above access to the new development should be from Sundance Drive and townhomes should be reduced by a third in size. I recommend a council member physically visit the community on Ensign Quay Lane to see how apparent these concerns will become.

In conclusion, it is not safe to add that number of dwellings to a blind drive, narrow single entrance/exit and non-turn around lane.

Yours truly,

A black rectangular box redacting the signature of Jamie Peterson.

Jamie Peterson
3617 Brown Road
West Kelowna, BC V1Y 6L2

From: 
To: [City of West Kelowna Submissions](#)
Subject: Attn city clerk DP-22-26
Date: May 12, 2023 6:31:21 PM
Attachments: [sundanceemail.pdf](#)

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Please see my letter attached below re: the development proposal for 3401 Sundance Drive

Thank you

Kaylee Fairman

RE: File # DP 22-26 3401 Sundance Drive

May 12, 2023

City Clerk

Municipal Hall 2760 Cameron Road, West Kelowna V1Z 2T6

Email: submissions@westkelownacity.ca

Dear City Hall & Council:

As a resident of the area and longtime resident of West Kelowna, I would like to express my concern over this development and the variances proposed. I have safety concerns regarding the traffic on Sundance and Upper Sundance, especially in case of fires or emergencies as this development will drastically increase the amount of traffic in the area and vehicles on the road. I also have worries regarding the loss of habitat and green space, as I routinely use that property as a thru road when walking my dog and see many animals and birds use the trees on that empty lot. Will there be a green space added to Shannon Lake or the Sundance Drive area to combat this? Will any of the trees remain on this lot? Additionally, I would like to express my concerns regarding the pressure that a development of this size will place on our local area and infrastructure as this is a very large increase in resources. In particular, water usage in our area, especially when we do not have a functioning water treatment plant and are on a boil water advisory constantly since I have lived here on Sundance. In addition, I also believe that having the appropriate turnaround near the terminus of this private hillside lane is important for drivers and local traffic, as well as for emergency vehicles such as firetrucks and ambulance. The ONE variance I do agree with in the development permit submitted is the increase in the retaining wall height. Thank you for your consideration and for taking the time to review this letter.

Sincerely,

Kaylee Fairman

2304-2200 Upper Sundance drive, West Kelowna BC, V4T 3E8

email: [REDACTED]

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk - DP 22-26
Date: May 13, 2023 8:26:08 AM

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Josh Hennig 2088 Rosealee Court West Kelowna

After reviewing the delivered Notice for Development Permit with Variance (DP 22-26) with the members of our community, we believe the decision to move forward with the proposed development variances for the construction of 58 townhouses would be unsafe and problematic for a number of reasons. Our primary concern is with safety. In the past several years, we have had very dry summers and with plans for these townhomes to be built so close together, along with the reduced boundary setbacks, any severe situation involving a fire would bring large-scale destruction to the homes and structures located nearby. In the event of an emergency, fire trucks could not properly tend to the buildings, as the lane has a sharp, blind corner with a narrow width under 17 feet and a sharp turning radius that a vehicle over 32 feet would not be able to navigate. Even if a small fire truck, ambulance or other emergency vehicle was able to manage their way up the lane, there is no turn around location, thus causing extended time concerns and even unnecessary deadly circumstances. As someone who has worked on this street I can't stress enough the complete lack of space and parking. Seven homes was already too much for this road. Another major concern is the likely degradation of the physical landscape on the hill side which we worry will cause some level of instability and even possibly water runoff concerns, erosion and potential flooding with landslides. In regard to maintenance and upkeep around the new properties during winter months, there should be a designated area on the property for snow removal and snow storage. We believe the situation of the resulting reduced parking, loading and visitor parking for the new properties should be considered a non-starter, as there is already insufficient space for parked vehicles on Ensign Quay Lane and Ensign Lane, with a width of under 17 feet. With the reduced driveway sizes, it is simply not feasible to suggest an alternative for additional parking. The city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there are serious concerns about

what will happen with emptying garbage, recycle and compostable bins for all of the newly proposed townhomes.

Ensign Quay Lane has a width of less than 17 feet wide with no turnaround and with the 7 current homes, the

proposal of an additional 20 homes, the expectation is a minimum of 60 additional vehicles using this laneway with no

parking, no turnaround and only one shared egress and ingress entryway.

To possibly remedy many mentioned safety issues above access to the new development should be from Sundance

Drive and townhomes should be reduced by a third in size. We recommend a council member physically visit our

community on Ensign Quay Lane to see how apparent these concerns will become.

The mixture of juvenile and adult trees will be a loss to the neighbourhood, not only in regard to screening, shade and

greenery but also with the loss of the roots would most likely destabilize the hillside. As a whole, this green area is a

visually enjoyable aspect for the entire community and will have a negative impact with insensitive and obstruction

overbuilding on greenery and openness.

In conclusion, it is not safe to add that number of dwellings to a blind drive, narrow single entrance/exit and non-turn

around lane.

Thank you for your assistance with this matter,

Josh Hennig 2088 Rosealee Court West Kelowna

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Cc: [REDACTED]
Subject: ATTN: City Clerk- DP 22-26
Date: May 13, 2023 4:10:48 PM

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Attn: City Clerk REGARDING DP 22-26
2023

May 13,

Hello Council,

My name is Richard Getty and my wife is Tracy Sinclair. We own a home at 2097 Ensign Quay Lane.

We have read the Notice for Development Permit with Variance (DP 22-26) and are totally against it. I hope you all have come by and had a look at our road and subject property.

- 1) We have a very small road (as you know it's referred to as a Lane)**
- 2) We have very little parking, no sidewalks or curbs. Now you want to take this away? Has anyone watched a family walking along the dark Ensign Quay Lane Road pushing a baby stroller and navigating moving vehicles? And now you want to add more homes and vehicles like large pickups and SUV's? This is a recipe for disaster.**
- 3) When you come up please have the Fire Department bring up a Fire Truck, or any Emergency vehicles and try to get around the sharp narrow corner onto the Lane. Another recipe for a major disaster. Have the City Trash and Recycling trucks try to make it onto the Lane. I would like to see how are they going to turn around. In fact, how are any vehicles going to turn around? All access for this development should be from Sundance Drive.**
- 4) And last, when the next big rain falls comes, there will be flooding. Who will be responsible for flooded homes? Taking away all those trees and cramming in more townhomes there will be huge water issues. This property is very steep.**

I would like all these safety concerns and issues to be on record if this goes through. If the council passes this Variance there could be big Legal

problems for the City down the road when one of these catastrophes happens.

Please come and have a look and thank you all for your time.

Regards
Richard Getty &
Tracy Sinclair

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk - DP 22-26
Date: May 13, 2023 5:06:06 PM

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Hello. My name is Karen Smiley and I am currently an owner of a home and an operator of a licensed daycare at 2125 Ensign Quay Lane.

I am writing because I have some serious concerns about the proposal put forward on the laneway. I am encouraging all members of council to please vote to reject this proposal. I have lived on this laneway for over 6 years and have seen multiple safety issues regarding the private laneway. Adding so many homes onto a road that was simply not designed for it is going to be disastrous.

Even for current residents, the laneway has been a contentious issue. Currently we have issues where people are parking on the laneway, blocking traffic as well blocking access for emergency vehicles. I have taken multiple pictures throughout the years and have even called in to complain that in the event of an emergency at our daycare it would be hard to get first responders to our location. During the winter months, this problem escalates so much that we no longer accept daycare clients without 4 wheel drive vehicles because it's simply not safe.

To add an additional 18 houses with short driveways and reduce parking is absolutely insane to me. We don't even have sidewalks or street lighting and all traffic is funnelled through a very small blind corner that comes to a steep cliff without barricades.

If you don't believe me then PLEASE and I mean PLEASE come and see this area for yourself before you agree to this proposal. Then imagine yourself with over 2x more traffic (There are 7 houses currently, adding 18 more to this laneway will take us from 7 to 25 dwellings!). It also then connects not to a road, but ANOTHER private laneway that is constantly blocked by vehicles parking where they should not be parking.

Parking bylaws are not enforced here by the city. Once this goes through and people park on the laneway, because of their small driveways and lack of guest parking, it's going to start WARS between neighbours. You're not supposed to park on the laneway, but many do. We have already had fights because people can't get home because they are blocked.

I can already foresee the other issues being brought forward with these variances. For example: All garbage bins are brought below to a lower road, garbage collection does not happen up here because garbage vehicles cannot turn around. Snow removal does not occur on the laneway or the road below. Where the heck is snow going to go? That's what loading zones can AND should be used for in winter.

The variance to forgo the requirement to install a turn around at the end of the lane, reduced loading zones, and parking sizes... Is this serious? Should we just

forgo every bit of infrastructure required for safe neighbourhoods? There is every opportunity to do the right thing and have sensible development in this area, but I would implore you that before we add this many units onto a small laneway, that we consider other safer options.

We should have a second egress connecting Ensign Quay Lane to Sundance Dr. This is because evacuating a daycare in the event of an emergency with 25 homes on a single vehicle corner is a risk that we should not be taking. Emergency vehicles should not have to spend time trying to reverse up laneways and having to compete with 25 homes trying to possibly leave. This is not something a first responder should be dealing with and is not a risk we should be taking.

We already live with these risks on this laneway but accepting this proposal will make things much worse. I support development, but we can do better than this.

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: (ENSURE ATTN; CITY CLERK - DP22-26 IS INCLUDED IN SUBJECT LINE OF EMAIL AS WELL, or it will not be read during meeting)
Date: May 13, 2023 7:20:01 PM
Attachments: [Concerns Ensign Quay.docx](#)

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Good Morning,
Please see attached, My concerns surrounding the proposed development project on Ensign Quay, West Kelowna.

Please feel free to reach out with any questions or concerns
Warmest Regards,
Lyndsay Anne Parrott

TO:

submissions@westkelownacity.ca

SUBJECT:

Attn: City Clerk - DP 22-26

Lyndsay Anne Parrott

3565 Paynter Road,

West Kelowna, B.C., V4T 1R1

After reviewing the delivered Notice for Development Permit with Variance (DP 22-26) with the members of our community, we believe the decision to move forward with the proposed development variances for the construction of 58 townhouses would be unsafe and problematic for a number of reasons.

Our primary concern is with safety. In the past several years, we have had very dry summers and with plans for these townhomes to be built so close together, along with the reduced boundary setbacks, any severe situation involving a fire would bring large-scale destruction to the homes and structures located nearby. In the event of an emergency, fire trucks could not properly tend to the buildings, as the lane has a sharp, blind corner with a narrow width under 17 feet and a sharp turning radius that a vehicle over 32 feet would not be able to navigate. Even if a small fire truck, ambulance or other emergency vehicle was able to manage their way up the lane, there is no turn around location, thus causing extended time concerns and even unnecessary deadly circumstances. There is barely enough room for one car to pass through some days and if there were to be more housing that would increase the amount of traffic on the road.

Another major concern is the likely degradation of the physical landscape on the hill side which we worry will cause some level of instability and even possibly water runoff concerns, erosion and potential flooding with landslides. In regard to maintenance and upkeep around the new properties during winter months, there should be a designated area on the property for snow removal and snow storage.

We believe the situation of the resulting reduced parking, loading and visitor parking for the new properties should be considered a non-starter, as there is already insufficient space for parked vehicles on Ensign Quay Lane and Ensign Lane, with a width of under 17 feet. With the reduced driveway sizes, it is simply not feasible to suggest an alternative for additional parking.

The city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there are serious concerns about what will happen with emptying garbage, recycle and compostable bins for all of the newly proposed townhomes.

Ensign Quay Lane has a width of less than 17 feet wide with no turnaround and with the 7 current homes, the proposal of an additional 20 homes, the expectation is a minimum of 60 additional vehicles using this laneway with no parking, no turnaround and only one shared egress and ingress entryway..

To possibly remedy many mentioned safety issues above access to the new development should be from Sundance Drive and townhomes should be reduced by a third in size. We recommend a council member physically visit our community on Ensign Quay Lane to see how apparent these concerns will become.

The mixture of juvenile and adult trees will be a loss to the neighbourhood, not only in regard to screening, shade and greenery but also with the loss of the roots would most likely destabilize the hillside. As a whole, this green area is a

visually enjoyable aspect for the entire community and will have a negative impact with insensitive and obstruction overbuilding on greenery and openness.

In conclusion, it is not safe to add that number of dwellings to a blind drive, narrow single entrance/exit and non-turn around lane.

Thank you for listening and have a good day.

Warmest Regards,

Lyndsay Anne Parrott

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk - DP 22-26
Date: May 13, 2023 11:15:44 PM
Attachments: [City Clerk DP 22-26 QP.pdf](#)

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Evening,

Attached below is my letter with concerns for the development DP 22-26.

Thank you for your time and have a great day!

Cheers,

Queena Pan

Queena Pan
2119 Ensign Quay Lane
West Kelowna, B.C, V4T 2Z4

Dear Attn: City Clerk – DP 22-26

I hope this letter find you well. My name is Queena and a resident on Ensign Quay Lane. I am writing to the council regarding the Development Permit with Variance (DP 22-26) as I have serious safety concerns with the proposal.

I live on a narrow private lane that can barely have two small vehicles driving at the same time. There is a sharp turn to enter our lane and you cannot see oncoming traffic. No emergency vehicles would ever be able to come up in case of an emergency. There is no area for turn around either. Vehicles need to use the driveways our homes to be able to turn around; assuming that there is an open driveway. It is already a chaos with just 7 houses, I cannot imagine having 18 more houses across from us.

In the winter it is not safe to drive up not just our lane that has a steep sharp turn, but another private steep road to get to our lane. My vehicles with good winter tires have troubles climbing those hills. Most of the time, I must leave my vehicles down the mountain. With an additional 18 houses with 2 vehicles per house, it is a death sentence to drive in on both private roads in the winter. There are no street barriers to protect us from the cliff we must drive up every day. There are also no street snow removals that come up to either private lane. We must shovel the snow across the street, but if there are going to be 18 houses, where are we to shovel the snow to.

On the end of our lane and the hill on the back of our houses are dry grasses. If there was a fire, how is 50+ vehicles supposed to evacuate, because there is no way a fire truck can come up Ensign Quay Lane. There is not even a sidewalk here if we wanted to evacuate by foot. We ask that the council visit our lane and the proposed site.

Thank you for your time and have a great day!

Cheers,

Queena Pan

May 12, 2023

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk - DP 22-26
Date: May 13, 2023 11:19:39 PM
Attachments: [City Clerk DP 22-MYC.pdf](#)

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Hello,

Attached is my letter to the council in regards to the development DP 22-26.

Thank you,
Mei

Mei Ying Cao
2119 Ensign Quay Lane
West Kelowna, B.C, V4T 2Z4

Dear Attn: City Clerk - DP 22-26,

My name is Mei Ying and I am a resident on Ensign Quay Lane. I am writing to you in regards to safety concern for the DP 22-26 proposal.

There are currently 7 houses on Ensign Quay Lane and it is already a safety concern as our street is narrow with a dangerous tight corner at the entrance. The proposed development will exponentially increase the traffic on our street, which can barely have two cars driving at once. There are no room for parking as is. Where would the vehicles from the proposed 18 townhouses go, there is no where to park.

Our street does not have a turning point, people use private drive ways to turn around. Service and emergency vehicles cannot come up to our street. They cannot fit the tight corner. This is the truth! Around 2017, one of our carbon monoxide detector went off. We called the West Kelowna fire station, they were sent over to check. They could not come up to our house. They parked the fire truck down the mountain and all 4 fire fighters walked up the mountain to our home. Ask them, they will have records of the incident and coming for an inspection.

If there were a situation where residents have to evacuate, how is our lane supposed to support everyone. The winter is brutal as is, we push the snow across the lane where the townhouses would be. I have no idea where the snow will go if there are town houses there; the city does clear our lane.

I hope you understand our concerns and make the appropriate decision for your residents and community.

Sincerely,
Mei Ying Cao

May 12, 2023

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk DP 22-26
Date: May 14, 2023 4:35:10 PM

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Todd Ruscheinsky/Jo-Anne Koral
2111 Ensign Quay Lane

To whom it may concern;

My husband and I built this home in 2006 when there was very few homes around! Our foundation has 20ft walls buried due to the unstable hillside! I can't imagine what damage will occur further to our homes if 20 more townhouses are put on it! We also were nervous about the lane considering how narrow it was and not having snow removal or garbage pick up was definitely a deterrent but we fell in love with the green area behind us and the beautiful view. We were told then that because it was a private lane there would be no chance of further building! To fantasize 20 more homes on it is beyond ludicrous but severely unsafe as there is no turn around for emergency vehicles, no actually no access because of the 17ft sharp, blind corner and absolutely no parking..no where for snow to go and unimaginable that it possibly could even be contemplated! Please someone from the city come drive up and just see what challenges even 7 homes have here!

We implore you not to allow this development to go through on this tiny little lane!

Thank you

Todd Ruscheinsky/ Jo-Anne Koral
2111 Ensign Quay Lane

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Cc: [MayorAndCouncil](#)
Subject: Attn: City Clerk (Regarding DP 22-26 from Ian & Carolyn Larratt of 3025 Ensign Lane)
Date: May 14, 2023 7:41:15 PM
Attachments: [DP 22-26 Ian & Carolyn Larratt 3025 Ensign Lane.pdf](#)

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Please find attached a letter from my wife and I regarding our concerns regarding DP 22-26. We are the owners of 3025 Ensign Lane, West Kelowna, BC.

Ian Larratt
250-868-1696

Ian & Carolyn Larratt
57 Eugene Ave
Whitehorse, YT Y1A 4A3

May 14, 2023

City of West Kelowna
2760 Cameron Rd
West Kelowna, BC V1Z 2T6

Attn: City Clerk (DP 22-26)

Dear Sir/Madam,

My wife and I are the owners of 3025 Ensign Lane, West Kelowna. We are writing to express our concerns regarding the proposed variances for DP 22-26 at 3401 Sundance Dr, West Kelowna.

The current iteration of the site plan contains a number of oversites, design flaws, and liabilities that we are confident all concerned parties would like to resolve. They are as follows:

- 1) **Winter safety.** The easement from Ensign Lane that would grant access to proposed units 39-54 goes up a very steep, narrow laneway that is impassable when covered with snow. The current residents clear the snow before attempting to drive on it. Failing to do so results in packed tire prints that make the laneway hazardous even for foot traffic. Hiring a snow removal company would not sufficiently mitigate this issue as there will still be periods of time where the driveway is impassible due to snow accumulation that has yet to be cleared. There is also no space to dump cleared snow. Hauling away the snow would require equipment that would be unreasonably disruptive to the neighborhood.
- 2) **Garbage disposal.** There is not enough physical room on Ensign Lane for the required number of garbage bins. Between the current parking and garbage bin requirements, space is already very cramped. It should also be noted that our lot spans Ensign Lane, and we **do not** give our consent for any residents of the proposed development to leave their garbage or yard waste bins anywhere on our property. Leaving bins on our land, even briefly, is not a right granted by the easement. Private garbage collection will have to be organized as part of the strata arrangement.
- 3) **Parking.** Space is very limited, and there have already been parking disputes among the current neighbors. There is insufficient parking for all of the lower units (21-58) as parking is not a privilege granted by easement access. When calculating off-street parking requirements, it is important to consider that there is no on-street parking for any of these units. Vehicles parked on laneways would be subject to towing at the discretion of the respective landowners on whose land the vehicle is parked. Access to proposed units 54-56 requires crossing a portion of

our land that is used for guest parking and also for winter parking when the laneway is impassible (see red area in Appendix 1). Additionally, we believe that the turning radius for entry into units 49-52 is insufficient given the steep grade of the laneway. We strongly recommend that a traffic engineer assess the proposed site plan for flow, parking, and safety.

- 4) **Access over land not covered by easement.** Access to proposed units 52-56 would require crossing a portion of our property not covered by an easement (see blue area in Appendix 1). We **do not** give our consent to this access. On May 11, 2023, we notified the City of West Kelowna and the developer's agent of this planning oversight. Neither party was aware. This failure to identify such an obvious constraint during the discovery phase of the project is deeply concerning. It casts serious doubt on the competency of the City of West Kelowna, the developer, and the developer's agents.
- 5) **Emergency access.** My wife and I both used to work as paramedics in the Okanagan, and it is our opinion that the proposed units 21-52 are not safely serviceable by first responders. There is insufficient room to maneuver emergency vehicles on the narrow laneways. Furthermore, units 39-52 would require a response on foot when snowy – this would be especially hazardous with stretchers and gear.
- 6) **Unreasonable disturbance to neighbors during construction.** Many of the proposed units would require extensive removal of bedrock. This process is long, loud, dusty, and is in very close proximity to existing homes. The bedrock removal for the recent development on the uphill side of Sundance Dr was much farther away, yet still created noise and vibrations sufficient to rattle our windows.
- 7) **Damage to existing property.** The aforementioned bedrock removal is likely to cause vibratory damages to nearby structures. Appropriate assessment and prescription of work should be completed by qualified engineers prior to development permit approval. Any development permit should also stipulate measures to prevent damage from rockfall.
- 8) **Unnamed laneway.** The laneway that would provide access to units 39-54 is not engineered to handle the required level of traffic. It is paved with thin asphalt that already shows signs of wear from the two residences currently using it. It would have to be widened and regraded. This would also mitigate some of the concerns regarding winter conditions, however we **do not** give our consent to these measures at this time.

From an urban planning perspective, the level of proposed housing density for the lower portion of the project (units 21-58) is not appropriate for such a steep slope with such limited access/egress. The density for this area should be evaluated separately, and not as fraction of the whole property. The overall design appears to have the singular goal of maximizing the developer's profits.

We strongly urge Council to conduct a site visit.

We are also concerned that neighbors have not been adequately notified. There is only one development proposal sign on the lower portion of the property. It has been placed on Ensign Quay Lane. There is no signage on Ensign Lane, or on the unnamed laneway that accesses proposed units 39-54. One of our tenants texted us saying, "I found the development sign hiding down the street beside us. Sneaky buggers." In lieu of any evidence to the contrary, we are inclined to agree with his assessment.

As for the variances requested, we formally oppose them all. Granting them would make way, either directly or indirectly, for the preceding concerns to become a reality. We have full confidence that the

City of West Kelowna will refrain from granting any variances or development permits that would adversely affect the property value, quality of life, and safety of its citizens.

Should the proposed development go ahead, we will hold all involved parties liable for any and all damages that arise.

The following are a few additional recommendations:

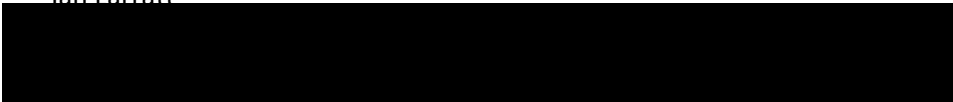
1. Hire an unbiased engineer to conduct pre- and post-construction structural assessments of those properties most likely to sustain vibratory damages during construction. The City should oversee this process at the expense of the developer.
2. Hire an unbiased property appraiser to assess the impact of the proposed development on surrounding property values. If said appraiser deems the resale value of the surrounding properties to be negatively impacted by the development, the City should refrain from granting the variances that would allow for the proposed development to be built OR compensation should be paid to affected home owners in the amount of the lost value. The site constraints on this project are such that withholding the requested variances would likely necessitate a complete redesign and would most likely rule out the possibility of building two parallel rows of townhomes (upper and lower). This is one of the reasons why we believe that the City of West Kelowna would incur substantial liability by granting the requested variances and development permit.
3. Require a deposit from the developer to be used as a means to pay for temporary accommodations and/or compensation for lost rental income for homes that may become uninhabitable during construction due to noise, vibration, etc.
4. Require that the entire project be redesigned such that all vehicular access is via Sundance Dr.

We are available for consultation and would welcome the chance to help this development become viable.

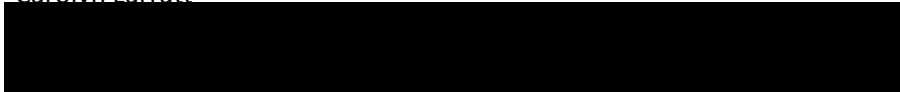
Thank you for your time and consideration,

Sincerely,

Ian Larratt



Carolyn Larratt



Appendix 1.



From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk DP 22-26
Date: May 14, 2023 9:34:46 PM
Attachments: [Ensign DP 22_26.pdf](#)

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Hello,

Attached is my submission for inclusion in the package to Council regarding DP 22-26.

Thanks,
Bruce Larratt

Bruce Larratt
2605 Campbell Rd
West Kelowna, BC V1Z 1T1

May 14, 2023

City of West Kelowna
2760 Cameron Rd
West Kelowna, BC V1Z 2T6

Attn: City Clerk (DP 22-26)

I am requesting that Council deny Development Permit (DP 22-26) and have the applicant redesign the proposal in accordance with the City's Official Community Plan Development Permit guidelines and provide access for all units from Sundance Drive.

Providing access for 38 additional units from private lanes that are already not functioning well is not realistic and is unsafe. The proposed development would displace parking for many existing residences. Alternative access has already been identified by the proponent as shown by the construction road identified in their Construction Management Plan.

My perspective is unique in that I built and lived in the first house on Ensign Lane (2994 Ensign Way) located 40 metres away from the proposed unit 56. My son subsequently built immediately adjoining the proposed units 45-56 (3025 Ensign Lane). I was actively involved in the construction of both houses and lived at the first for five years then at the second for six years. I currently assist my son, who lives in the Yukon, with the property management of 3025 Ensign Lane. Both houses were built on steep and challenging lots without any need for variances and both have more than the required parking. Even so, parking is fully utilized. I have had my share of unpleasant exchanges over parking issues.

DP 22-26 proposes that units 39-54 would be accessed from what is being called an Ensign Lane extension, which in actual use is a driveway accessing 3025 and 3033 Ensign Lane. It rises steeply from Ensign Lane then levels off. This steep section faces north so snow or ice does not melt easily. That section must be kept clear and sanded to be kept safe in the winter. Any vehicle driving down that "extension" and turning right onto Ensign Lane will drive their front bumper into the roadway unless they swing wide to the left first. Any larger vehicle should only exit to the left as the turn is too sharp to the right. Any vehicle with a low backend such as a motorhome or ambulance can only enter the steep "extension" by driving over the gravel parking spot in front of proposed units 55/56.

Figure 7 of the Development Services Council Report shows a rendering including a lovely, grassed area opposite the lane townhomes. The only units where it may be possible to enact that rendering are 39-42. Photo 1 is the driveway servicing 3025 and 3033 Ensign Lane and referred to as the Ensign Lane extension by the proponent.

Proposed units 53-56 have no legal access as their driveways cross 3025 Ensign Lane. The proponent was unaware of this oversight until my son pointed it out a few days ago. This space is a useful parking space especially in the winter if the driveway is too icy.

When my mother-in-law purchased both lots in 2004 there was no “extension”. Chris Ensign explained to me that the easement was necessary so that 3033 Ensign Lane could access from above as the lot was too steep to have direct access from Ensign Lane. Easement access for the DP subject property was included only to keep options open. It was never intended or designed to be the primary access for so many units.

There are other issues on Ensign Lane including drainage, address confusion and snow removal all of which will be compounded by the scale of the proposed development.

Being very familiar with the subject property and neighbourhood, I see two ways forward for the developer that would benefit the community. The first is to build only the 20 proposed townhomes fronting Sundance Drive. The second is to purchase both 3025 and 3033 Ensign Lane and include that land in a development that has proper street access.

I plan to attend the council meeting on Tuesday and look forward to hearing the presentations and discussion.

Kind Regards,
Bruce Larratt

Photo 1: Driveway for 3025 and 3033 Ensign Way taken from the far side of Ensign Lane.



From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attn: City Clerk
Date: May 15, 2023 7:17:38 AM

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File number DP 22-26

Good morning,

I am writing against the proposed development on Sundance Drive, specifically the number of variances requested and the large volume of homes being added to the single family neighborhood.

I understand that development will happen, but would rather see single family homes. Upgrades to road infrastructure needs to happen first. Especially sidewalks added to Shannon lake rd and a roundabout on Daimler to be able function and flow with the already increased development in the neighborhood.

We moved to the okanagan 7 years ago, specifically to West Kelowna to get away from crowded neighborhoods and to enjoy the naturally beauty West Kelowna and would hate to see council turn our neighborhood into crowed car lined streets. Plus where would all these cars park during snow events when there are no parking allowed on the streets?

Thank you for accepting my letter against the development.

Heather Manaog
2130 Shamrock Dr

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: ATTN: City Clerk DP 22-26
Date: May 15, 2023 7:57:40 AM

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Alexander Tyabji
2125 Ensign Quay Lane

I have reviewed the notice of development for DP 22-26 and I am writing with serious concerns over the safety for the residents in this area going forward. I urgently ask you to do a site visit before even considering such variances.

Just last winter I came dangerously close to driving off the cliff at Ensign Quay Lane. The ice was packed due to light traffic and even though we collectively paid for snow removal on the road, it was a dangerous thick sheet of ice. My 4 wheel drive f150 came inches away from going down the embankment. If I was in my 2 Wheel Drive Vehicle, I know I would have gone down the cliff. This corner is a serious risk during the winter months as I have seen vehicles stuck blocking the only access point into and from our laneway. I have pictures and have documented these issues over the years.

In the summer months, this laneway backs onto a slope of dried grass and dead weeds. We literally had a giant grassfire on shamrock this year, which is one of the roads below. We are at a serious risk of having a grass fire of our own and if you don't believe me, come for a visit and take a look at the end of the laneway yourself in July or August.

The closest fire hydrant is at the end of Ensign Quay Lane. How are we going to evacuate 25 homes and a daycare when competing firetrucks are trying to drive up? If a fire truck is parked at the hydrant, how are parents going to come and get their children from the daycare during an evacuation.

There is no reason to grant any of the variances for this development. It is a large piece of land and these requests are clearly to maximize profits and leave the aftermath to the residents of this area. When I built my home, I changed my design to fit the rules of the area and this developer should do the same. Every request is to put more and more houses on a laneway that clearly wasn't designed for it.

In writing this letter, I am documenting that I have informed you of this serious safety risk. If something happens because of an emergency and there is a serious injury or death as the result of not having proper emergency vehicle access then you and the city should be held responsible.

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Fwd: Attn: CITY CLERK - DR 22-26 ENSIGN DEVELOPMENT
Date: May 15, 2023 3:21:59 PM

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To whom it may concern

My name is Andreea Savan, and I am the owner of 2115 Ensign Quay Lane.

After reviewing the Notice for Development Permit with Variance (DP 22-26) with members of our community, I believe the decision to move forward with the proposed development variances for the construction of 58 townhouses would be unsafe and problematic for many reasons.

Our primary concern is with safety. In the past several years, we have had very dry summers and with plans for these townhomes to be built so close together, along with the reduced boundary setbacks, any severe situation involving a fire would bring large-scale destruction to the homes and structures located nearby. In the event of an emergence, fire would be unable to properly tend to the buildings, as the lane has a very sharp, blind corner with a narrow width under 17' and a very sharp turning radius that a vehicle over 32' would not be able to navigate. Even in the event that a small fire truck or an ambulance would be able to make their way up there, there is no turn around location which would cause further time delays and even unnecessary deadly circumstances, especially with the additional vehicles belonging to residents of these townhomes.

Another major concern is the likely degradation of the physical landscape on the hillside, which we worry will cause some level of instability, and even possibly water runoff concerns, erosion and potential flooding or landslides. In regards to maintenance and upkeep around the new properties during winter months, there should be a designated area on the property for snow removal and snow storage. Not only that but who would be responsible for snow removal? The city of West Kelowna? Currently the city of West Kelowna does not take responsibility for Ensign Quay lane, so if these homes will have access from Ensign Quay lane, will they be responsible or the city. We have already approached this issue with the city

in the past and we were told a plow does not fit up Ensign Quay Lane, and also has no way to turn around.

We believe the situation of the reduced parking, loading and visitor parking for the new townhomes should be considered a non-starter, as there is already insufficient space for parked vehicles on Ensign Quay Lane, and Ensign Lane, with a width of under 17'. With the reduced driveway sizes, it is simply not feasible to suggest an alternative so additional parking.

The city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there is serious concern about what will happen with emptying garbage, recycle and compostable bins for all these newly proposed residences.

Ensign Quay Lane has a width of 17' with no turn around and with the 7 current homes, the proposal for an additional 20 homes, the expectation is a minimum of 6- additional vehicles using this laneway with no parking, no turnaround and only one shared egress and ingress entryway.

To possibly remedy many of the above mentioned safety concerns, access to the new development should be from Sundance Drive and townhomes should be reduced by 1/3 in size. We recommend a council member physically visit our community on Ensign quay Lane to see how apparent these concerns will become.

The mixture of juvenile and adult trees will be a loss to the neighborhood, not only in regard to screening, shade, and greenery but also with the loss of the roots would most likely destabilize the hillside. As a while, this green area is a visually enjoyable aspect of this entire community and will have a negative impact with the insensitive and obstruction overbuilding on greenery and openness.

I find it very interesting how laws, bylaws, regulations as well as building requirements can change depending on what is convenient and for whom. I built my home in 2016, I had just moved out here from Ontario, it was my first home, we were starting a life, starting a family, planning and building for our future. 2115 Ensign Quay Lane was the last home to be built on that Lane. It was a challenging build due to the limited amount of space, for materials as well as for machinery. But it was a challenged we welcomed, it was a start to our life and a way to prove to ourselves what we were capable of accomplishing. I cannot even begin to imagine what building 58 new townhomes, 28 of them off Ensign Quay lane, I cannot imagine what that would be like, or how you would even get an excavator in there, or a pumper for the concrete, or even a delivery truck to deliver materials or lumber or trusses.

When we build this home we were told that it was legal for us to build a legal suite, which we did. When it came to occupancy, we were granted occupancy for the main part of the house, but not the suite. It was not a matter of construction or building code, everything was perfect in that regard, but it was a matter of signatures. We have a double car garage with a double driveway as well as enough space for another car beside the driveway. We met all the requirements, except for signatures, which were never mentioned to us before. We were required to have a signed approval from each homeowner Ensign Quay Lane, as well as Ensign Lane, indicating they approved for us to have a legal suite, which would create more, people and of course more traffic. Now this makes no sense, especially when you look at the big picture now, if a small 1 bedroom legal suite was an issue at that time, how is it that 58 new townhomes is all of a sudden ok. 2125 Ensign Quay Lane, they were building a beautiful space in the basement for childcare, a necessary and much sought after service in the community, and they were also required to get the same signatures due to the increased traffic.

It seems like we fast forward a few years and we are now looking at 58 more townhomes, 28 of each accessing from Ensign Quay lane, which just a few short years ago had difficulties accepting increased traffic of a 1 bedroom legal suite and a childcare space. How is this fair, how is this an example of West Kelowna treating all members of it's community the same way? Let's have this developer get signed approvals from all homeowners affected by this development. Let's have the homeowners have a real say in the decision making, just like they would've had a real say in my home. The small portion of the West Kelowna community is already underserved. We pay property taxes just like any other residents, yet we do not have the privilege of having out garbage picked up in front of our house, or our street snow plowed.

I'd like to propose a trial run. Let's have an emergency drill, just with the homes and residents which currently reside on Ensign Quay Lane. Lets hypothesize (god forbid) a fire and a 911 call. What would you have there 2 firetrucks, min 1 ambulance and min 1 police vehicle. How would that turn out, where would they go, how would they park, how would they work around each other? Let's see how that goes and now let's imagine 28 more residence on the opposite side of the currently existing homes on Ensign Quay Lane

In conclusion I, and we, do not feel that it would be safe to add that number of dwellings in this area, with such a blind drive, narrow single entry/exit, as well as no turn around

Thank you for your assistance in this matter.



owner of 2115 Ensign Quay Lane

West Kelowna, BC



Attachment 3

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023
DP REVISIONS	04	JULY 28, 2023
DP REVISIONS	05	AUGUST 18, 2023

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PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
FULL SITE PLAN
PROJECT DETAILS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A1.0

SCALE
1:300



SITE INFORMATION

PARCEL ZONING	R4
PARCEL AREA	13,043m2
TOTAL FOOTPRINT AREA OF HOMES	4,474m2
LOT COVERAGE	34%

AMENITY SPACE

FOR MORE THAN 1 BEDROOM UNIT	25.0m2 (269.1 ft2)
TOTAL NUMBER OF UNITS	57
REQUIRED AMENITY AREA	25 X 57 = 1,425m2 OF AMENITY SPACE
AMENITY AREA PROVIDED	4,012 m2 for central court area and patios

LOADING	
REQUIRED	PROVIDED
1 PER 15 UNITS TOTAL UNITS = 57 REQUIRED = 3.8 = 4	PROVIDE 2 LOADING ZONES, ONE ON ENSIGN QUAY LANE ADJACENT TO HAMMERHEAD AND ONE ON ENSIGN LANE WITH TURN AROUND ON SUNDANCE 8 HOMES HAVE ADEQUATE DRIVEWAY AREA TO ALLOW FOR LOADING

TANDEM UNIT TYPE A: SHORT UNIT WITH ONE DEDICATED STALL IN GARAGE	TANDEM UNIT TYPE B: DEEP UNIT WITH 2 CAR TANDEM GARAGE	TANDEM UNIT TYPE C: SINGLE UNIT 39
--	---	---------------------------------------

SUNDANCE DRIVE UNITS - SIDE BY SIDE	QUAY LANE UNITS - TANDEM		
TYPICAL UNIT AREA = 112.4m2	TYPICAL UNIT AREA TYPE A = 59.92m2	TYPICAL UNIT AREA TYPE B = 60.29m2	TYPICAL UNIT AREA TYPE C = 60.29m2
TOTAL NUMBER OF UNITS = 20	TOTAL NUMBER OF UNITS TYPE A = 6	TOTAL NUMBER OF UNITS TYPE B = 30	TOTAL NUMBER OF UNITS TYPE C = 1
TOTAL AREA FOR ALL UNIT 1 TO 20 = 2,248 m2	TOTAL AREA FOR ALL UNITS = 360 m2	TOTAL AREA FOR ALL UNITS = 1,206 m2	TOTAL AREA FOR ALL UNITS = 60.29 m2

UNIT PARKING			
REQUIRED	PROVIDED SUNDANCE	REQUIRED	PROVIDED QUAY
PER UNIT = 2 TOTAL UNITS = 20 REQUIRED = 40	GARAGE PARKING = 40 DRIVEWAY STALLS = 23 TOTAL = 63	PER UNIT = 2 TOTAL UNITS = 37 REQUIRED = 74	GARAGE PARKING = 68 DRIVEWAY STALLS = 14 TOTAL = 82
VISITOR PARKING		TOTAL STALLS AVAILABLE = 13 ALSO AVAILABLE IS STREET PARKING ALONG SUNDANCE ROAD	
DEVELOPMENT VISITOR PARKING REQUIREMENT = 10% OF REQ. TOTAL REQUIRED = 40 + 74 = 114 x 10% = 11.4 OR 11			

ALL REQUIRED PARKING STALL SIZES SHOWN ARE DESIGNED AS PER BYLAW STALL SIZE REQUIREMENTS

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
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DP REVISIONS	03	APRIL 17, 2023
DP REVISIONS	04	AUGUST 18, 2023

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PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

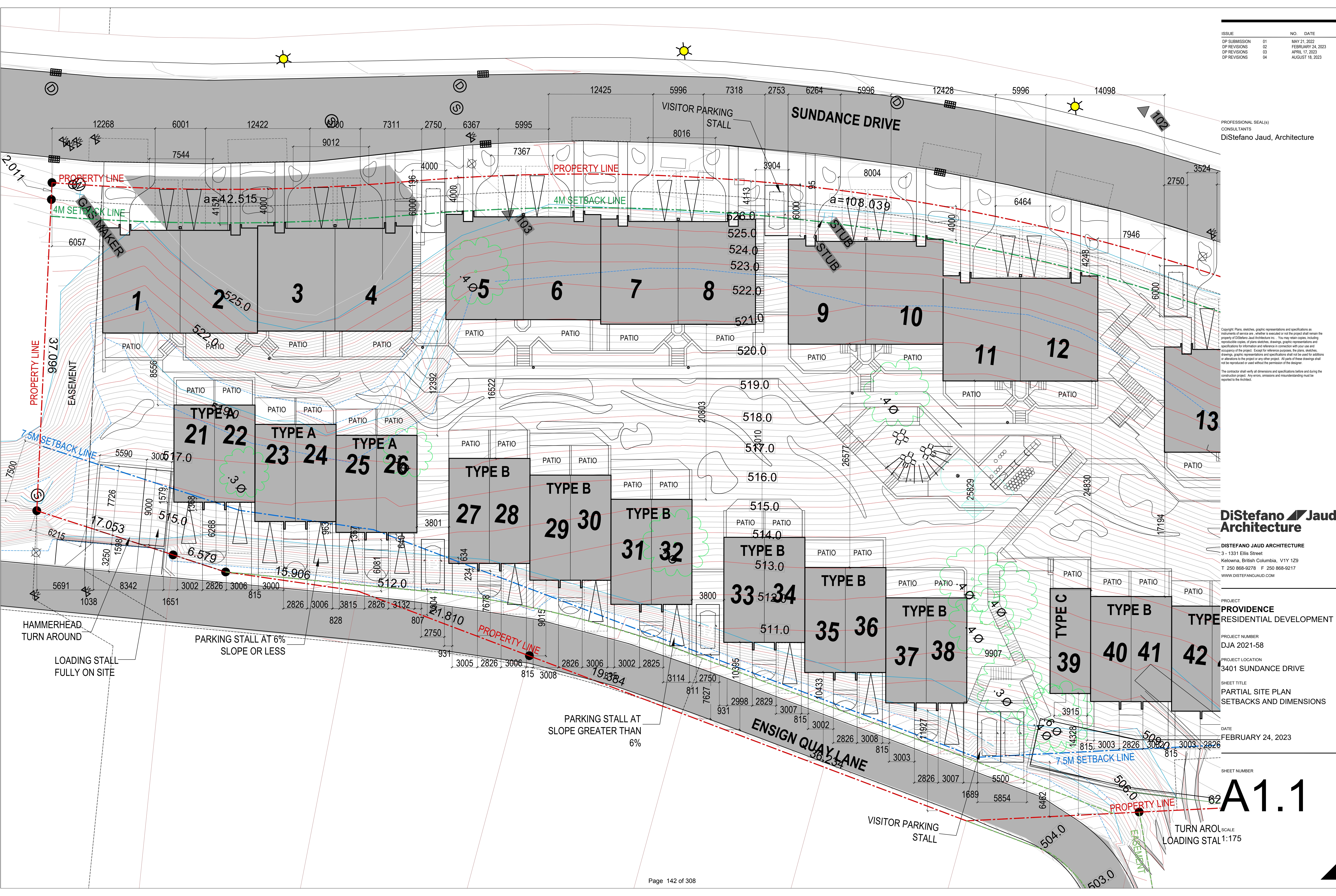
SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

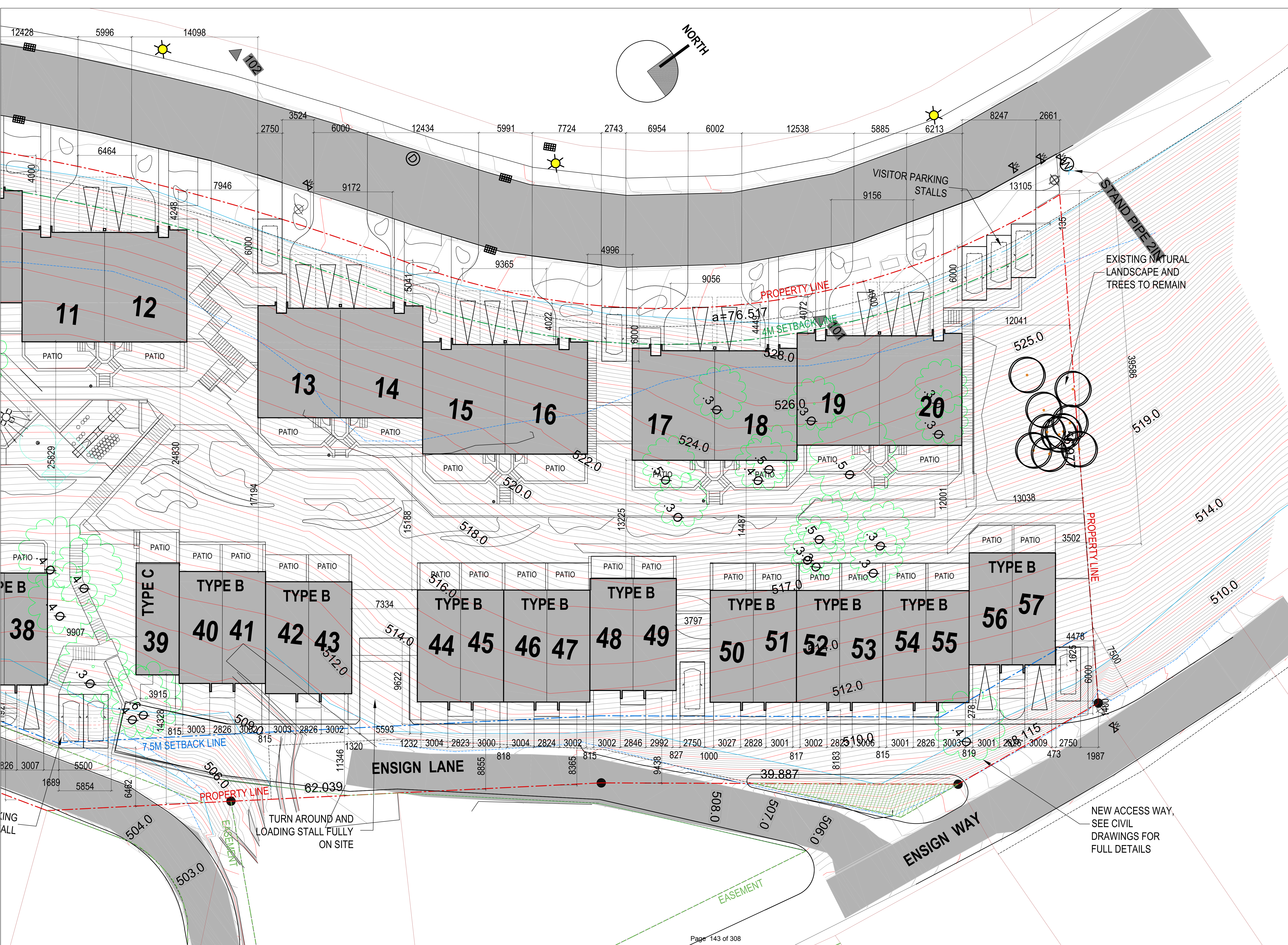
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A1.1

TURN AROUND
LOADING STALL
SCALE 1:175





ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023
DP REVISIONS	04	AUGUST 18, 2023

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PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A1.2

SCALE
1:175

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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CONSULTANTS
DiStefano Jaud, Architecture

UNIT AREAS:

MAIN FLOOR (ENTRY LEVEL) = 1,210 sq.ft.
MID-LOWER LEVEL = 738 sq.ft.
BOTTOM LEVEL = 738 sq.ft.

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED IN 2 GROUPS OF 2 FOR A TOTAL OF 4 UNITS AS PER THE SITE PLAN.
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
ENTRY LEVEL PLAN
SUNDANCE ROAD

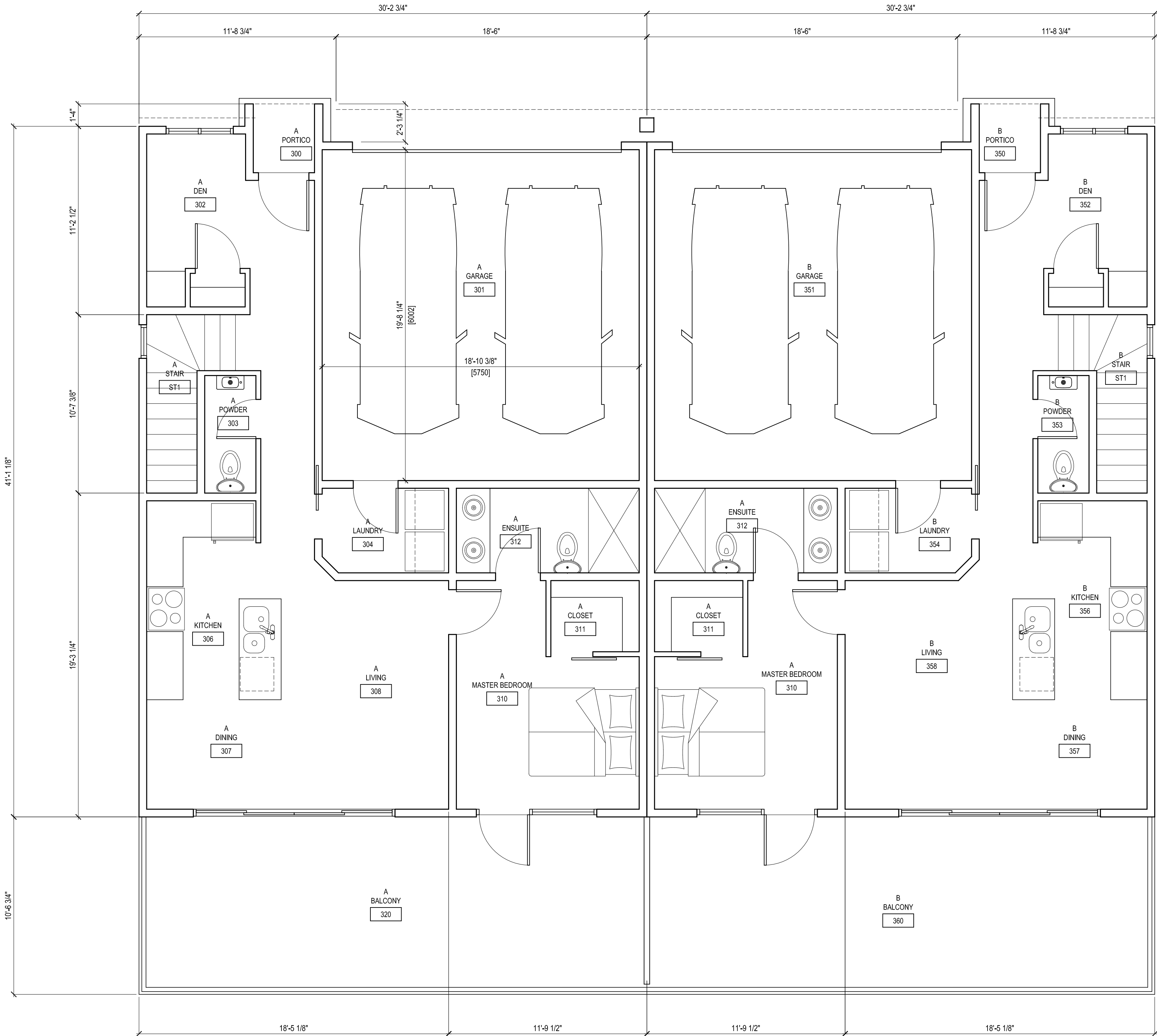
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.1

SCALE
1/4"=1'-0"

SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE



1
A2.1
ENTRY LEVEL
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

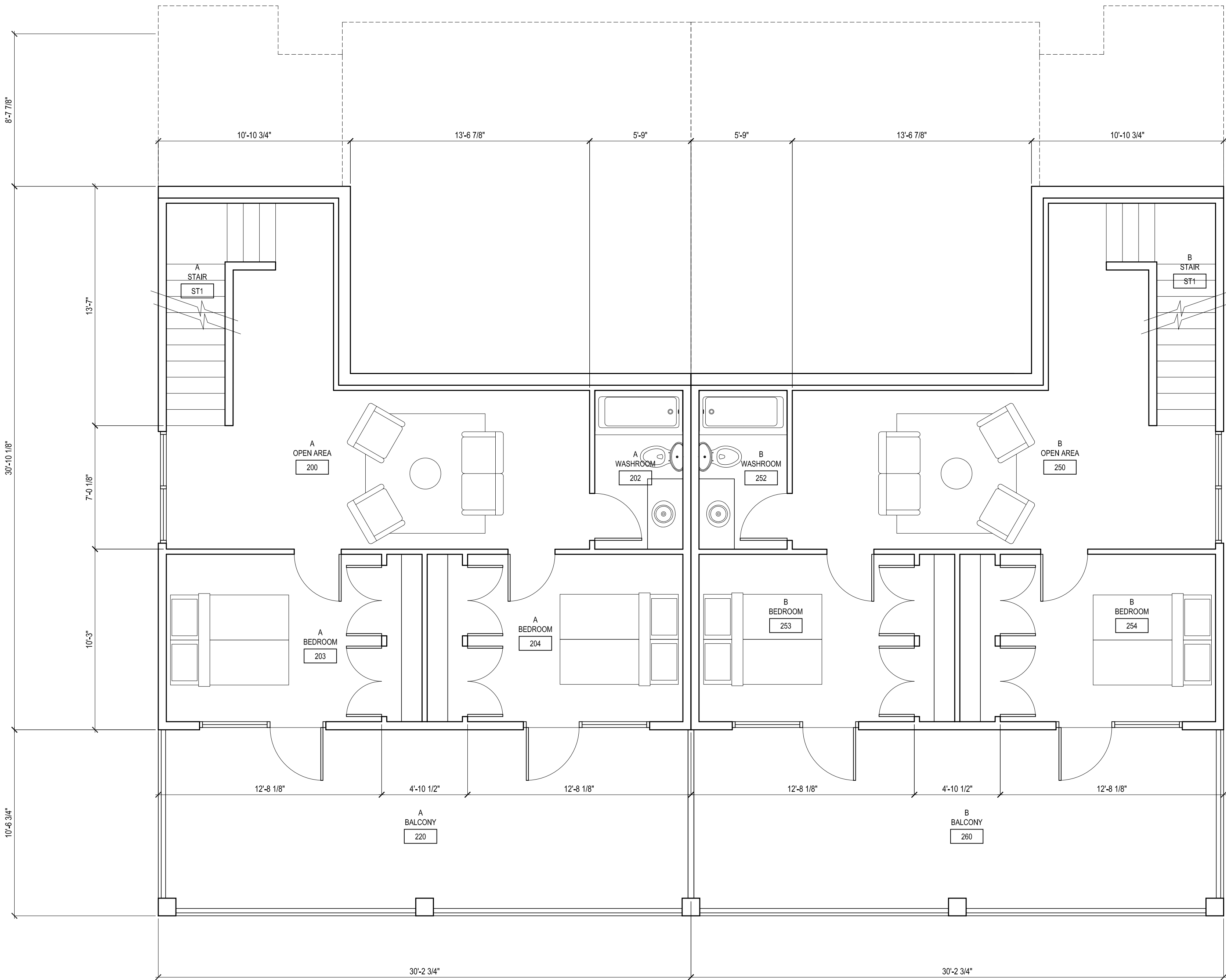
SHEET TITLE
SIDE BY SIDE UNITS
LOWER MID-LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.2

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.2
MID-LOWER LEVEL
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 11, 2023

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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

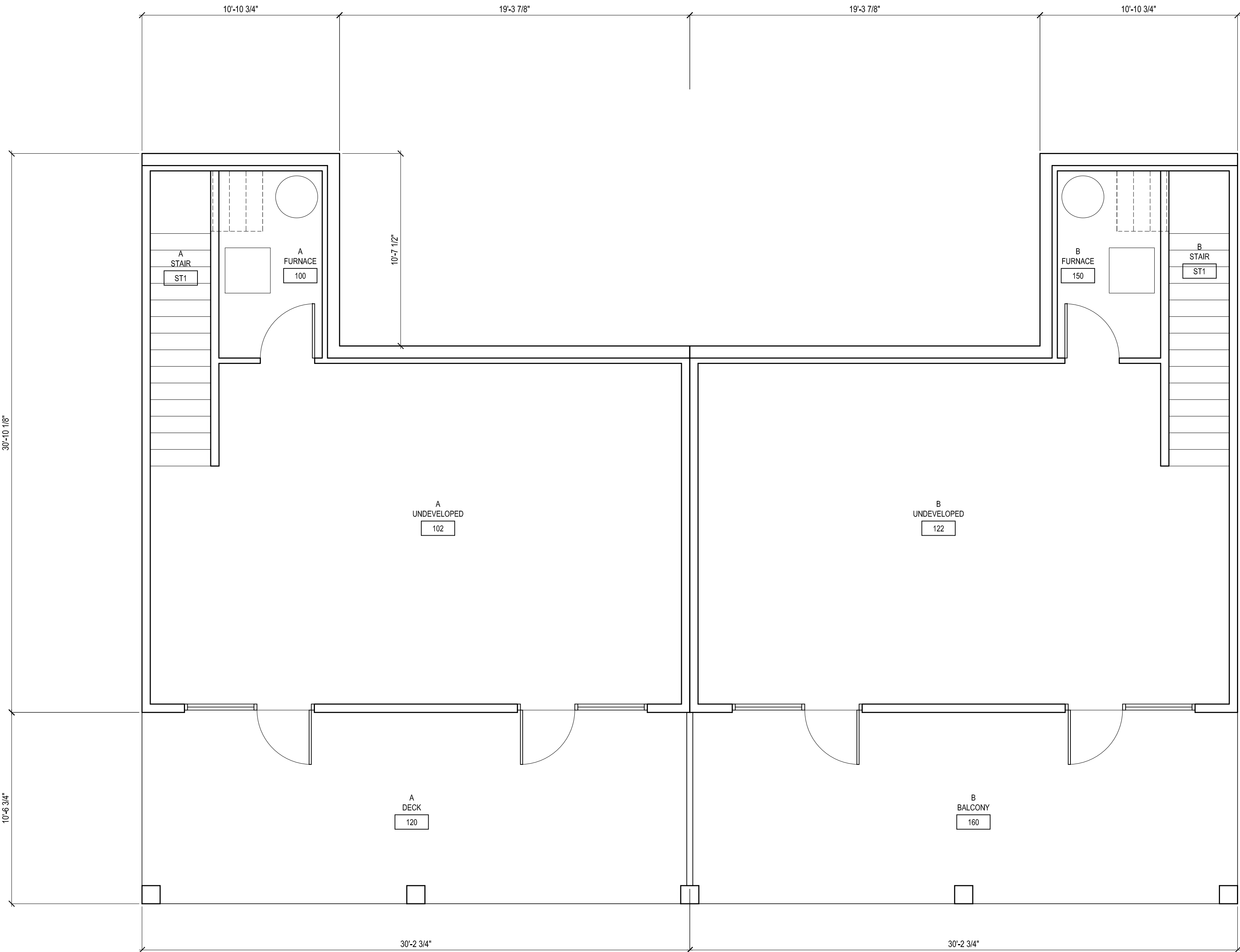
SHEET TITLE
SIDE BY SIDE UNITS
BOTTOM LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.3

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.3 LOW LEVEL
1/4"=1'-0"

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

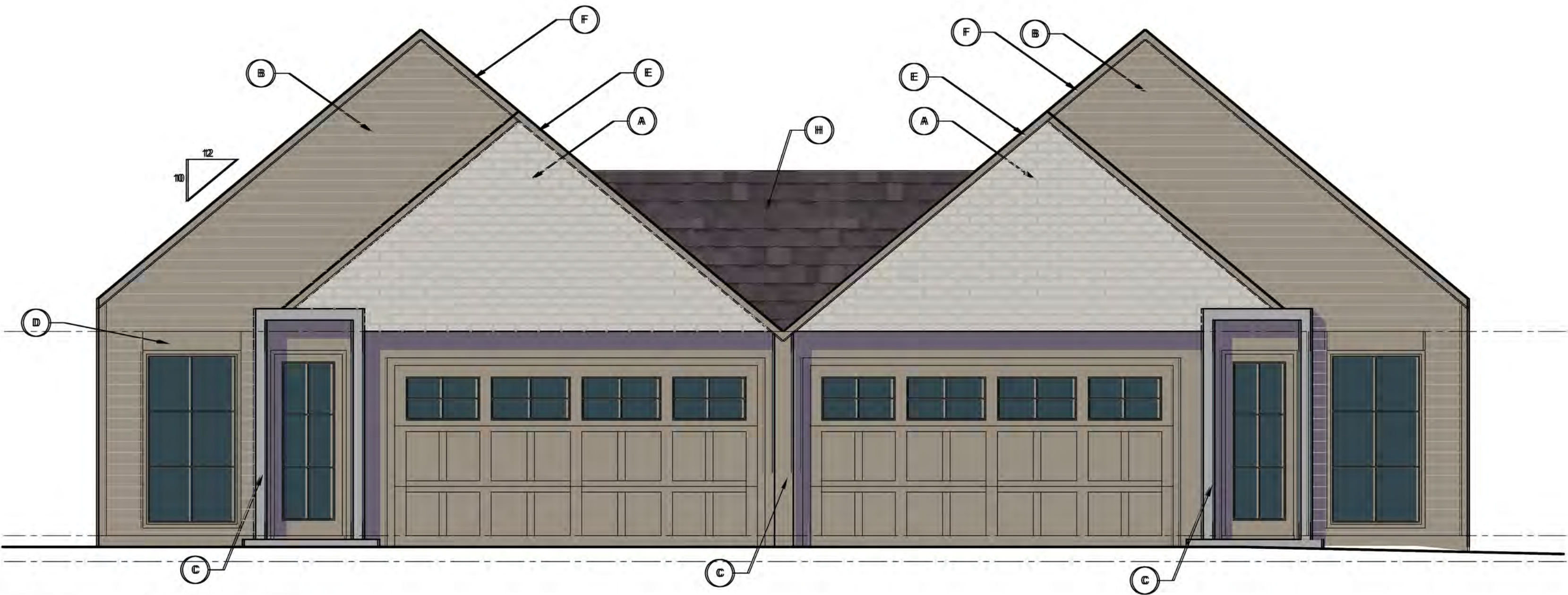
SHEET TITLE
SIDE BY SIDE UNITS
ELEVATIONS

DATE
FEBRUARY 24, 2023

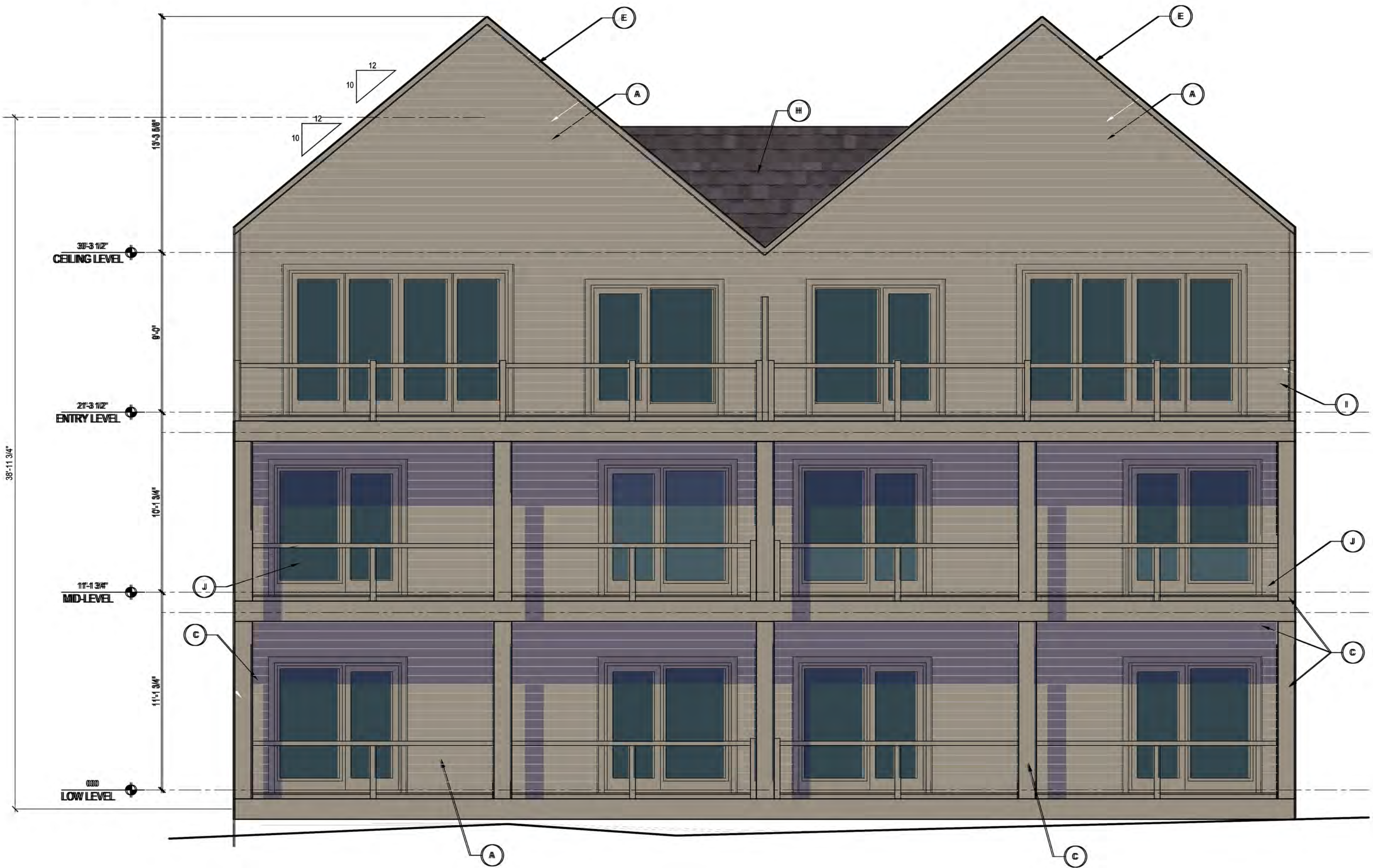
SHEET NUMBER

A2.4

SCALE
1/4"=1'-0"



1
A2.4
STREET ELEVATION - COMBINED
1/4"=1'-0"



2
A2.4
REAR ELEVATION - DOUBLE UNIT
1/4"=1'-0"

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION		
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS		GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUP OF UNITS USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE IN COLOUR TONES FROM LIGHT TO MID AND THEN TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS. THE COLOURS SHOWN IN THIS ELEVATION REPRESENT THE MID-TONE COLOUR OPTION.

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PROJECT NUMBER
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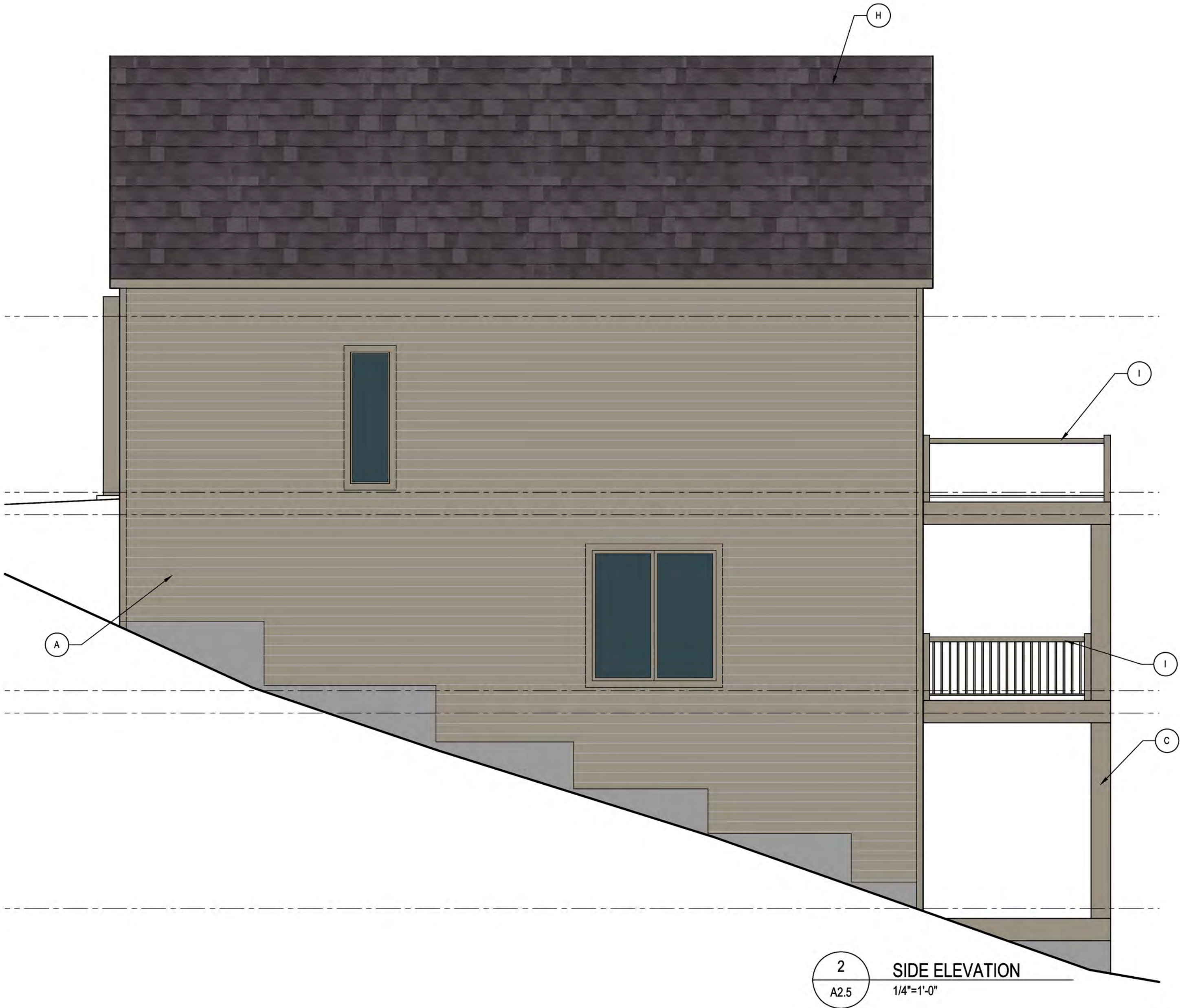
SHEET TITLE
SIDE BY SIDE UNITS ELEVATIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.5

SCALE
1/4"=1'-0"



- GENERAL NOTES:**
- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
 - REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
 - DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAPE ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION	TYPE	DESCRIPTION
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS		GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
STREET SCAPE
ELEVATIONS

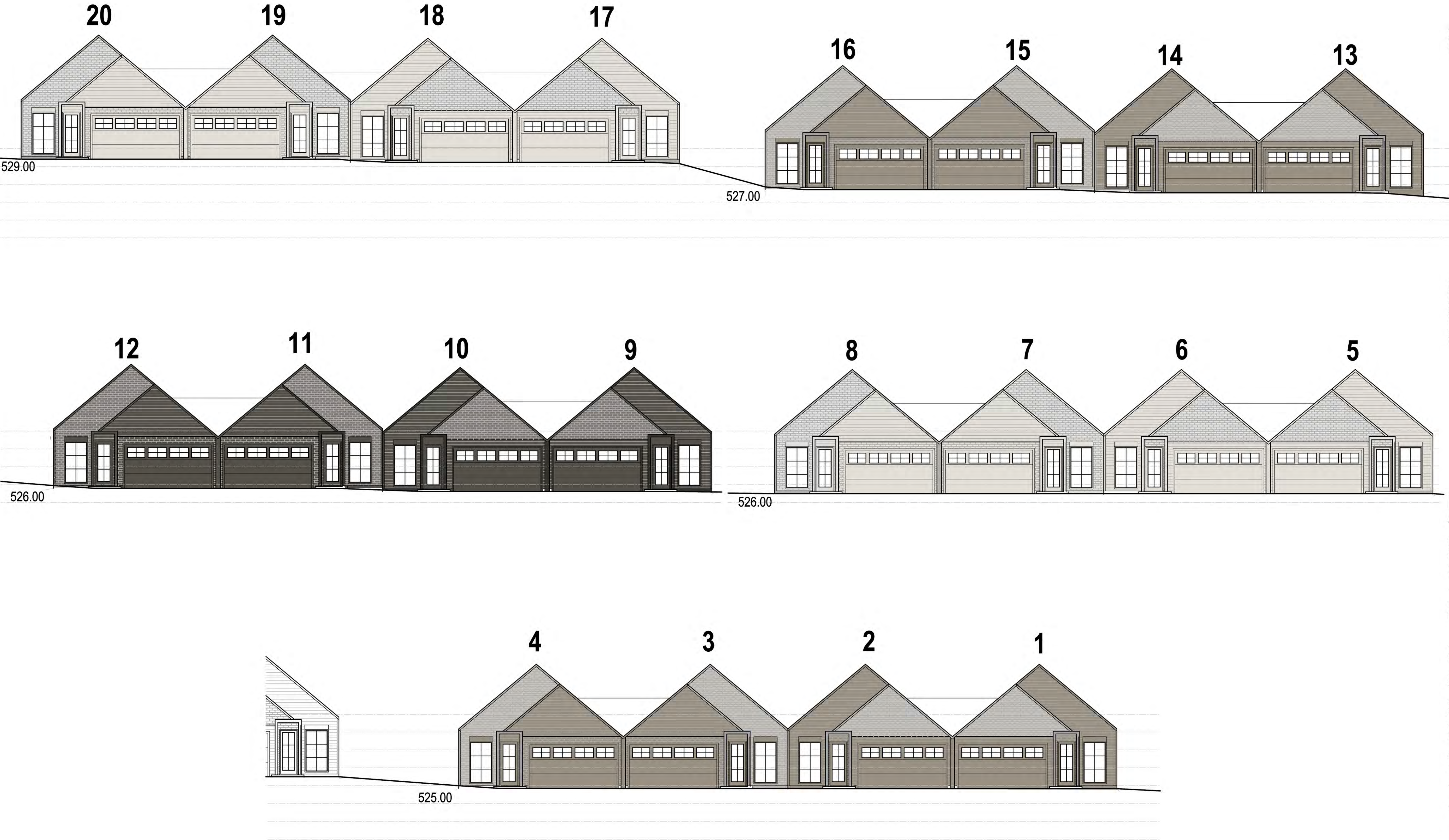
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.6

SCALE
1/8"=1'-0"

1
A2.6
STREET SCAPE
1/8" = 1'-0"



ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
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FINAL RENDERINGS BEING COMPLETED AND WILL BE SUBMITTED AS SOON AS DONE. MINOR CHANGES ONLY TO OCCUR TO IMAGES

PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPINGS OF LIGHT TONE UNITS AND MID TONE UNITS IN DISTANCE



PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPING OF DARK TONE UNITS

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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
RENDERED PERSPECTIVES

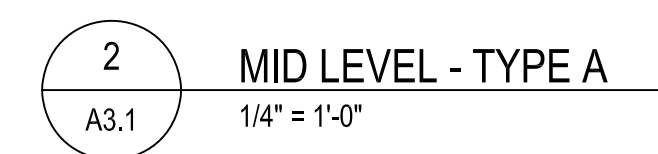
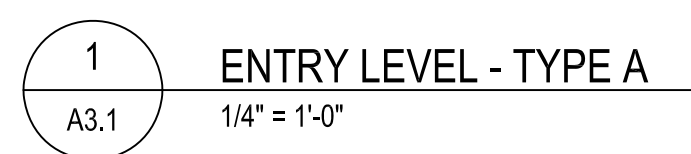
DATE
FEBRUARY 24, 2023

SHEET NUMBER
A2.7

SCALE
N.T.S.

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- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS



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DATE
FEBRUARY 24, 2023

A3.1

SCALE
1/4"=1'-0"

SIDE A	SIDE B
ENTRY LEVEL (GARAGE) 645 sq.ft.	ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 668 sq.ft.	MID LEVEL = 645 sq.ft.
TOP FLOOR = 679 sq.ft.	TOP FLOOR = 678 sq.ft.

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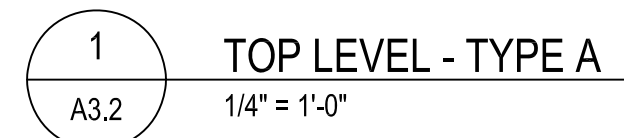
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A3.2

100%

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS



**TANDEM GARAGE UNIT
UNIT TYPE B - ELONGATED PLAN
QUAY LANE FRONTAGE**



SIDE B

ENTRY LEVEL (GARAGE) 649 sq.ft.
MID LEVEL = 663 sq.ft.
TOP FLOOR = 711 sq.ft.

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

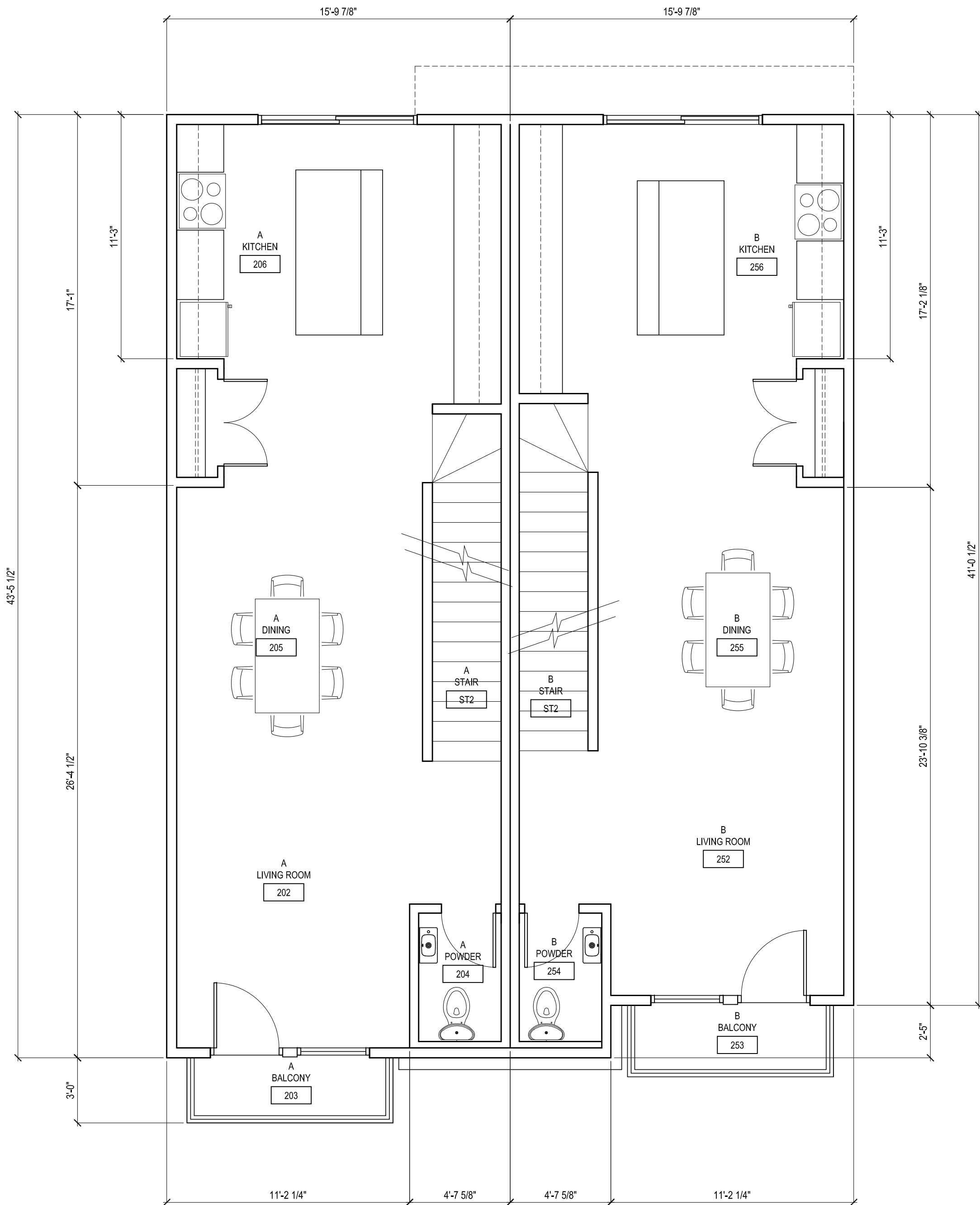
SHEET TITLE
TANDEM UNITS - TYPE B
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

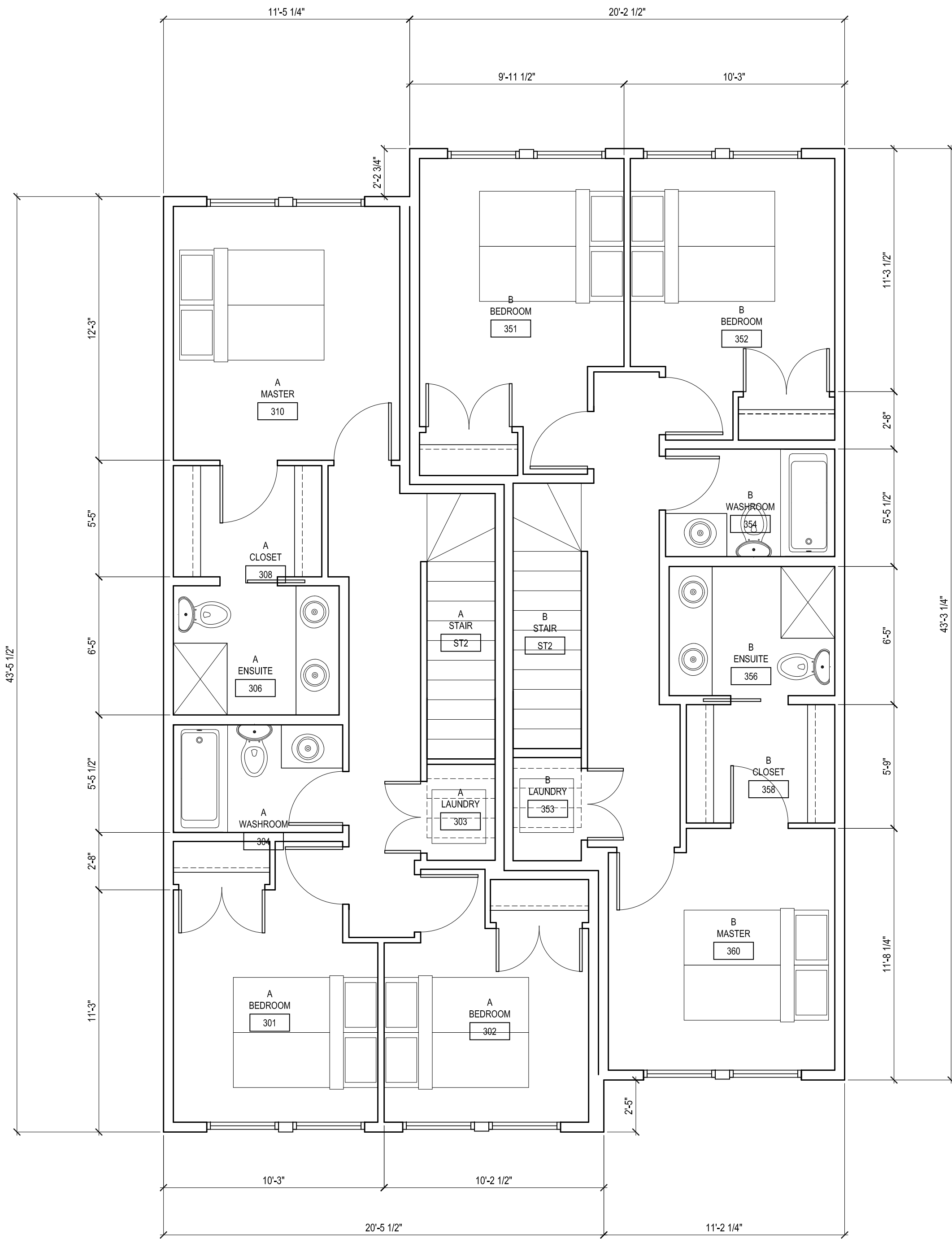
SHEET NUMBER

A3.3

SCALE
1/4"=1'-0"



1
A3.3
MID LEVEL - TYPE B
1/4" = 1'-0"



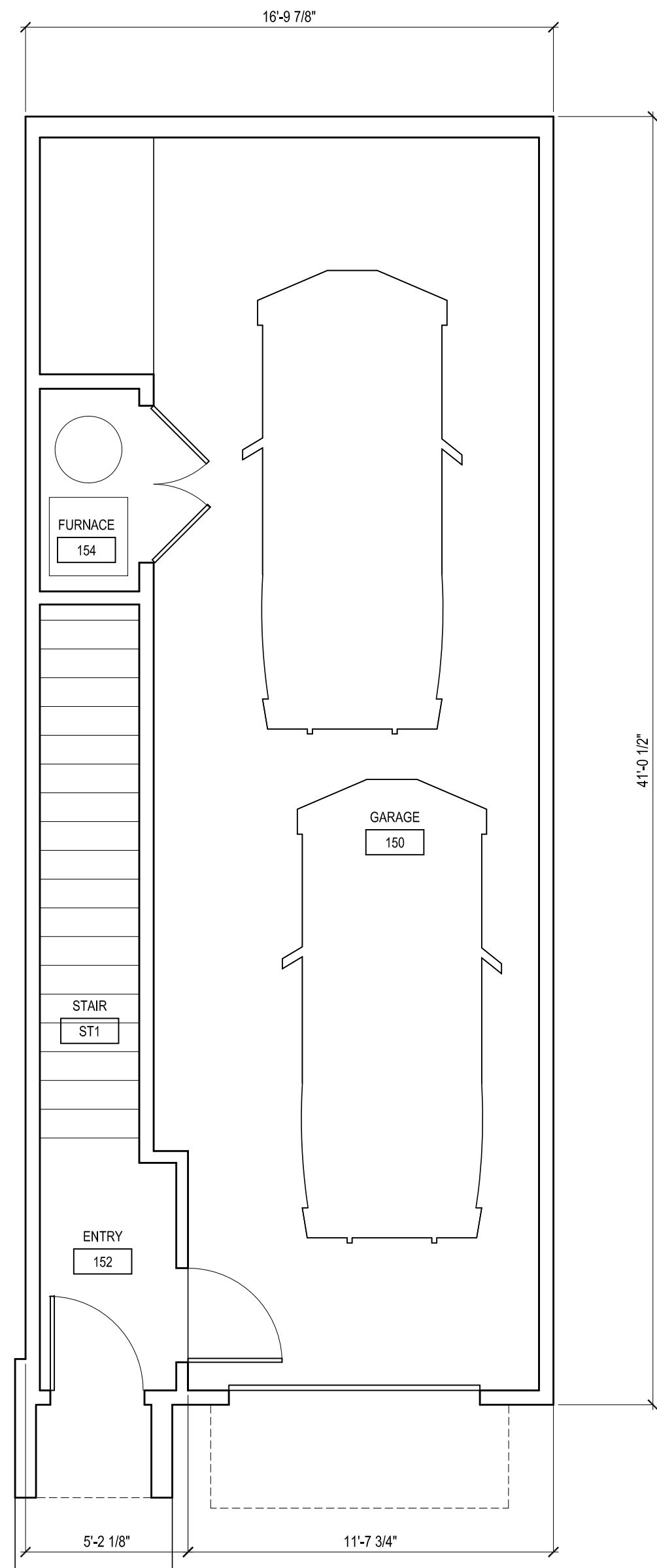
2
A3.3
TOP LEVEL - TYPE B
1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE C - SINGLE UNIT

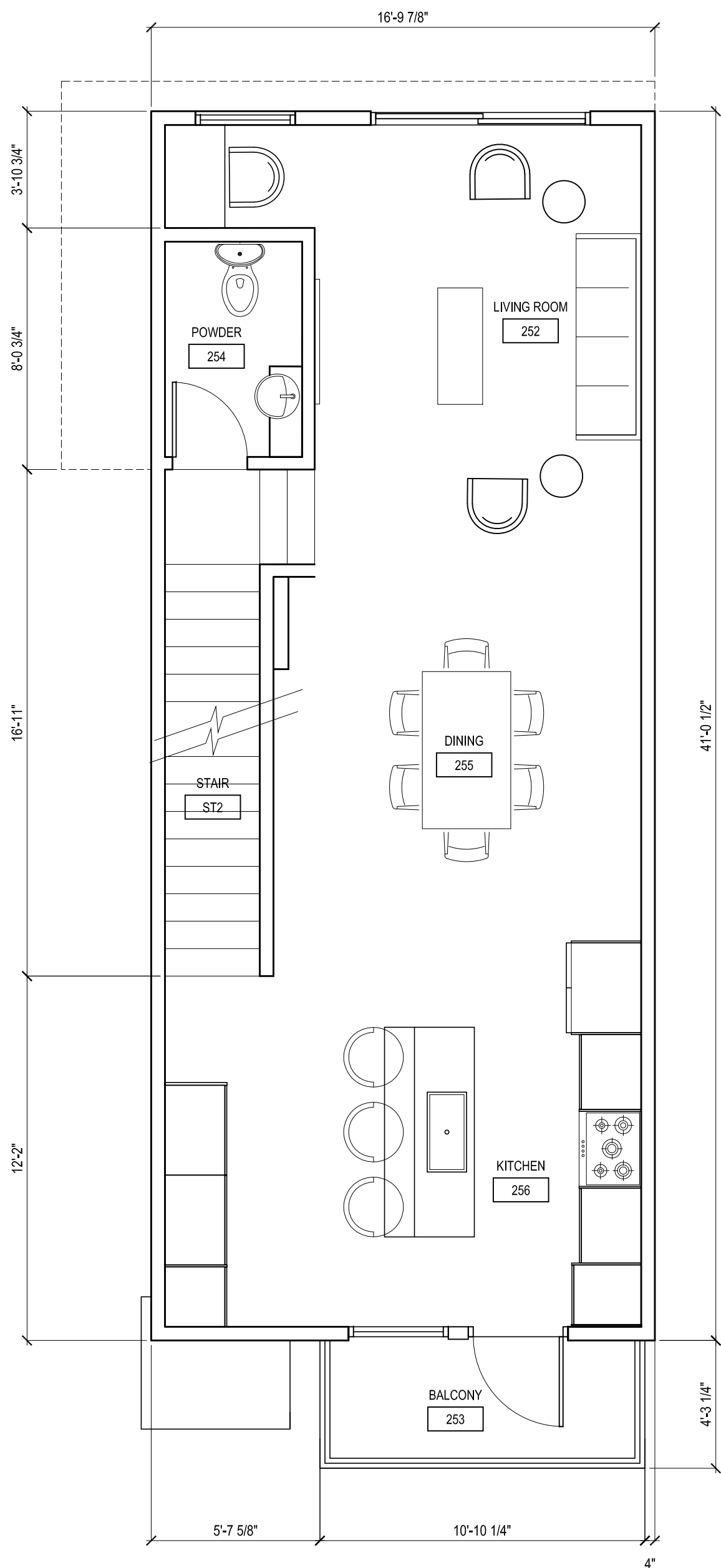
UNIT AREAS TYPE C:

SIDE C
ENTRY LEVEL (GARAGE) 690 sq.ft.
MID LEVEL = 690 sq.ft.
TOP FLOOR = 746 sq.ft.

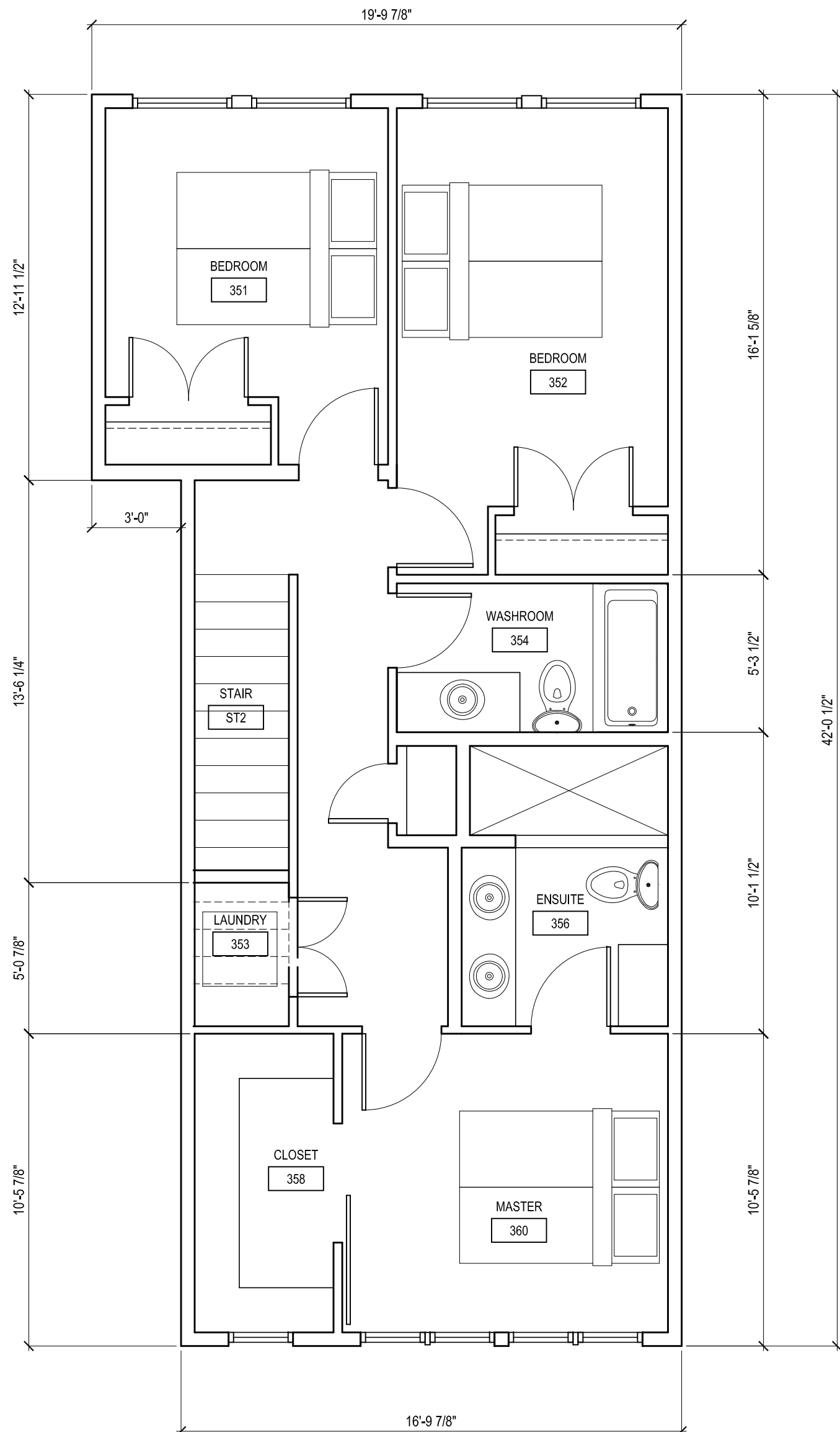
TANDEM UNIT 39



1 ENTRY LEVEL - TYPE C
A3.4 1/4" = 1'-0"



2 MID LEVEL - TYPE C
A3.4 1/4" = 1'-0"



3 TOP LEVEL - TYPE C
A3.4 1/4" = 1'-0"

ISSUE	NO.	DATE
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DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023
DP REVISIONS	04	AUGUST 18, 2023

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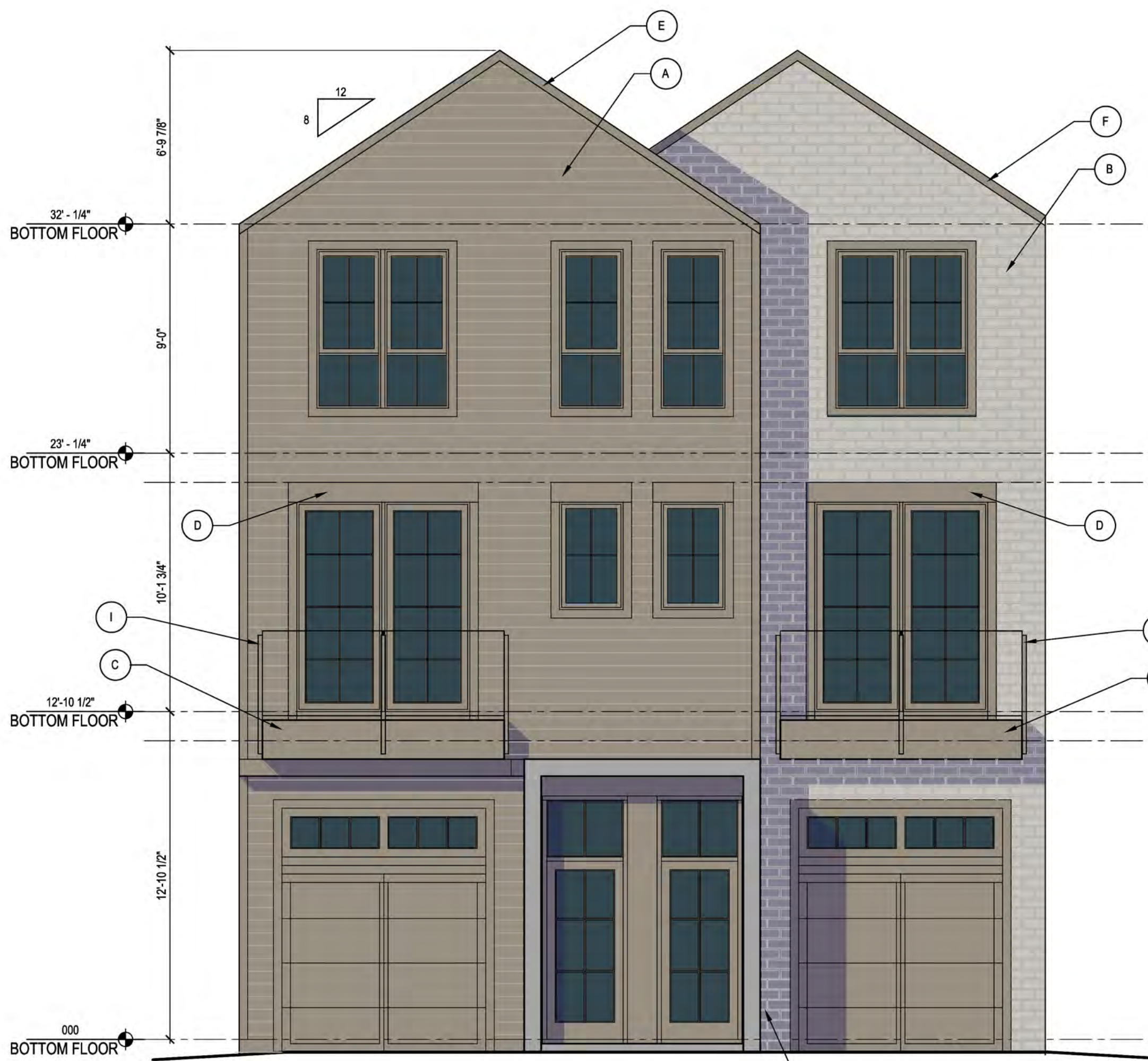
PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS - TYPE C
FLOOR PLANS
QUAY LANE

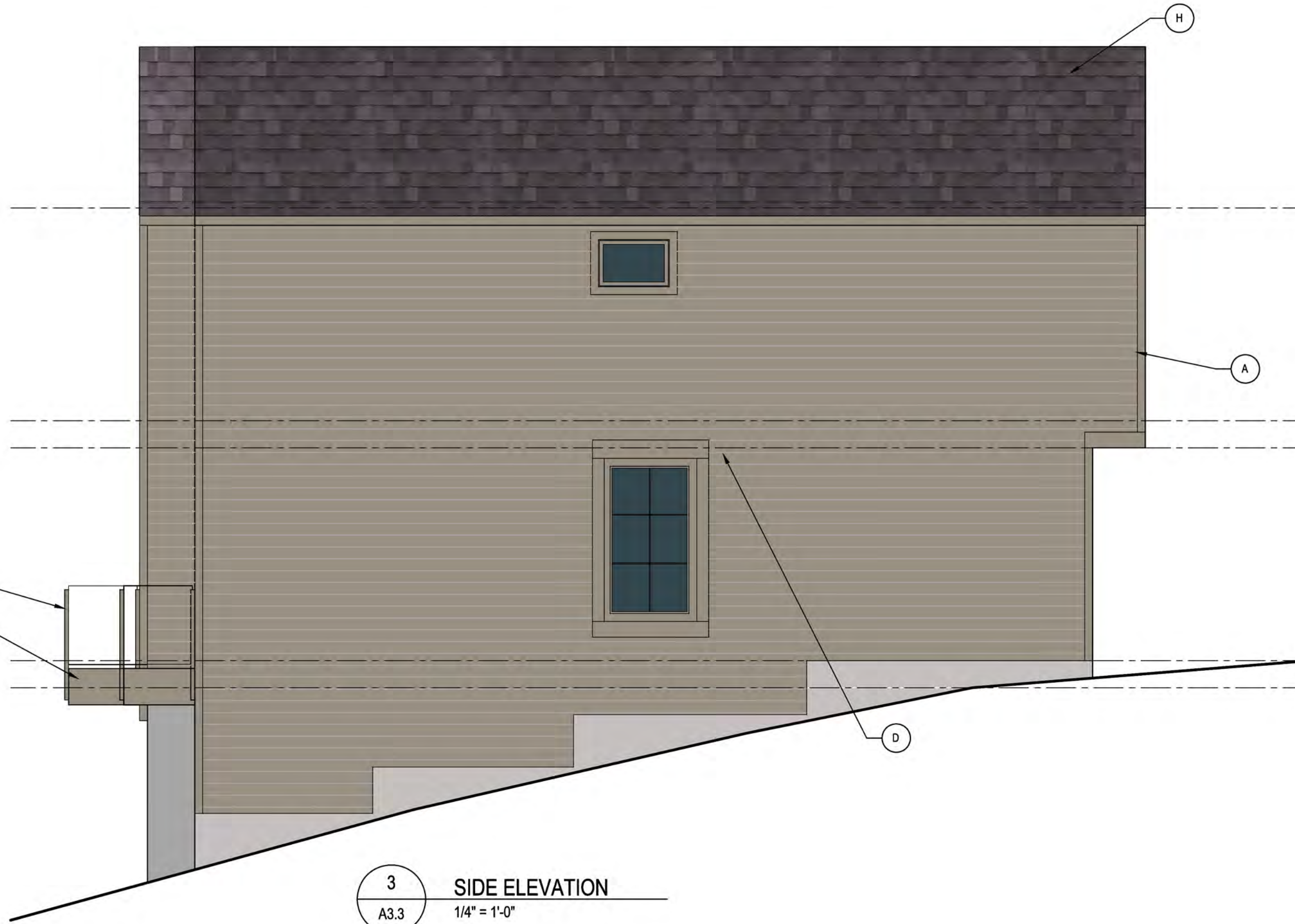
DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.4

SCALE
1/4"=1'-0"



1 STREET ELEVATION
A3.3 1/4" = 1'-0"



3 SIDE ELEVATION
A3.3 1/4" = 1'-0"



2 REAR ELEVATION
A3.3 1/4" = 1'-0"



4 SIDE ELEVATION
A3.3 1/4" = 1'-0"

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS
C	HARDIE PLANK CLAD ELEMENT COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS
F	PAINTED FASCIA COLOUR: 3 VARIATIONS
H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
	GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

GENERAL NOTES:

- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
- REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
- DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAPE ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

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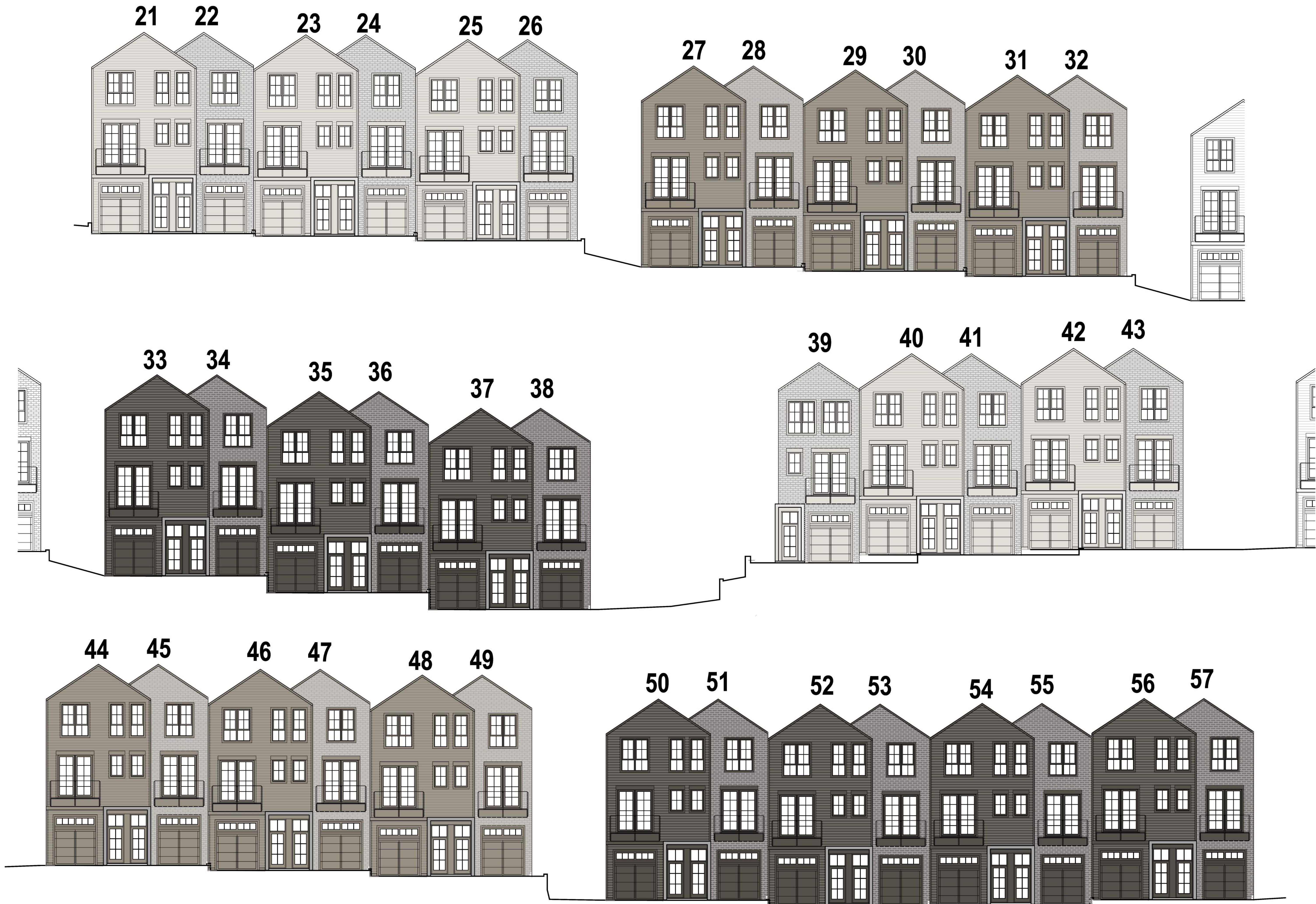
SHEET TITLE
**TANDEM UNITS - TYPE C
TYPICAL ELEVATIONS
QUAY LANE**

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A3.5

SCALE
1/4"=1'-0"



ISSUE	NO.	DATE
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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS - TYPE C
STREETSCAPE ELEVATION
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.6

SCALE
1/8"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PERSPECTIVE STREET VIEW OF
SUNDANCE DRIVE SIDE-BY-SIDE
UNITS SHOWING GROUPINGS OF
LIGHT TONE UNITS AND MID
TONE UNITS IN DISTANCE

FINAL RENDERINGS BEING
COMPLETED AND WILL BE
SUBMITTED AS SOON AS DONE.
MINOR CHANGES ONLY TO
OCCUR TO IMAGES

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PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS
RENDERED VIEW
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.7

SCALE
N.T.S.

GATEHOUSE SUNDANCE DEVELOPMENT

Re-Issued for Development Permit

Contact Information

VDZ+A

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Fort Langley Studio
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Fort Langley, British Columbia, V1M 2R8

Mount Pleasant Studio
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Vancouver, British Columbia, V5T 3J7

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Alternate contacts (incase away):
Stephen Heller
Associate Landscape Architect
stephen@vdz.ca
604 546 0925

Key Project Contacts

GATEHOUSE

Project Owner

PO BOX 2585 Stn Sardis
Chilliwack, BC V2R 1A8

Mike Shepherd
mike@madebygatehouse.com
604 819 2192

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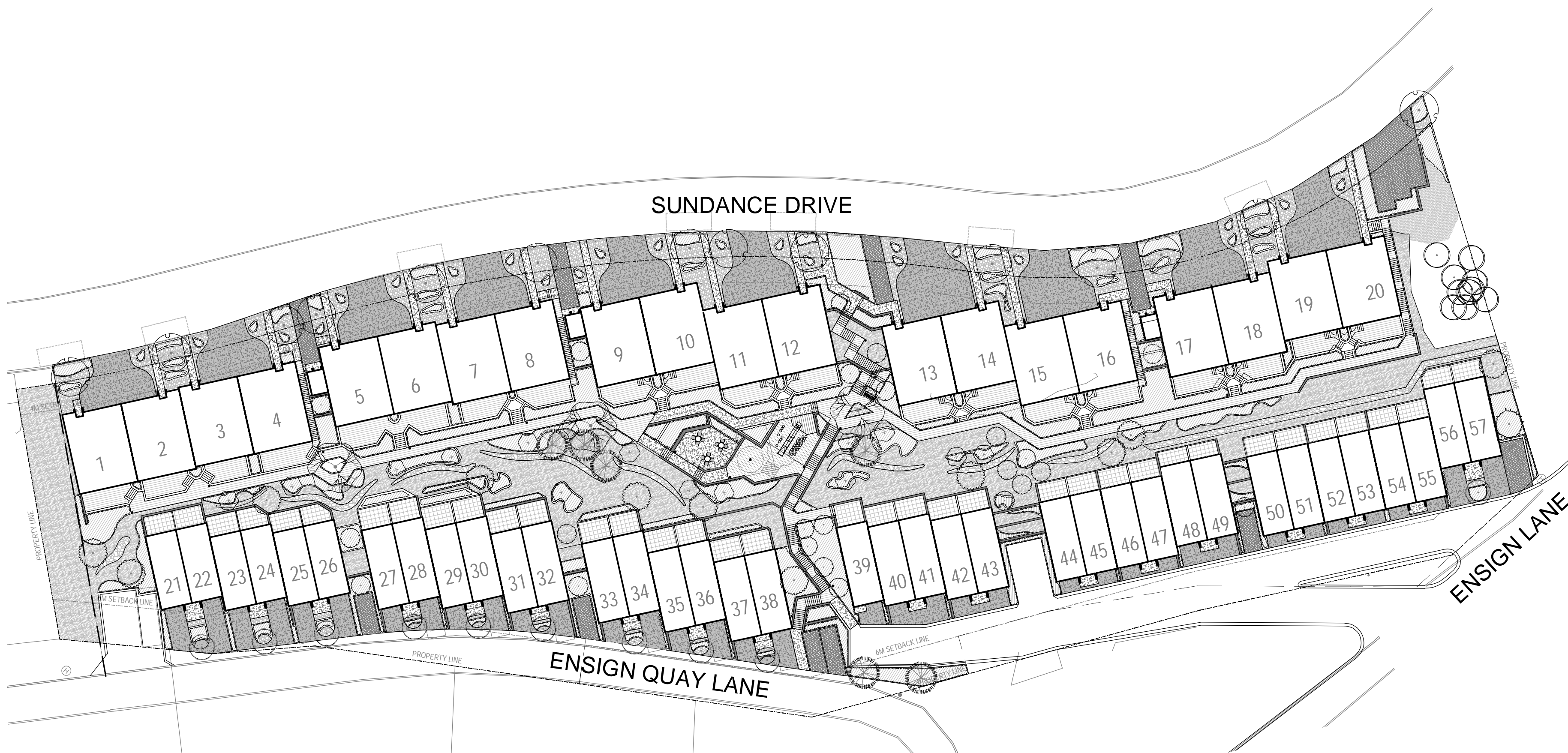
Project Architect

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Kelowna, BC V1Y 1Z9

Carlo Distefano
carlo@distefanojud.com
250 868 9278

Sheet List Table

Sheet Number	Sheet Title
L-00	COVER PAGE
L-01A	LANDSCAPE SITE PLAN WEST
L-01B	LANDSCAPE SITE PLAN CENTER
L-01C	LANDSCAPE SITE PLAN EAST
L-02	PLANT PALETTE
L-03A	FENCE AND GRADING PLAN WEST
L-03B	FENCE AND GRADING PLAN CENTER
L-03C	FENCE AND GRADING PLAN EAST
LS-01	SECTION A & B
LS-02	SECTION C & D
LS-03	SECTION E & F
LD-01	DETAILS
LD-02	DETAILS
LD-03	DETAILS
LD-04	DETAILS



1 SITE PLAN OVERVIEW
Scale 1:500



2 LOCATION MAP
NTS

9	KM	Re-Issued for Development Permit	2023-08-21
8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18

No.	By:	Description	Date
REVISIONS TABLE FOR DRAWINGS			
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1	KM	Re-DP Update	2023-08-22
No.	By:	Description	Date

No.	By:	Description	Date
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No.	By:	Description	Date
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Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn:
AH
ET

Checked:
SH

Approved:
MVDZ

Scale:
AS SHOWN

Stamp:

Original Sheet Size:
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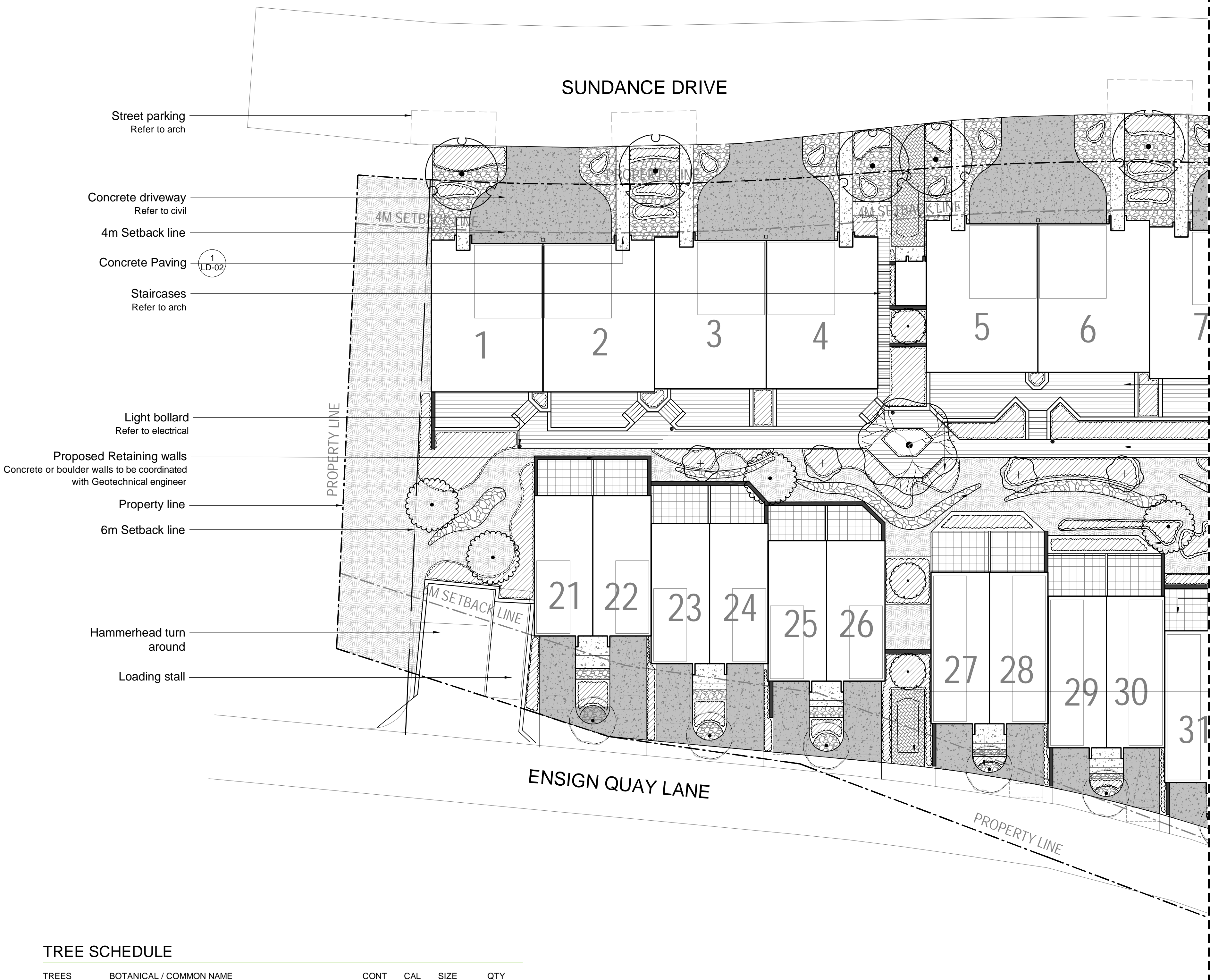
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VDZ Project #:
DP2022-16

Drawing #:
L-00

Z:\PROJECTS\DEVELOPMENT PERMIT\DP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWGS\SHEETS\L-01 SITE PLAN.DWG



REFER TO L-01B

MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 2" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

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FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO

100-9181 Church St | 102-355 Kingsway

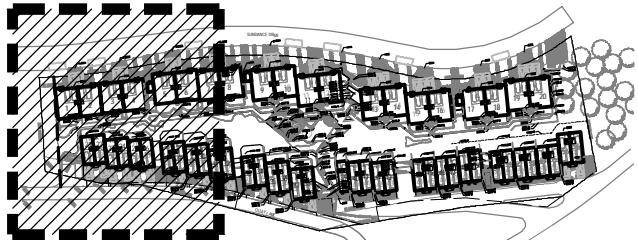
Fort Langley, BC | Vancouver, BC

VIM 2881 VST 317

www.vdz.ca

604-882-0024

Key Map (NTS)



No.	By:	Description	Date
9	KM	Re-Issued for Development Permit	2023-08-21
8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
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1	KM	Re-DP Update	2023-08-22
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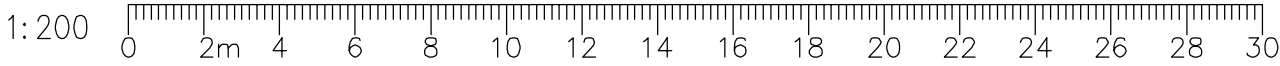
Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn: KM ET	Stamp:
Checked: SH	
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TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	10
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		23
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	11
	Existing Tree	-			10
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3



Drawing Title: LANDSCAPE SITE PLAN WEST



DP2022-16

L-01A

MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 2" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

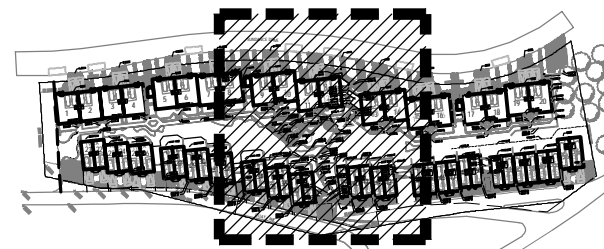
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		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	10
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		23
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	11
	Existing Tree	-			10
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

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Key Map (NTS)



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2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18

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No.	By:	Description	Date
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REVISIONS TABLE FOR SHEET

Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

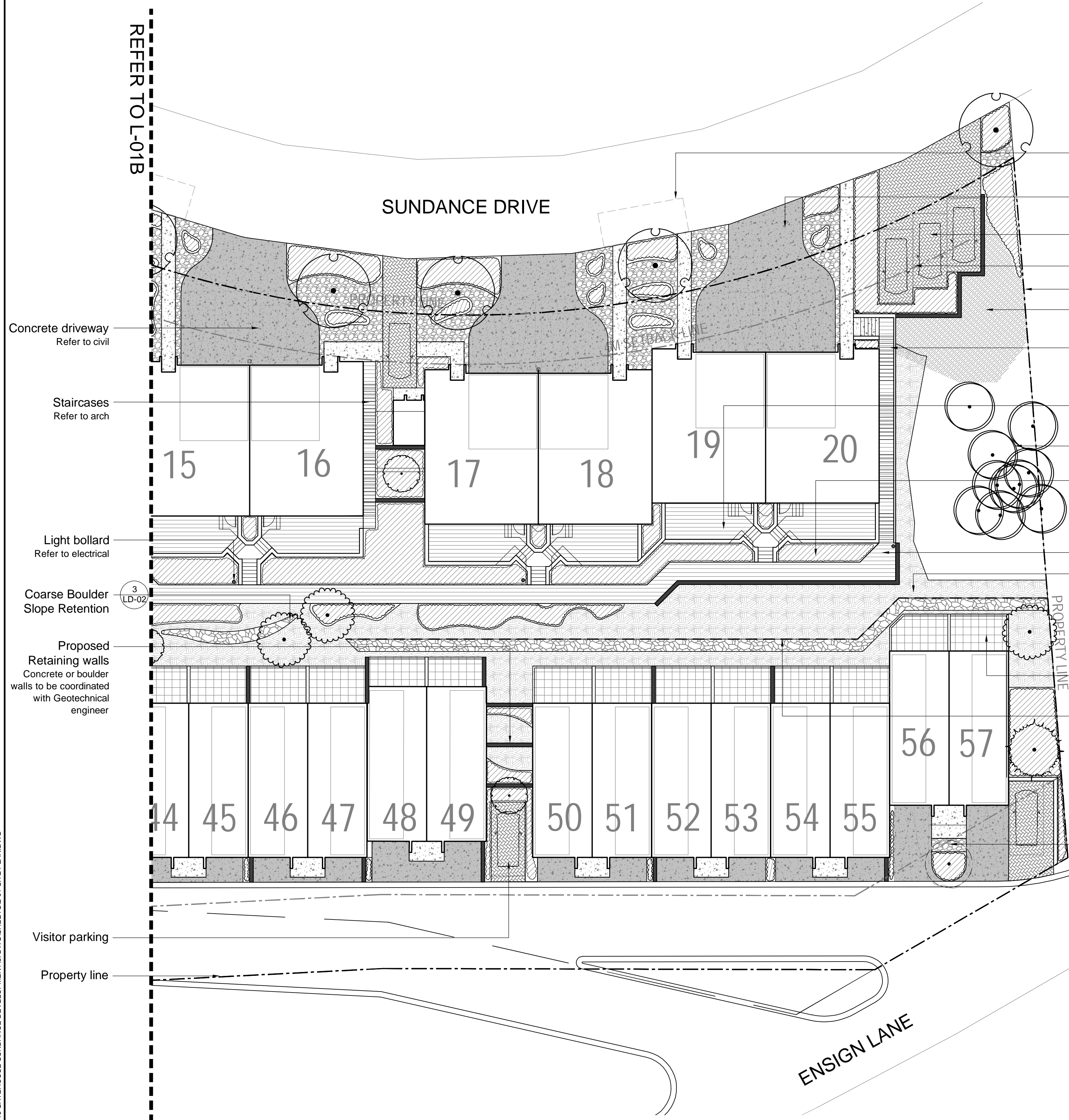
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- Street parking
Refer to arch
- Concrete driveway
Refer to civil
- Visitor parking
- 4m Setback line
- Property line
- Structural fill slope; per
geotechnical recommendation
- Boardwalk stairs with
handrails
Refer to grading plan

- Private Deck
Composite decking typ.
- Existing Trees
- Xeriscape Shrub Beds

- Boardwalk w/ guardrail
- Dry Meadow Slope
Retention Hydroseed
- Lower patio
Hydra Pressed Slabs
- Bedrock wall; typ

Decorative Rock

MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 3" Basaltic River Stone
		DRY MEADOW/ SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
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		PROPOSED STRUCTURAL RETAINING WALL; REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	10
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		23
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	11
	Existing Tree	-			10
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

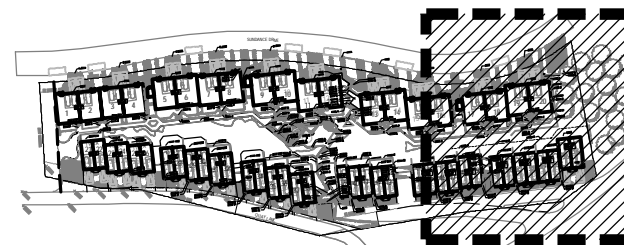
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Key Map (NTS)



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2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18

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1	KM	Re-DP Update	2023-08-22

REVISIONS TABLE FOR SHEET

Project:
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3401 Sundance Drive
West Kelowna, B.C.

Drawn: KM ET	Stamp:
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Drawing Title:
LANDSCAPE SITE PLAN EAST



Drawing Project #:
DP2022-16













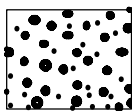

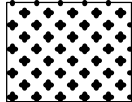
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L-01C

Z:\PROJECTS\DEVELOPMENT PERMIT\ACTIVEDP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWGS\SHEETS\L-02 PLANTING PLAN.DWG

TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	10
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		23
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	11
	Existing Tree	-			10
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

PLANT SCHEDULE

SHRUBS	BOTANICAL / COMMON NAME	CONT	
	Artemisia tridentata / Big Sagebrush	#2	
	Caryopteris x clandonensis / Bluebeard	#2	
	Ericameria nauseosa / Rubber Rabbitbrush	#2	
DWARF CONIFERS	BOTANICAL / COMMON NAME	CONT	
	Juniperus chinensis 'Torulosa' / Hollywood Juniper	#3	
	Juniperus sabina / Savin Juniper	#2	
	Pinus mugo 'Pumilio' / Mugo Pine	#2	
GRASSES	BOTANICAL / COMMON NAME	CONT	
	Chasmanthium latifolium / Wood Oats	#1	
	Elymus mollis / American Dunegrass	#1	
PERENNIALS	BOTANICAL / COMMON NAME	CONT	
	Eryngium alpinum 'Blue Star' / Sea Holly	#1	
	Perovskia x 'Little Spire' / Russian Sage	#1	
	Sedum x 'Thunderhead' / Thunderhead Sedum	#1	
	Yucca glauca / Narrowleaf Yucca	#2	
GROUND COVERS	BOTANICAL / COMMON NAME	CONT	SPACING
	Arctostaphylos uva-ursi / Kinnikinnick	#1	300mm
	Juniperus horizontalis 'Andorra' / Andorra Juniper	#1	600mm
	Thymus pseudolanuginosus / Woolly Thyme	#1	300mm



Perovskia Atriplicifolia



Eryngium Alpinum 'Blue star'



Artemisia Tridentata



Ericameria nauseosa



Juniperous Horizontalis 'Andorra'



Thymus Pseudolanuginosus



Coreopsis 'Moonbeam'



Juniperous Sabina



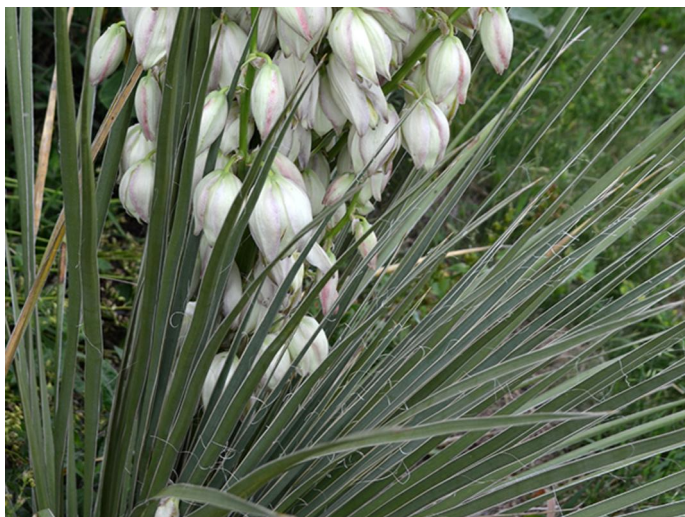
Arctostaphylos Uva-Ursi



Chasmanthium latifolium



Caryopteris x Clandonensis



Yucca Glauca



Elymus mollis



Sedum x 'Thunderhead'



Sedum x 'Thunderhead'



Dry meadow seed mix

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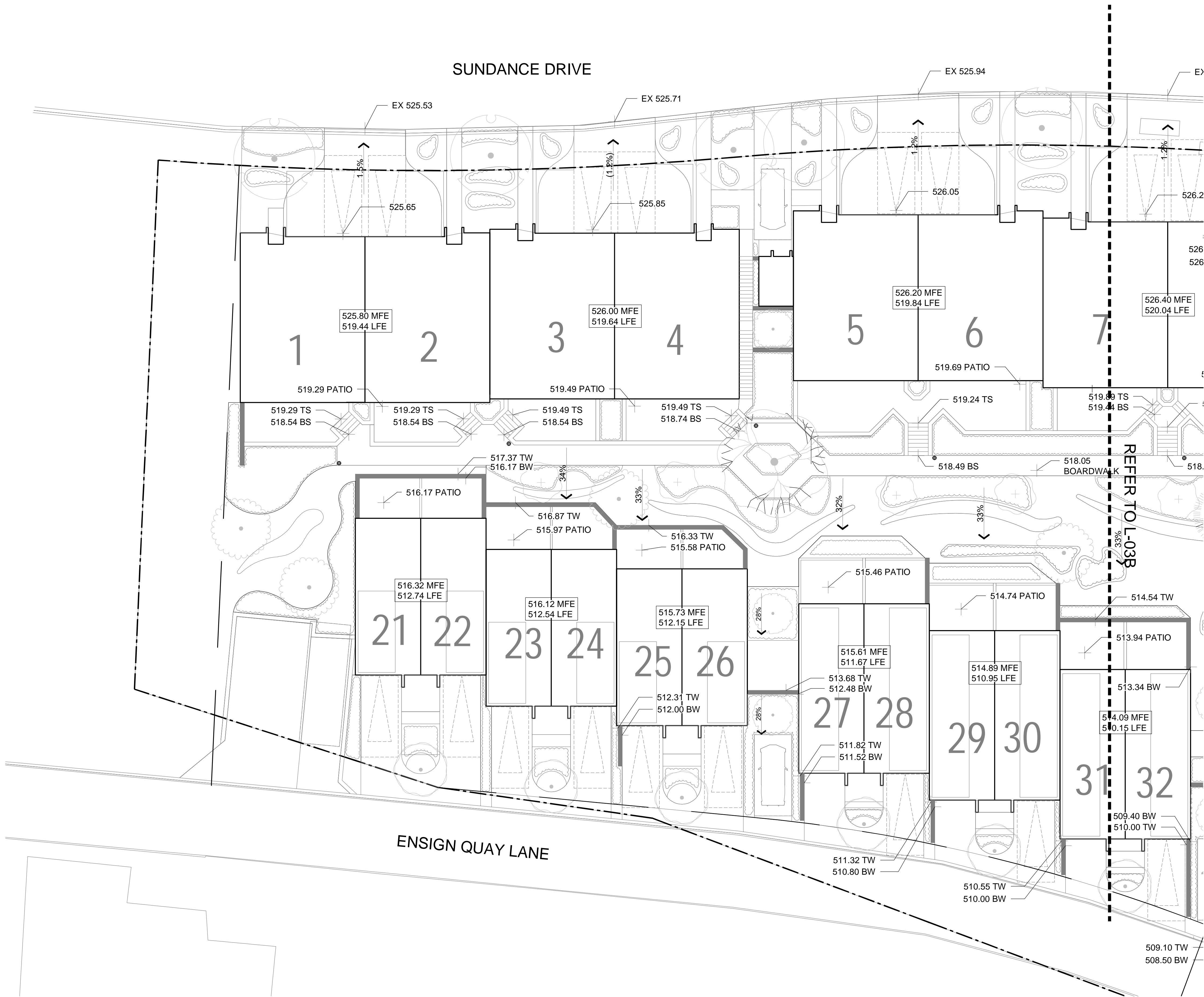
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PLANT PALETTE



VDZ Project #:
DP2022-16

Drawing #:
L-02

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Key Map (NTS)



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REVISIONS TABLE FOR SHEET

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3401 Sundance Drive
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Drawing Title:

FENCE AND GRADING PLAN WEST

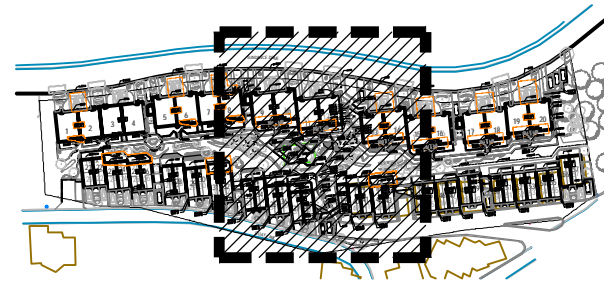
VDZ Project #:

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Drawing #:

L-03A

Key Map (NTS)



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Drawing Title:

FENCE AND GRADING PLAN CENTER

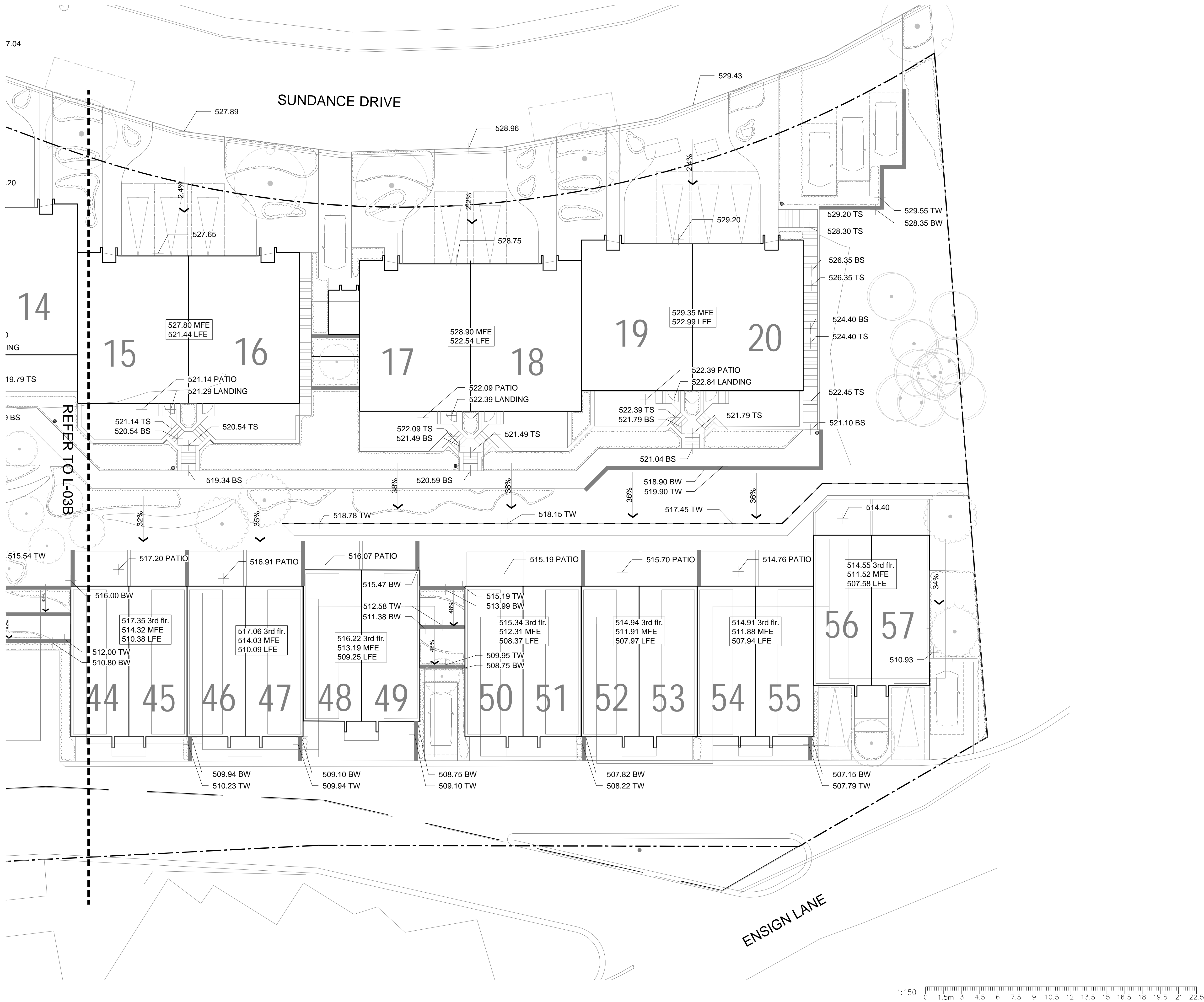
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DP2022-16

Drawing #:

L-03B

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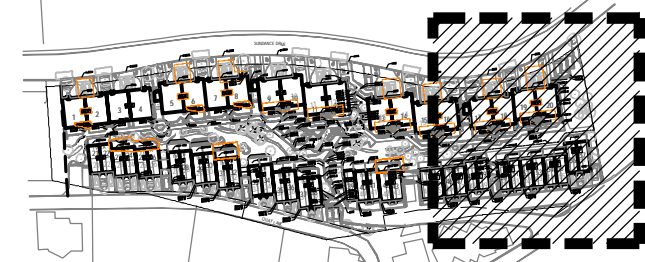


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Key Map (NTS)



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Scale: 1:150	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS TO THE DRAWINGS MUST BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

Drawing Title:
FENCE AND GRADING PLAN EAST



VDZ Project #:
DP2022-16

Drawing #:
L-03C

LEGEND

---	EXISTING GRADE
---	EXISTING BEDROCK

9	KM	Re-Issued for Development Permit	2023-08-21
8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
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5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
No.	By:	Description	Date

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No.	By:	Description	Date
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REVISIONS TABLE FOR SHEET

Project:
PROVIDENCE / SUNDANCE

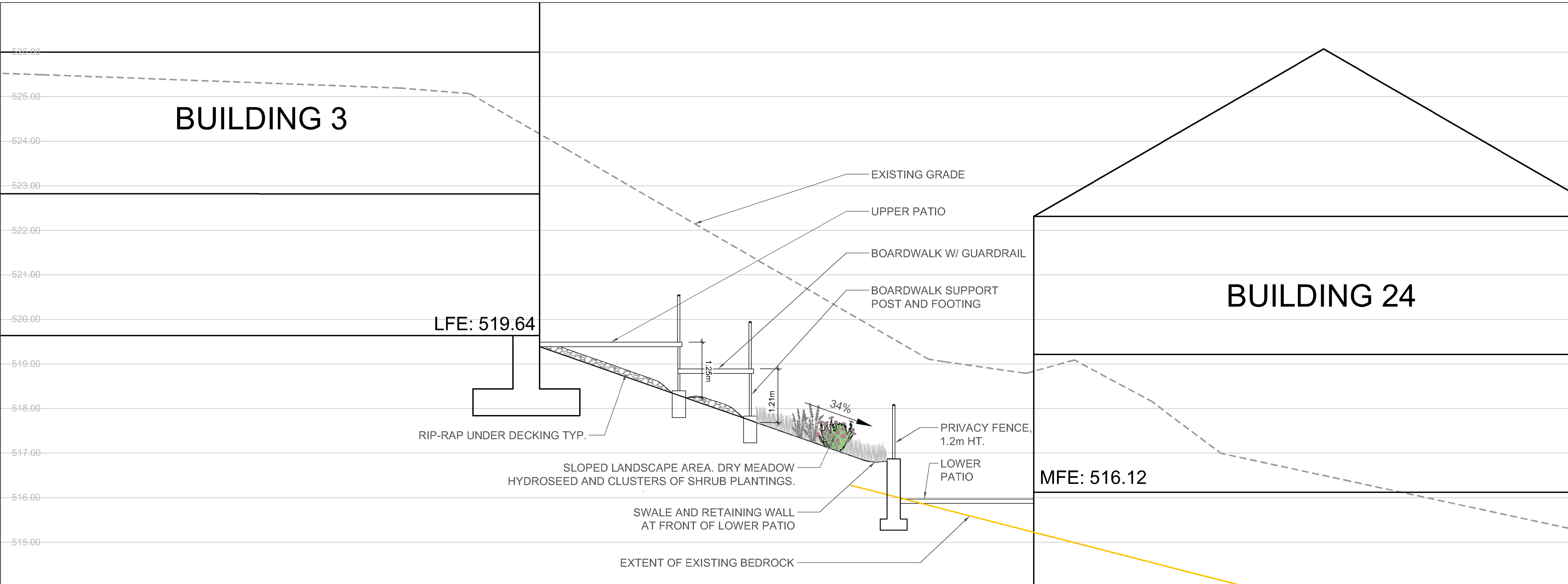
Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn: KM	Stamp:
Checked: SH	
Approved: MVDZ	Original Sheet Size: 24"x36"
Scale: AS SHOWN	CONTRACTOR SHALL CHECK ALL DIMENSIONS OF THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS TO DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

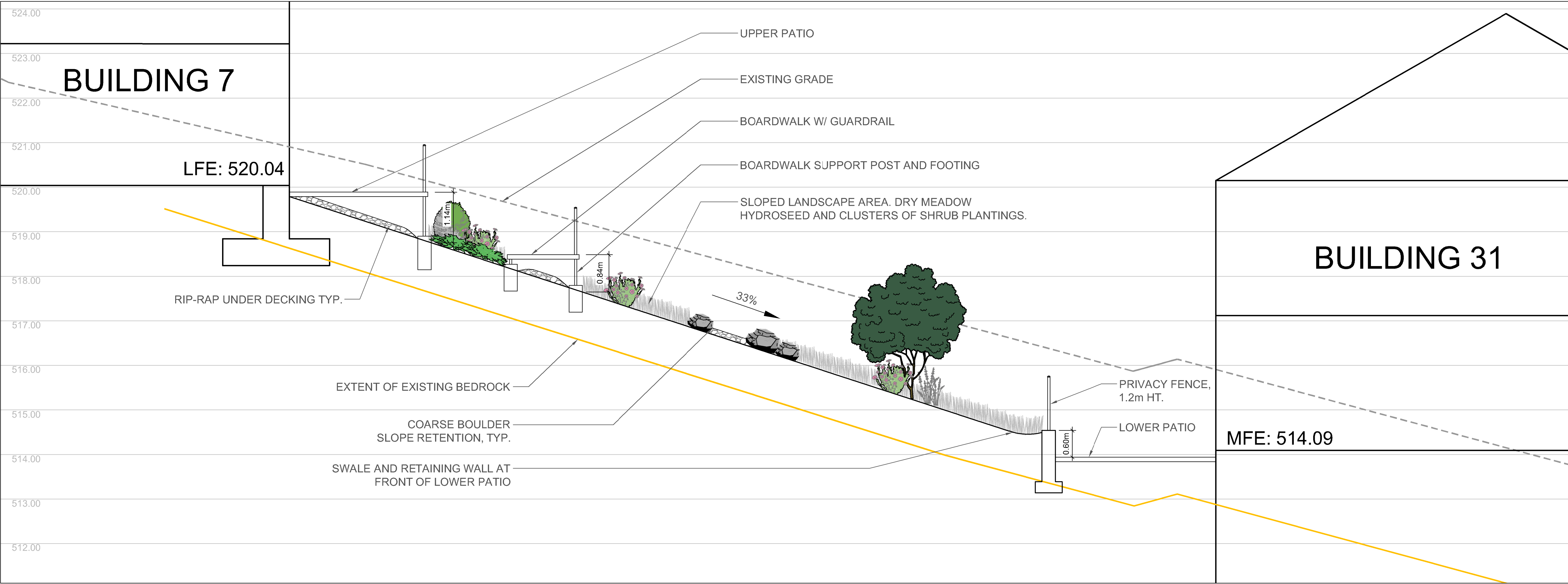
Drawing Title:
LANDSCAPE SECTIONS

VDZ Project #:
DP2022-16

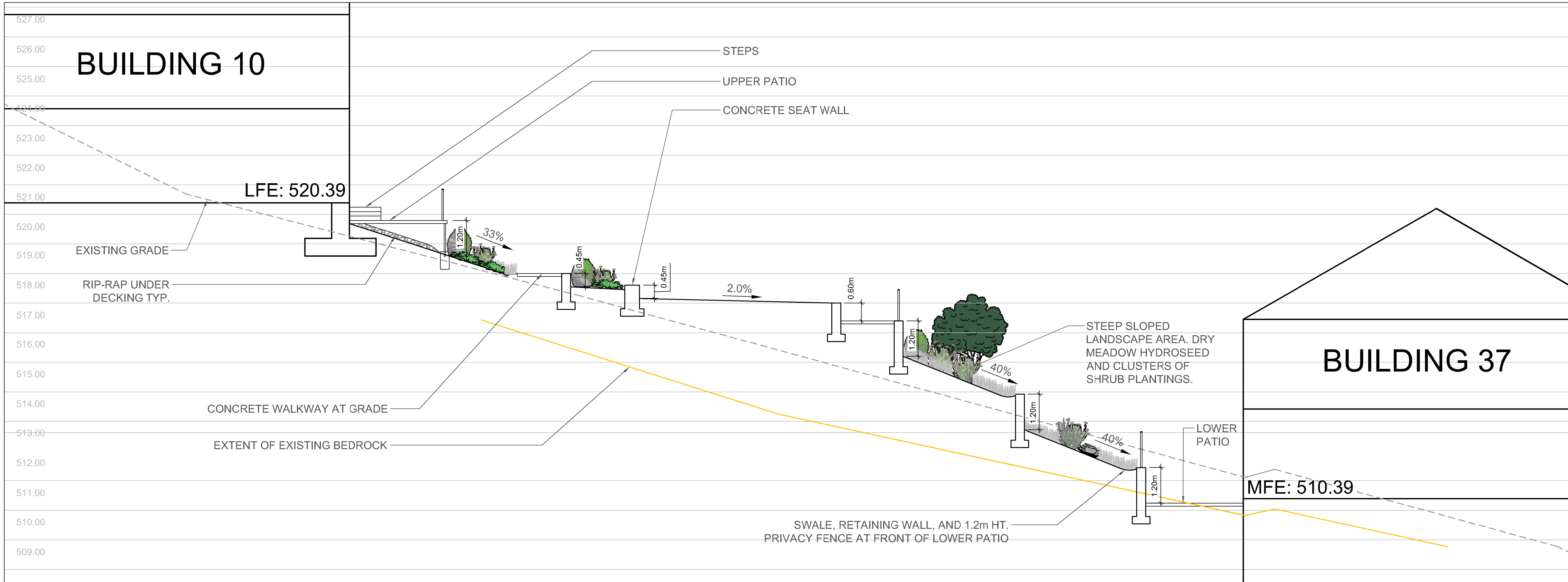
Drawing #:
LS-01



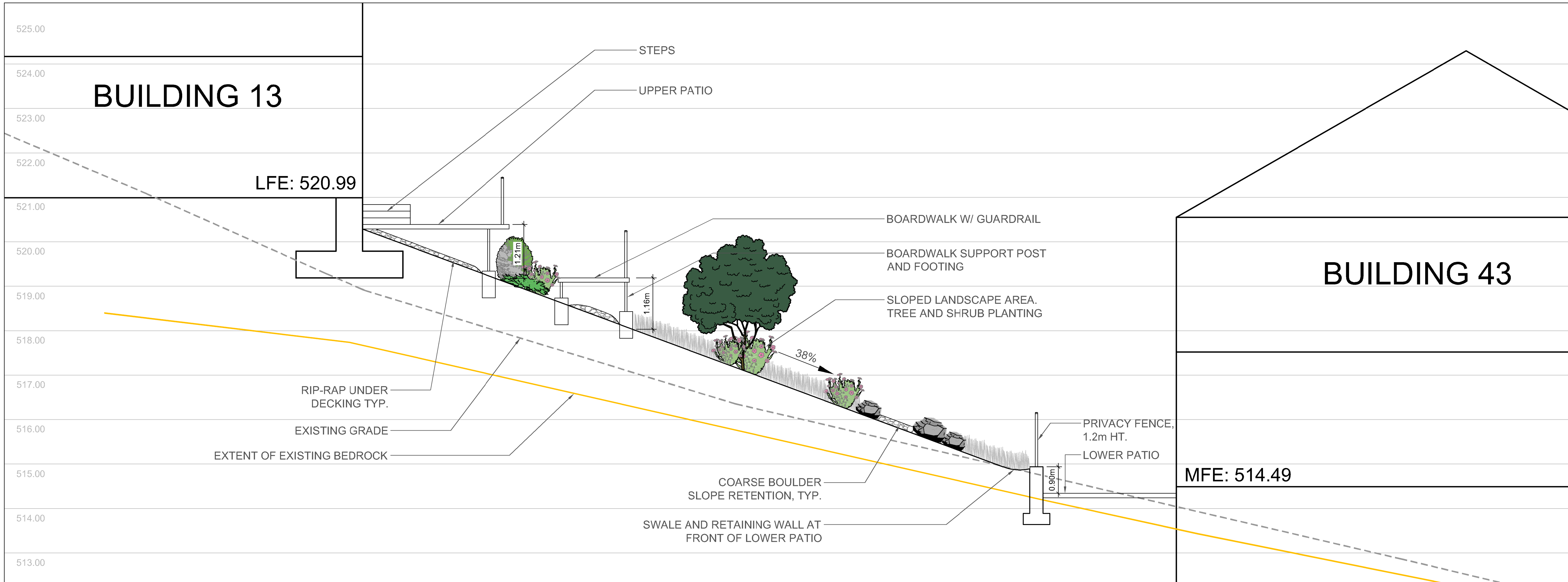
A SECTION THROUGH UNIT 3 AND 24
Scale 1:50



B SECTION THROUGH UNIT 7 AND 31
Scale 1:50



C SECTION THROUGH UNIT 10 AND 37
Scale 1:75



D SECTION THROUGH UNIT 13 AND 43
Scale 1:50

LEGEND	
	EXISTING GRADE
	EXISTING BEDROCK

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LEGEND

	EXISTING GRADE
	EXISTING BEDROCK

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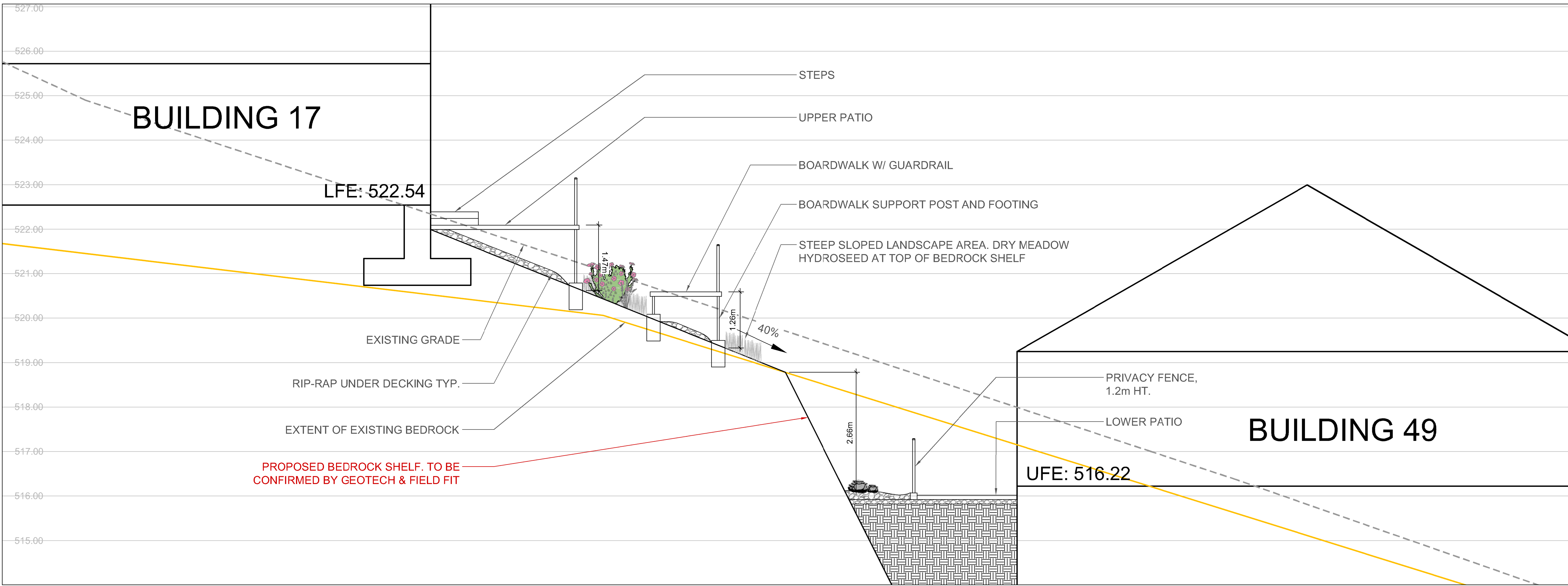
No.	By:	Description	Date

REVISIONS TABLE FOR SHEET

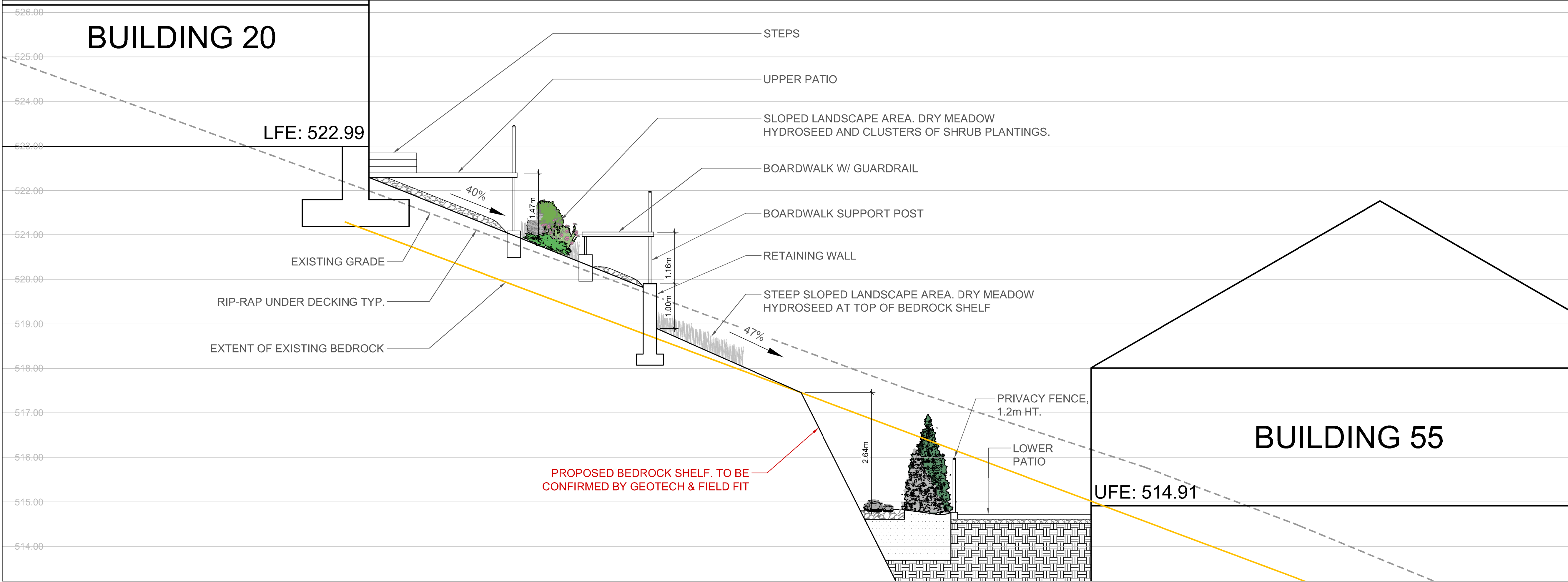
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PROVIDENCE / SUNDANCE

Location:
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Drawn: KM	Stamp:
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E SECTION THROUGH UNIT 17 AND 49
Scale 1:50



F SECTION THROUGH UNIT 20 AND 55
Scale 1:50

Drawing Title:
LANDSCAPE SECTIONS

VDZ Project #:
DP2022-16

Drawing #:
LS-03

Z:\PROJECTS\DEVELOPMENT PERMIT\ACTIVE\DP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWG\SHEET\LD-01 DETAILS.DWG

Do not trim leader

50mm wide woven polyweb banding attached to stake with shingle nails.
Strapping sources: Fabritech Manufacturing Ltd.
#201 - 20540 Duncan Way, Langley, ph: 604-534-5300, fx:604-534-6306
George n. Jackson Ltd.
600 derwent way (annacis island), Delta, B.C.
V3M 5P8
(t) 1-800-663-1369
(f) 1-800-663-3736
www.Jackson.ca

2 pressure treated Ø50-Ø75mm round wood stakes 2m lengths. Stakes should be aligned parallel to sidewalk/road

'Arborgard+' tree trunk protector - AG-9-4 or 'nds' - tree trunk protector - tp 128 ; nds

50mm deep saucer formed in topsoil for initial first year watering. Fill saucer with bark mulch

300mm minimum of topsoil around root ball compacted to 85% MPD

Pedestal

Stakes to not penetrate root ball. Stakes to penetrate native soil by 300mm

300 MIN ROOT BALL 300 MIN

Notes:

1. Sacking/burlap to be loosened and dropped to the bottom of the planting hole. all string, twine, etc. to be removed.
2. All wire baskets shall have the top 1/3 of the wire removed prior to planting.
3. All trees shall be single stem
4. 450mm depth root guard to be installed when tree planted within 2m of hardscape

1 DECIDUOUS TREE PLANTING

Scale 1:25

Do not damage or cut leader

50mm wide woven nylon binding attached to stake with shingle nails

2 x Pressure treated round wood stakes 50 mm - 75mm in diameter 2m in length

Crown of root ball shall be even with finished grade

75 mm decomposed bark mulch

Create saucer around tree

Cut & remove top 1/3 of burlap from root ball

B&B root ball

Compacted top soil with pedestal to Canadian Landscape Standards

Scarify pit bottom minimum of (150mm)

Minimum of (150mm) on all sides of root ball

Notes:

1. Do not damage main roots or destroy root ball when installing tree stake.
2. Water thoroughly after installation
3. Remove tree rings and stakes two years after installations
4. Provide drainage for planting pit in impermeable soil
5. 450mm depth root guard to be installed when tree planted within 2m of hardscape

2 CONIFEROUS TREE PLANTING

Scale 1:25

Prune plant to remove dead or broken branches

Decomposed Bark Mulch 75mm min

450mm deep Gently Compacted Topsoil Mixture to Canadian Landscape Standard: "Well Groomed" soil

Compacted sub base

Notes:

1. Remove all string, twine, pots, tags from plant.
2. All soil, mulch and plant material to meet latest Canadian Landscape Standard.
3. Topsoil saucers diameter to be 1.5X diameter of plant container.
4. O.C. spacing per planting plan.

3 SHRUB PLANTING

Scale 1:25

Maintain saucer on lower sides of plant to retain water

Top of root ball level with finished grade

Prune plant to remove dead or broken branches

Decomposed Bark Mulch 75mm min

Original grade of slope

450mm deep Gently Compacted Topsoil Mixture to Canadian Landscape Standard: "Well Groomed" soil

Compacted sub base

Notes:

1. Remove all string, twine, pots, tags from plant.
2. All soil, mulch and plant material to meet latest Canadian Landscape Standard.
3. Topsoil saucers diameter to be 1.5X diameter of plant container.
4. O.C. spacing per planting plan.

4 SHRUB PLANTING ON SLOPE

Scale 1:25

Plant material installation @ 50mm higher than surroundings to facilitate mulch installation

Min. 2% Slope to Edge

Plant spacing as per plan

Granular herbicide ronstar 9 or approved pre-emergent herbicide. Apply according to manufacturers directions above and below mulch

50mm decomposed bark mulch

Prepared planting soil 150mm min.

Existing subsoil

5 GROUND COVER PLANTING

Scale 1:10

Building wall. Refer to Architects detail

Protection board & membrane. Refer to Architects detail

200 x 50 pressure treated timbers, fixed in place with 400 x 50 pressure treated stakes every 1000mm

Topsoil and mulch (As per specification)

Prepared Subsoil (Free draining).

25mm round river rock

20mm (3/4") clear crushed gravel, compact to 95% MPD.

Landscape fabric

6 DRIP STRIP

Scale 1:20

200mm x 300mm concrete curb. Subbase to be 15mm thick of 19mm minus road base compacted to 95% MPD. Subgrade to be compacted to 95% MPD.

10M rebar top and bottom

10mm Landsafe wearing course

90mm Landsafe base mat

100mm base course permeable aggregate compacted to 95% MPD

Geotextile to be placed around pea gravel and underside of base course aggregates

Existing subgrade compacted to 95%MPD

Note:
Rubber colour to be:
Softline solutions
Rainbow Blue 590, Patina Green 400 Pattern per plan.

7 RUBBER SAFETY SURFACE

Scale 1:10

Tree planting details. Refer to Detail 1, and 2, Sheet LD-01

Boulder retaining wall. Refer to Detail 3, Sheet LD-02

1000 MIN

20% SLOPE MAX

600 MAX

Compacted sub base

Notes:

1. Refer to plans for extents of shrub planting build-up
2. To be used in interior west-east corridor of site where soil build-up and slope leveling is required

8 PLANTING BEDS ON SLOPE

Scale 1:25

VDZ+A

LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO

100-9181 Church St | 102-355 Kingsway
Fort Langley, BC | Vancouver, BC
V1M 2R8 | VST 317
www.vdz.ca | 604-882-0024

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Project: PROVIDENCE / SUNDANCE			
Location: 3401 Sundance Drive West Kelowna, B.C.			
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Checked:			
SH			
Approved:	Original Sheet Size:		
APPROVED	24"x36"		
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MVDZ			

Drawing Title:

DETAILS

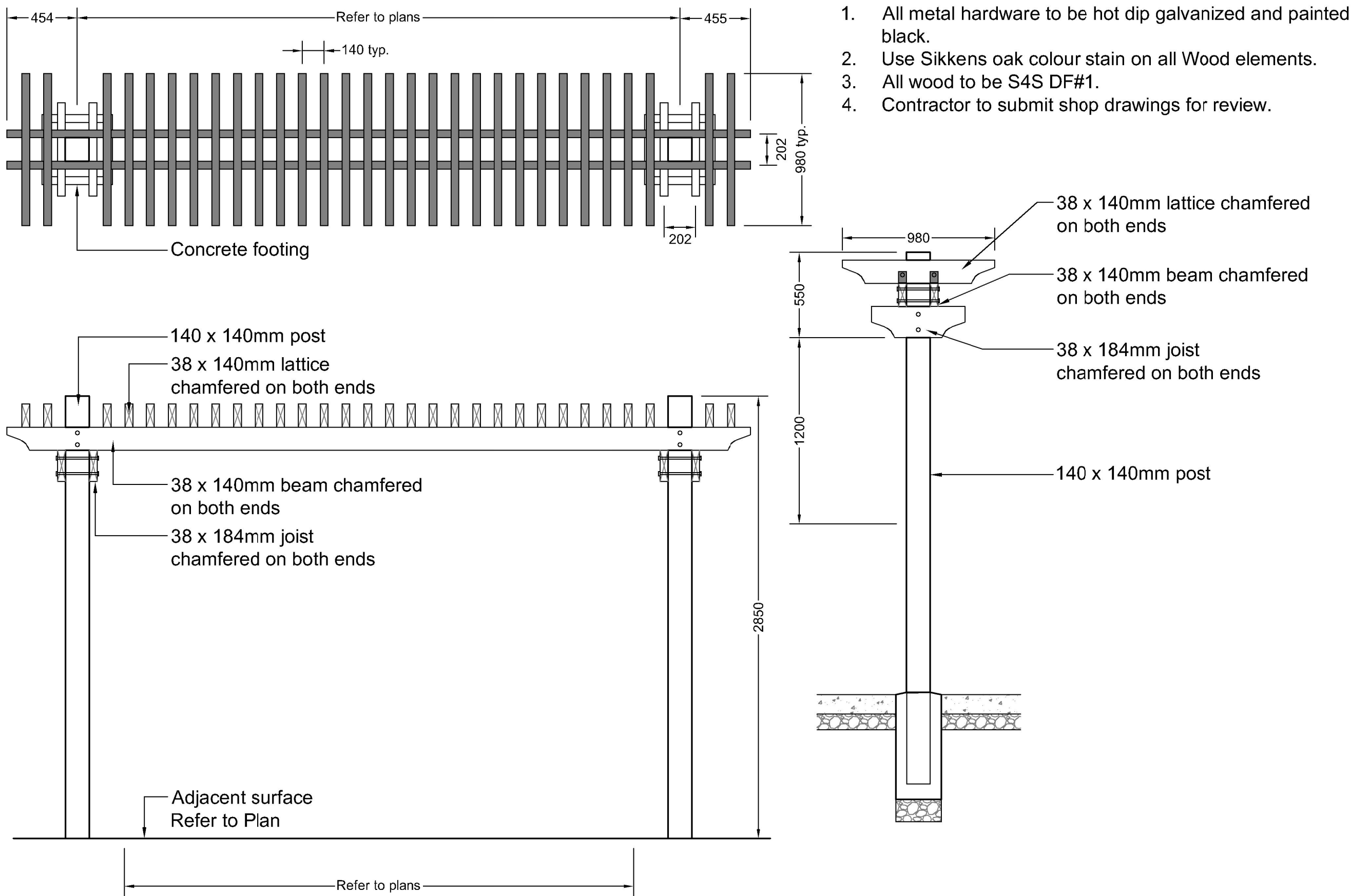
Drawing #:

LD-01

VDZ Project #:

DP2022-16

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1 LINEAR TRELLIS

Scale 1:25

NOTES:

1. Install to manufacturers specifications.
2. Or approved substitute.



Picnic Table
Model Number: MLPT400-S-W
Size: 749mm x 1581mm x 445mm
Options: Surface Mount
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approve equal

3 PICNIC TABLE

N.T.S



Bar Counter

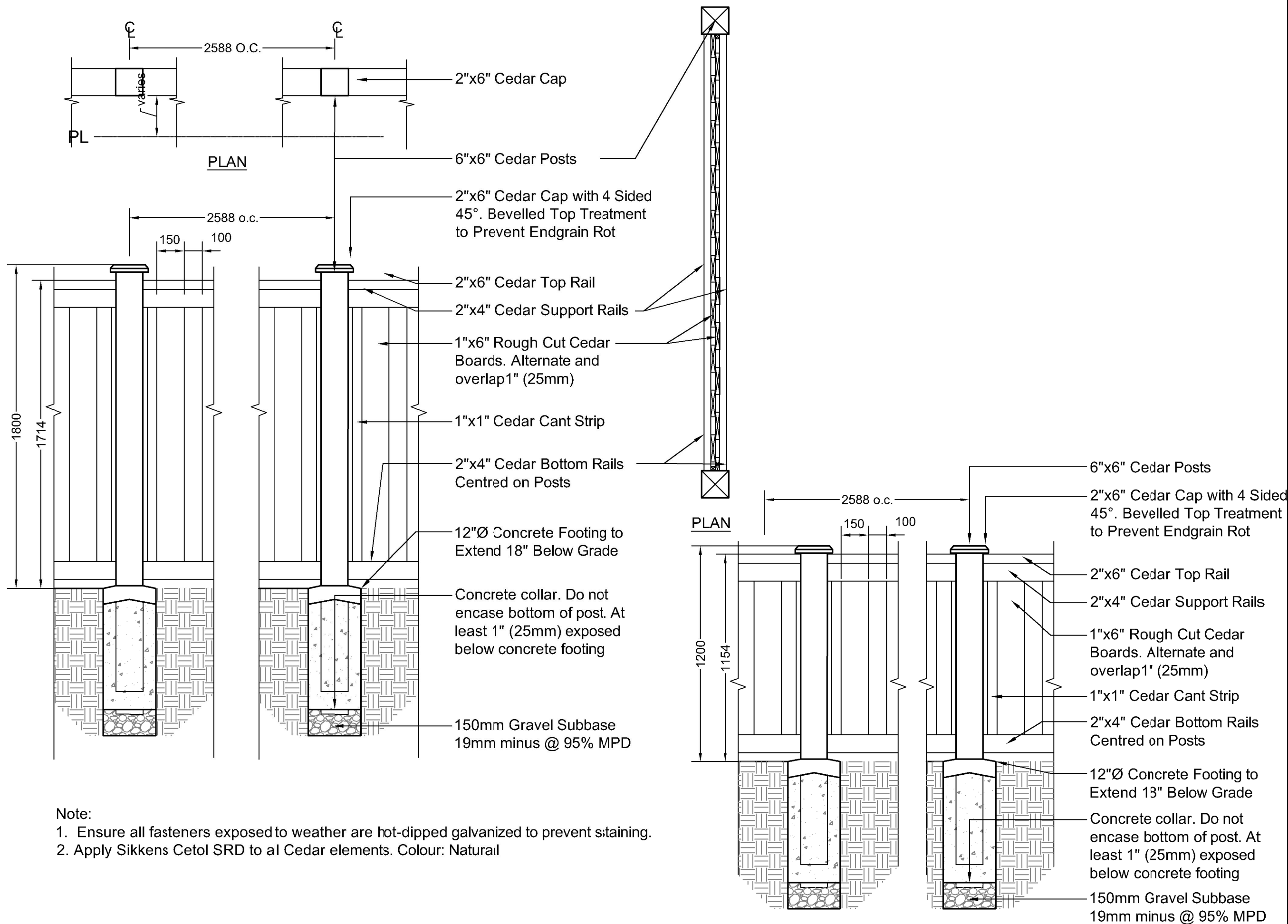
Material: Polished concrete top

Chairs

Details: Bar stool height
Material: Metal

4 BAR TABLE

N.T.S

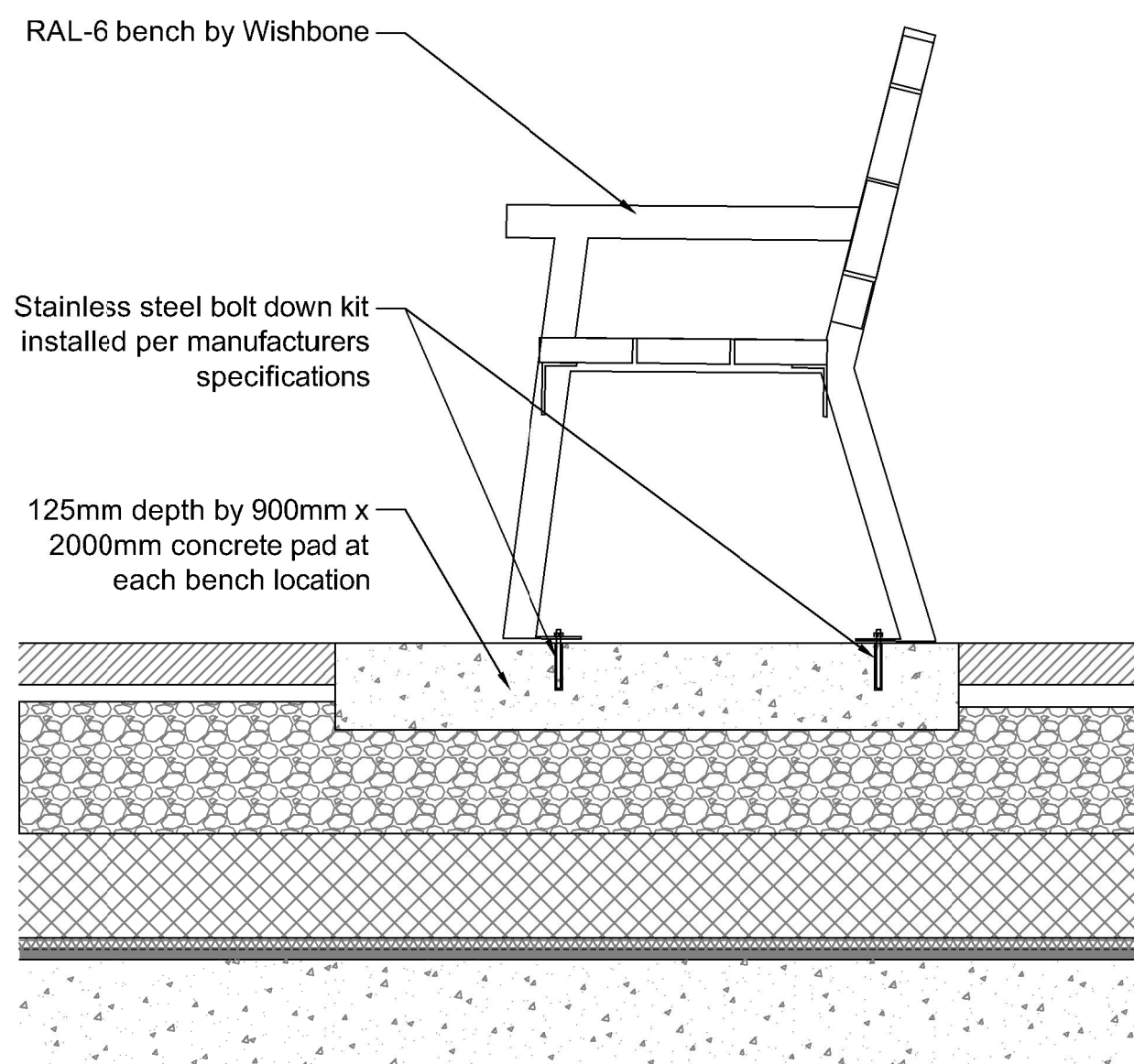


2 CEDAR FENCE

Scale 1:20

NOTES:

1. Install bench to manufacturers specifications.
2. Concrete pad to be included at all bench locations.
3. Size of concrete pad as noted on details unless shown otherwise on plans.
4. Or approved substitute.
5. Refer to concrete paving detail for additional information on concrete pad.



Bench
Model Number: MLB370-W
Size: 1765mm
Options: Center Arm
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approved equal



5 BENCH

N.T.S

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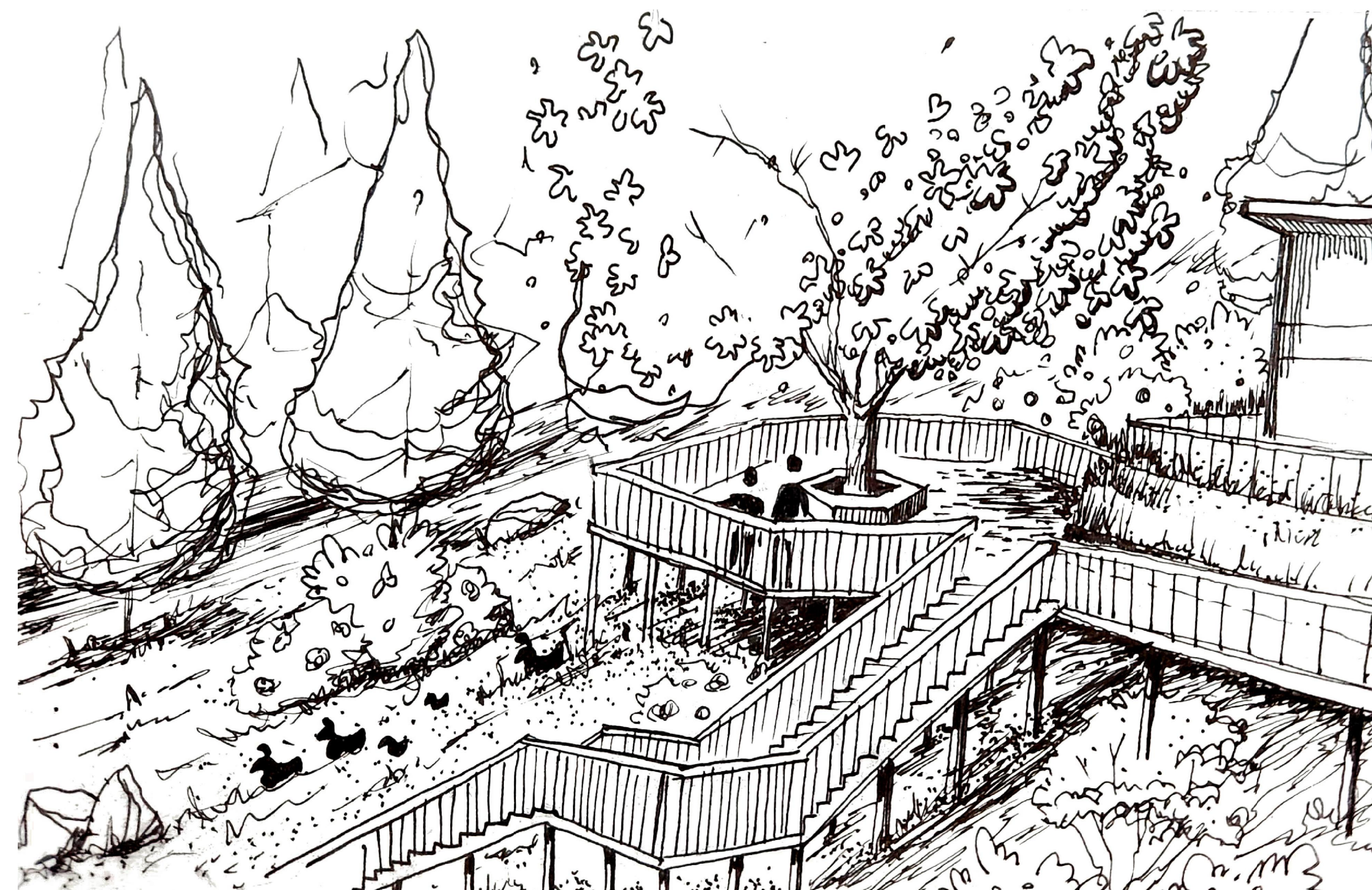
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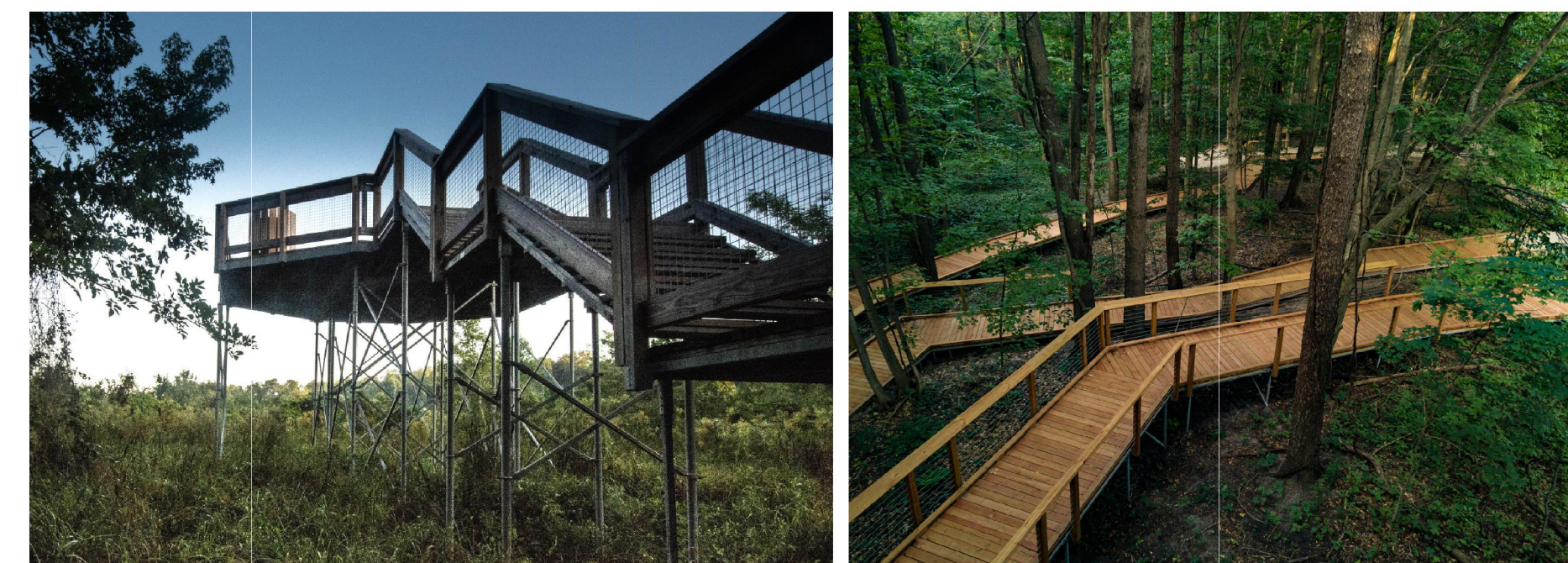
Drawing Title:
DETAILS

VDZ Project #:
DP2022-16

Drawing #:
LD-03



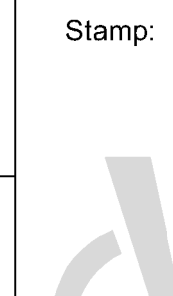
1 BOARDWALK AND STAIRS



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Drawing #: **LD-04** VDZ Project #: **DP2022-16** Drawing Title: **DETAILS**



CITY OF WEST KELOWNA
DEVELOPMENT PERMIT WITH VARIANCES
DP 22-26

To: Gatehouse Developments (Providence) Inc., BC1304810
 c/o Mike Sheppard
 PO Box 2585 STN Sardis Main
 Sardis, BC, VR2 1A8

CC: Distefano Jaud Architecture
 c/o Carlo Distefano
 3-1331 Ellis Street
 Kelowna, BC, V1Y 1Z9

1. This Permit is issued subject to compliance with all of the Bylaws of the City of West Kelowna applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Permit applies to and only to those lands within the City of West Kelowna described below, and any and all buildings, structures and other developments thereon:

Lot 37 District Lot 2044 ODYD Plan KAP90501
(3401 Sundance Drive)

3. This **Multiple Family and Intensive Residential (DPA 3), Hillside (DPA 4), and Sensitive Terrestrial Ecosystem (DPA 5) Development Permit Area** allows the construction of 57 townhouses contained in 11 buildings, subject to the following conditions and related Schedules:
 - The siting and phasing of buildings are to be in accordance the **Site Plan Drawings A1.0-A1.2**, prepared by DiStefano Jaud Architecture, dated April 17, attached as Schedule "A";
 - The exterior design and finish of buildings are to be in accordance with the **Architectural submission** (elevations, material and colour schedule, renderings) prepared by DiStefano Jaud Architecture, dated February 24 and April 17, 2023, attached as Schedule "A";
 - The dimensions and siting of retaining walls and servicing to be constructed on the land shall be in accordance with **Civil Engineering Drawings** (site servicing and grading plans, pre- and post-contours, hammerhead cross sections, lane extension cross sections, stormwater management, erosion and sediment control plan, turning movement plan), package prepared by Aplin & Martin Consultants Ltd., prepared Aug. 22, 2023, Rev. 2, "Issued for Development Permit", Project No. 20-3099, attached as Schedule "B";
 - Landscaping to be provided on the land in general accordance with **Landscape Drawings**, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-L01C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, dated August 21, 2023, attached as Schedule "C";

- Construction activities to be in accordance with the **Environmental Assessment Reports**, prepared by Makonis Consulting Ltd., dated June 2021, attached as Schedule “E”, including but not limited to:
 - i. Bird nest surveys must be done in advance of clearing of trees between March 15 and August 15, and in accordance with the *Migratory Birds Act* and any other provincial and federal regulations;
 - ii. Spoil piles left for longer than two weeks are to be tarped;
 - iii. Spill-containment kits will be on site and on each equipment/machine; and
 - iv. Work site will be maintained in clean state. Oil containers, cans, grease tubes, rags, etc. and any other materials packaging will be removed from the work area to an approved disposal location immediately on completion of the servicing job.
- All construction activities and associated mitigation measures to be conducted on the land be in general accordance with the **Geotechnical Report**, dated February 8, 2021, and Geotechnical Memo, dated July 8, 2021, prepared by Interior Testing Services Ltd, attached as Schedule “F”, and the **Construction Management Plan**, prepared by Gatehouse Developments Inc., dated September 2, 2020, attached as Schedule “G”, including but not limited to:
 - i. Rock slopes to be finished no steeper than 0.5(H):1(V) and scaled to remove loose particles at the rock face.
 - ii. 3 m clearing of overburden above the top of the rock cuts with chain link fencing installed above the rock cut.
 - iii. With rock slopes less than 2.5 m in height, a catchment buffer is not required, however where rock cuts are 2.5 m to 6 m in height, provide a 1.5 m wide catchment buffer area.
 - iv. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - v. Finished grades should be sloped away from the building in order to minimize infiltration of water into the backfill zone. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - vi. ITSL should be called to review the stripped subgrade prior to placing new grading fills, as well as during fill placement to carry out periodic field density testing.
- The following **variances to Zoning Bylaw No. 0265** are included as part of this Development Permit, in accordance with the Site Plan attached as Schedule “A”, and Civil Package, attached as Schedule “B”:
 - i. S.10.11.4(g)(i) – Reduce the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies);
 - ii. S.10.11.5(g)iii – Reduce the minimum required interior parcel boundary setback from 4.5 m to 3.5 m;
 - iii. S.10.11.5(g)ii – Reduce the minimum required rear parcel boundary setback from 7.5 m to 5.97 m (varies);

- iv. S.4.3.2(b) – Reduce the minimum required distance between the parking and loading spaces from the front parcel boundary (Sundance Drive) from 3.0 m from 0.0 m (varies);
 - v. S.4.10.1(a) – Reduce the minimum required number of loading spaces from four to two space(s);
 - vi. S.4.10.2(a) – Reduce the minimum required dimensions of a loading space from 3.0 m x 9.0 m to 2.89 m to 9.0 m; and
 - vii. S.3.14.1 – Increase the maximum height of three retaining walls from 2.5 m to 3.5 m.
 - viii. S.4.4.3(ii)(b) – Increase the width of driveway crossing at the property line from a maximum 7.0 m to 9.4 m (varies).
4. As a condition of the issuance of this Permit, the property owner shall deposit 125% of the cost estimate for the **Landscaping (\$400,080.90)** as performance security to ensure installation of the required landscape plantings, in accordance with the City's Development Application Procedures Bylaw No. 0260 and Schedule "D".
 5. Should any interest be earned upon the security, it shall accrue to the Permittee and be paid to the Permittee if the security is returned. The condition of the posting of the security is that should the Permittee fail to carry out the development hereby authorized, according to the terms and conditions of the Permit within the time provided, the City of West Kelowna may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Permittee, or should the Permittee carry out the development permitted by this Permit within the time set out below, the security shall be returned to the Permittee. There is filed accordingly:
 6. The land described herein shall be developed strictly in accordance with the terms and conditions of this Permit and any plans and specifications attached to this Permit, which shall form a part hereof. Should any changes be required to this permit, please ensure that you obtain written approval from City of West Kelowna prior to making any changes.
 7. If this Development Permit has not been issued within one year from approval, Development Permit DP 22-26 shall be deemed to have been refused and the file will be closed.
 8. **This Permit is not a Building Permit.**
 9. **This is not a Municipal Highways Permit.**
 10. As a condition of the issuance of this Permit, any works proposed within the access easement or adjacent private property must be approved by the affected easement holder(s) or landowner(s), to the satisfaction of the city.
 11. All archaeological sites in B.C. are protected under the Heritage Conservation Act. This applies to whether sites are located on public or private land and whether the site is known or unknown. If you think you have uncovered an archaeological site during a building project or renovation, please do not disturb the site further and call B.C.'s Archaeology Branch immediately at (250) 953-3334. Branch archaeologists will review your project plans and make recommendations to manage site impacts and secure the required permitting.

12. Subject to the terms of the permit, where the holder of a permit issued under the *Local Government Act* does not substantially commence any construction with respect to which the permit was issued within one year after the date it is issued, the permit lapses.

AUTHORIZING RESOLUTION NO. **CXXX/XX** PASSED BY THE MUNICIPAL COUNCIL ON _____, 2023.

Signed on _____

Corporate Officer

I hereby confirm that I have read and agree with the conditions of Development Permit DP 22-26 with Variances and will ensure that copies of the Permit will be provided to onsite personnel at time of construction.

Signed on _____

Property Owner or Agent

ISSUED on _____

Schedules:

- A. Architectural submission by DiStefano Jaud Architecture, Drawing No. A1.0-1.2, A2.1-2.3, A3.1-3.3, & 3.4, Rev. 3, dated April 17, 2023, and Drawings 2.4-2.7, A3.3, & A3.5-3.7, Rev. 2, dated February 24, 2023.
- B. Civil drawings, prepared by Aplin & Martin Consultants Ltd., Drawing Package prepared August 22, 2023, Rev. 2, Issued for Development Permit, Project No. 20-3099.
- C. Landscape Plan, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-LO1C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, Rev. 9, dated August 21, 2023.
- D. Landscape Estimate, prepared van der Zal + Associates, dated May 5, 2023, Project File Reference 2021-128.
- E. Environmental Reports, prepared by Makonis Consulting Ltd., dated June 2021.
- F. Geotechnical report, dated February 8, 2021, and Geotechnical Memo dated, July 8, 2021, prepared by Interior Testing Services Ltd.
- G. Construction management plan, prepared by Kyle Weiher, Senior Project Manager, Gatehouse Construction Inc., no date.

H:\DEVELOPMENT SERVICES\PLANNING\3060 Development Permits\3060-20 Permits\2022\DP 22-26 3401 Sundance Dr\Permit_Security

From: noreply@esolutionsgroup.ca
To: [Delegation Requests](#)
Subject: New Response Completed for Request to Appear As a Delegation
Date: September 6, 2023 5:47:23 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe. **The City will never ask for personal or account information or account password through email.** If you feel this email is malicious or a scam, please forward to westkelowna@phishforward.beauchersecurity.com.

Hello, Please note the following response to Request to Appear As a Delegation has been submitted at Wednesday September 6th 2023 5:44 PM with reference number 2023-09-06-004.

- **Request to appear as a delegation on:**
9/26/2023
- **Name of person making the presentation:**
Ryan Jones
- **Name of the group or organization that the person is representing:**
Gatehouse Developments
- **Daytime phone number:**
7789680230
- **Email address:**
rjones@stewartland.ca
- **Presentation title:**
Providence Development
- **What is your request to Council?**
Presentation to outline the changes made to the conceptual plan.
- **Presentation points:**
Powerpoint presentation will be provided.

[This is an automated email notification -- please do not respond]

From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: DP 22-26
Date: September 21, 2023 3:34:36 PM

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Todd/Jo-Anne Ruscheinsky
2111 Ensign Quay Lane

Dear Council;

I'm writing a second letter or better plea that you will truly understand and maybe come see our little skinny lane to know that adding all those new townhouses to a already compromised and poorly constructed road is going to be a disaster for our little neighborhood! Ensign Quay that leads up to our road is used frequently for parking although bylaw says it shouldn't! With a air bnb on the right side and a tall home on the left which has been turned into 3 apartments you obviously know there isn't enough parking! This in turn makes it a hazard for us turning up into Ensign Quay Lane and emergency vehicles. We have huge parking problems already without adding more homes! Next huge problem is snow removal! These developers do not realize how much we get! We do not have the luxury or a snow removal truck or sand truck when it's slippery..this is up to only the owners! With all these new townhouses, where do we put it? Not to mention a abundance of garbage/ recycling/ yard waste bins..where will these go? I truly do not believe these developers care about this or even slightly addressed it! Sundance has ample room for these townhouses, Ensign Quay Lane doesn't..please do not allow this to go ahead!

Please visit our little lane for yourself..you will understand then!

Thank you!



DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

File No: Z 17-02

Subject: **Application No. Z 17-02 – Application Extension (3290 Glenrosa Road, 3280 Glenrosa Road and Salmon Road)**

Report Prepared By: Yvonne Mitchell, Planner II

RECOMMENDATION to Consider and Resolve:

THAT in accordance with Development Application Procedures Bylaw No. 0260, Council approve an additional extension of one (1) year for File: Z 17-02, subject to the reservoir costs being brought to 2023 standards.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

Council gave third reading to Zoning Bylaw Amendment Application Z 17-02 on May 8, 2018. The amendment proposes to rezone 3 lots in Upper Glenrosa from the Rural Residential Large Parcel Zone (RU4) to the Parks and Institutional Zone (P1), Single Detached Residential Zone (R1), and Low Density Multiple Residential Zone (R3) to facilitate approximately 92 single family and 26 multi-family units.

The Zoning Bylaw Amendment included a variance to exempt the developer from constructing a water storage reservoir and is to be scheduled for consideration of adoption once conditions are met, which include submission of technical/design, legal, and financial matters (Attachment 1). Given deficiencies in the Glenrosa Road system, one of Council's conditions included improvements to ~700m of Glenrosa Road to the urban arterial road standard (modified) for the section past Gates Road. These improvements included 400m of required frontage improvements and improvements to the 300m of roadway between the subject properties and Gates Road. These

improvements were identified in the Council-adopted Upper Glenrosa/Salmon Road Comprehensive Development Plan (CDP).

This is the second extension request for this application. A previous extension request was made in 2022 (Attachment 1), which extended the deadline for Council consideration for final adoption to May 10, 2023. This extension request was to allow time for the applicant to address the dedication of land to accommodate improvement to Glenrosa Road.

The applicant then made another extension request to further review the level of Glenrosa Road improvements required. On June 27, 2023, this second extension request was presented to Council. At this meeting, discussion was had over the feasibility of meeting the Glenrosa road improvements condition as currently understood. As a result, consideration of the extension request was postponed for 3 months to provide time for the developer to work with staff to find alternative improvements.

DISCUSSION

The applicant requested additional changes to the proposed road improvements, given concerns with the inability to negotiate land acquisitions required with surrounding property owners and the cost of off-site upgrades. The applicant has now provided new plans for the road improvements that avoid the need for land acquisition and reduce costs. The changes are summarized below. For more detail refer to the applicant's revised drawings and explanation in Attachment 2.

- re-alignment of a short section of utilities – water and storm;
- reduction in boulevard width on east side to shift the roadway; and extend the storm discharge approx. 60 meters at Gates Road intersection.

The effect of the modified cross section is the reduction in boulevard width on the east side of the roadway from 2.75m to 1.25m as illustrated in Figure 1 and 2. The new plans have been reviewed by staff and do not impact safety or consistency with the CDP.

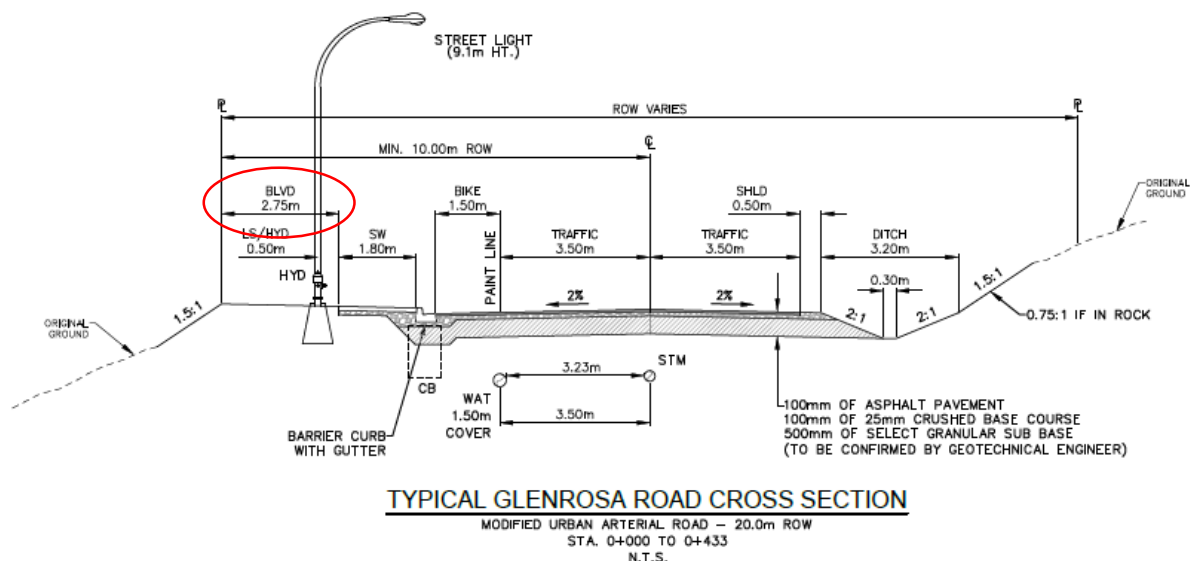


Figure 1 – previously accepted modified Glenrosa Road cross section.

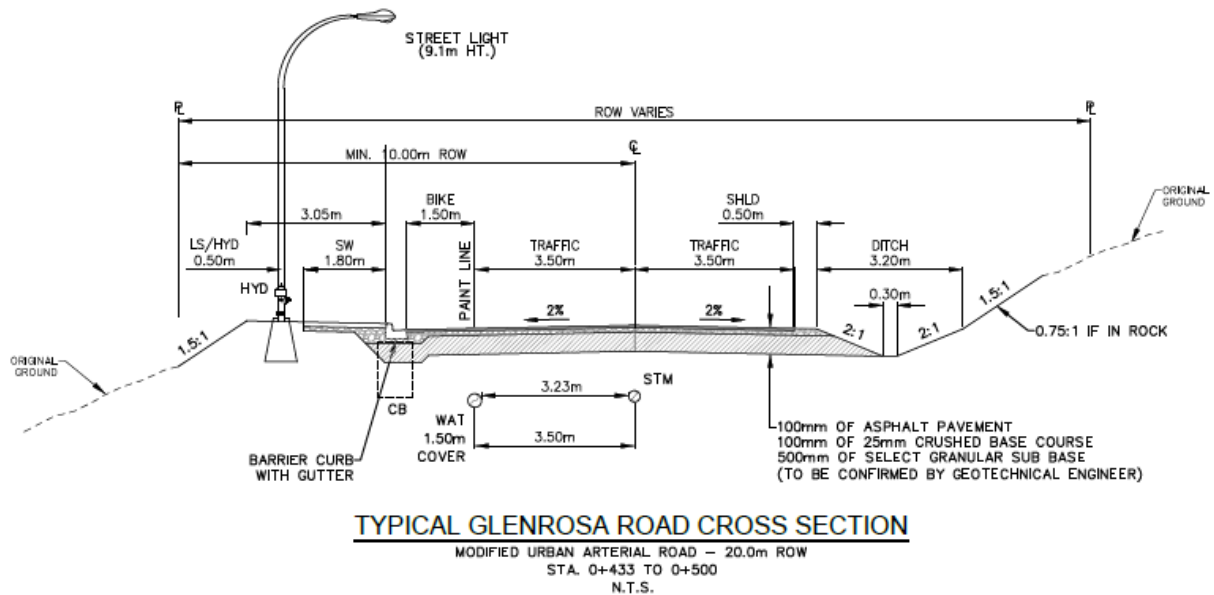


Figure 2 – Updated modified Glenrosa Road cross section with boulevard width reduced to 1.25 m.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
June 27, 2023	THAT Council defer consideration of Z 17-02, Application Extension, 3290 Glenrosa Road, 3280 Glenrosa Road and Salmon Road for a period of three months.	C257/23
May 10, 2022	THAT Council approve an extension of one (1) year to resolve the zoning conditions for File: Z 17-02, in order for the application to move forward for final adoption, subject to the reservoir costs being brought to 2022 standards.	C129/22
May 8, 2018	THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and THAT Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and THAT Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290	C127/18

	Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezoning application (Zoning Amendment Bylaw No. 0154.56, 2017).	
February 27, 2018	A Public Hearing was held for the bylaw amendment.	N/A
August 22, 2017	<p>THAT Council give first and second readings to the Zoning Amendment Bylaw; and</p> <p>THAT Council direct staff to schedule the Public Hearing pending submission of the following to the satisfaction of the General Manager of Development Services:</p> <ul style="list-style-type: none"> • Proposed design and cross-section for the Glenrosa Road improvements; • Storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities; • Revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard; and • Proposal to assist with the construction of park improvements at Stonegate Park. 	C401/17

CONCLUSION

Given the applicant has provided a new plan for the improvements to Glenrosa Road that will not affect safety or consistency with the CDP, it is recommended that Council approve the extension request.

As per the Development Application Procedures Bylaw No. 0260, Council may extend the deadline for a period of one (1) year by passing a resolution to that effect to enable the applicant to complete the requirements for final adoption. A maximum of two (2) one-year time extensions may be granted by Council. This extension request is the second extension request submitted for the application. The request would extend the deadline to May 10, 2024, for the applicant to complete the requirements for final adoption. The requirements for final adoption can be found in Attachment 1; however the \$3,071 per lot/unit contribution to the future reservoir project is recommended to be updated based on current costs.

Alternate Recommendation to Consider and Resolve:

THAT Council deny an extension request for File: Z 17-02;

THAT Council rescind first, second and third readings to Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02), and abandon the bylaw; and

THAT Council direct staff to close File Z 17-02 and associated file DVP 17-15.

If Council chooses this option the file would be closed, and the applicant would not be able to apply for a similar application for a period of six months.

REVIEWED BY

Bob Dargatz, Development Engineering Manager

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☒ No ☐

Attachments:

- A. June 27 2023 Extension #2 Council Report
- B. Applicant's Revised Road Improvement Plans and Explanation



DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: June 27, 2023

From: Paul Gipps, CAO

File No: Z 17-02

Subject: **Z 17-02; Application Extension; 3290 Glenrosa Road, 3280 Glenrosa Road and Salmon Road**

Report Prepared By: Yvonne Mitchell, Planner II

RECOMMENDATION to Consider and Resolve:

THAT Council approve an extension of one (1) year and maintain the previously identified zoning conditions for File: Z 17-02, subject to the reservoir costs being brought to 2023 standards.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity

We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

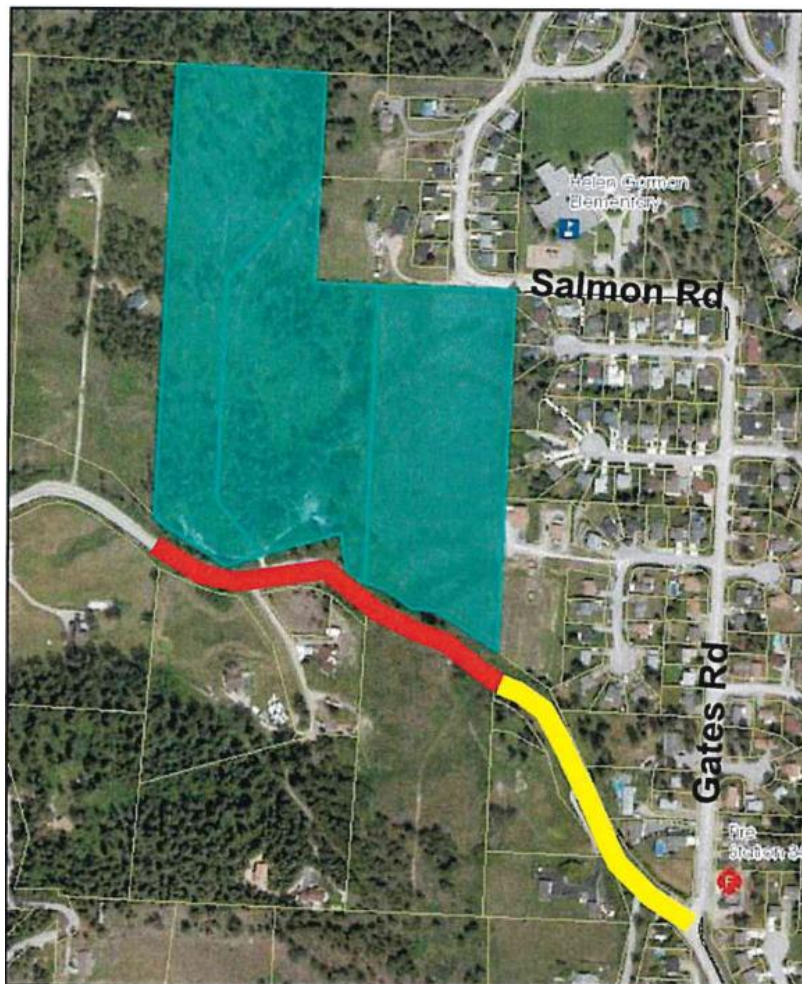
Council gave third reading to Zoning Bylaw Amendment Application Z 17-02 on May 8, 2018. The amendment proposes to rezone 3 lots in Upper Glenrosa from the Rural Residential Large Parcel Zone (RU4) to the Parks and Institutional Zone (P1), Single Detached Residential Zone (R1), and Low Density Multiple Residential Zone (R3) to facilitate approximately 92 single family and 26 multi-family units.

The Zoning Bylaw Amendment included a variance to exempt the developer from constructing a water storage reservoir and is to be scheduled for consideration of adoption once conditions are met which include submission of technical/design, legal, and financial matters (Attachments 1 - 3). Given deficiencies in the Glenrosa road system, one of Council's conditions included improvements to ~700m of Glenrosa Road to the urban arterial road standard for the section past Gates Road (Figure 1). These improvements were also identified in the Council-adopted Upper Glenrosa/Salmon Road Comprehensive Development Plan (CDP). In recognition of site constraints, the City

previously provided the following concessions to the urban arterial standard to the applicant:

- Reduction of the road right of way width from 24.0m to 20.0 metres;
- Re-location of the 2.0 metre wide sidewalk to back of curb;
- Re-location of the 2.0 metre wide boulevard to back of sidewalk;
- Elimination of the street tree requirements from the cross-section;
- Permission for cut and fill slopes and/or retaining walls to be located within road right of way; and
- increase of the maximum road grade from 8% to 10%.
- Require only curb, gutter, and sidewalk on the north side of Glenrosa Road.

The above concessions result in a half modified arterial road standard and reduce the expected improvement costs by approximately \$600,000 (Attachment 4). Despite these concessions the applicant expressed concern with the Glenrosa Road improvement requirements and associated costs at third reading; however, Council maintained the requirement for the improvements. It should be noted the CDP identifies the costs associated with the urbanization of Glenrosa Road are to be borne by the developer.



This is the second extension request for this application. A previous extension request was made in 2022 (Attachment 5), which extended the deadline for Council consideration for final adoption to May 10, 2023. This extension request was to allow time for the applicant to address the dedication of land to accommodate improvement to Glenrosa Road. The applicant has now made another extension request to further review the level of Glenrosa Road improvements required. The current request would extend the deadline to May 10, 2024.

Figure 1: Extent of Glenrosa Road Improvements at property frontage (red) and extended/off-site improvements (yellow)

DISCUSSION

Applicant Rationale

The applicant has provided a rationale letter for their extension request (Attachment 6). As per the letter, the current upgrades required to Glenrosa Road are too costly to pursue the development, and the intent of the extension request is to provide time to undergo a redesign exercise that will look at opportunities to reduce costs along select sections of the roadway upgrade where appropriate.

Impact of Proposed Extension

There are a number of zoning conditions related to improvements to Salmon Road, water reservoir contributions, improvements to Stonegate Park, and stormwater management. One of the detailed zoning conditions related to Glenrosa Road is as follows:

- 1. Submission of designs and cross-sections, cost estimate, and development agreement for the full width of the Glenrosa Road improvements from and including the subject properties to Gates Road to the satisfaction of the General Manager of Development Services (full width improvements include curb, gutter, sidewalk on the north side of the road and ditch on the south side).***

The identified Glenrosa Road improvements are outlined as requirements in the Council-adopted CDP and enhance public safety for both vehicle and pedestrian traffic. The identified works to Glenrosa Road are located outside of the City's current Growth Boundary, and therefore are not anticipated in the City's DCC program or Capital plans for road improvements. The CDP states the road is to be constructed to the full Urban Arterial standard and that all traffic improvements identified at the detailed design stage (i.e. rezoning) and any associated costs will be borne by the developer. In recognition of site constraints, the City has previously worked with the developer to provide multiple concessions to this standard, (see Third Reading Report Attachment 3); however, it is anticipated that further modifications will have an impact on safety and consistency with the CDP.

It is also anticipated that proposed changes to the road improvements may result in a request to amend the conditions established at 3rd reading.

CONCLUSION

Given the applicant has indicated they are not able to meet the Glenrosa Road Improvements condition outlined at zoning, and further concessions to both this condition, as well as the requirement to provide adequate water storage (i.e. build reservoir) have the potential to impact safety and consistency with the CDP, it is recommended that Council not approve the extension request intended to further review required road improvements. It is further recommended that Council maintain the previously identified improvements to Glenrosa Road.

As per the Development Application Procedures Bylaw No. 0260, Council may extend the deadline for a period of one (1) year by passing a resolution to that effect to enable the applicant to complete the requirements for final adoption. A maximum of two (2) one-year time extensions may be granted by Council. This extension request is the second extension request submitted for the application.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
May 10, 2022	THAT Council approve an extension of one (1) year to resolve the zoning conditions for File: Z 17-02, in order for the application to move forward for final adoption, subject to the reservoir costs being brought to 2022 standards.	C129/22
May 8, 2018	THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and THAT Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and THAT Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezoning application (Zoning Amendment Bylaw No. 0154.56, 2017).	C127/18
February 27, 2018	A Public Hearing was held for the bylaw amendment.	N/A
August 22, 2017	THAT Council give first and second readings to the Zoning Amendment Bylaw; and THAT Council direct staff to schedule the Public Hearing pending submission of the following to the satisfaction of the General Manager of Development Services:	C401/17

-
- Proposed design and cross-section for the Glenrosa Road improvements;
 - Storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities;
 - Revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard; and
 - Proposal to assist with the construction of park improvements at Stonegate Park.
-

Alternate Recommendation(s) to Consider and Resolve:

Option 1: This option reflects the applicant's request to extend the application to further review the road design for additional costs savings. Should Council choose this option the applicant would be granted an extension, and the opportunity to present an alternative to the existing Glenrosa Road improvements as part of a future consideration.

THAT Council approve an extension of one (1) year for File: Z 17-02 to allow the applicant an opportunity to provide alternate road design options for further review and consideration, subject to the reservoir costs being brought to 2023 standards.

Option 2:

THAT Council deny an extension request for File: Z 17-02;

THAT Council rescind first, second and third readings to Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02), and abandon the bylaw; and

THAT Council direct staff to close File Z 17-02 and associated file DVP 17-15.

REVIEWED BY

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Council Minutes Excerpt - May 8, 2018
2. Detailed Zoning Conditions
3. Third Reading Council Report
4. Glenrosa Road Improvements Cost Estimate
5. Applicant Rationale Extension 1
6. Applicant Rationale Extension 2

It was moved and seconded

THAT Council defer second reading to City of West Kelowna Official Community Plan Bylaw Amendment No. 0100.18, 2017 (File: Z 11-15); and

THAT Council defer second reading to City of West Kelowna Zoning Bylaw Amendment No. 0154.15, 2017 (File: Z 11-15).

C216/18 CARRIED Opposed: Councillor Winsby and Mayor Findlater

Bylaws for Consideration of Third Reading and Adoption

9.2.2 **Z 17-02, Zoning Amendment Bylaw No. 0154.56, 2017 (File Z 17-02), 3280 and 3290 Glenrosa road and Block 105 Salmon Road**

It was moved and seconded

THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and

THAT Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and

THAT Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezoning application (Zoning Amendment Bylaw No. 0154.56, 2017).

C217/18 CARRIED Opposed: Councillors Ensign and Zanon

9.2.3 **LUCA 18-01, Land Use Contract Discharge Bylaw No. 0251, 2018, 3195 Sunset Place**

It was moved and seconded

THAT Council rescind first and second readings of Land Use Contract Discharge Bylaw No. 0251, 2018 and abandon the bylaw.

C218/18 CARRIED UNANIMOUSLY

Detailed Zoning Conditions

As the application has progressed to consideration of third reading without satisfactory resolution to numerous development issues, the following list itemizes the requirements that are recommended to be addressed prior to consideration of adoption:

Technical/Design Matters

1. **Submission of designs and cross-sections, cost estimate, and development agreement** for the full width of the Glenrosa Road improvements from and including the subject properties to Gates Road to the satisfaction of the General Manager of Development Services (full width improvements include curb, gutter, sidewalk on the north side of the road and ditch on the south side).
2. **Submission of designs, cost estimate, and development agreement** for all required off-site improvements related to a revised storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities, and a revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard, to the satisfaction of the General Manager of Development Services.
3. **Submission of a design, cost estimate, and development agreement** for the installation of curb extensions at crosswalk on Salmon Road fronting the elementary school.

Legal Matters

4. **Registration of a covenant** to require the construction of the Glenrosa Road improvements at time of subdivision (both on-site and off-site works in accordance with the Comprehensive Development Plan).
5. **Registration of a covenant** to require the construction of a pedestrian trail through the proposed P1 area at time of subdivision (the agreement may note that the length of the 1.5 metre wide trail through the property will contribute to the parkland dedication requirements in accordance with s. 510 of the *Local Government Act*). A portion of the pedestrian trail may also need to be secured through right of way if it cannot be accommodated within the P1 area (note that a trail cannot be established within the riparian area).
- ~~6. **Registration of a covenant** to require at time of subdivision a Dam Breach and Inundation Analysis prepared in accordance with the Dam Safety Regulation, Water Sustainability Act.~~
7. **Registration of a covenant** to require a \$3,071 per lot/unit contribution to the future reservoir project (with single family lot contribution at subdivision and multiple family unit contribution at building permit).
8. **Registration of a statutory right of way** to establish a pedestrian connection between Stonegate Park and the proposed residential subdivision (blanket SRW at zoning with final SRW determined through subdivision approval).
9. **Dedications of land** to accommodate improvements to Glenrosa Road.
10. **Dedication of land** for the proposed Parks and Open Space (P1) area.

Financial Matters

11. **Financial contribution** of \$35,000 to assist with the construction of park improvements at Stonegate Park.



COUNCIL REPORT
Development Services
For the May 8, 2018 Council Meeting

DATE: May 1, 2018 **File:** Z 17-02

TO: Jim Zaffino, CAO

FROM: Darin Schaal, Planner III

RE: Bylaw No.: Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02)
 Legal: Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777
 Address: 3280 & 3290 Glenrosa Road & Block 105 Salmon Road
 Owners: L. & G. Gulyas, J. & B. Lima, 595338 BC Ltd.
 Agent: D.E. Pilling and Associates Ltd.

RECOMMENDED MOTION

THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and

THAT Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and

THAT Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezoning application (Zoning Amendment Bylaw No. 0154.56, 2017).

RATIONALE

The following considerations provide rationale for the recommended motion:

- The proposed rezoning plan is consistent with the OCP land use designations.
- Although the applicant has yet to address the technical aspects of the proposal, the recommended motion outlines the information that is still required prior to consideration of adoption.
- Glenrosa Road improvements are required by bylaw, outlined as zoning requirements in the Council-adopted Comprehensive Development Plan, and also enhance public safety for both vehicle and pedestrian traffic.
- The Comprehensive Development Plan stipulates the expectations for parkland dedication and pedestrian connectivity.
- Financial contributions to the future reservoir project and the adjacent undeveloped park are consistent, fair, and best practices and help relieve the taxpayer burden that development places on municipal infrastructure.

LEGISLATIVE REQUIREMENTS

Council has the authority under Part 14 (s. 479) of the *Local Government Act* to create and amend the Zoning Bylaw and under Part 12 (s. 498) to issue a variance permit to alter the provisions of subdivision and development requirements.

EXECUTIVE SUMMARY

Although the Recommended Motion supports Council giving third reading to the bylaw amendment, the applicant is not in agreement with staff on the conditions of zoning (Attachment 1). The areas of contention are the Glenrosa Road improvements, parkland dedication requirements at subdivision, and Stonegate Park improvements. The Recommended Motion is based on the policies adopted in the Comprehensive Development Plan (CDP), the requirements of the *Local Government Act*, the BC Parkland Acquisition Best Practices Guide, and City bylaws, plans, and strategies, and standard practices. However, several alternate motions are provided for Council consideration, including Alternate Motion #2, which illustrates the applicant's proposal for no immediate improvements to Glenrosa Road (cash-in-lieu for frontage improvements at subdivision) and no contribution to park improvements.

BACKGROUND

On September 20, 2016, Council adopted a Comprehensive Development Plan (CDP) and associated Official Community Plan (OCP) amendments (Resolution C324/16) for three large rural properties in Upper Glenrosa.

Location & Surrounding Uses

The subject properties are located three kilometres northwest of the intersection of Glenrosa Road and Highway 97 (Attachment 2). One parcel has frontage on both Glenrosa Road and Salmon Road, while the other two parcels only have frontage on Glenrosa Road.

The surrounding area is predominantly rural residential uses with some single detached residential uses to the north and east (Figure 1). Helen Gorman Elementary School is located to the northeast and Stonegate Park (an undeveloped park space) abuts the site to the east.

Proposal

The proposed zones are consistent with the OCP land use designations adopted as part of the Comprehensive Development Plan (Files: OCP 12-01 & CDP 13-01). The combined area of the subject properties is 12.2 hectares (30.1 ac). Based on the proposed rezoning plan, the neighbourhood will be comprised of 67% single family zoning, 8% multiple family zoning, and 9% parks and open space, with roads accounting for the remaining area (Attachment 4). The applicant estimates the population of the development at built-out to be 328, which is

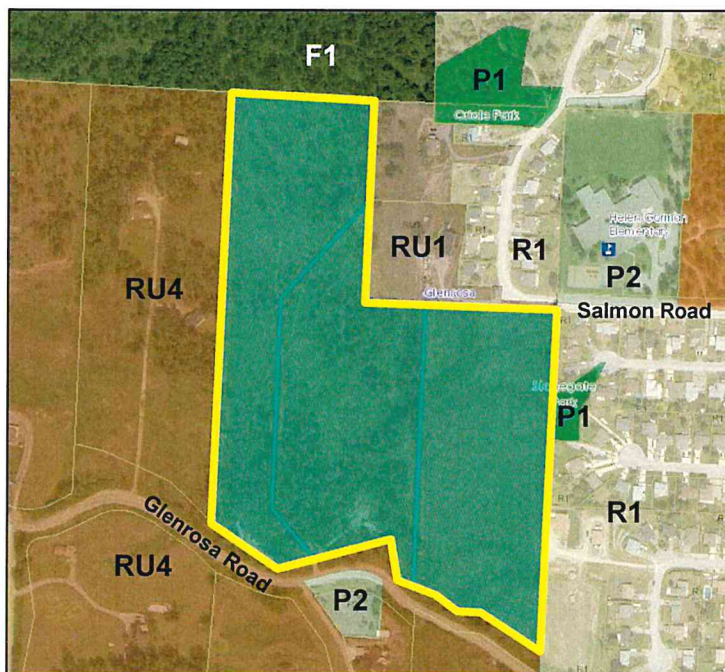


Figure 1 - Map of subject properties (delineated in yellow) and surrounding uses (see also: Attachment 3)

based on 92 single family lots and 26 multiple family units (the actual number of lots and/or units will be determined through future subdivision and development permit approval).¹

Public Hearing Overview

A Public Hearing was held on February 27, 2018. Two residents, in addition to the agent and property owners (for two of the three subject properties), addressed Council at the Public Hearing. Three letters were also received as late items: one from the Ministry of Forests, Lands, Natural Resource Operations, and Rural Development, one from another resident (a former Chair of the Westbank Irrigation District), and one from the applicants.

Feedback from the public included:

- Concerns about public safety, particularly regarding traffic in the area of Helen Gorman Elementary School and also construction traffic (i.e. heavy equipment) and access to the development site.
- Discontent with changes to the Official Community Plan (land use designation) following incorporation.
- Concerns about the proposed water booster station concept and the water storage deficiency in the Glenrosa neighbourhood.

Concerns from the applicants (agent and owners) included:

- The land dedication of environmentally sensitive areas is not credited toward the 5% parkland dedication requirement at subdivision.
- The existing alignment of Glenrosa Road and specifically land acquisition and road improvement requirements and the associated costs.
- The acknowledgment that improvements to Stonegate Park would be an advantage for future property owners but that improvements should be voluntary.

Referral comments from the Ministry of Forests, Lands, Natural Resource Operations, and Rural Development (MFLNRO) were also received as a late item addition to the agenda. MFLNRO comments included:

- The subject properties are located in the flood path of Glenrosa Lake (i.e. Ficke's Pond) and development will change the risk classification and consequence rating of the dams and have significant consequences for the dam owners.
- No water licenses will be issued for the future properties created through subdivision and development.

Response to Feedback from Public Hearing

The following sections are in response to the issues expressed at the Public Hearing:

Public Safety & Construction Traffic

The public safety/traffic concerns in the vicinity of Helen Gorman Elementary School noted at the Public Hearing were also studied recently as a component of a nearby development proposal. The study identified several safety issues associated with traffic congestion during peak school pick-up/drop-off times. The study recommended several mitigation measures to improve public safety—one of which suggests the construction of curb extensions at the crosswalk to address visibility concerns (Figure 2). As the proposed development will increase the pedestrian and vehicle traffic in the vicinity of the elementary school, the applicant proposes to construct off-site works in the form of curb extensions at the crosswalk fronting the elementary school to address the public safety/traffic concerns. It is

¹ The estimated population of the Glenrosa neighbourhood is 6,155 (neighbourhood profile based on 2011 census data). Glenrosa Road is the single access to the neighbourhood.

recommended the commitment be secured through the execution of a development agreement as a condition of zoning.

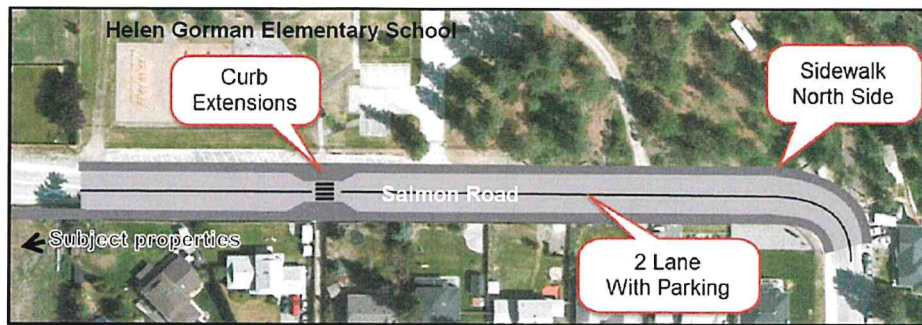


Figure 2 - Illustration of proposed curb extension at crosswalk to improve pedestrian safety

In addition to the proposed off-site works, one of the subject properties has approximately 70 metres of parcel frontage on Salmon Road and standard frontage improvements are required at time of development, which include curb, gutter, sidewalk (one side), and street lighting for that portion of the road fronting the subject property.² The construction of a sidewalk in the future will connect to the existing sidewalk on the south side of Salmon Road that leads to the existing crosswalk to the elementary school.

In response to the concern about construction traffic and access to the development site, the applicant acknowledged that Salmon Road (via Gates Road and passing by the elementary school) would be used for access. Following the Public Hearing, the applicant responded that the issue would be dealt with at time of subdivision. As such, a Construction Management Plan will be required at the development permit stage (concurrently with subdivision).

West Kelowna Official Community Plan (OCP)

The West Kelowna OCP was adopted on July 26, 2011. Prior to the adoption of the West Kelowna OCP, the Regional District of Central Okanagan OCP Bylaw No. 1050 guided development in the area. At that time, numerous properties in the Upper Glenrosa area, including the subject properties, had a future land use designation of Low Density/Single Detached Residential. When the West Kelowna OCP was adopted, a Rural Reserve (or urban growth boundary) was established based on the existing extent of community servicing. Properties outside the areas that could be serviced by community infrastructure were thus re-designated as Rural Reserve in the OCP due particularly to the lack of water. The existing zoning designations did not change. The planning process for the West Kelowna OCP was conducted in accordance with the *Local Government Act* and involved extensive public consultation and notification over two and half years of bylaw preparation.

Land Dedication

The applicant requests that the land dedication of environmentally sensitive areas (areas proposed to be rezoned to P1) be credited toward the parkland dedication requirement at subdivision. In response, it is noted that the Comprehensive Development Plan (CDP) reaffirms that the environmentally sensitive areas are not considered to fulfill parkland dedication requirements under s. 510 of the *Local Government Act*.³ Instead, as the City has adequate neighbourhood parkland inventory in the area, a cash-in-lieu contribution will be determined at time of subdivision.

² Salmon Road is classified as an Urban Local road. For design/cross-section details see: [Works and Services Bylaw No. 0120](#), Schedule 9, Drawing No. 115.

³ Upper Glenrosa/Salmon Road Comprehensive Development Plan, s. 5.0 Parks and Open Space, p. 8

The dedication of environmentally sensitive areas is not considered to fulfill parkland dedication requirements because the areas do not constitute useable parcel area in accordance with the *Local Government Act* and the BC Parkland Acquisition Best Practices Guide. However, the applicant will receive credit for the east/west recreational trail connection as the trail is intended to serve an active parkland function (Figure 3). The 1.5 metre wide pedestrian trail is approximately 350 metres in length, which constitutes about a tenth of the total parkland dedication requirement. The remaining parkland dedication requirement will be collected as cash-in-lieu at time of subdivision.

Glenrosa Road Improvements

The applicant expressed concern with the Glenrosa Road improvement requirements and the associated costs (Figure 4). The level and extent of road improvement requirements were considered in the CDP, which undertook to identify the opportunities and constraints of the proposed development. The CDP policy states (s. 9.2, p. 11):

Glenrosa Road will be constructed to full urban arterial standard in accordance with the City of West Kelowna Works and Services Bylaw.

The CDP notes the purpose of the urbanization of Glenrosa Road is to improve safety and address concerns related to increased traffic generated by the proposed development. More recently the City's traffic engineering consultant provided a Safety Performance Review and suggested some modifications to the road design (Attachment 5). In recognition of the site constraints, the following concessions to the urban arterial standard were then provided to the applicant:

- Reduction of the road right of way width from 24.0 metres to 20.0 metres;
- Re-location of the 2.0 metre wide sidewalk to back of curb;
- Re-location of the 2.0 metre wide boulevard to back of sidewalk;

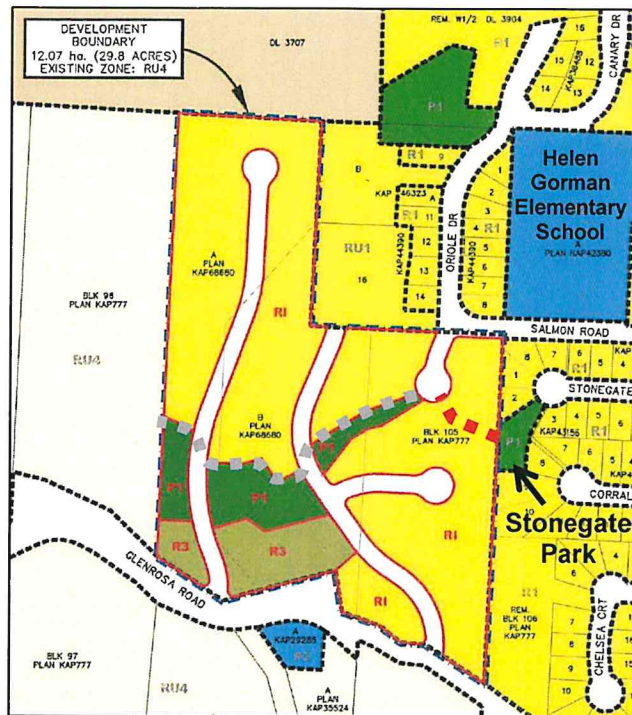


Figure 3 - Map illustrating conceptual pedestrian connections: east/west trail (grey) through lands to be dedicated and pedestrian right of way to Stonegate Park (red)

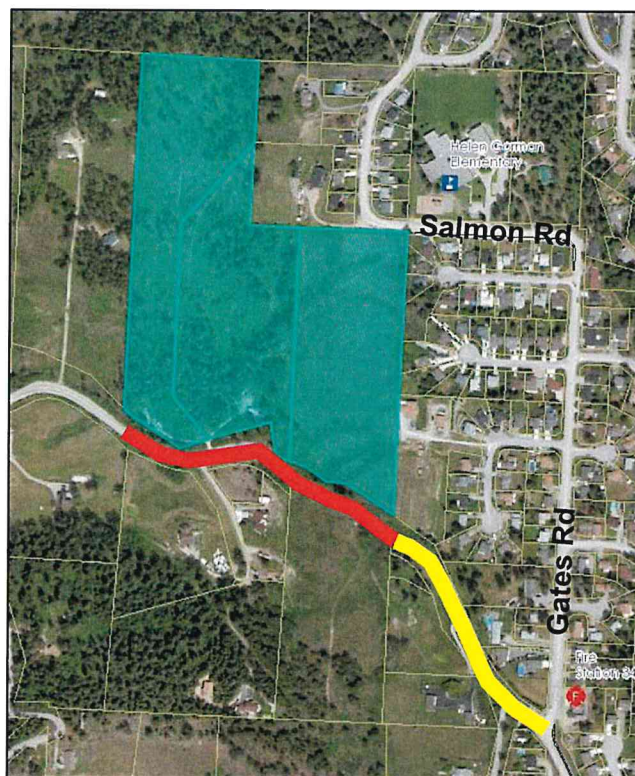


Figure 4 - Map showing extent of Glenrosa Road improvements at property frontage (red) and extended/off-site improvements (yellow)

- Elimination of the street tree requirement from the cross-section;
- Permission for cut and fill slopes and/or retaining walls to be located within road right of way; and
- Increase of the maximum road grade from 8% to 10%.

The arterial road standard has since been further modified to require only curb, gutter, and sidewalk on the north side of Glenrosa Road. Despite all these concessions to the urban arterial road standard, the applicant indicates the cost of the road improvement requirements is approximately \$3.2 million.⁴ The CDP also recognized the costs associated with the urbanization of Glenrosa Road and thus contains the following transportation policy (s. 9.5, p. 11):

Traffic improvements are to be identified at the detailed design stage of development (i.e. rezoning) and any associated costs will be borne by the developer.

The road improvement considerations were included in the CDP based on recommendations in Traffic Impact Assessment and with the objective to ensure roadway connections transition effectively and efficiently between the subject properties and adjacent neighbourhoods. With that in mind, at the detailed design stage, staff will continue to work with the applicant to identify additional opportunities to address the technical challenges in realization that the full width urban arterial road standard may not be feasible for the length of the required road improvements.

Stonegate Park Improvements

The applicant acknowledged that improvements to Stonegate Park would be advantageous for future property owners, but also suggested that any improvements should be voluntary. In response, it is noted that the proposed development anticipates a population increase of 328 residents, which further increases the need for a developed neighbourhood park in the area, and Stonegate Park is directly abutting the proposed development. Moreover, it is a common and standard practice for development to contribute to park improvements, especially when the development increases the demand for such improvements. Improvements to Stonegate Park are thus rational and directly connected to the proposed development. As DCCs cannot be applied to neighbourhood park improvements, the financial burden is either placed on the taxpayer or the developer. In addition, Council resolution has already directed the applicant to assist with the construction of park improvements (C401/17).

In lieu of a proposal for park improvements from the applicant, a modest improvement plan for Stonegate Park has been prepared on their behalf (Attachment 7). The guiding principle for the improvement plan is pedestrian connectivity, as emphasized in the Comprehensive Development Plan (CDP), with Stonegate Park functioning as trail hub (s. 9.4, p. 11). In addition to gravel pathways that connect the subject property to Corral Court



Figure 5 - Streetview image of Stonegate Park (undeveloped)

⁴ At the Public Hearing, the applicant stated that the estimated cost of the required Glenrosa Road improvements is \$3.4 million (for the modified road standard at that time). For comparison, the land value generated through the proposed rezoning (for only the single detached residential portion of the proposal) is estimated to be approximately \$5 million (for un-serviced raw land) and between \$10.3 million and \$13.7 million (for serviced developable land) (Attachment 6).

and Stonegate Court, the improvement plan includes a bike skills/pump track, two benches, fencing and signage. A cost estimate for the improvement plan is approximately \$35,000 (Attachment 7).

There are six municipal parks within 1000 metres walking distance of the subject properties. Including Stonegate Park, four are undeveloped, one is underdeveloped (turf only), and one is a baseball field. As there is no capital identified for neighbourhood park development or improvement, and the Glenrosa neighbourhood has an ample supply of undeveloped parks (Figure 5), staff recommend that park improvements should be a condition of zoning adoption.

Dam Safety

The Ministry of Forests, Lands, Natural Resource Operations, and Rural Development provided referral comments (late items agenda) noting that the subject properties are located in the flood path of Ficke's Pond. The Ministry also noted that the proposed development will change the risk classification and consequence rating of the two dams that create the pond and have significant consequences for the dam owners. Staff later followed up with the Ministry and confirmed that the dam owners, as well as the water license holder, share liability and responsibility for dam breach and inundation risks. It is also the responsibility of the dam owners to have a re-determination of dam classification report prepared annually. As the proximity of development—in this case a residential neighbourhood—to a dam decreases, the consequence rating increases. The classification/rating of the dam (e.g. low, significant, high, very high, extreme consequence) then affects the extent of work and costs involved in the preparation of Dam Safety Reviews.

As downstream economic damage and potential loss of life are factors in the assessment of risk classification, staff recommend a covenant be registered on title to require at time of subdivision a Dam Breach and Inundation Analysis prepared in accordance with the Dam Safety Regulation, *Water Sustainability Act*. The analysis is intended to consider flood protection measures as part of future site re-grading plans and limit potential economic damage and loss of life.

Variance: Water Servicing

As a consideration of the rezoning proposal, the applicant requests a variance (File: DVP 17-15) to the servicing requirements in Works and Services Bylaw No. 0120. More specifically, to meet bylaw requirements, a new reservoir at a higher elevation is required to supply water to the development. As an alternative, however, the applicant proposes to construct a booster station adjacent to the existing reservoir to supply both domestic water and fire flow and will also require standby power to ensure constant functioning and supply.⁵ The recommended motion thus includes authorization to issue the variance permit subject to adoption of the associated zoning.

Public notification for the variance application has been conducted in accordance with the *Local Government Act*. Notification letters (62 letters) were sent to property owners and their tenants within 100 metres of the subject property (Attachment 8). At the time of writing this report, no correspondence from the public has been received.

⁵ Council previously accepted the applicant's proposed alternative to providing water to the development; see: [Council Report, File: CDP 13-01, October 27, 2015](#) (Resolution C374/15) and [Council Report, Files: OCP 12-01 & CDP 13-01, September 20, 2016](#) (Resolution C324/16).

Discussion

On September 20, 2016, Council adopted the Upper Glenrosa/Salmon Road Comprehensive Development Plan (CDP) and associated Official Community Plan (OCP) amendments (Resolution C324/16). The CDP is intended to guide the development process (i.e. rezoning, subdivision, development permit) and thus it contains policies intended to address issues identified in the preliminary planning stages. The issues anticipated from the CDP process include: transportation and general safety issues, the protection of environmentally sensitive areas, and infrastructure/servicing deficiencies.

On August 22, 2017, Council gave first and second readings to the Zoning Amendment Bylaw and directed staff to schedule the Public Hearing pending submission of outstanding plans and designs associated with required Glenrosa Road improvements (design and storm water) and Stonegate Park improvements (Resolution C401/17). No plans or designs have been submitted since first and second readings. Instead, the applicant proposes an alternative to the required improvements to Glenrosa Road and appeals that park improvements at Stonegate Park be voluntary and requests that the land dedication of environmentally sensitive areas be considered to fulfill the 5% parkland dedication requirement at subdivision. The applicant's proposal is inconsistent with the Comprehensive Development Plan, Council direction, and bylaw regulations. Despite these inconsistencies, an alternate motion (#2) is provided as an option for Council to consider as well as to illustrate the applicant's proposal.

COUNCIL REPORT/RESOLUTION HISTORY

Date	Report Topic/Resolution	Resolution No.
February 27, 2018	A Public Hearing was held for the bylaw amendment.	N/A
August 22, 2017	<p>THAT Council give first and second readings to the Zoning Amendment Bylaw; and</p> <p>THAT Council direct staff to schedule the Public Hearing pending submission of the following to the satisfaction of the General Manager of Development Services:</p> <ul style="list-style-type: none">• Proposed design and cross-section for the Glenrosa Road improvements;• Storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities;• Revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard; and• Proposal to assist with the construction of park improvements at Stonegate Park.	C401/17

FINANCIAL IMPLICATIONS

The City will hold the \$35,000 for improvements to Stonegate Park in the Improvement Restricted Reserve fund until the time the park improvements are completed.

ALTERNATE MOTIONS

Four alternate motions have been drafted for Council consideration. Alternate Motion #1 is a staff recommended alternative to postpone consideration of third reading until further details are submitted for the Glenrosa Road improvements. Alternate Motion #2 reflects the applicant's proposal and is not recommended. Alternate Motion #3 postpones consideration pending the outcome of the Council Workshop on the road DCC priorities and Alternate Motion #4 rescinds previous readings and abandons the bylaw. A summary list of the applicant's proposal in comparison to the recommended conditions and requirements is provided in Attachment 9.

Alternate Motion #1 – Postpone Third Reading

THAT Council postpone consideration of third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and

THAT Council direct staff to schedule the bylaw for consideration of third reading conditional upon submission and approval of the following items to the satisfaction of the General Manager of Development Services:

- Proposed designs and cross-sections for the Glenrosa Road improvements (both sides);
- Revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard; and
- Storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities; and
- Commitment of financial assistance in the amount of \$35,000 for the construction of park improvements at Stonegate Park.

Should Council postpone third reading to the bylaw amendment, the application will not be brought forward for consideration of third reading until the aforementioned issues have been satisfactorily addressed.

Alternate Motion #2 – Give Third Reading (Applicant Proposal: Represents No Immediate Improvements to Glenrosa Road (cash-in-lieu at subdivision) & No Contribution to Park Improvements)

THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and

THAT Council direct staff to schedule the bylaw for consideration of adoption conditional upon the following:

Glenrosa Road Improvements

1. Dedications of land to accommodate future improvements to Glenrosa Road;

Parks and Open Space Requirements

2. Dedication of land for the proposed Parks and Open Space (P1) area;
3. Registration of statutory rights of way to establish a pedestrian connection between Stonegate Park and the proposed residential subdivision and an east/west pedestrian trail (if the trail cannot be established through the proposed P1 area);
4. Registration of a covenant to require the construction of a pedestrian trail through the proposed P1 area;

Other Covenants (Staff Recommended)

5. Registration of a covenant to require a \$3,071 per lot/unit contribution to the future reservoir project;
6. Registration of a covenant to require at time of subdivision a Dam Breach and Inundation Analysis prepared in accordance with the Dam Safety Regulation, *Water Sustainability Act*; and

THAT Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezoning application (Zoning Amendment Bylaw No. 0154.56, 2017).

Alternate Motion #3 – Postpone Consideration of Third Reading Pending Completion of Council Workshop on Arterial Road Development Cost Charges Priorities

THAT Council postpone consideration of third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02) pending the outcome of the Council Workshop on the Arterial Road Development Cost Charges Priorities.

Alternate Motion #4 – Rescind Readings and Abandon Bylaw

THAT Council rescind first and second readings to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02) and abandon the bylaw.

Should Council rescind first and second readings to the bylaw amendment, the application will be closed in accordance with Development Applications Procedures Bylaw No. 0131. Council shall not reconsider an application of this nature for the property for a period of six months.

Respectfully submitted,


Darin Schaal
Planner



Bob Dargatz
Development Manager / Approving
Officer


Rob Hillis
Engineering Manager


Brent Magnan
FOR: Planning Manager


Nancy Henderson
General Manager of Development
Services

PowerPoint: x Yes No

Approved for Agenda	
 Jim Zaffino / CAO	<u>May 2, 2018</u> Date

Attachments:

1. Detailed Zoning Conditions
2. Context Map
3. Subject Property Map
4. Rezoning Plan
5. Safety Performance Review, Watt Consulting
6. Land Value Analysis
7. Stonegate Park Improvement Plan and Cost Estimate
8. Public Notification Mailing Area Map
9. Summary List of Applicant Proposal v. Recommended Motion
10. Zoning Amendment Bylaw No. 0154.56

Detailed Zoning Conditions

As the application has progressed to consideration of third reading without satisfactory resolution to numerous development issues, the following list itemizes the requirements that are recommended to be addressed prior to consideration of adoption:

Technical/Design Matters

1. **Submission of designs and cross-sections, cost estimate, and development agreement** for the full width of the Glenrosa Road improvements from and including the subject properties to Gates Road to the satisfaction of the General Manager of Development Services (full width improvements include curb, gutter, sidewalk on the north side of the road and ditch on the south side).
2. **Submission of designs, cost estimate, and development agreement** for all required off-site improvements related to a revised storm water management plan for the Glenrosa Road improvements, including details on storm detention facilities, and a revised servicing report, including design criteria and rationale for Glenrosa Road and any deficiencies associated with the required road standard, to the satisfaction of the General Manager of Development Services.
3. **Submission of a design, cost estimate, and development agreement** for the installation of curb extensions at crosswalk on Salmon Road fronting the elementary school.

Legal Matters

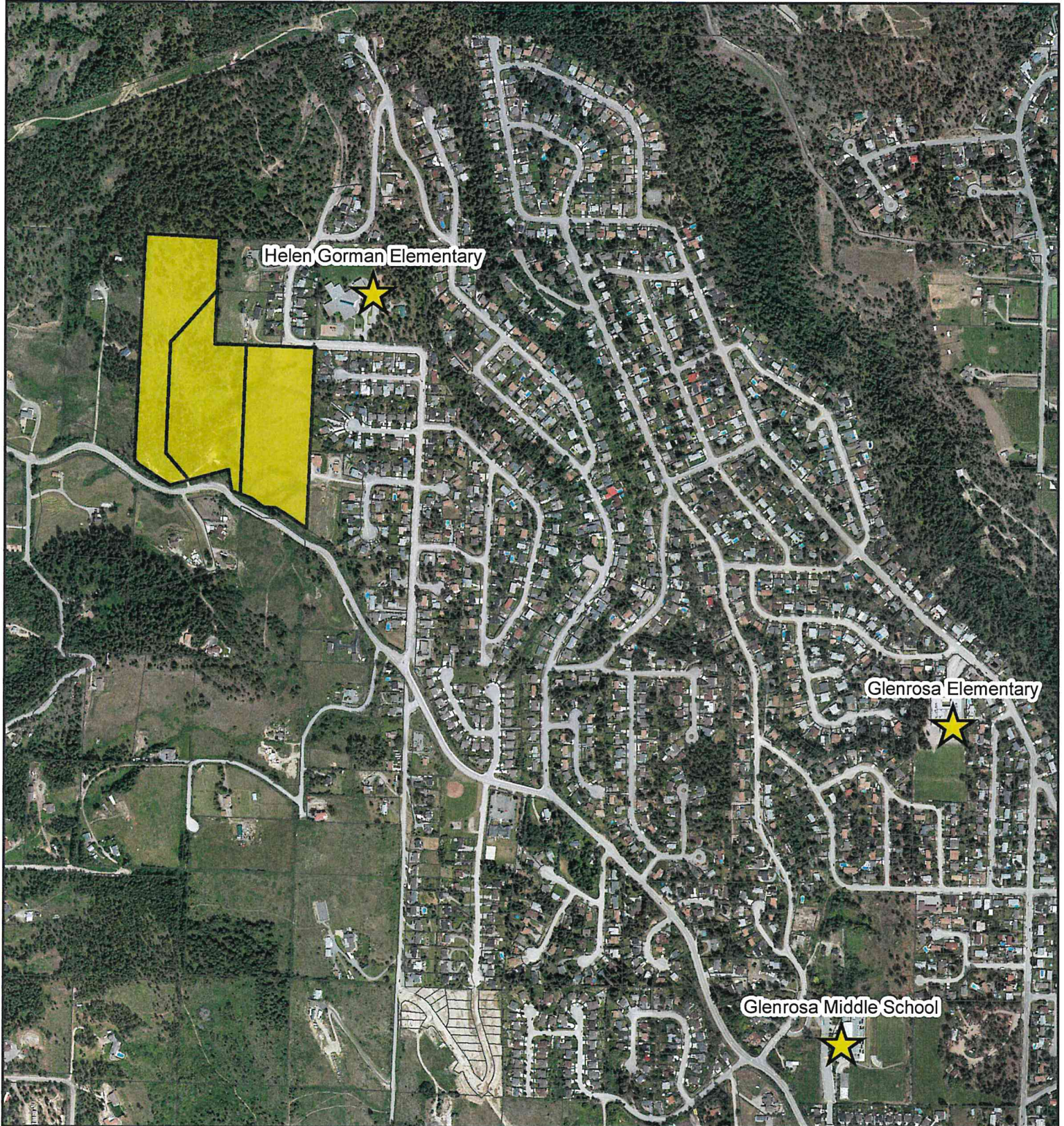
4. **Registration of a covenant** to require the construction of the Glenrosa Road improvements at time of subdivision (both on-site and off-site works in accordance with the Comprehensive Development Plan).
5. **Registration of a covenant** to require the construction of a pedestrian trail through the proposed P1 area at time of subdivision (the agreement may note that the length of the 1.5 metre wide trail through the property will contribute to the parkland dedication requirements in accordance with s. 510 of the *Local Government Act*). A portion of the pedestrian trail may also need to be secured through right of way if it cannot be accommodated within the P1 area (note that a trail cannot be established within the riparian area).
6. **Registration of a covenant** to require at time of subdivision a Dam Breach and Inundation Analysis prepared in accordance with the Dam Safety Regulation, *Water Sustainability Act*.
7. **Registration of a covenant** to require a \$3,071 per lot/unit contribution to the future reservoir project (with single family lot contribution at subdivision and multiple family unit contribution at building permit).
8. **Registration of a statutory right of way** to establish a pedestrian connection between Stonegate Park and the proposed residential subdivision (blanket SRW at zoning with final SRW determined through subdivision approval).
9. **Dedications of land** to accommodate improvements to Glenrosa Road.
10. **Dedication of land** for the proposed Parks and Open Space (P1) area.

Financial Matters

11. **Financial contribution** of \$35,000 to assist with the construction of park improvements at Stonegate Park.



CONTEXT MAP: Z 17-02



File: Z 17-02

Legal Description: Lots A & B, DL 3190, ODYD,
Plan KAP68680 and Block 105, DL 3190, ODYD, Plan 777



Subject Property



District Boundary

0 150 300 600
Metres







SUBJECT PROPERTY: Z 17-02



File: Z 17-02

Legal Description: Lots A & B, DL 3190, ODYD,
Plan KAP68680 and Block 105, DL 3190, ODYD, Plan 777

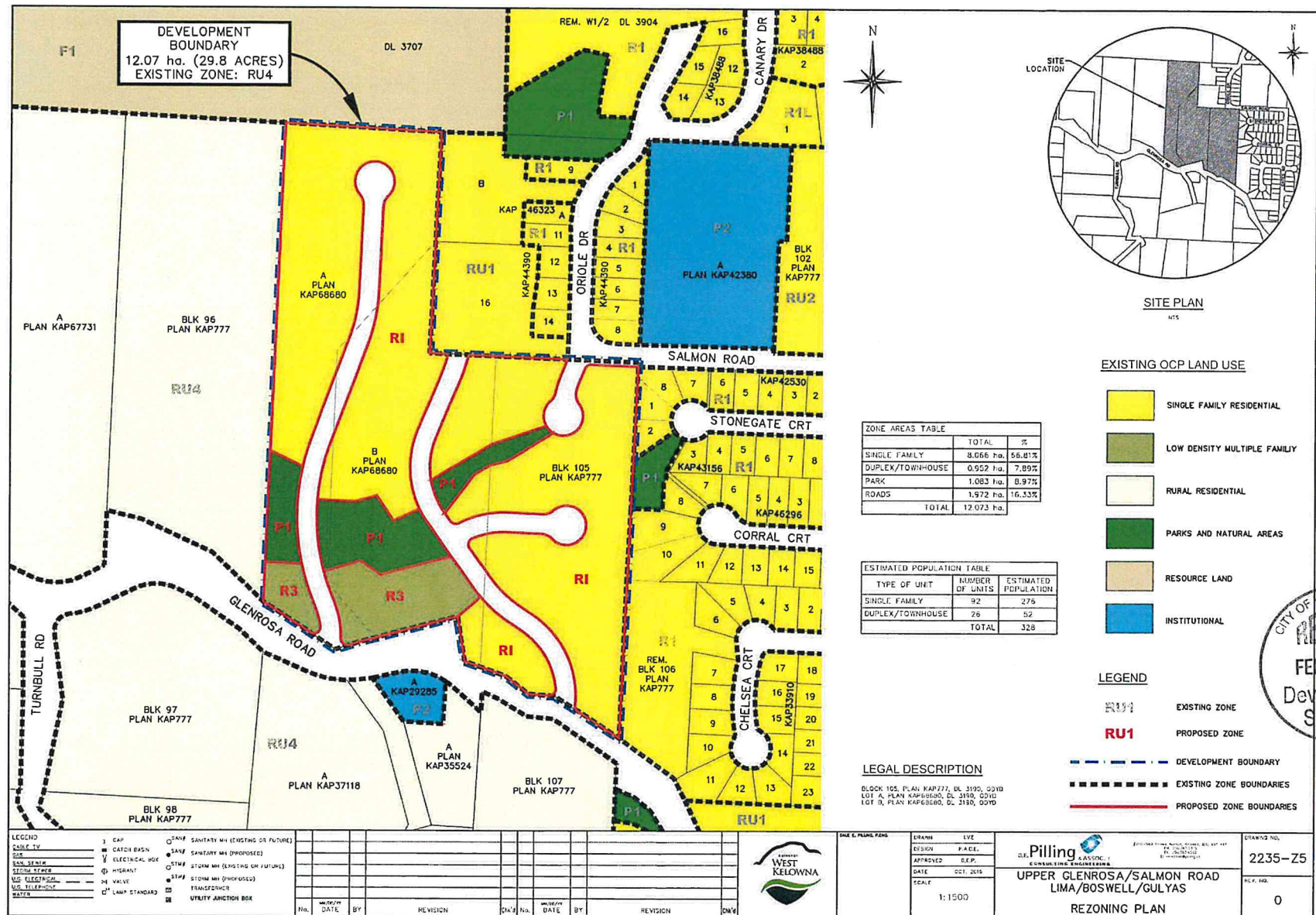
LEGEND

-  Subject Property
-  Zoning Boundary

0 40 80 160
Metres

1:3,000





MEMORANDUM

To: Bob Dargatz – City of West Kelowna
From: Tom Baumgartner, M.Sc., P.Eng.
Our File #: 2115.B01
Project: Upper Glenrosa / Salmon Road
Date: December 20, 2017
RE: Safety Performance

Watt Consulting Group was retained by the City of West Kelowna to scope and review the traffic impact assessment for the Upper Glenrosa / Salmon Road development. This memorandum further assesses:

1. Safety performance of the upper Glenrosa Road segment from Gates Road to Turnbull Road; and
2. The outstanding items from the TIA to the design stage.

1.0 SAFETY PERFORMANCE

The April 2015 *Upper Glenrosa / Salmon Road Traffic Impact Assessment* by Opus International Consultants Ltd (Opus report) identified several design issues with road safety for Glenrosa Road. Specific to the segment from Turnbull Road to Gates Road, they are as follows:

- 2.5m westbound traffic lane width (it was indicated in 2014 that this should be corrected);
- Inconsistent gravel shoulders 0-2.5m wide;
- Winding road alignment;
- Maximum grades of 12.2%;
- Minimum centreline horizontal radii below 185m (52m measured); and
- Deficient vertical curves.

1.1 Collision History

ICBC data from 2006-2015 (10 years) was reviewed for Glenrosa Road from east of Turnbull Road to Gates Road. During this time there were 13 collisions at the intersection of Glenrosa Road / Gates Road and three (3) collisions along the 670m segment of Glenrosa Road from Turnbull Road to Gates Road. The collision statistics are summarized in **Table 1**.

TABLE 1: COLLISION SUMMARY

	Collisions			Collision Frequency (C/Yr)	Collision Rate
	PDO	Injury	Total		
Glenrosa Rd / Gates Rd Intersection	10	3	13	1.3	1.1 C/MEV
Glenrosa Rd: Gates Rd to Turnbull Rd	2	1	3	0.3	1.8 C/MVKT

PDO – Property Damage Only

C/Yr – Collisions per Year

C/MEV – Collisions per Million Entering Vehicles

C/MVKT – Collisions per Million Vehicle Kilometres Travelled

With the increased development volumes from the Opus report, collision frequencies are expected to increase with the trips generated by the proposed development. These are expected to be:

- 1.7 collisions per year at Glenrosa Road / Gates Road (additional 0.4 collisions per year);
- 0.5 collisions per year on Glenrosa Road from Gates Road to Turnbull Road (additional 0.2 collisions per year).

Collision data indicates that a high portion of collisions were due to icy / slippery surface conditions. Equating to 56% of total collisions and 75% of injury collisions. Two of the total injury collisions were related to icy conditions and the vehicle travelling off-road down the embankment. In order to improve road safety, it was recommended that the urbanized update to the road cross-section for the development also improve horizontal and vertical alignment along the roadway.

The Opus report noted that a higher percentage of collisions were attributed to snow / ice conditions with a need to improve winter maintenance and geometry of Glenrosa Road for vehicle recovery under slippery conditions.

Improvements to the corridor design can have the following safety implications to reduce collisions¹:

- Provide Lighting – Improves visibility of road during nighttime to reduce 10%-40% of collisions.
- Flatten Horizontal Curve – 10%-85% reduction in collisions depending on degree of curve.

¹ Source: TAC Canadian Guide to In-service Road Safety Reviews

1.2 Road Design

As the internal road network plan for the development did not provide connectivity, the priority was to improve Glenrosa Road design. The road design was initially requested to meet the City's urban arterial standard anticipating the potential for future connections through to Trepanier. As the existing road rights of way were constrained and there were challenges to tie-in to the existing roadway, the following changes to the design standard were permitted:

- Reduction of road right-of-way from 24m to 20m
- Increase of maximum grade from 8% to 10%
- Reduction of minimum lane width from 3.7m to 3.5m
- Reduction of minimum sidewalk width from 2.0m to 1.8m

2.0 REMAINING TIA ELEMENTS

The Opus report indicated that outstanding items would be addressed in the design stage. Remaining items include:

- More details on collision history and analysis (provided above in this memo);
- Information on linear parkway connection and level of accessibility needs to be provided.
- Identify sidewalk connectivity improvements for development to Salmon Road sidewalk network.

Please contact me at 778-313-1014 (ext 431) if there are any questions or comments. Thank you.

Sincerely,

Watt Consulting Group



Tom Baumgartner, M.Sc., P.Eng.
Transportation Engineer

Land Value Analysis: Proposed R1 Parcels

The estimates in the table below do not account for servicing costs, which are bylaw requirements to develop R1-zoned land.* Servicing costs must be factored into consideration to estimate potential profit.

Average Land Value (R1 Parcels)	Average Lot Size		Average Value per Lot		Proposed No. of Lots	Potential Revenue	Existing Land Value	Estimated Lift in Land Value
\$732,000 / ac	Min.	0.17 ac (703 m ²)	Min.	\$124,000	92	\$11.4 M	\$1,116,080	\$10.3 M
	Max.	0.22 ac (877 m ²)	Max.	\$161,000		\$14.8 M		\$13.7 M

* The applicant has requested a variance to the servicing requirement to provide a new reservoir to supply water to the development

- For comparison, the City's Land Agent estimates the value of the raw, un-serviced R1-zoned land is approximately \$5 million to \$6 million.
- The average land value per acre for R1-zoned properties in the Glenrosa neighbourhood is \$732,000 (based 2017 BC Assessment data from a random sample of eleven properties with an average parcel area of 0.24 acres).
- The average land value per acre for RU4-zoned properties in the Glenrosa neighbourhood is \$56,000 (based 2017 BC Assessment data from a random sample of ten properties with an average parcel area of 10.4 acres)
- The existing land value noted in the table above is based on the average land value for RU4-zoned parcels and the overall parcel area proposed to be rezoned to R1.
- The minimum/maximum lot sizes are based on the Comprehensive Development Plan (CDP) land use plan with a 20% reduction to account for future roads, parks, etc.

Stonegate Park Improvement Plan

Stonegate Court

Subject Properties
(File: Z 17-02)

West Kelowna



Legend

x-x-x Post and Rail Fence

 Park Bench

 Bike Skills Pump Track

 Compacted Gravel Pathway

All disturbed areas to be top-dressed and hydroseeded



Scale 1 : 500

Notes

Stonegate Park Improvement Plan

Stonegate Park

Status: Undeveloped

Type: Natural Area

Area: 0.21 hectares

Future Pedestrian SRW

Trail Hub

Existing
Dedicated
Walkway

Corral
Court

0 13 25 Meters

Map Projection: NAD_1983_UTM_Zone_11N
© City of West Kelowna (2015)

Map Produced on: 3/29/2018 12:59:49 PM



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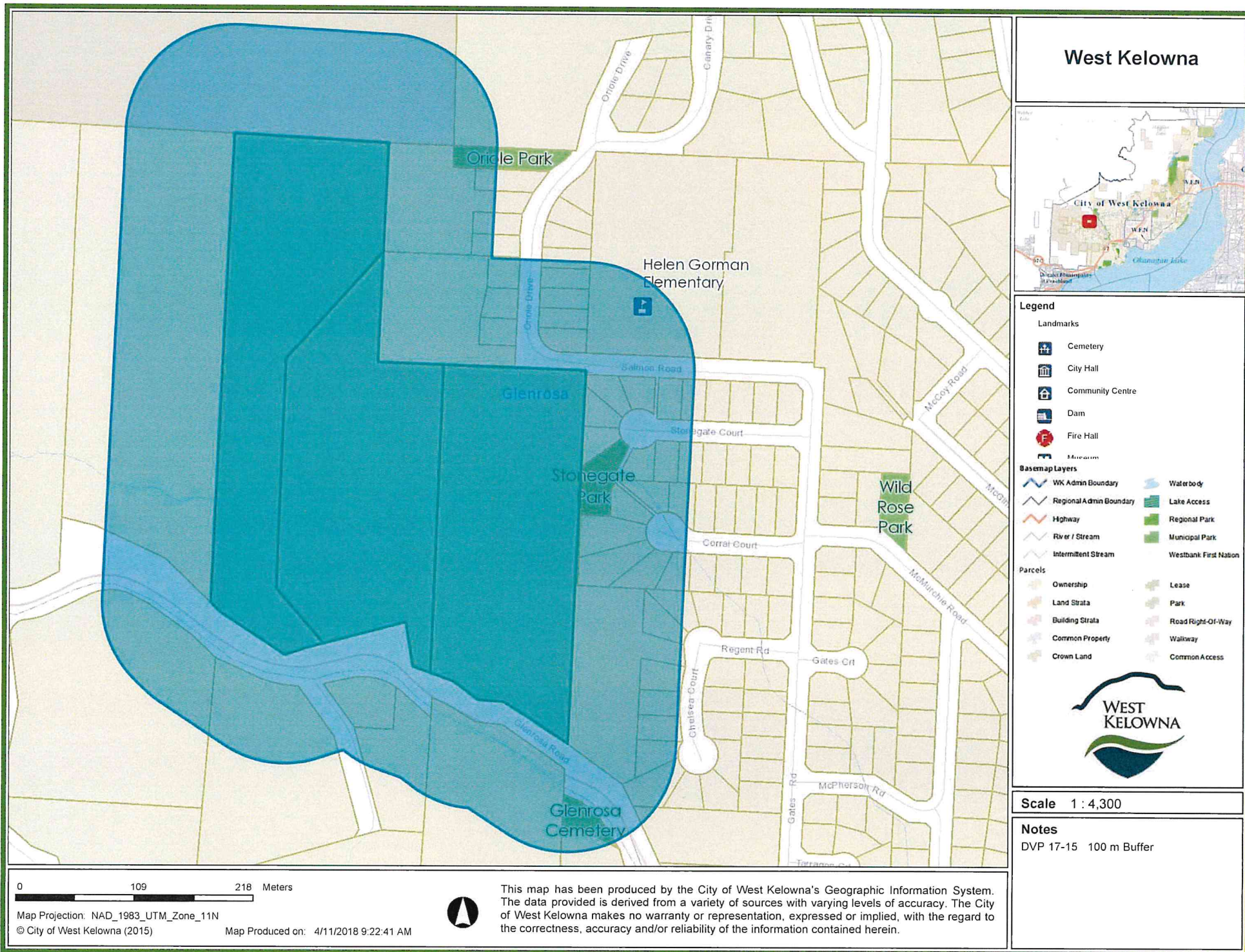
Stonegate Park Improvement Plan

Estimated costs to improve Stonegate Park:

Item	Cost	Quantity	Subtotal	Contingency (15%)	Total
Pathway, compacted gravel	\$104/metre	80 metres	\$8,320	\$1,248	\$9,568
Bike skills/pump track	\$10,000 to \$15,000	1	\$12,500	\$1,875	\$14,375
Restoration, top dressing and hydroseeding	\$3/m ²	400 m ²	\$1,200	\$180	\$1,380
Fencing, post and rail	\$3,000	1	\$3,000	\$450	\$3,450
Signage	\$1,500	1	\$1,500	\$225	\$1,725
Benches	\$2,200	2	\$4,400	\$660	\$5,060
TOTAL COST			\$30,920	\$4,638	\$35,558

Upper Glenrosa Park Analysis

Park Name	Park Type	Status	Distance (via public road or trail)	Area (m ²)
Stonegate Park	Natural Area	Undeveloped	0 metres	2110
Wild Rose Park	Natural Area	Undeveloped	300 metres	1600
Oriole Park	Neighbourhood	Undeveloped	130 metres	2030
McMorland Park	Neighbourhood	Underdeveloped (turf only)	700 metres	1830
Ranch Park	Baseball	Athletic	600 metres	8100
Glenrosa Park	Neighbourhood	Undeveloped	1000 metres	5860



**Summary List: Applicant Proposal v.
Development Requirements & Recommended Conditions**

Item	Applicant Proposal	City Standard
Parkland dedication / ESA	<ul style="list-style-type: none"> • P1 area to be dedicated = 9% of properties • No additional dedication at subdivision 	<ul style="list-style-type: none"> • P1 area dedicated as ESA • ESA not creditable for parkland dedication requirement at subdivision
East/west pedestrian connection	<ul style="list-style-type: none"> • Credit for ESA and pedestrian trail 	<ul style="list-style-type: none"> • Credit toward parkland dedication
Glenrosa Road improvements	<ul style="list-style-type: none"> • Cash-in-lieu for frontage works • Half width • Not extended to Gates Road (only for parcel frontage) 	<ul style="list-style-type: none"> • Modified arterial standard (site specific concessions supported) • Full width • Extended (off-site) works to Gates Road
DCC program	<ul style="list-style-type: none"> • Extend DCC road status for Glenrosa Road from Gates Road past subject properties 	<ul style="list-style-type: none"> • DCC program applies as is; extending DCC road status requires: <ul style="list-style-type: none"> ○ Re-examination of DCC program ○ Amendment to DCC Bylaw (with approval from BC Inspector of Municipalities) ○ Multi-year process
Booster station	<ul style="list-style-type: none"> • Instead of new reservoir 	<ul style="list-style-type: none"> • Council support/approval of variance required • Does not meet Works and Services Bylaw No. 0120
Glenrosa Road sidewalks (between McIver & McGinnis)	<ul style="list-style-type: none"> • Install only sidewalks between McIver and McGinnis ahead of DCC project 	<ul style="list-style-type: none"> • Existing DCC project in DCC program • Not cost effective or efficient to install only sidewalks (not curb or gutter) • No DCC credits for only sidewalks

CITY OF WEST KELOWNA

BYLAW NO. 0154.56

A BYLAW TO AMEND "ZONING BYLAW NO. 0154"

WHEREAS the Council of the City of West Kelowna desires to amend "CITY OF WEST KELOWNA ZONING BYLAW NO. 0154" under the provisions of the *Local Government Act*.

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna, in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as "CITY OF WEST KELOWNA ZONING AMENDMENT BYLAW NO. 0154.56, 2017".

2. Amendments

"Zoning Bylaw No. 0154" is hereby amended as follows:

- 2.1 By changing the zoning on Lots A & B, District Lot 3190, ODYD, Plan KAP 68680 and Block 105, District Lot 3190, ODYD, Plan 777 as shown on Schedule 'A' attached to and forming part of this bylaw from Rural Residential Large Parcel (RU4) to Low Density Multiple Residential (R3), Single Detached Residential (R1), and Parks and Open Space (P1).
- 2.2 By depicting the change on "Zoning Bylaw No. 0154 Schedule B" (Zoning Bylaw Map).

READ A FIRST AND SECOND TIME THIS 22ND DAY OF AUGUST, 2017
PUBLIC HEARING HELD THIS 27TH DAY OF FEBRUARY, 2018
READ A THIRD TIME
ADOPTED

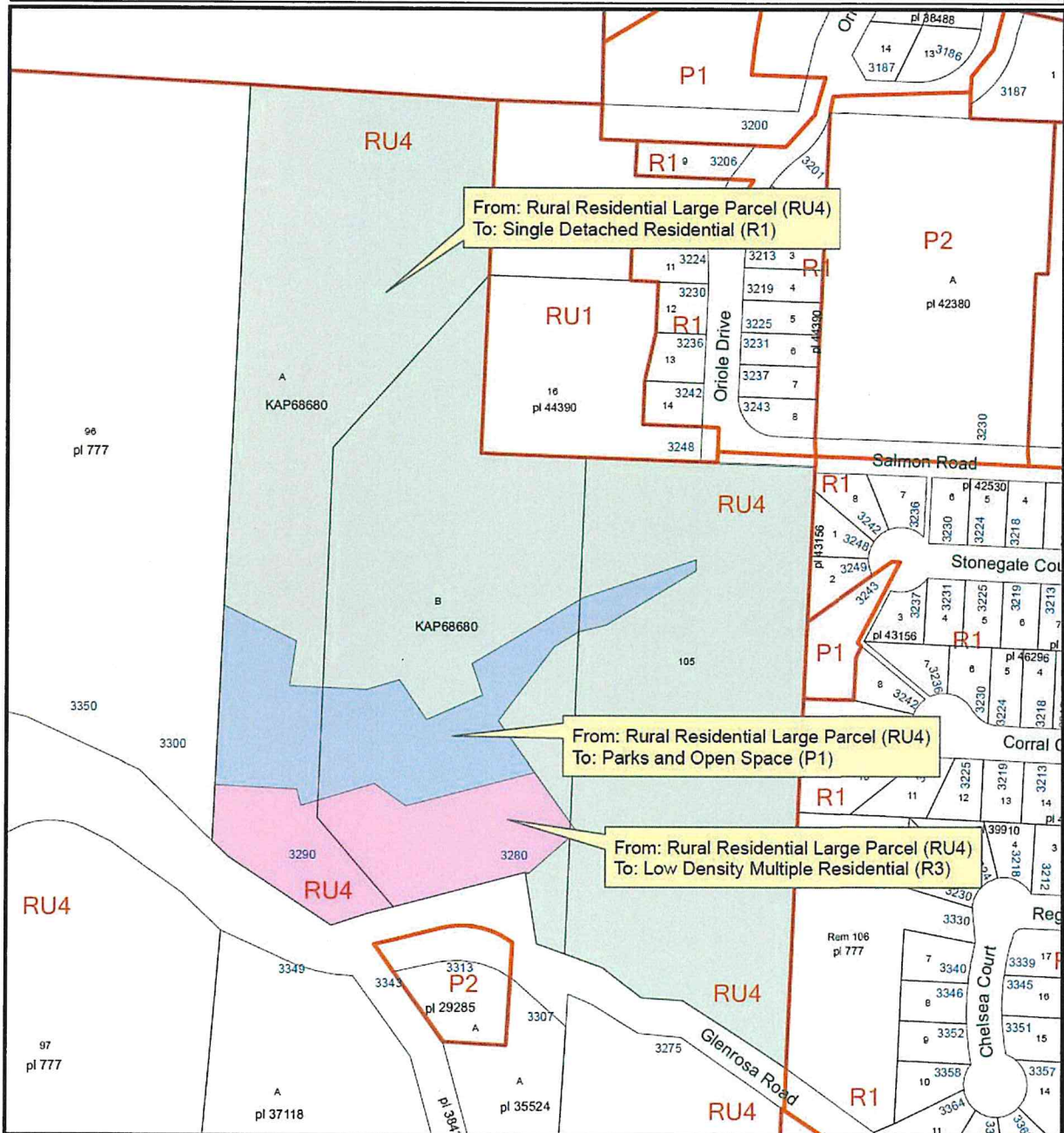
MAYOR

CITY CLERK



CITY OF
WEST
KELOWNA

SCHEDULE 'A' of BYLAW NO. 0154.56



LEGEND

- From RU4 to R3
- From RU4 to P1
- From RU4 to R1
- Zoning Boundary

0 45 90 180 Metres

1:3,000



Date: 24/08/2017

City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

April 30, 2018
File: 2235
CoWK: Z 17-02

Attention: Darin Schaal, Planner III

Dear Mr. Schaal,

**Re: Upper Glenrosa/Salmon Road – Zoning Bylaw Amendment and Development
Variance Permit Application – Lot A & B, KAP68680 and Block 105, KAP777, ODYD**

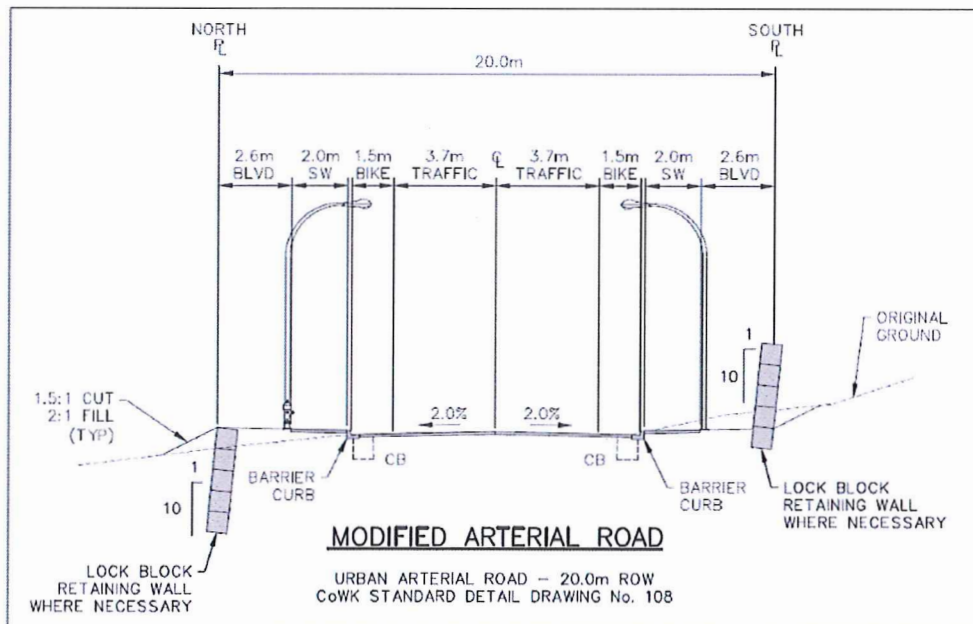
Regarding your report to Council for 3rd Reading, please find attached five construction estimates pertaining to various Glenrosa Road designs discussed with Staff.

It is important to note the following:

- These estimates are based on preliminary design and although our linework is considered accurate, the actual storm sewer size and detention requirements could vary greatly depending on the storm model and Environmental requirements.
- Jason Schleppe with Ecoscape Environmental Consultants provided the following advice regarding Staff's requirement to relocate the existing stream on the south side of Glenrosa Road to a piped system. Initially, he said that this is not a process that should be undertaken by a developer. He indicated that it would be an extensive process that the Municipality should be spearheading. Upon further review, Jason's professional opinion is that the process will take at least 6 months, involve First Nations, cost \$10,000 and that piping is unlikely to get approved.
- The land acquisition was estimated based on the 2017 BC Land Assessment values. Actual costs are expected to inflate significantly during negotiations.
- A Geotechnical Investigation has not been completed for Glenrosa Road. As such, rock cut volumes were estimated based on a visual assessment of existing conditions.
- Costs for shallow utilities and traffic control was not included.
- As per Staff's request, the Half Modified Arterial Road has been divided into two estimates specifying Frontage Works and Offsite Works separately. This information has been provided for discussion purposes only. Due to the current topography and arterial geometry requirements, the road design ties into the existing Glenrosa Road location and elevation at the beginning/end only. As such, these works cannot be constructed separately.

Below is a summary of each construction estimate including a location map and road standard:

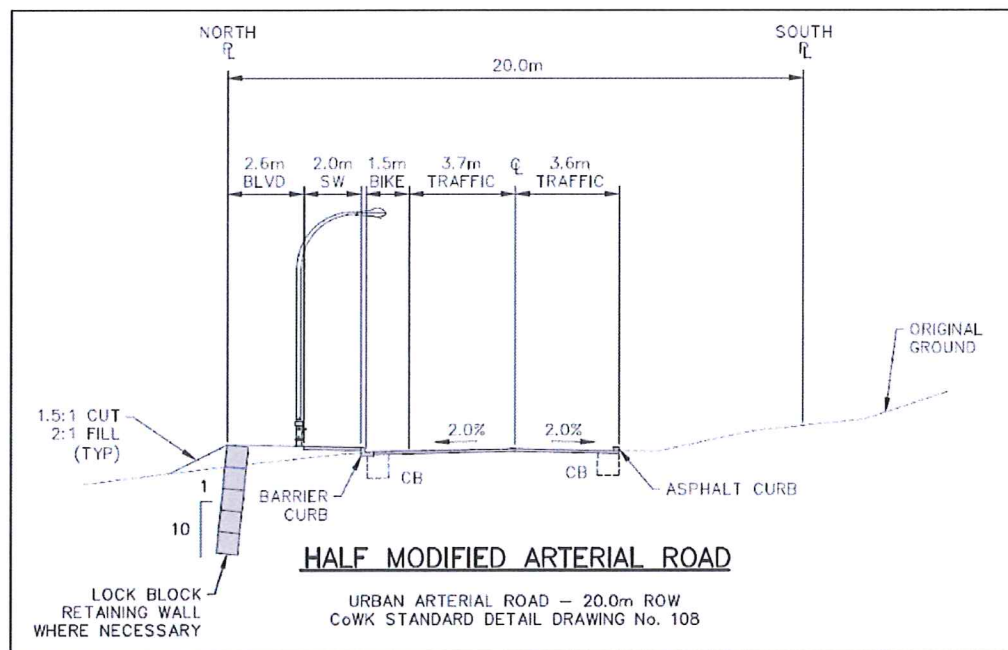
Modified Arterial Road



Modified Arterial Road Estimate

\$3,385,278.86

Half Modified Arterial Road

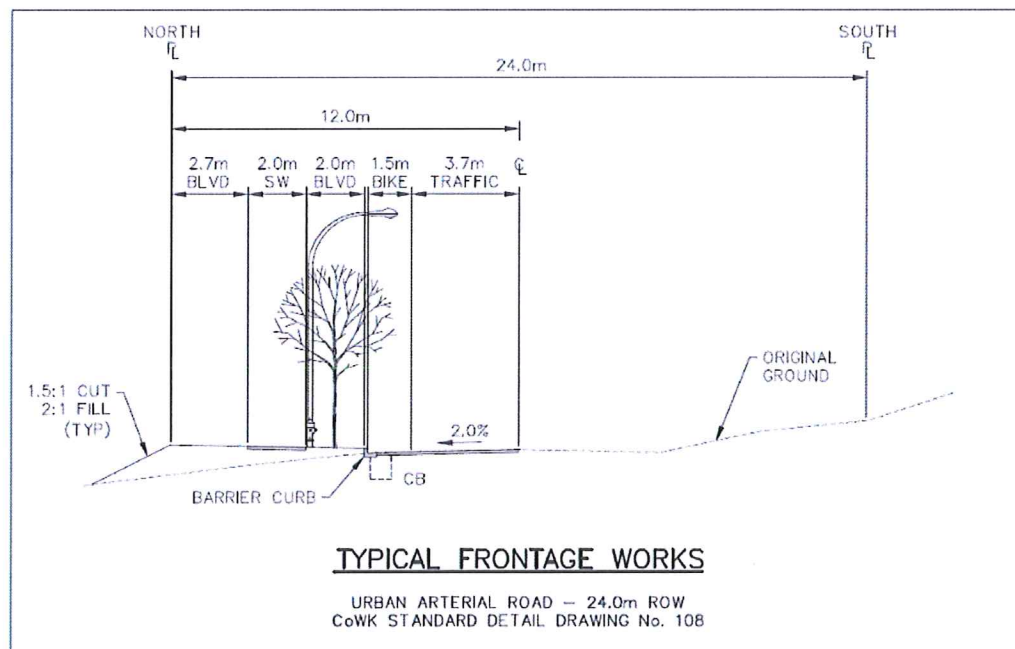


Half Modified Arterial Road Estimate \$2,793,100.68

Frontage Only \$1,378,166.27

Offsite Only \$1,414,939.29

Typical Frontage Works



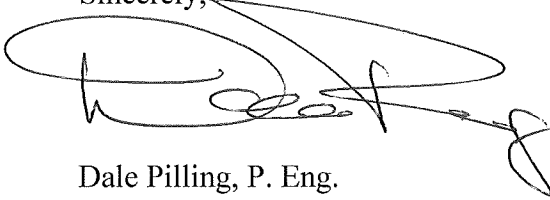
Typical Frontage Works Estimate

\$770,022.44

Closing

We trust this is what you require to proceed to Council for consideration in the timeliest manner. If you require anything further, please do not hesitate to contact us.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dale Pilling', with a large, sweeping loop at the end.

Dale Pilling, P. Eng.

C.c. Joe & Bernadette Lima
Keith & Gail Boswell
Les & Gail Gulyas

**Upper Glenrosa/Salmon Road
 Glenrosa Road
 Modified Arterial Road
 Construction Estimate
 April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
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Part 1. Earth Works

1.	Clearing and grubbing including tree removal	13,393	m ²	\$ 5.00	\$ 66,965.00
2.	Topsoil stripping including onsite stockpile and re-use	2,370	m ³	\$ 11.50	\$ 27,255.00
3.	Common excavation, cut to fill	5,049	m ³	\$ 10.00	\$ 50,490.00
4.	Rock cut	1,499	m ³	\$ 31.00	\$ 46,469.00
5.	Placement of rock as fill material	1,959	m ³	\$ 8.00	\$ 15,672.00
6.	Import and placement of structural fill material	7,038	m ³	\$ 25.00	\$ 175,950.00
7.	Supply and install lock block retaining wall	1,343	m ²	\$ 400.00	\$ 537,200.00

Subtotal Part 1					\$ 920,001.00
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Part 2. Road Works

1.	Sawcut, remove and dispose existing asphalt & curb	5,317	m ²	\$ 3.85	\$ 20,470.45
2.	Supply and place 150mm crush base course - 300mm depth - roads	8,198	m ²	\$ 12.20	\$ 100,015.60
3.	Supply and place 75mm crush base course - 150mm depth - roads	7,553	m ²	\$ 7.99	\$ 60,348.47
4.	Supply and place 25mm crush base course - 150mm depth - roads	7,553	m ²	\$ 7.99	\$ 60,348.47
5.	Supply and place asphaltic pavements - 75mm depth - roads	7,553	m ²	\$ 23.85	\$ 180,139.05
6.	Supply and place 0.45m barrier style curb and gutter c/w base gravels and curb prep.	1,333	lm	\$ 91.05	\$ 121,369.65
7.	Supply and place 2.00m sidewalk c/w base gravels and curb prep.	1,333	lm	\$ 171.25	\$ 228,276.25

Subtotal Part 2					\$ 770,967.94
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Part 3. Watermains

1.	Supply and install watermain pipe - 300mmØ C900 PVC	243	lm	\$ 500.00	\$ 121,500.00
2.	Supply and install valves and fittings including thrust blocks, valve boxes and risers - 300x300x150mmØ HHF tee, 1-150mm HF gate valve and hydrant assembly c/w hydrant lead	4	ea.	\$ 10,000.00	\$ 40,000.00

Subtotal Part 3					\$ 161,500.00
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Part 4. Storm Sewer

1.	Supply and install storm sewer pipe - 300mmØ ultra-rib PVC	673	lm	\$ 270.00	\$ 181,710.00
2.	Supply and install catch basin leads - 250mmØ SDR35 PVC	10	lm	\$ 140.00	\$ 1,400.00
	- 200mmØ SDR35 PVC	230	lm	\$ 120.00	\$ 27,600.00

**Upper Glenrosa/Salmon Road
Glenrosa Road
Modified Arterial Road
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
3. Supply and install storm sewer manhole including base, lid, frame and cover				
- 1050mmØ	12	ea.	\$ 2,800.00	\$ 33,600.00
- 1050mmØ solid barrel sections	24	vm	\$ 800.00	\$ 19,200.00
4. Supply and install catch basins				
- 900mmØ side inlet	23	ea.	\$ 2,000.00	\$ 46,000.00
- 900mmØ twin inlet	1	ea.	\$ 3,600.00	\$ 3,600.00
5. Storm detention system	210	m ³	\$ 1,000.00	\$ 210,000.00
Subtotal Part 4				\$ 523,110.00

Part 5. Miscellaneous				
1. Supply and install street lights and bases c/w all trenching, conduit, bedding and backfill	33	ea.	\$ 5,950.00	\$ 196,350.00
2. Topsoil & hydroseed	6,880	m ²	\$ 5.00	\$ 34,400.00
3. Fencing	392	lm	\$ 107.00	\$ 41,944.00
4. Land Acquisition	1	LS	\$ 17,836.83	\$ 17,836.83
5. Environmental	1	LS	\$ 10,000.00	\$ 10,000.00
Subtotal Part 5				\$ 300,530.83

Construction Cost Estimate: Limitation of Liability

The parties agree that the D.E. Pilling & Associates Ltd. cannot and does not warrant or represent that bids of negotiated prices will not vary from the Construction Cost Estimate. The parties further agree that nothing in their agreement shall be deemed to be a cost condition or representation that the project can be completed for the amount of the Construction Cost Estimate or any other amount, and the Client expressly waives its right to withhold the D. E. Pilling & Associates Ltd.'s fees, whether in whole or in action or bring any other proceedings in any court of law against the D.E. Pilling & Associates Ltd. in connection with advise or information relating to the Construction Cost Estimate, whether in contract, tort or otherwise.

Hard Cost Subtotal	\$ 2,676,109.77
Engineering (10%)	\$ 267,610.98
Subtotal	\$ 2,943,720.75
Contingency (15%)	\$ 441,558.11
Total Construction Estimate	\$ 3,385,278.86

**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
Part 1. Earth Works				
1. Clearing and grubbing including tree removal	9,225	m ²	\$ 5.00	\$ 46,125.00
2. Topsoil stripping including onsite stockpile and re-use	1,840	m ³	\$ 11.50	\$ 21,160.00
3. Common excavation, cut to fill	2,535	m ³	\$ 10.00	\$ 25,350.00
4. Rock cut	1,086	m ³	\$ 31.00	\$ 33,666.00
5. Placement of rock as fill material	1,412	m ³	\$ 8.00	\$ 11,296.00
6. Import and placement of structural fill material	10,515	m ³	\$ 25.00	\$ 262,875.00
7. Supply and install lock block retaining wall	955	m ²	\$ 400.00	\$ 382,000.00
Subtotal Part 1				\$ 782,472.00
Part 2. Road Works				
1. Sawcut, remove and dispose existing asphalt & curb	5,286	m ²	\$ 3.85	\$ 20,351.10
2. Supply and place 150mm crush base course - 300mm depth - roads	7,405	m ²	\$ 12.20	\$ 90,341.00
3. Supply and place 75mm crush base course - 150mm depth - roads	6,281	m ²	\$ 7.99	\$ 50,185.19
4. Supply and place 25mm crush base course - 150mm depth - roads	6,281	m ²	\$ 7.99	\$ 50,185.19
5. Supply and place asphaltic pavements - 75mm depth - roads	6,281	m ²	\$ 23.85	\$ 149,801.85
6. Supply and place 0.45m barrier style curb and gutter c/w base gravels and curb prep.	678	lm	\$ 91.05	\$ 61,731.90
7. Supply and place 0.15m asphalt curb c/w base gravels and curb prep.	706	lm	\$ 56.50	\$ 39,889.00
8. Supply and place 2.00m sidewalk c/w base gravels and curb prep.	678	lm	\$ 171.25	\$ 116,107.50
Subtotal Part 2				\$ 578,592.73
Part 3. Watermains				
1. Supply and install watermain pipe - 300mmØ C900 PVC	243	lm	\$ 500.00	\$ 121,500.00
2. Supply and install valves and fittings including thrust blocks, valve boxes and risers				
- 300x300x150mmØ HHF tee, 1-150mm HF gate valve and hydrant assembly c/w hydrant lead	4	ea.	\$ 10,000.00	\$ 40,000.00
Subtotal Part 3				\$ 161,500.00
Part 4. Storm Sewer				
1. Supply and install storm sewer pipe - 300mmØ ultra-rib PVC	673	lm	\$ 270.00	\$ 181,710.00

**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
2. Supply and install catch basin leads				
- 250mmØ SDR35 PVC	10	lm	\$ 140.00	\$ 1,400.00
- 200mmØ SDR35 PVC	213	lm	\$ 120.00	\$ 25,560.00
3. Supply and install storm sewer manhole including base, lid, frame and cover				
- 1050mmØ	12	ea.	\$ 2,800.00	\$ 33,600.00
- 1050mmØ solid barrel sections	24	vm	\$ 800.00	\$ 19,200.00
4. Supply and install catch basins				
- 900mmØ side inlet	23	ea.	\$ 2,000.00	\$ 46,000.00
- 900mmØ twin inlet	1	ea.	\$ 3,600.00	\$ 3,600.00
5. Storm detention system	210	m³	\$ 1,000.00	\$ 210,000.00
Subtotal Part 4				\$ 521,070.00

Part 5. Miscellaneous

1. Supply and install street lights and bases c/w all trenching, conduit, bedding and backfill	17	ea.	\$ 5,950.00	\$ 101,150.00
2. Topsoil & hydroseed	5,094	m²	\$ 5.00	\$ 25,470.00
3. Fencing	275	lm	\$ 107.00	\$ 29,425.00
4. Land Acquisition	1	LS	\$ 8,305.00	\$ 8,305.00
Subtotal Part 5				\$ 164,350.00

Construction Cost Estimate: Limitation of Liability

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Hard Cost Subtotal	\$ 2,207,984.73
Engineering (10%)	\$ 220,798.47
Subtotal	\$ 2,428,783.20
Contingency (15%)	\$ 364,317.48
Total Construction Estimate	\$ 2,793,100.68

**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road - Frontage Only
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
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Part 1. Earth Works

1. Clearing and grubbing including tree removal	6,596	m ²	\$ 5.00	\$ 32,980.00
2. Topsoil stripping including onsite stockpile and re-use	1,319	m ³	\$ 11.50	\$ 15,168.50
3. Common excavation, cut to fill	1,097	m ³	\$ 10.00	\$ 10,970.00
4. Rock cut	470	m ³	\$ 31.00	\$ 14,570.00
5. Placement of rock as fill material	611	m ³	\$ 8.00	\$ 4,888.00
6. Import and placement of structural fill material	9,156	m ³	\$ 25.00	\$ 228,900.00
Subtotal Part 1				\$ 307,476.50

Part 2. Road Works

1. Sawcut, remove and dispose existing asphalt & curb	2,831	m ²	\$ 3.85	\$ 10,899.35
2. Supply and place 150mm crush base course - 300mm depth - roads	3,990	m ²	\$ 12.20	\$ 48,678.00
3. Supply and place 75mm crush base course - 150mm depth - roads	3,365	m ²	\$ 7.99	\$ 26,886.35
4. Supply and place 25mm crush base course - 150mm depth - roads	3,365	m ²	\$ 7.99	\$ 26,886.35
5. Supply and place asphaltic pavements - 75mm depth - roads	3,365	m ²	\$ 23.85	\$ 80,255.25
6. Supply and place 0.45m barrier style curb and gutter c/w base gravels and curb prep.	364	lm	\$ 91.05	\$ 33,142.20
7. Supply and place 0.15m asphalt curb c/w base gravels and curb prep.	397	lm	\$ 56.50	\$ 22,430.50
8. Supply and place 2.00m sidewalk c/w base gravels and curb prep.	364	lm	\$ 171.25	\$ 62,335.00
Subtotal Part 2				\$ 311,513.00

Part 3. Watermains

1. Supply and install watermain pipe - 300mmØ C900 PVC	174	lm	\$ 500.00	\$ 87,000.00
2. Supply and install valves and fittings including thrust blocks, valve boxes and risers - 300x300x150mmØ HHF tee, 1-150mm HF gate valve and hydrant assembly c/w hydrant lead	3	ca.	\$ 10,000.00	\$ 30,000.00
Subtotal Part 3				\$ 117,000.00

Part 4. Storm Sewer

1. Supply and install storm sewer pipe - 300mmØ ultra-rib PVC	365	lm	\$ 270.00	\$ 98,550.00
2. Supply and install catch basin leads - 200mmØ SDR35 PVC	108	lm	\$ 120.00	\$ 12,960.00

**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road - Frontage Only
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
3. Supply and install storm sewer manhole including base, lid, frame and cover				
- 1050mmØ	6	ea.	\$ 2,800.00	\$ 16,800.00
- 1050mmØ solid barrel sections	12	vm	\$ 800.00	\$ 9,600.00
4. Supply and install catch basins				
- 900mmØ side inlet	12	ea.	\$ 2,000.00	\$ 24,000.00
5. Storm detention system	113	m ³	\$ 1,000.00	\$ 113,000.00
Subtotal Part 4				\$ 274,910.00

Part 5. Miscellaneous				
1. Supply and install street lights and bases c/w all trenching, conduit, bedding and backfill	9	ea.	\$ 5,950.00	\$ 53,550.00
2. Topsoil & hydroseed	3,341	m ²	\$ 5.00	\$ 16,705.00
3. Land Acquisition	1	LS	\$ 8,305.00	\$ 8,305.00
Subtotal Part 5				\$ 78,560.00

Construction Cost Estimate: Limitation of Liability

The parties agree that the D.E. Pilling & Associates Ltd. cannot and does not warrant or represent that bids of negotiated prices will not vary from the Construction Cost Estimate. The parties further agree that nothing in their agreement shall be deemed to be a cost condition or representation that the project can be completed for the amount of the Construction Cost Estimate or any other amount, and the Client expressly waives its right to withhold the D. E. Pilling & Associates Ltd.'s fees, whether in whole or in action or bring any other proceedings in any court of law against the D.E. Pilling & Associates Ltd. in connection with advise or information relating to the Construction Cost Estimate, whether in contract, tort or otherwise.

Hard Cost Subtotal	\$ 1,089,459.50
Engineering (10%)	\$ 108,945.95
Subtotal	\$ 1,198,405.45
Contingency (15%)	\$ 179,760.82
Total Construction Estimate	\$ 1,378,166.27

**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road - Offsite Only
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
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Part 1. Earth Works

1.	Clearing and grubbing including tree removal	2,629	m ²	\$ 5.00	\$ 13,145.00
2.	Topsoil stripping including onsite stockpile and re-use	521	m ³	\$ 11.50	\$ 5,991.50
3.	Common excavation, cut to fill	1,438	m ³	\$ 10.00	\$ 14,380.00
4.	Rock cut	616	m ³	\$ 31.00	\$ 19,096.00
5.	Placement of rock as fill material	801	m ³	\$ 8.00	\$ 6,408.00
6.	Import and placement of structural fill material	1,359	m ³	\$ 25.00	\$ 33,975.00
7.	Supply and install lock block retaining wall	955	m ²	\$ 400.00	\$ 382,000.00

Subtotal Part 1

\$ 474,995.50

Part 2. Road Works

1.	Sawcut, remove and dispose existing asphalt & curb	2,456	m ²	\$ 3.85	\$ 9,455.60
2.	Supply and place 150mm crush base course - 300mm depth - roads	3,415	m ²	\$ 12.20	\$ 41,663.00
3.	Supply and place 75mm crush base course - 150mm depth - roads	2,916	m ²	\$ 7.99	\$ 23,298.84
4.	Supply and place 25mm crush base course - 150mm depth - roads	2,916	m ²	\$ 7.99	\$ 23,298.84
5.	Supply and place asphaltic pavements - 75mm depth - roads	2,916	m ²	\$ 23.85	\$ 69,546.60
6.	Supply and place 0.45m barrier style curb and gutter c/w base gravels and curb prep.	314	lm	\$ 91.05	\$ 28,589.70
7.	Supply and place 0.15m asphalt curb c/w base gravels and curb prep.	309	lm	\$ 56.50	\$ 17,458.50
8.	Supply and place 2.00m sidewalk c/w base gravels and curb prep.	314	lm	\$ 171.25	\$ 53,772.50

Subtotal Part 2

\$ 267,083.58

Part 3. Watermains

1.	Supply and install watermain pipe - 300mmØ C900 PVC	69	lm	\$ 500.00	\$ 34,500.00
2.	Supply and install valves and fittings including thrust blocks, valve boxes and risers - 300x300x150mmØ HHF tee, 1-150mm HF gate valve and hydrant assembly c/w hydrant lead	1	ea.	\$ 10,000.00	\$ 10,000.00

Subtotal Part 3

\$ 44,500.00

Part 4. Storm Sewer

1.	Supply and install storm sewer pipe - 300mmØ ultra-rib PVC	308	lm	\$ 270.00	\$ 83,160.00
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**Upper Glenrosa/Salmon Road
Glenrosa Road
Half Modified Arterial Road - Offsite Only
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
2. Supply and install catch basin leads				
- 250mmØ SDR35 PVC	10	lm	\$ 140.00	\$ 1,400.00
- 200mmØ SDR35 PVC	105	lm	\$ 120.00	\$ 12,600.00
3. Supply and install storm sewer manhole including base, lid, frame and cover				
- 1050mmØ	6	ea.	\$ 2,800.00	\$ 16,800.00
- 1050mmØ solid barrel sections	12	vm	\$ 800.00	\$ 9,600.00
4. Supply and install catch basins				
- 900mmØ side inlet	11	ea.	\$ 2,000.00	\$ 22,000.00
- 900mmØ twin inlet	1	ea.	\$ 3,600.00	\$ 3,600.00
5. Storm detention system	97	m³	\$ 1,000.00	\$ 97,000.00
Subtotal Part 4				\$ 246,160.00

Part 5. Miscellaneous				
1. Supply and install street lights and bases c/v all trenching, conduit, bedding and backfill	8	ea.	\$ 5,950.00	\$ 47,600.00
2. Topsoil & hydroseed	1,753	m²	\$ 5.00	\$ 8,765.00
3. Fencing	275	lm	\$ 107.00	\$ 29,425.00
Subtotal Part 5				\$ 85,790.00

Construction Cost Estimate: Limitation of Liability

The parties agree that the D.E. Pilling & Associates Ltd. cannot and does not warrant or represent that bids of negotiated prices will not vary from the Construction Cost Estimate. The parties further agree that nothing in their agreement shall be deemed to be a cost condition or representation that the project can be completed for the amount of the Construction Cost Estimate or any other amount, and the Client expressly waives its right to withhold the D. E. Pilling & Associates Ltd.'s fees, whether in whole or in action or bring any other proceedings in any court of law against the D.E. Pilling & Associates Ltd. in connection with advise or information relating to the Construction Cost Estimate, whether in contract, tort or otherwise.

Hard Cost Subtotal	\$ 1,118,529.08
Engineering (10%)	\$ 111,852.91
Subtotal	\$ 1,230,381.99
Contingency (15%)	\$ 184,557.30
Total Construction Estimate	\$ 1,414,939.29

**Upper Glenrosa/Salmon Road
Glenrosa Road
Typical Frontage Works - Arterial Road
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
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Part 1. Earth Works

1. Clearing and grubbing including tree removal	3,017	m ²	\$ 5.00	\$ 15,085.00
2. Topsoil stripping including onsite stockpile and re-use	603	m ³	\$ 11.50	\$ 6,934.50
3. Import and placement of structural fill material	2,476	m ³	\$ 25.00	\$ 61,900.00
Subtotal Part 1				\$ 83,919.50

Part 2. Road Works

1. Sawcut, remove and dispose existing asphalt & curb	1,241	m ²	\$ 3.85	\$ 4,777.85
2. Supply and place 150mm crush base course - 300mm depth - roads	2,075	m ²	\$ 12.20	\$ 25,315.00
3. Supply and place 75mm crush base course - 150mm depth - roads	1,738	m ²	\$ 7.99	\$ 13,886.62
4. Supply and place 25mm crush base course - 150mm depth - roads	1,738	m ²	\$ 7.99	\$ 13,886.62
5. Supply and place asphaltic pavements - 75mm depth - roads	1,738	m ²	\$ 23.85	\$ 41,451.30
6. Supply and place 0.45m barrier style curb and gutter c/w base gravels and curb prep.	355	lm	\$ 91.05	\$ 32,322.75
7. Supply and place 2.00m sidewalk c/w base gravels and curb prep.	355	lm	\$ 171.25	\$ 60,793.75
Subtotal Part 2				\$ 192,433.89

Part 3. Watermains

1. Supply and install watermain pipe - 300mmØ C900 PVC	164	lm	\$ 500.00	\$ 82,000.00
2. Supply and install valves and fittings including thrust blocks, valve boxes and risers - 300x300x150mmØ HHF tee, 1-150mm HF gate valve and hydrant assembly c/w hydrant lead	2	ea.	\$ 10,000.00	\$ 20,000.00
Subtotal Part 3				\$ 102,000.00

Part 4. Storm Sewer

1. Supply and install storm sewer pipe - 300mmØ ultra-rib PVC	164	lm	\$ 270.00	\$ 44,280.00
2. Supply and install catch basin leads - 200mmØ SDR35 PVC	49	lm	\$ 120.00	\$ 5,880.00
3. Supply and install storm sewer manhole including base, lid, frame and cover - 1050mmØ	6	ea.	\$ 2,800.00	\$ 16,800.00
- 1050mmØ solid barrel sections	12	vm	\$ 800.00	\$ 9,600.00
4. Supply and install catch basins - 900mmØ side inlet	6	ea.	\$ 2,000.00	\$ 12,000.00

**Upper Glenrosa/Salmon Road
Glenrosa Road
Typical Frontage Works - Arterial Road
Construction Estimate
April 30, 2018**

Description	Quantity	Unit	Unit Price	Total
5. Storm detention system	52	m ³	\$ 1,000.00	\$ 52,000.00
Subtotal Part 4				\$ 140,560.00

Part 5. Miscellaneous				
1. Supply and install street lights and bases c/v all trenching, conduit, bedding and backfill	9	ea.	\$ 5,950.00	\$ 53,550.00
2. Topsoil & hydroseed	1,650	m ²	\$ 5.00	\$ 8,250.00
3. Landscaping	1	LS	\$ 28,000.00	\$ 28,000.00
Subtotal Part 5				\$ 89,800.00

Construction Cost Estimate: Limitation of Liability

The parties agree that the D.E. Pilling & Associates Ltd. cannot and does not warrant or represent that bids of negotiated prices will not vary from the Construction Cost Estimate. The parties further agree that nothing in their agreement shall be deemed to be a cost condition or representation that the project can be completed for the amount of the Construction Cost Estimate or any other amount, and the Client expressly waives its right to withhold the D. E. Pilling & Associates Ltd.'s fees, whether in whole or in action or bring any other proceedings in any court of law against the D.E. Pilling & Associates Ltd. in connection with advise or information relating to the Construction Cost Estimate, whether in contract, tort or otherwise.

Hard Cost Subtotal	\$ 608,713.39
Engineering (10%)	\$ 60,871.34
Subtotal	\$ 669,584.73
Contingency (15%)	\$ 100,437.71
Total Construction Estimate	\$ 770,022.44



City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

April 18, 2022
File: 2463
CoWK: Z 17-02 / DVP 17-15

Attention: Christine De Silva, Development Technician
Cc: Carla Eaton, Planner

Dear Ms. De Silva,

**Upper Glenrosa/Salmon Road – Lot A & B, KAP68680 and Block 105, KAP777, ODYD
Zoning Bylaw Amendment and Development Variance Permit Application
Extension Request**

On behalf of Mr. Ralph Berezan of Berezan Management (Glenrosa) Ltd. D.E. Pilling & Associates Ltd. (Pilling) submits this extension request to zoning file Z 17-02.

The May 1, 2018, Council Report for the May 8, 2018 Council Meeting included the following recommended motion:

- 1) **THAT** Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and
- 2) **THAT** Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and
- 3) **THAT** Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezone application (Zoning Amendment Bylaw No. 0154.56, 2017).

Legal Matter Item 9. “Dedications of land to accommodate improvements to Glenrosa Road” is currently the sticking point. Land acquisition required for Glenrosa Road Improvements as designed with agreed upon design criteria is shown on Pilling’s attached drawing 2463 – P4 (rev.0). The status of the land acquisition discussions for the affected properties is summarized below:

- 1) 3275 Glenrosa Road – (Majoros)
 - The developer met with the owners’ parents and their representatives on site to review proposed changes to Glenrosa and the land acquisition required.
 - It was further discussed with the property owner that in exchange for the land, the owner wanted a domestic water service to Block 107.

- The owner wanted the developer to include the water service variance request under the rezoning/develop variance application (DVP), including the required petition to council to allow the property to enter the irrigation district and allow the water service.
 - The city required a backup purchase agreement in place in the event the water service variance request was rejected. At the time, the owner listed the property for sale and indicated to the development team that he will not be proceeding with negotiations at this time.
- 2) 3255 Glenrosa Road – (Majoros)
- The developer met with the owners and their representatives on site to review proposed changes to Glenrosa and the land acquisition required. The owners' parents and their representative did not seem to have an issue with the land acquisition or access easement. No agreement was reached.
- 3) 3235 Glenrosa Road – (Quist)
- This property requires relocating an existing driveway to the property to facilitate design grading requirements of the Glenrosa Road cross section. There is also a parking spot located at the northeast corner of the subject property built over a steep fill slope that the owners requested be maintained.
 - The developer met with the owners of the property, and they were unable to reach an agreement to purchase the property or on the proposed driveway alterations.
 - Pilling and Development Engineering reviewed this matter and discussed options to redesign this section of the road to avoid this driveway relocation. Due to the state of the negotiations with 3275 Glenrosa Road, the developer put these alternative design options on hold.
- 4) 3205 Glenrosa Road – (BC Telephone)
- Coordination efforts were carried out with representatives from BC Telephone Co. to obtain their approvals to purchase the land. At the time, outstanding items were preparation of the legal plan and the executed purchase agreement.
- 5) 3410 Gates Road – (Huot)
- The developer met with the owners and their representatives on site to review proposed changes to Glenrosa and the land acquisition required. At that time, a verbal agreement was reached for compensation. Outstanding items were the legal plan and the executed purchase agreement.

Currently, we request an extension to the file as the developer wishes to resurrect these discussions and negotiations and requires more time to reach agreements. We trust this is what you require to proceed with this request. If you require anything further, please do not hesitate to contact us.

Sincerely,
D.E. Pilling & Associates Ltd.



David Mori, P.Eng
 Project Engineer

C.c. Ralph Berezan



City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

May 9, 2023
File: 2463
CoWK: Z 17-02 / DVP 17-15

Attention: Yvonne Mitchell, Planner II

Dear Ms. Mitchell,

**Upper Glenrosa/Salmon Road – Lot A & B, KAP68680 and Block 105, KAP777, ODYD
Zoning Bylaw Amendment and Development Variance Permit Application
Extension Request**

On behalf of Mr. Ralph Berezan of Berezan Management (Glenrosa) Ltd. D.E. Pilling & Associates Ltd. (Pilling) submits this extension request to zoning file Z 17-02.

The May 1, 2018, Council Report for the May 8, 2018 Council Meeting included the following recommended motion:

- 1) **THAT** Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.56, 2017 (File: Z 17-02); and
- 2) **THAT** Council direct staff to schedule the bylaw for consideration of adoption conditional upon submission of all technical/design matters, legal matters, and financial matters to the satisfaction of the General Manager of Development Services (refer to Attachment 1: Detailed Zoning Conditions); and
- 3) **THAT** Council authorize the issuance of a Variance Permit (File: DVP 17-15) for 3280 and 3290 Glenrosa Road and Block 105 Salmon Road (Lots A & B, DL 3190, ODYD, Plan KAP 68680 & Block 105, DL 3190, ODYD, Plan 777) to vary Works and Services Bylaw No. 0120, Appendix A – Design and Construction Standards, Schedule 3 – Water Supply and Distribution Systems, Section 3.4.2 – Reservoirs, to exempt the developer from the requirement to construct a water storage reservoir subject to adoption of the associated rezone application (Zoning Amendment Bylaw No. 0154.56, 2017).

A previous extension request was made in April 2022. The extension request and summary of land acquisition challenges were discussed under Pilling's extension request letter dated April 18, 2022, attached to this letter for reference.

The developer and Pilling met with West Kelowna Planning & Engineering staff on May 2, 2023, to discuss the land acquisition challenges and consider concepts that would help with providing a safe and improved roadway upgrade while better utilizing the existing roadway alignment and working within topographical constraints. As the developer finds the current offsite upgrade too costly to pursue the development, the intent of the re-design exercise will look at opportunities to reduce costs along select sections of the roadway upgrade where appropriate.

Thank you for your time and understanding of the challenges this file faces. With this extension, we hope through the coming months we will arrive at a balance that works for all parties. We trust this is what you require to proceed with this request. If you require anything further, please do not hesitate to contact us.

Sincerely,
D.E. Pilling & Associates Ltd.

A handwritten signature in black ink, appearing to read 'David Mori', with a stylized flourish at the end.

David Mori, P.Eng
Project Engineer

C.c. Ralph Berezan

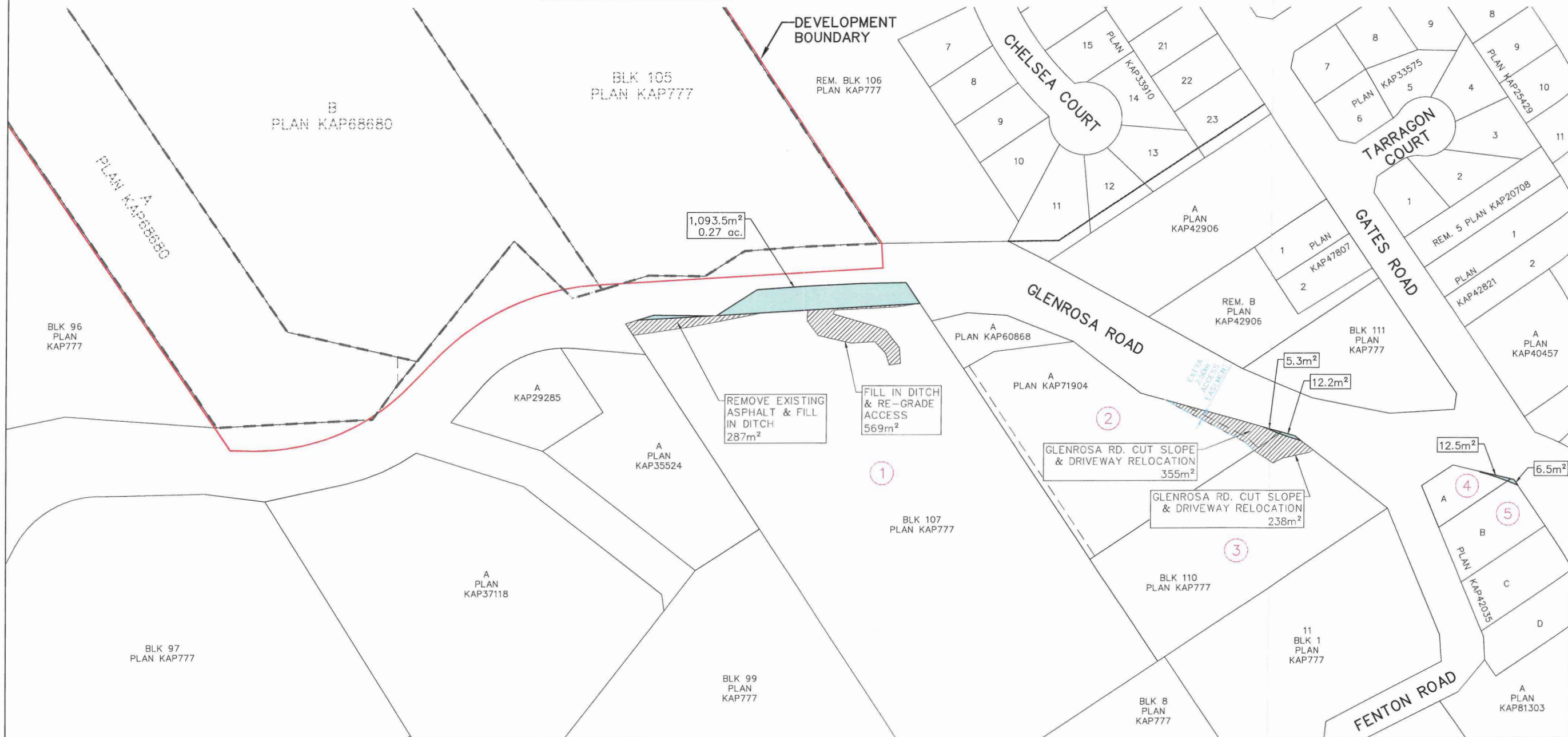


LEGEND

- EX PARCEL BOUNDARY
- DEVELOPMENT BOUNDARY
- PROPOSED LAND ACQUISITION FOR ROAD DEDICATION
- PROPOSED LIMIT OF DISTURBANCE

LAND ACQUISITION NOTES

No.	OWNERS/CONTACT	PARCEL	AREA	DESCRIPTION
1	ZOLTAN MAJOROS / AGENT: RUSTY ENSIGN 250-769-6366 250-469-3675 zollimajoros@gmail.com rusty@ensignbros.com	BLK 107 PLAN KAP777 3275 GLENROSA ROAD	856m ²	LIMIT OF DISTURBANCE
2		LOT A PLAN KAP71904 3255 GLENROSA ROAD	1,093.5m ² 355m ² 5.3m ²	LAND ACQUISITION FOR ROAD DEDICATION LIMIT OF DISTURBANCE LAND ACQUISITION FOR ROAD DEDICATION
3	ROBERT & DEBRA QUIST 250-870-7366 excelsundecks@gmail.com	BLK 110 PLAN KAP777 3235 GLENROSA ROAD	238m ² 12.2m ²	LIMIT OF DISTURBANCE LAND ACQUISITION FOR ROAD DEDICATION
4		LOT A PLAN KAP42035 3205 GLENROSA ROAD	17.4m ²	LAND ACQUISITION FOR ROAD DEDICATION
5	TY & MELISSA HUOT 250-469-2811 ty.huot@tygoconstruction.ca	LOT B PLAN KAP42035 3410 GATES ROAD	6.5m ²	LAND ACQUISITION FOR ROAD DEDICATION



LEGEND

- CABLE TV
- GAS
- SAN. SEWER
- STORM SEWER
- U.G. ELECTRICAL
- U.G. TELEPHONE
- WATER

- CAP
- CATCH BASIN
- ELECTRICAL BOX
- HYDRANT
- VALVE
- LAMP STANDARD

- SAN# SANITARY MH (EXISTING OR FUTURE)
- SAN# SANITARY MH (PROPOSED)
- STM# STORM MH (EXISTING OR FUTURE)
- STM# STORM MH (PROPOSED)
- TRANSFORMER
- UTILITY JUNCTION BOX

No.	MM/DD/YY	DATE	BY	REVISION	Chk'd	No.	MM/DD/YY	DATE	BY	REVISION	Chk'd

DAVID K.Y. WONG, P.ENG.

DRAWN: LVZ

DESIGN: P.A.C.E.

APPROVED: D.K.Y.M.

DATE: JULY 2020

SCALE: 1:1,000

WEST KELOWNA

D.E. Pilling & ASSOC. CONSULTING ENGINEERING

250-763-2315

250-763-6559

reception@pilling.ca

250-540 Groves Avenue, Kelowna, B.C. V1Y 4Y7

UPPER GLENROSA/SALMON ROAD

BEREZAN MANAGEMENT (GLENROSA) LTD.

LAND ACQUISITION PLAN

DRAWING NO. 2463-P4

REV. NO. 0



City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

September 11, 2023
File: 2463
CoWK: Z 17-02 / DVP 17-15

Attention: Mayor and Council of West Kelowna

Bob Dargatz, P.Eng., Development Manager / Approving Officer

Upper Glenrosa/Salmon Road – Zoning Bylaw Amendment and Development Variance Permit Application – Lot A & B, KAP68680 and Block 105, KAP777, ODYD

In response to the rezoning application extension our office made on May 9, 2023, West Kelowna's Planning Department brought forward the application to Council on June 27, 2023. This application was the second rezoning application extension for the file, in accordance with West Kelowna's policy that allows for two rezoning file extension requests.

The application is faced with two issues that require solution.

- 1) Unable to reasonably negotiate land acquisition required to facilitate the construction of a 680m section of Glenrosa Road north of the Gates Road and Glenrosa Road intersection, as identified on D.E. Pilling & Associates Ltd. (Pilling) drawing 2463 – P4 (rev.0), dated July 2020.
- 2) Cost of offsite upgrades.

For reference, after the 3rd reading, the subject property and its rezoning application was taken over by new ownership during 2019.

The June extension request pointed out that the extension would allow the development team to explore opportunities to address the above issues provided the land acquisition hurdles could first be solved.

On Tuesday, June 27, 2023 the Municipal Council of the City of West Kelowna passed the following resolution (C257/23):

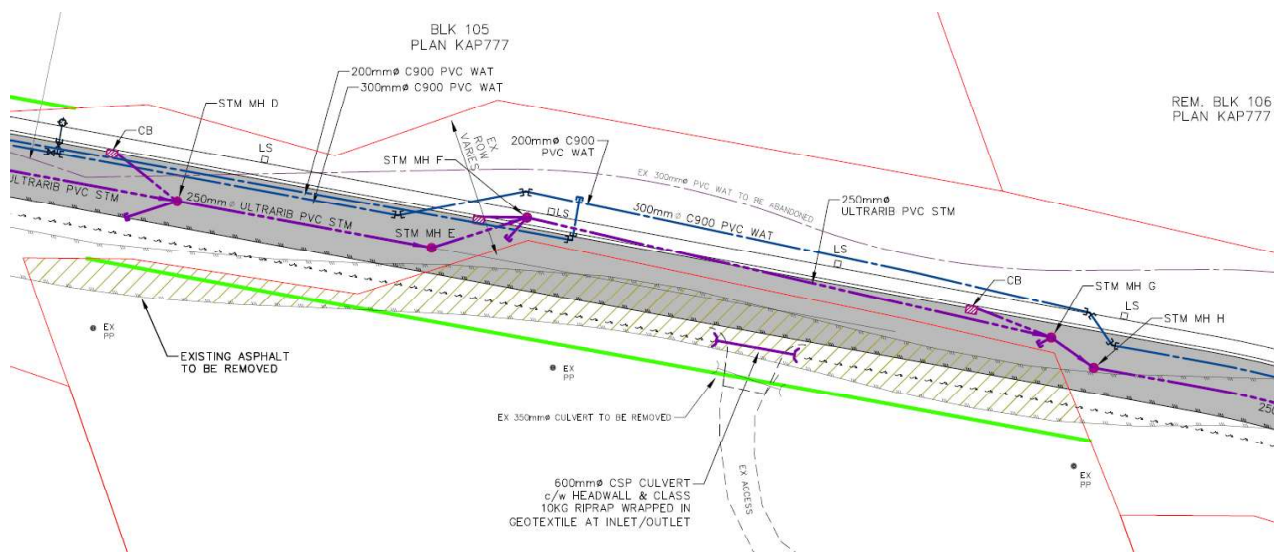
THAT Council defer consideration of Z17-02, Application Extension, 3290 Glenrosa Road, 3280 Glenrosa Road and Salmon Road for a period of three months.

Submission materials include the below listed documents and the trailing text portion of this letter.

- 2463 – P4 (rev.0) – Land Acquisition Plan (From 2020)
- 2463 – R1d (rev.0) – Glenrosa Road Plan/Profile 0+000 to 0+340
- 2463 – R2d (rev.0) – Glenrosa Road Plan/Profile 0+340 to 0+700
- 2463 – X2c (rev.0) – Glenrosa Road Cross Sections 0+490 to 0+660

Following the resolution, our office reviewed the design to explore options to address the above noted issues. Referring to the attached Land Acquisition Map 2463-P4, land acquisition shown on properties labeled 1 to 5 have been eliminated. A summary of the changes that have been reviewed with West Kelowna's reviewing departments are as follows:

- Property #1: 3275 Glenrosa Road (BLK 107 PLAN KAP777)
 - Figure 1 below shows the existing property line in red, the proposed property lines in green, the proposed Glenrosa Road asphalt as grey, and the existing Glenrosa Road travelled surface overlapping private property hatched in olive green.
 - Section 42 of the BC Transportation Act states that
 - *"42 (1) Subject to subsection (2), if public money is spent on a travelled road that is not a highway, the travelled road is deemed and declared to be a highway."*
 - Consequently, this section of Glenrosa Road is deemed a highway.
 - To allow for road surface works (realignment, new pavement, etc.) as generally proposed on the Pilling civil drawings, the re-design includes jogging the proposed storm sewer and water system works off their preferred alignments so that they avoid containment within Property #1. The jogged utilities result in utilities located within the current Glenrosa Road right-of-way which we understand will avoid land acquisition requirements to complete the upgrade.



- Property #2 & #3: 3275 Glenrosa Road (BLK 107 PLAN KAP777) & 3235 Glenrosa Road (BLK 110 PLAN KAP777)
 - The challenge here was the balance between retaining wall heights on the east side of the road and reconstructing a functional driveway to these two properties on the west side of the road.
 - Map 2463-P4 notes driveway relocation and embankment cuts that when discussed on site with survey layout was not met with approval by the owners.
 - Long story short: The revised design avoids the driveway and embankment. Revisions included raising the road, narrowing the boulevard on the east side to offset the effect that the road lift had on the east retaining wall height, and shifted the centerline of the road to the east away from the subject driveway.

- Property #4 & #5:
 - The Glenrosa Road upgrade comes with nearly 700 m of barrier curb which would direct storm water to catch basin inlets, a piped storm sewer, a large storm water detention tank, and a slow controlled release downstream of the upgrade boundary.
 - By extending the storm discharge some 60 m, shoulder work and dedication on properties #4 & 5 is avoidable. Figure 2 below, shows the previous design on the left and the revised design on the right.

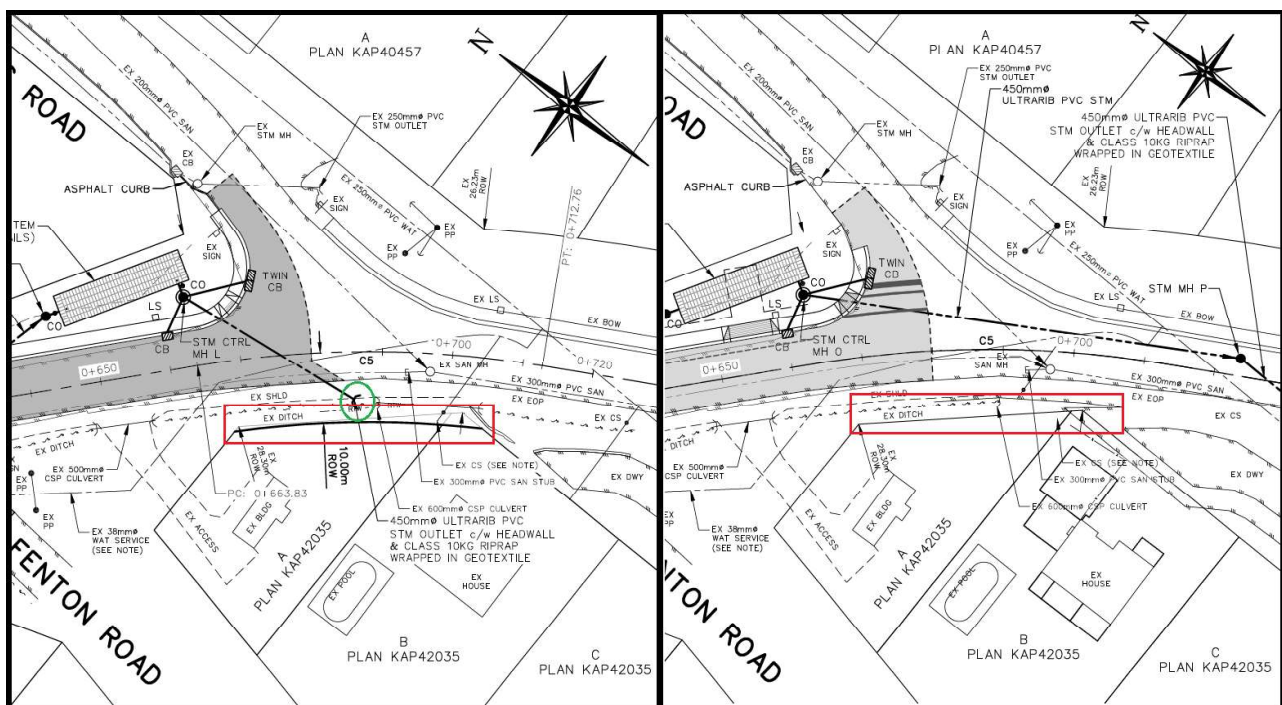


Figure 2: Property #4 & #5 Land Acquisition Avoidance

We trust this is what you require to approve this extension request. If you require anything further, please do not hesitate to contact us.

Sincerely,
D.E. Pilling & Associates Ltd.



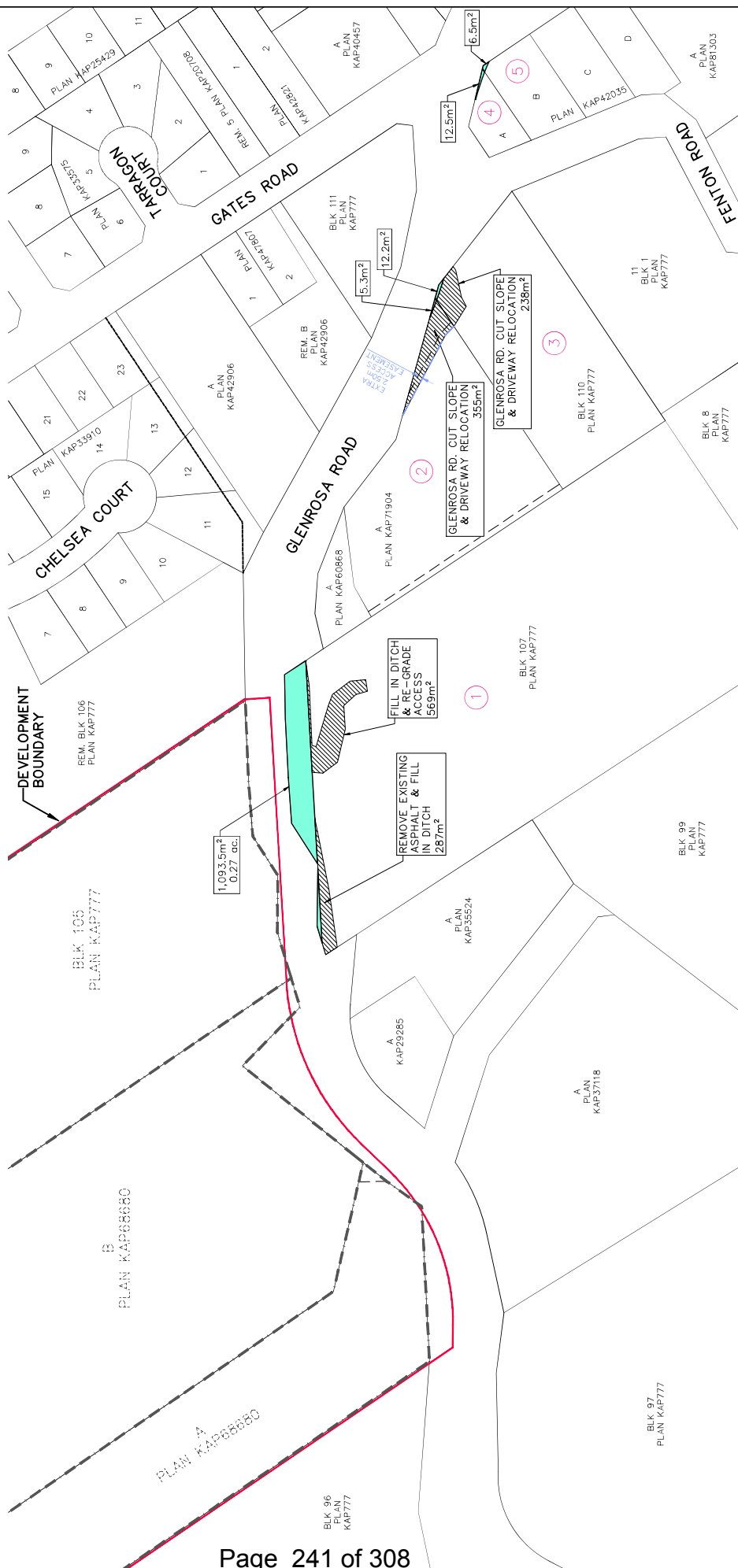
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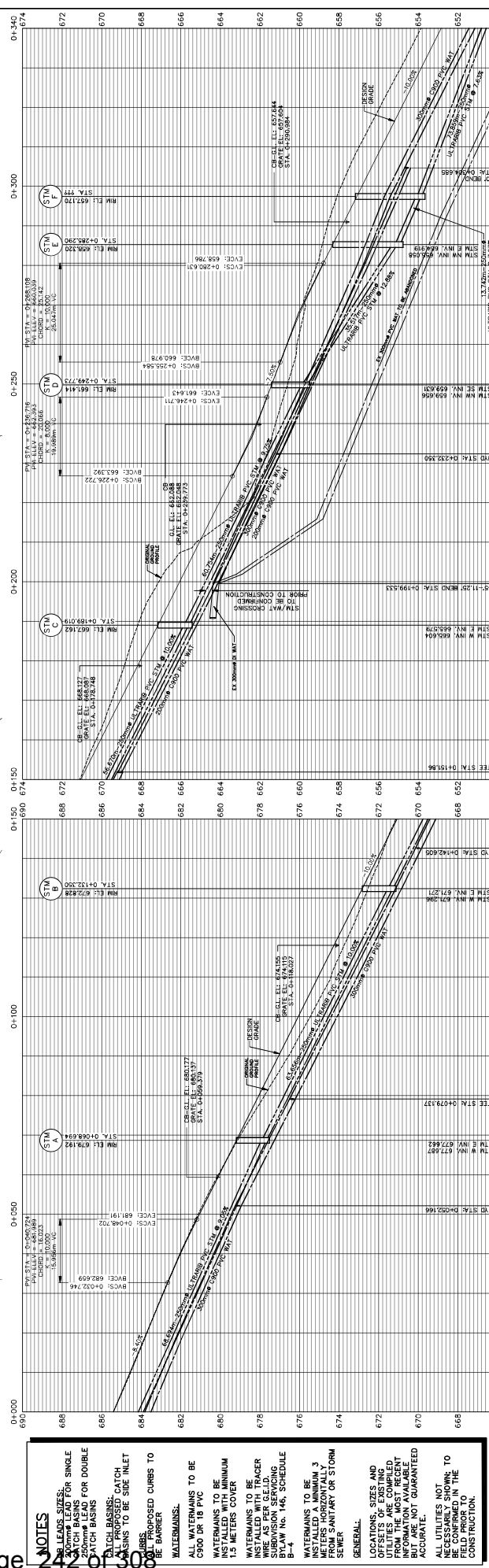
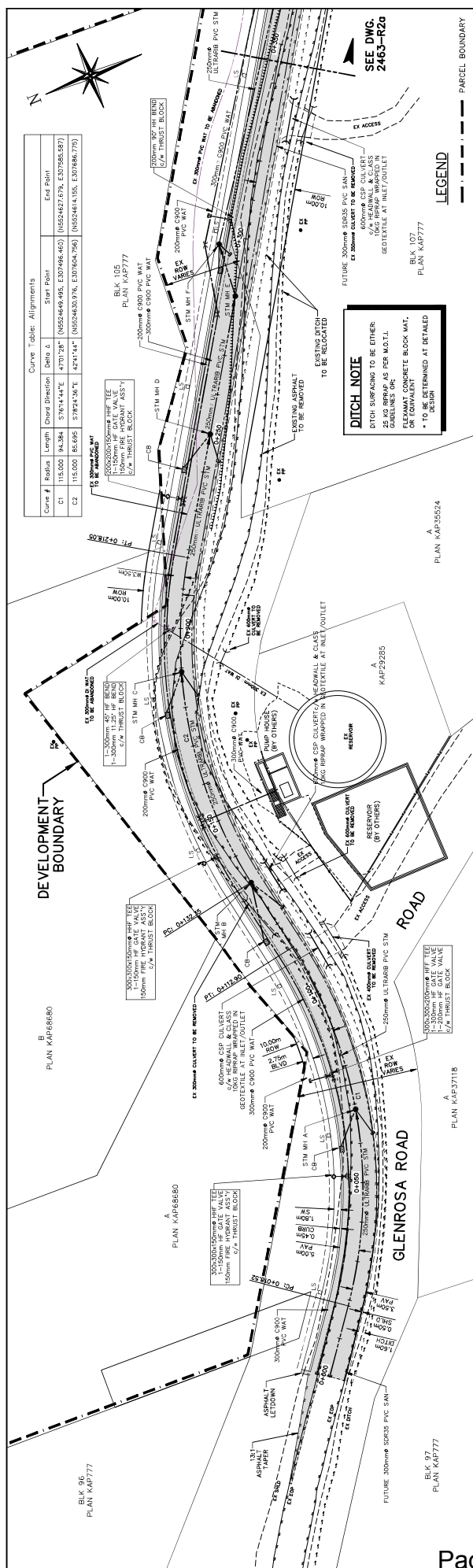
David Mori, P.Eng
Project Engineer





C.c. Ralph Berezan

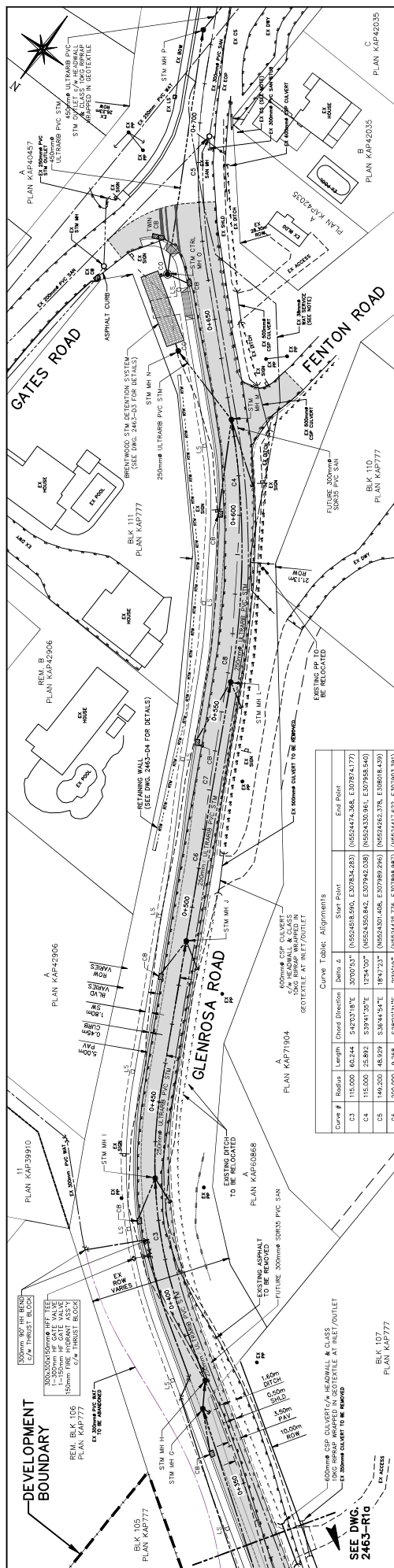
LAND ACQUISITION NOTES

No.	OWNERS / CONTACT	PARCEL	AREA	DESCRIPTION
1	ZOLTAN MAJOROS / AGENT, RUSTY ENSIGN 3275 GLENROSA ROAD Zoltan.j.pro@gmail.com rusty@ensigntrucks.com	BLK 107 PLAN KAP777 3275 GLENROSA ROAD	856m ² 1,093.5m ²	LIMIT OF DISTURBANCE LAND ACQUISITION FOR ROAD DEDICATION
2	ZOLTAN MAJOROS / AGENT, RUSTY ENSIGN 3255 GLENROSA ROAD Zoltan.j.pro@gmail.com	LOT A PLAN KAP71904 3255 GLENROSA ROAD	355m ² 5.3m ²	LIMIT OF DISTURBANCE LAND ACQUISITION FOR ROAD DEDICATION
3	ROBERT & DEBRA QUIST 250-870-7356 exclanducks@gmail.com	BLK 110 PLAN KAP777 3235 GLENROSA ROAD	238m ²	LIMIT OF DISTURBANCE LAND ACQUISITION FOR ROAD DEDICATION
4	BB TELEPHONE Co. / ROBERT KATES 778-874-5066 Robert.Kates@vellsa.com	LOT A PLAN KAP42035 3205 GLENROSA ROAD	12.2m ² 17.4m ²	LAND ACQUISITION FOR ROAD DEDICATION
5	TY & MELISSA HUOT 250-469-2811 ty.huot@tygeonconstruction.ca	LOT B PLAN KAP42035 3410 GATES ROAD	6.5m ²	LAND ACQUISITION FOR ROAD DEDICATION

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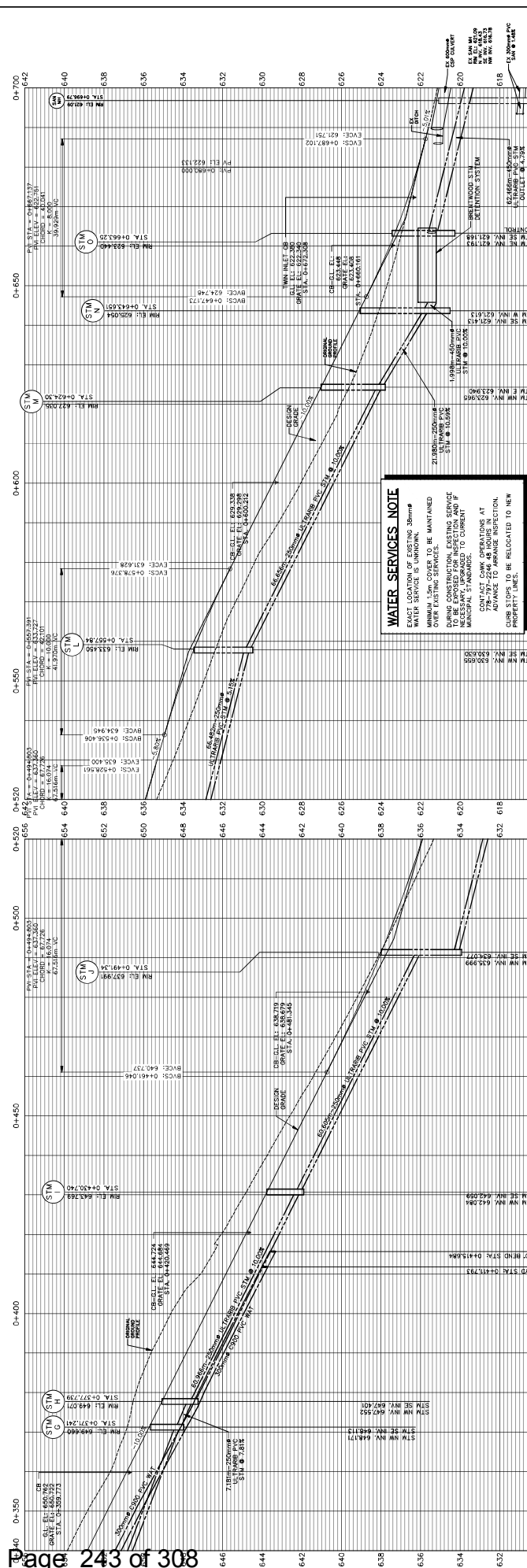


																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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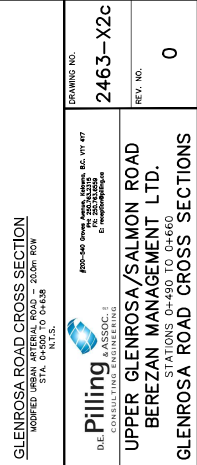
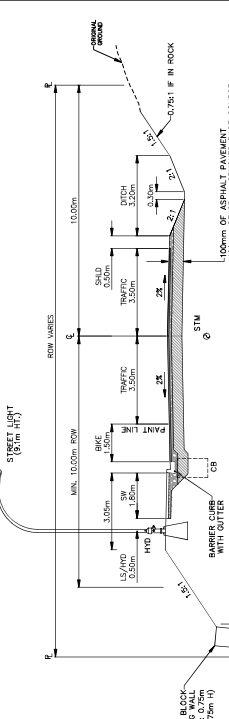
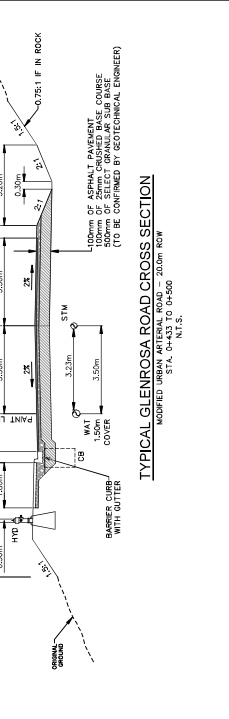
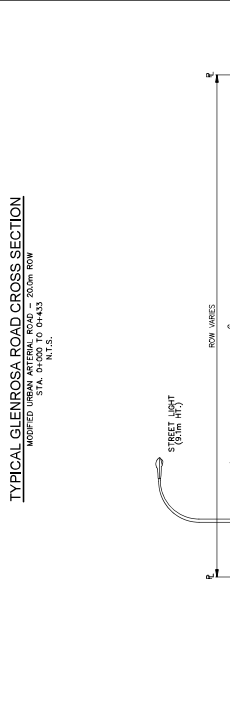
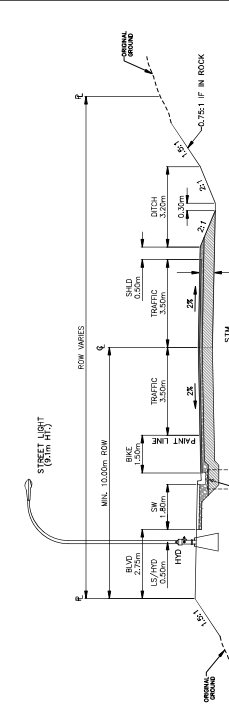
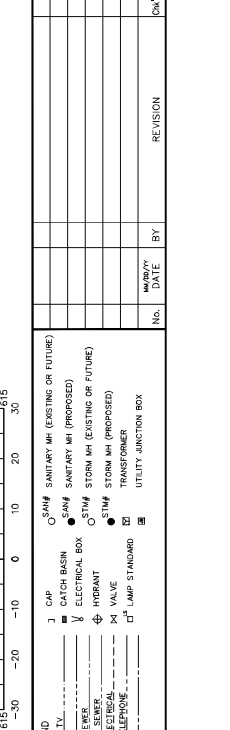
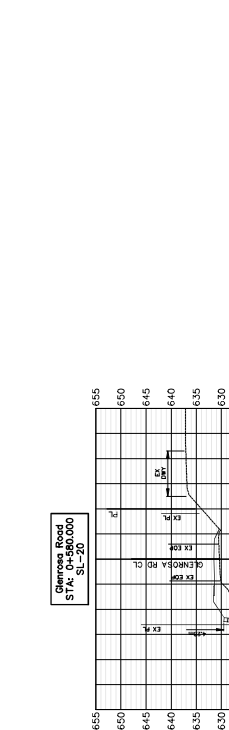
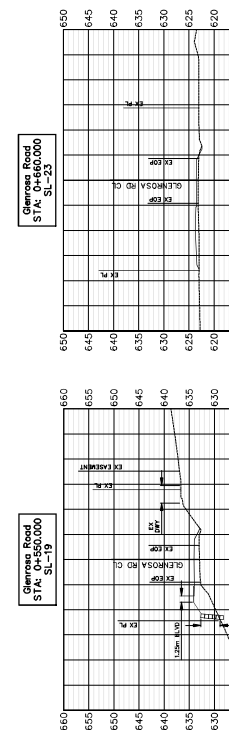
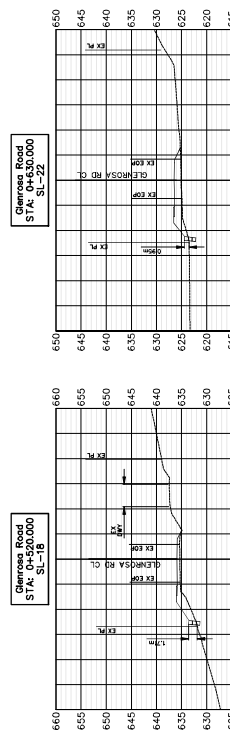
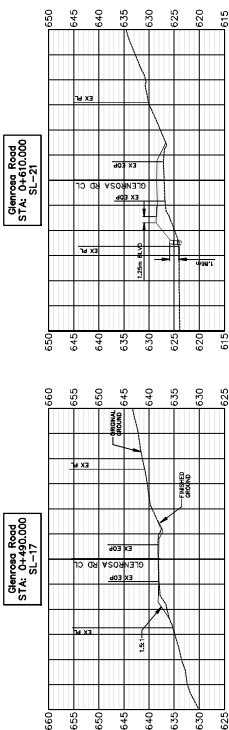
Curve Table: Alignments							
Curve #	Curve E	Radius	Length	Chord Distance	Delta Δ	Start Point	End Point
01	01	115.000	62.244	54.020(18.4)	307°03'	(N52547.848, E30783.283)	(N52547.438, E30874.777)
02	02	115.000	62.244	54.020(18.4)	307°03'	(N52547.438, E30874.777)	(N52547.848, E30783.283)
03	03	115.000	25.892	19.994(15.5°)	129°40'	(N52543.550, E42709.628)	(N52543.501, E42995.540)
04	04	115.000	25.892	19.994(15.5°)	129°40'	(N52543.501, E42995.540)	(N52543.550, E42709.628)
05	05	140.250	48.529	33.544(24.4)	184°12'	(N52630.148, E37989.290)	(N52642.612, E38160.439)
06	06	200.000	9.268	5.852(3.3°)	239°19'	(N52642.612, E38160.439)	(N52644.162, E38094.339)
07	07	115.000	62.244	54.020(18.4)	215°48'	(N52644.162, E38094.339)	(N52644.162, E38094.339)
08	08	115.000	71.691	53.027(16.2°)	5°48'	(N52644.162, E38094.339)	(N52645.856, E38079.115)

REFER TO DWG. 2463-R1 FOR GENERAL NOTES



WATER SERVICES NOTE
EXACT LOCATION OF EXISTING 38mm
WATER SERVICE IS UNKNOWN.
MINIMUM 1.5m COVER TO BE MAINTAINED
OVER EXISTING SERVICES.

[illegible]



DRAWING NO.										2463-X2c										REV. NO.										0									
Pilling & Associates Inc. Consulting Engineering Ltd.										UPPER GLENROSA/SALMON ROAD BEREZAN MANAGEMENT LTD.										STATIONS 0+430 TO 0+660										GLENROSA ROAD CROSS SECTIONS									
DRAWN										L.V.Z.										H = 1:500										V = 1:500									
DESIGN										P.A.C.E.																													
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DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

File No: Z 22-08

Subject: **Z 22-08; Zoning Amendment Bylaw No. 0265.12, 2023 (Second Reading); 2741 Auburn Road**

Report Prepared By: Jayden Riley, Planner III

RECOMMENDATION to Consider and Resolve:

THAT Council give second reading to Zoning Amendment Bylaw No. 0265.12, 2023; and

THAT Council direct staff to schedule a public hearing regarding the proposed amendment bylaw.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

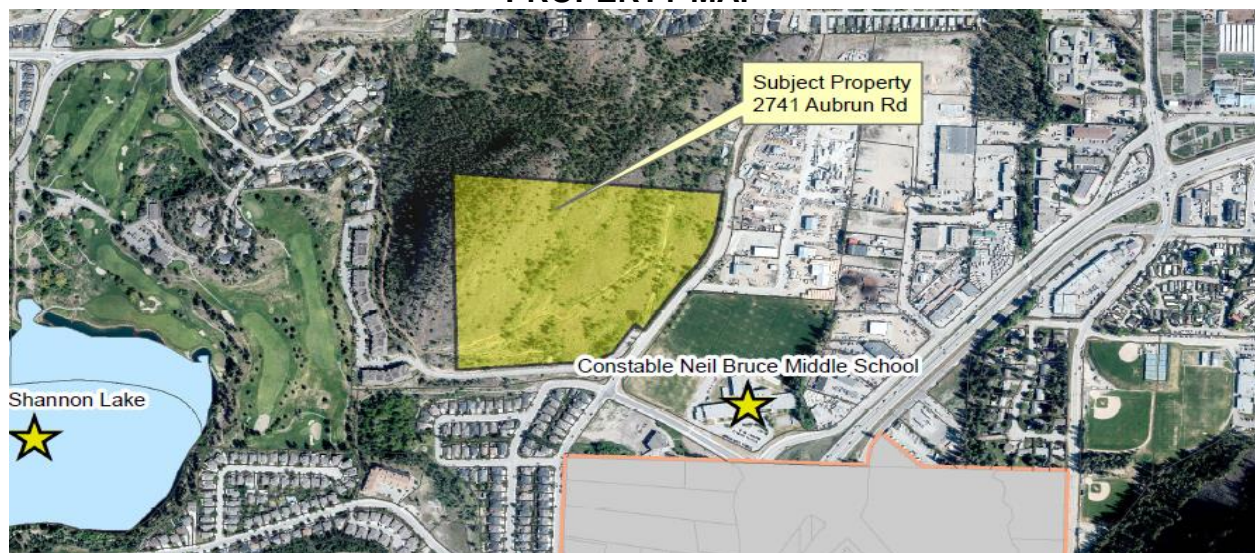
BACKGROUND

This zoning amendment bylaw received first reading on February 14, 2023. At that meeting, Council directed staff to schedule the bylaw for second reading subject to further traffic analysis to determine necessary off-site improvements to mitigate intersection failure and safety concerns near the surrounding site and the steps required to initiate those improvements. The results of the traffic analysis were presented at the time of second reading on July 25, 2023 (Attachment 1) where Council postponed second reading pending additional information on a proposed cost sharing proposal related to the off-site traffic improvements. A cost sharing proposal has since been prepared by the applicant and is attached to this report (Attachment 2). A summary of the proposal is outlined in the following section of this report.

PROPERTY DETAILS			
Address	2741 Auburn Road		
PID	026-159-601		
Folio	36414135.233		
Lot Size	12.4 ha		
Owner	0761681 BC LTD.	Agent	ParaMorph Architecture Inc. (formerly David Eaton Inc. Architecture)
Current Zoning	Low Density Multiple Residential (R3)	Proposed Zoning	Comprehensive Development (CD 10); Parks and Open Space (P1)
Current OCP	Medium Density Multiple Family, Parks and Natural Areas	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	Apartment / Condo;
Development Permit Areas	Hillside, Sensitive Terrestrial		
Hazards	Hillside, Sensitive Terrestrial, Traffic		
Agricultural Land Reserve	None		

ADJACENT ZONING & LAND USES		
North	^	Rural Residential Large Parcel (RU4)
East	>	Institutional and Assembly (P2) Light Industrial (I1) Heavy Industrial (I2) Gravel Extraction (I4)
West	<	Low Density Multiple Residential (R3)
South	v	Compact Single Detached Residential (RC3)

PROPERTY MAP



DISCUSSION

Summary of Applicant's Cost Sharing Proposal

The applicant has submitted a cost sharing proposal for the off-site road improvements related to their development (Attachment 2). The proposal includes a phased approach with a proposed 50% cost share between the City and the developer for approximately \$1.7 million of off-site improvements, based on a preliminary estimate – see Table 1.

Table 1: Cost share proposal from applicant

Phase	Unit / Building Trigger	Item	Estimated Cost (\$)	Cost Share (%)
1	Unit 1 / Building 1 of 5	Auburn Road frontage lane widening for on-street parking	Unknown	100% Developer
		Dual left turn lane on Daimler Drive to Hwy 97	\$300k	50% Developer, 50% CWK
2	Unit 236 / Building 4 of 5	Auburn Road Roundabout	\$1.15 million	50% Developer, 50% CWK
		Old Okanagan / Byland / Daimler intersection through restriction	\$250k	50% Developer, 50% CWK

The applicant's rationale for cost sharing, as noted in the proposal, is that the development site is located adjacent to a road network already experiencing pressures from traffic and parking demands. With intersections in the area already experiencing near failing levels of services without development traffic, the applicant has proposed a 50/50 cost share for all off-site improvements, including those located outside the City's jurisdiction. The dual left turn lanes onto Hwy 97 and the intersection improvement to restrict through access from Byland Road to Old Okanagan Highway are primarily located within Ministry of Transportation and Infrastructure (MoTI) jurisdiction. The proposal also outlines a trigger for cost sharing based on the number of units, which coincides with the existing covenant on title.

There are concerns with the cost sharing proposal as a portion of the works are located outside of the City's jurisdiction where improvements are not generally funded by the City. Further, the proposed improvements are not currently identified as part of the City's Capital Works Plan and are not eligible for Development Cost Charge credits. As such final determination of the cost sharing proposal will require additional consideration at third reading following public input.

FINANCIAL IMPLICATIONS

The off-site road improvements are estimated at \$1.7 million, however, estimated costs will be more clearly known once a design review is completed. Any cost sharing would need to be supported through budgetary discussions and prioritized accordingly in the City's capital works program.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
February 14, 2023	THAT Council resolve to give first reading to City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023; and THAT Council resolve to direct staff to schedule the bylaw for second reading pending further traffic review and analysis to determine necessary off-site improvements to mitigate intersections failures near the surrounding stie and the steps required to initiate those improvements.	C125/23
July 25, 2023	THAT Council postpone second reading to the City of West Kelowna Bylaw No. 0265.12, 2023 (File Z 22-08).	C289/23

CONCLUSION

Staff recommend that Council give second reading to the proposed amendment bylaw and allow the opportunity for the public to comment on the proposal at time of public hearing. Staff position regarding the cost sharing proposal would be outlined at time of third reading.

Alternate Recommendation to Consider and Resolve:

1. That Council postpone consideration of the amendment bylaw.

THAT Council postpone second reading to the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08).

Should Council postpone consideration of the proposed amendment bylaw, further direction to staff on how to proceed is requested.

2. That Council deny the amendment bylaw.

THAT Council deny the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08) and abandon the bylaw and direct staff to close the file.

Should Council deny the amendment bylaw, the file will be closed in accordance with the Application Procedures Bylaw No. 0260.

REVIEWED BY

Bob Dargatz, Development Manager

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Second Reading Report, July 25, 2023 (File Z 22-08)
2. Applicant's Cost Sharing Proposal
3. Zoning Amendment Bylaw No. 0265.12, 2023



DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: July 25, 2023

From: Paul Gipps, CAO

File No: Z 22-08

Subject: **Z 22-08; Zoning Amendment Bylaw No. 02565.12, 2023 (Second Reading);
2741 Auburn Road**

Report Prepared By: Jayden Riley, Planner III

RECOMMENDATION to Consider and Resolve:

THAT Council give second reading to City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023; and

THAT Council direct staff to schedule a Public Hearing for the proposed amendment bylaw pending submission of a phased cost sharing proposal from the applicant.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

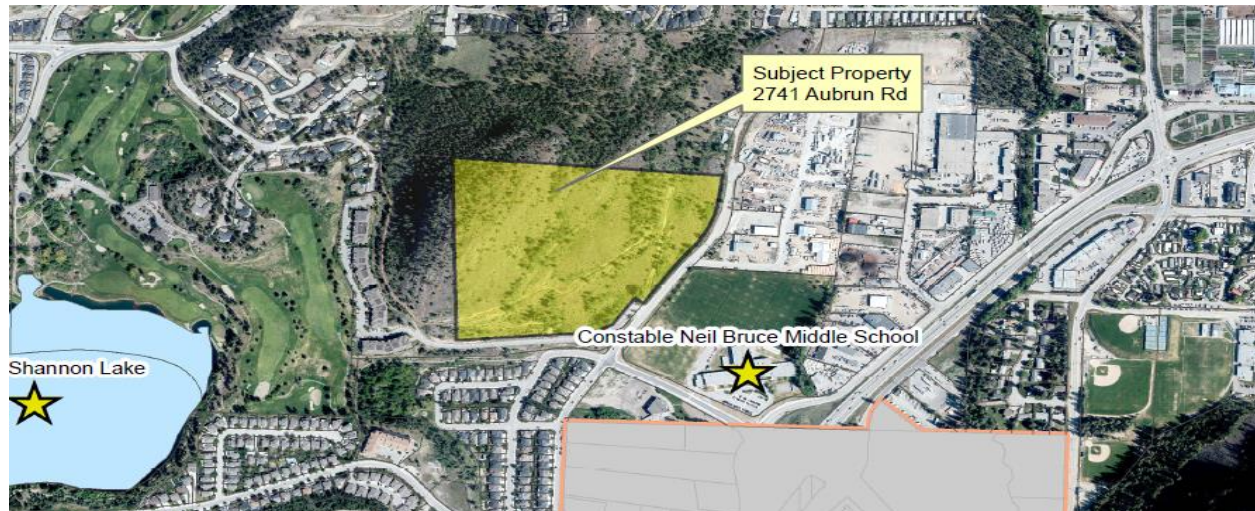
BACKGROUND

This zoning amendment bylaw received first reading on February 14, 2023 (*Attachment 1*). At that meeting, Council directed staff to schedule the bylaw for second reading subject to further traffic analysis to determine necessary off-site improvements to mitigate intersection failure and safety concerns near the surrounding site and the steps required to initiate those improvements. Further analysis has since been completed and the results of which are outlined in this report for Council's consideration. The City's traffic consultant will also be presenting the results to Council in conjunction with this report.

PROPERTY DETAILS			
Address	2741 Auburn Road		
PID	026-159-601		
Folio	36414135.233		
Lot Size	12.4 ha		
Owner	0761681 BC LTD.	Agent	David Eaton Architecture
Current Zoning	Low Density Multiple Residential (R3)	Proposed Zoning	Comprehensive Development (CD 10); Parks and Open Space (P1)
Current OCP	Low Density Multiple Family, Parks and Natural Areas	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	Apartment / Condo
Development Permit Areas	Hillside, Sensitive Terrestrial		
Hazards	Hillside, Sensitive Terrestrial, Traffic		
Agricultural Land Reserve	None		

ADJACENT ZONING & LAND USES			
North	^	Rural Residential Large Parcel (RU4)	
East	>	Compact Single Detached Residential (RC3) Light Industrial (I1), Heavy Industrial (I2), Gravel Extraction (I4)	
West	<	Low Density Multiple Residential (R3)	
South	v	Institutional and Assembly (P2)	

PROPERTY MAP



DISCUSSION

Traffic Analysis Results

Following first reading, the City retained Align Engineering to review traffic operations related to a potential 401-unit multi-residential development at 2741 Auburn Road and make recommendations to mitigate interim traffic conditions. The traffic analysis included assessment of the intersections of Old Okanagan Highway / Byland Road / Daimler Drive, Daimler Drive / Auburn Road, and Daimler Drive / Hwy 97.

The traffic analysis compared traffic conditions amongst three options with different off-site traffic improvements. Each option included the proposed dual left turn lane at Daimler Drive and Highway 97, as recommended by the original traffic impact assessment completed by the applicant. The three options that were assessed in the traffic analysis included:

1. Restrict Old Okanagan Hwy and Byland Road through and left turn with roundabout at Auburn Road;
2. Signal at Old Okanagan and Daimler Drive; and
3. Roundabout at Old Okanagan and Daimler Drive.

The traffic analysis concluded that (Option 1) to restrict Old Okanagan Highway – Byland Road ‘through’ traffic and ‘left turn’ movement from Daimler Drive to Byland Road and allow Byland Road traffic to circulate at a new roundabout at Daimler Drive / Auburn Road could provide an opportunity to resolve noted safety conflicts at the Old Okanagan / Daimler Drive / Byland Road intersection – See Figure 1. It is noted that this option increases traffic fronting Constable Neil Bruce Middle School.

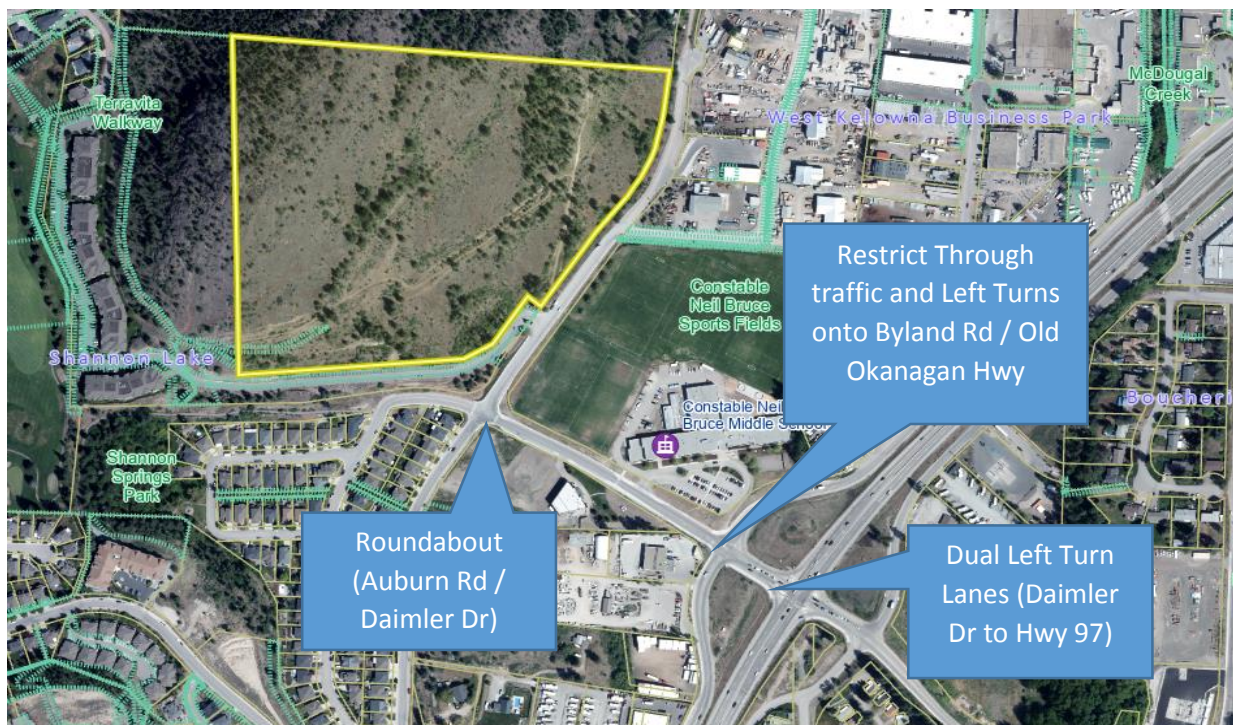


Figure 1: Option 1 from the traffic analysis

Options 2 & 3 modeled for a signal or roundabout at the Daimler Drive / Old Okanagan Highway / Byland Road intersection were determined not to function well due to the proximity to Highway 97, causing both options to fail.

Financial and Capital Works Implications

The improvements included with Option 1 are estimated at \$1.7 million. The intersection improvements are not currently outlined in the City's capital plan, nor are these considered eligible for Development Cost Charge (DCC) credits. Two of these improvements (the dual left turn lanes onto Hwy 97 and restricting access to Byland Rd / Old Ok Hwy) are located within Ministry of Transportation and Infrastructure (MoTI) intersections where improvements are not funded by the City.

It is anticipated that the applicant will submit a cost sharing proposal for Council's consideration. Any proposed cost sharing would likely need to be phased and be supported through budgetary discussions and prioritized accordingly in the City's capital works program. Further information regarding cost sharing will be outlined in more detail at time of public hearing, following receipt and review of the applicant's proposal.

Phased Approach to Off-Site Traffic Improvements

Through consultation with the City's traffic consultant, it was determined that the Option 1 improvements may be phased to lessen the initial cost burden for the developer and/or City. A possible approach to phasing may include constructing the dual left turn lanes at Daimler Drive and Highway 97 in Phase 1, followed by a second phase consisting of a roundabout at Auburn Road and restricting Old Okanagan and Byland Road through and left turn movements.

Further information regarding these improvements and the trigger points for their construction would be outlined at time of public hearing, following receipt and review of the applicant's proposal. The phasing of the proposed works will be reviewed in more detail and in consideration of the existing covenant limiting the development to 220 units.

Ministry of Transportation and Infrastructure Approval

Option 1 includes works that would be located within MoTI jurisdiction, specifically those at the intersection of Old Okanagan / Daimler Drive / Byland Road. MoTI has provided support in principle for Option 1, although a standard permitting process would apply. MoTI has also noted that consultation with WFN will be required.

Additional Auburn Road Frontage Improvements

Staff have noted existing challenges with Auburn Road caused by vehicles parking on the shoulder of the road during events occurring on the adjacent school grounds. Staff will be recommending this be addressed through additional frontage works in the form of road widening to allow on-street parking along the frontage of the subject property and that this be secured via covenant as a condition of zoning. This is intended to help reduce conflict between heavy industrial land uses and the institutional/park land uses.

CONCLUSION

Staff recommend that Council give second reading based on the following rationale:

- The City's traffic consultant has indicated the noted traffic improvements could provide an opportunity to resolve noted safety conflicts;
- A phased approach to the transportation improvements provides opportunity for both the developer and City to plan for additional costs;
- MoTI has confirmed support in principle for Option 1 off-site improvements;
- Prior to public hearing, the applicant will submit a proposal related to any cost sharing and phasing triggers of the off-site improvements for Council's consideration. A better understanding of financial and capital works related implications will be outlined in more detail at that time;
- The proposed use is consistent with the existing Land Use designation of Medium Density Residential; and
- The proposed multi-family use of the site is determined to be generally suitable for the neighbourhood, notwithstanding potential traffic issues.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
February 14, 2023	THAT Council consider and resolve to give First Reading to City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023; and, THAT Council consider and resolve to direct staff to schedule the bylaw for Second Reading pending further traffic review and analysis to determine necessary off-site improvements to mitigate noted intersection failures near the surrounding site and the steps required to initiate those improvements.	C125/23

Alternate Recommendation to Consider and Resolve:

1. That Council postpone consideration of the amendment bylaw.

THAT Council postpone second reading to the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08).

Should Council postpone consideration of the proposed amendment bylaw, further direction to staff on how to proceed is requested.

2. That Council deny the amendment bylaw.

THAT Council deny the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08) and abandon the bylaw; and

THAT Council direct staff to close the file.

Should Council deny the amendment bylaw, the file will be closed in accordance with the Application Procedures Bylaw No. 0260.

REVIEWED BY

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

Trevor Seibel, Deputy CAO

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Zoning Amendment Bylaw No. 0256.12
2. First Reading Report (File Z 22-08), February 14, 2023

CITY OF WEST KELOWNA

BYLAW NO. 0265.12

A BYLAW TO AMEND “ZONING BYLAW NO. 0265”

WHEREAS the Council of the City of West Kelowna desires to amend “CITY OF WEST KELOWNA ZONING BYLAW NO. 0265” under the provisions of the *Local Government Act*.

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna, in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as “CITY OF WEST KELOWNA ZONING AMENDMENT BYLAW NO. 0265.12, 2023”.

2. Amendments

“Zoning Bylaw No. 0265” is hereby amended as follows:

2.1 By adding the attached zoning regulations for the CD 10 – Comprehensive Development Zone (Auburn Road) to Part 14 – Comprehensive Development Zones.

2.2 By adding the zone “Comprehensive Development Zone (Auburn Road)” and zone abbreviation “CD10” to Part 5 – Establishment of Zones, Table 5.1.

2.3 By adding “CD10 – Comprehensive Development Zone (Auburn Road)” to the Table of Contents.

2.4 By changing the zoning of Lot 2 District Lot 2601 Osoyoos Division Yale District Plan KAP77227, as shown on Schedule ‘A’ attached to and forming part of the bylaw from Low Density Multiple Family (R3) to Comprehensive Development (CD 10) and Parks and Open Space (P1).

2.5 By depicting the change on “Zoning Bylaw No. 0265 Schedule B” (Zoning Bylaw map).

READ FOR A FIRST TIME THIS 14TH DAY OF FEBRUARY, 2023

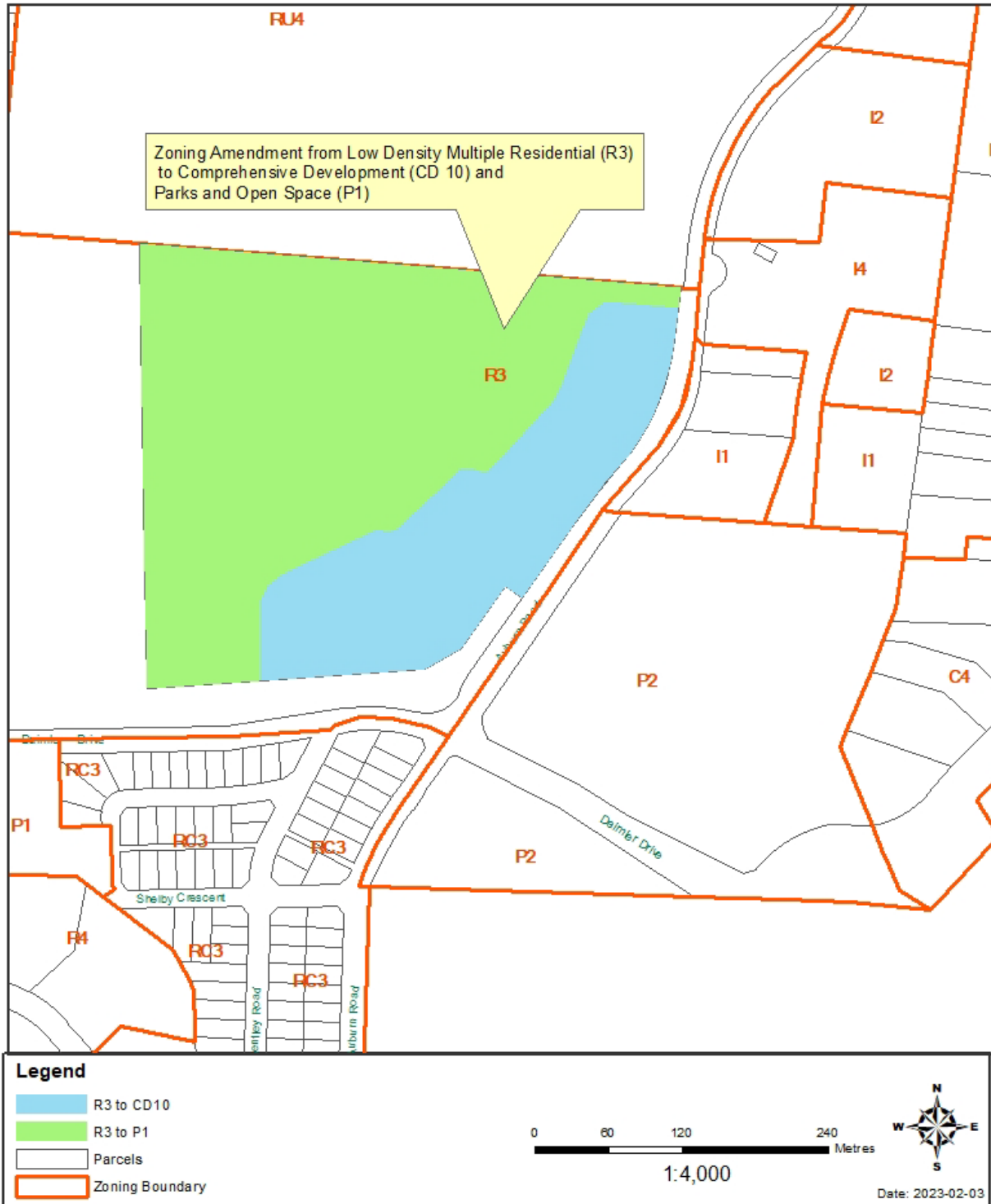
MAYOR

CORPORATE OFFICER



CITY OF
WEST
KELOWNA

SCHEDULE 'A' of BYLAW NO. 0265.12



CD10 – Comprehensive Development Zone (Auburn Road)

.1 Purpose

To accommodate multiple residential units the Shannon Lake neighbourhood.

.2 Principal Uses, Buildings and Structures

(a) Apartment

.3 Secondary Uses, Buildings and Structures

(a) Accessory uses, buildings, and structures

(b) Home based business, minor

.4 Regulations Table

SUBDIVISION REGULATIONS		
(a)	Minimum parcel area	48,349 m ²
(b)	Minimum parcel frontage	291 m
DEVELOPMENT REGULATIONS		
(c)	Maximum density	316,930 ft ² (29,443 m ²) FAR 0.236
(d)	Maximum coverage	98,855 ft ² (9,184 m ²) 7.36%
(e)	Maximum gross floor area and maximum units by building	
.1	Building 1 Building 2 Building 3 Building 4 Building 5	5,537 m ² (59,597 ft ²) up to 74 Units 5,906 m ² (63,572 ft ²) up to 78 Units 6,000 m ² (64,587 ft ²) up to 83 Units 6,000 m ² (64,587 ft ²) up to 83 Units 6,000 m ² (64,587 ft ²) up to 83 Units
	Total Gross Floor Area and Unit Count	29,443 m² (316,930 ft²) up to 401 Units
(f)	Maximum building height <ul style="list-style-type: none"> • Building 1. <i>(all floor levels to be considered as nominal 3.15m). (See Schedule B for schematic building sections)</i> • Building 2 • Building 3 • Building 4 • Building 5 	<ul style="list-style-type: none"> • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 3 storeys (9 m) exposed on-grade at the west facing internal street, 4 storeys (12 m) exposed at the east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade.

SITING REGULATIONS		
(h)	Buildings and structures shall be sited at least the distance from the feature indicated in the middle column below, that is indicated in the right-hand column opposite that feature:	
.1	Minimum front parcel boundary	4.50m (14'-6" ft)
.2	Rear parcel boundary	32.45m (106'-6")
.3	Interior side parcel boundary	44.50m (146'-0")
.4	Exterior side parcel boundary	4.50m (14'-6" ft)

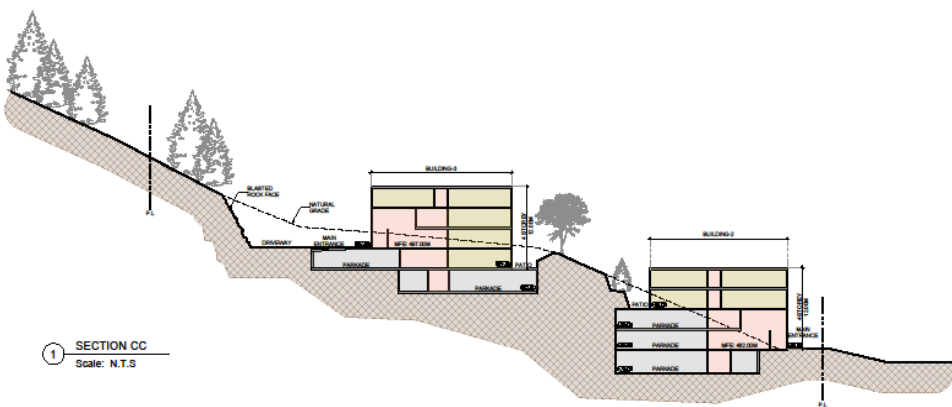
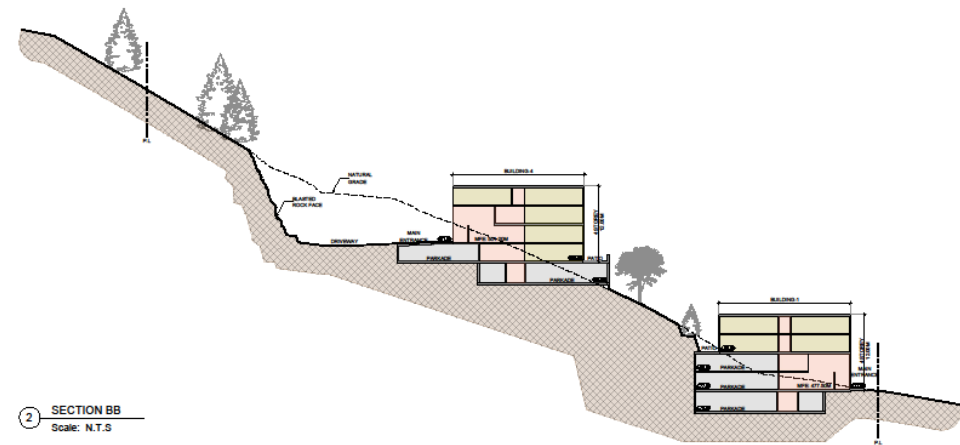
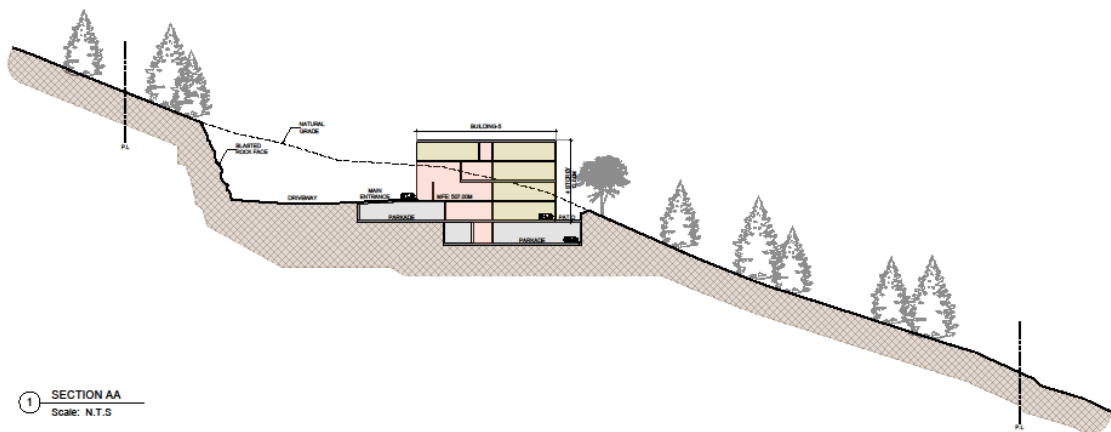
.5 Other Regulations:

AMENITIES		
(j)	The required areas of amenities are as follows: <i>(See Schedule A)</i>	
.1	Outdoor Amenity-1	3,925 ft ² (365 m ²)
.2	Outdoor Amenity-2	7,850 ft ² (729 m ²)
.3	Indoor Amenity	4,850 ft ² (451 m ²)

Schedule A



Schedule B





COUNCIL REPORT

To: Mayor and Council

Date: February 14, 2023

From: Paul Gipps, CAO

File No: Z 22-08

Subject: **Z 22-08; Zoning Amendment Bylaw No. 265.12, 2023 (First Reading); 2741 Auburn Road**

Report Prepared by: Jayden Riley, Planner III

RECOMMENDATION

THAT Council consider and resolve to give First Reading to City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023; and

THAT Council consider and resolve to direct staff to schedule the bylaw for Second Reading pending further traffic review and analysis to determine necessary off-site improvements to mitigate noted intersection failures near the surrounding site and the steps required to initiate those improvements.

STRATEGIC AREA(S) OF FOCUS

Economic Growth and Prosperity – We will work with our partners on the Westside and throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

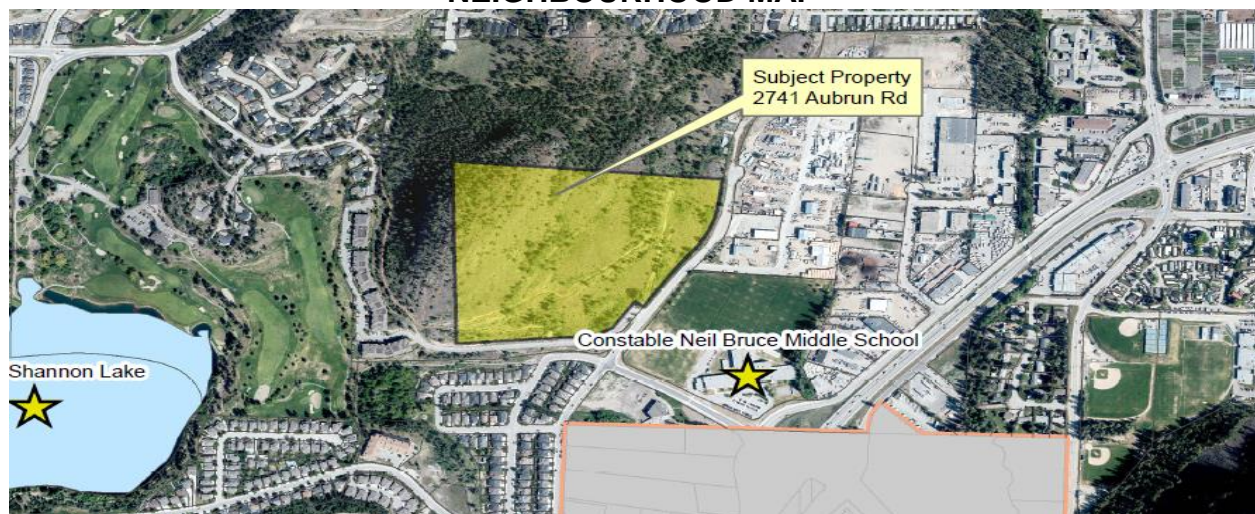
This application is seeking to rezone the subject property from Low Density Multiple Residential (R3) to Comprehensive Development (CD 10) and Parks and Open Space (P1) to accommodate a 401-unit apartment development. At the request of the applicant, staff are presenting the application for First Reading to receive feedback on the density limiting covenant and initial support before further investigating potential traffic improvements.

As a condition of Second Reading, staff are recommending that further traffic analysis is completed to confirm if there is potential for off-site improvements and the process of implementation.

PROPERTY DETAILS			
Address	2741 Auburn Road		
PID	026-159-601		
Folio	36414135.233		
Lot Size	12.4 ha		
Owner	0761681 BC LTD.	Agent	David Eaton Architecture
Current Zoning	Low Density Multiple Residential (R3)	Proposed Zoning	Comprehensive Development (CD 10); Parks and Open Space (P1)
Current OCP	Low Density Multiple Family, Parks and Natural Areas	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	Apartment / Condo
Development Permit Areas	Hillside, Sensitive Terrestrial		
Hazards	Hillside, Sensitive Terrestrial, Traffic		
Agricultural Land Reserve	None		

ADJACENT ZONING & LAND USES		
North	^	Rural Residential Large Parcel (RU4)
East	>	Compact Single Detached Residential (RC3) Light Industrial (I1), Heavy Industrial (I2), Gravel Extraction (I4)
West	<	Low Density Multiple Residential (R3)
South	v	Institutional and Assembly (P2)

NEIGHBOURHOOD MAP



PROPERTY MAP



Legislative Requirements

Council has the authority under Part 14, S.497 of the *Local Government Act* to amend its Zoning Bylaw.

DISCUSSION

Application History

The subject property was rezoned under File Z 07-12 from RU2 to R3A to accommodate a 220-unit development in the form of three apartment buildings, including a private recreational amenity facility. As a condition of rezoning, the City required the owner to 1) register a covenant restricting the density of the property to 220 units and 2) register a covenant to require the donation of a 7.6 ha portion of the site to the City as parkland at time of building permit.

The density covenant was intended to better align the proposal with the densities proposed under Shannon Lake Neighbourhood Plan (SLNP) (Appendix B-3 of the Westside Official Community Plan), which has since been repealed with the subsequent adoption of the current Official Community Plan Bylaw No. 0100. It should also be noted that the Auburn Road / Shannon Ridge Drive Road Connection was not constructed at the time the SNLP was adopted.

The density and parkland covenants are still registered on the property. The 401 units proposed under this current zoning amendment application would require Council to discharge the density covenant prior to adoption.

Proposal

This application is proposing to rezone a 4.8 ha portion of the subject property from Low Density Multiple Residential (R3) to a Comprehensive Development Zone (CD 10) and a 7.9 ha portion Parks and Open Space (P1) to support a 401-unit apartment development

contained within five buildings. Two of the five building are proposed to be located at the Auburn Road frontage with three buildings accessed via private lane at the upper elevation, further west of the site (*Figure 1*).

The apartment buildings are proposed to contain 74 to 83 units each. Specifically, 'Building 1' is proposed to contain 74 units; 'Building 2' is proposed to contain 78 units; and 'Buildings 3-5' are proposed to contain 83 units each. The CD Zone proposes each building to be four storeys, with two or three storeys exposed on the upland side and four storeys exposed on the street fronting façade of the buildings (*Attachment 1*). Each building is proposed to consist of mix of one- and two-bedroom units. Total parking requirements are anticipated to be met with a mixture of underground and surface parking.

The CD zone and site plan also include two outdoor amenity spaces over a total area of 1,094 sq. m. and an indoor amenity space contained within a 451 sq. m. area.



Figure 1: Site plan

Given that the proposal is to rezone to a CD zone, additional controls can be applied to the siting of building and other elements of a proposal that are more commonly identified

at Development Permit, as the site plan would be attached to the amendment bylaw as a schedule, once finalized.

Should Council give First Reading to the proposed amendment bylaw, the application will return with additional amendments/refinements to the CD zone criteria that reflect additional staff review and Council direction.

Policy and Bylaw Review

Official Community Plan Bylaw No.0100

The subject property contains Medium Density Multiple Family (MDMF) and Parks and Natural Areas Land Use Designations (LUDs). The MDMF LUD is intended to consist of semi-detached, attached, townhouse and/or low-rise apartment housing. Its purpose is to provide a broader range of housing opportunities in areas served by transit and in walking distance to community amenities, shops, and services, where the density is appropriate to the adjacent land use.

The subject property also contains a Parks and Natural Areas LUD over a 7.6-ha portion of the property¹. This LUD is intended to contain a variety of public, accessible green and natural space, as well as preserve significant natural areas and provide diverse recreation opportunities. No amendments the LUDs are proposed with this application.

The property is subject to multiple Development Permit Areas, including Steep Hillside, Sensitive Terrestrial Ecosystems, and Form and Character (Multiple Family and Intensive Residential). Submitted with the application includes a geotechnical report and environmental assessment. These reports are intended to provide information on general feasibility of the proposed CD zone. Subject to rezoning, more detailed reporting will be required at time of Development Permit.

Zoning Bylaw No. 0265

The subject property is currently zoned Low Density Multiple Residential (R3). This zone permits care facility, duplex, group home, and townhouse use up to three storeys. The purpose of this zone is to accommodate multiple residential in low density housing form. Apartment is not a permitted use in the R3 zone.

The proposed CD 10 zone (*Attachment 1*) is intended to accommodate 401 apartment units within five buildings. The CD zone criteria is intended to be specific to this proposal and outlines the number of units and storeys for each building. The CD zone criteria will also include a site plan as a schedule. What is presented in the existing CD Zone criteria is not necessarily final and may be amended prior to second reading.

In addition to establishing the siting of roads, amenity space, buildings and other proposed site features, the CD zone is also proposing to accommodate four-storey buildings otherwise not permitted in the existing R3 zone.

¹ This area is consistent with the parkland dedication area and proposed P1 zone boundary

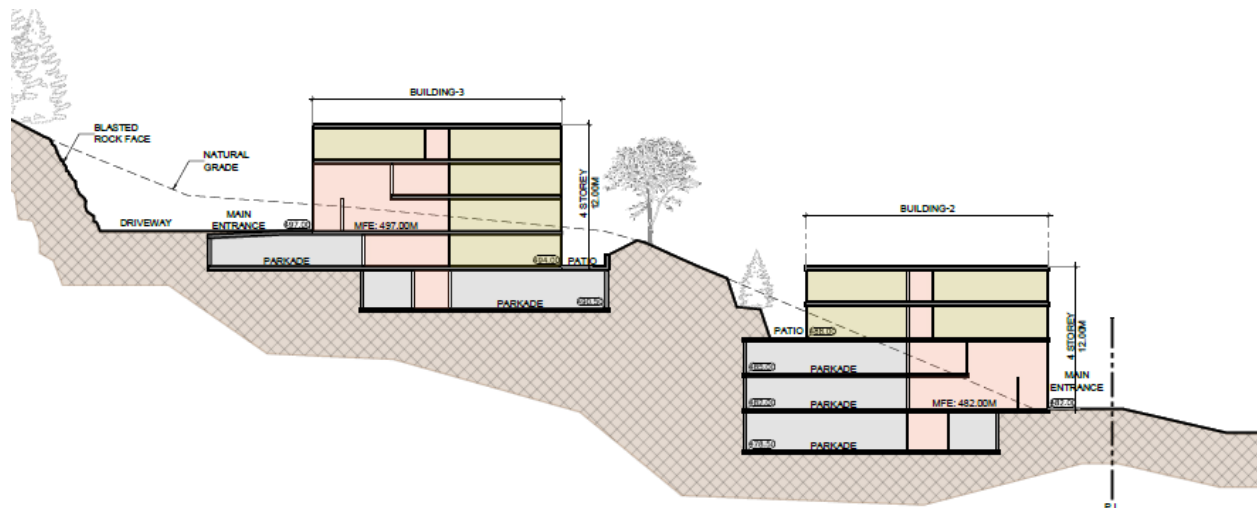


Figure 2: Sections of Buildings 2 and 3

Technical Review

Servicing

A Functional Servicing Report² was submitted with the application that outlines the existing and proposed services required to accommodate the proposal. The following is a summary of the servicing conditions:

- **Roads** – Auburn Road is an Urban Collector. The owner is responsible for any frontage improvements to meet Bylaw No. 0249 CWK Standard Detail Dwg 203, including streetlighting. The property is proposing two regular accesses from Auburn Road plus an emergency access road to 2780 Auburn Rd.
- **Water** – The site is not currently serviced. There is an existing looped 400 mm water main on Auburn Road which will provide a new 250 mm water service to the property. New off-site hydrants will be required along the property frontage spaced 90 m apart.
- **Sewer** – The site is not currently serviced. There is an existing 200 mm sewer main that runs through 2575 Auburn Road within a Statutory Right of Way. It is proposed to be extended across Auburn Road to the property line of the subject property. In anticipation of future development, a sanitary main is to be constructed on Auburn Road up to the north property line of this development.
- **Stormwater** – The site is not currently serviced. There is an existing 250 mm storm main located on Auburn Road. It is expected that this storm main will be extended along the full frontage of the subject property to provide a connection to the

² Prepared by Ecora Engineering & Resources Group Ltd., dated July 2022

development site and to provide storm drainage to the catch basins required for the frontage improvements. A new storm service off this main will be provided to the proposed development.

Geotechnical Investigation

A geotechnical report was conducted by Geopacific Consultants, dated November 8, 2021. The intention of the report was to confirm general feasibility of the proposed development via assessment of site conditions, and to provide general recommendations for future consideration. Subject to rezoning, more detailed geotechnical report information related to site grading would be provided at time of Development Permit.

The report notes the site contains gradients between 20 - 60% with small gravel access roads on the southeast portion of the site, vegetated with sparse mature coniferous trees and indigenous grass and shrubs, with bedrock outcrops over the majority of the property. The investigation found the site to be underlain by bedrock, encountered at all 12 test pits that were excavated in the development area.

The report also notes that a significant amount of rock removal and a series of retaining walls will be required to achieve final design grades on site, including excavation of several metres of bedrock, blasting, rock cuts and rock fall catchment ditches. Rock cuts are noted to potentially leave unstable rock masses which require additional work to stabilize, such as heavy scaling, meshing, or rock bolting. In conclusion, the report notes that from a geotechnical perspective the site is generally well-suited for the proposed development.

Environmental Assessment Report

The Environmental Assessment Report³ submitted with the application identifies ESA1 (high sensitivity) and ESA 2 (moderate) areas located within the proposed Parks and Open Space (P1) area of the site, northwest of the proposed development / CD area. The development area is determined to contain primarily ESA 3 (low sensitivity). A small portion of ESA 2 (moderate) is anticipated to be impacted by the proposed development; however, these impacts are generally aligned with the City's retention objectives of 40-80%, subject to the ecology of the surrounding ESA 1 areas not being impacted.

The report concludes with general recommendations for mitigation measures, habitat restoration, and environmental monitoring. Further information would be required at time of development permit, following rezoning.

Traffic Impact Assessment

The TIA⁴ prepared as part of the application noted that the Byland Road and Old Okanagan Highway intersection is expected to perform poorly for all scenarios along the stop-controlled approaches with and without site-generated traffic. The report states that future improvement options, including roundabouts or signalization at Daimler Drive, may be desirable in conjunction with improvements to the Highway 97 / Daimler Drive intersection (Figure 2).

³ Report prepared by Ecoscape Environmental Consultants Ltd., dated May 4, 2022

⁴ Report prepared by McElhanney Consulting Services Ltd., dated June 29, 2022

Highway 97 is identified to be near capacity by Opening Day (2024) and is expected to exceed capacity by 2034. Without capacity improvements along the Highway 97 corridor (e.g., six highway through-lanes), poor intersection performance at the Highway 97 / Daimler Drive intersection is expected.

The traffic improvements proposed include:

- All-way stop control at Auburn Road & Daimler Drive (within City's jurisdiction)
- Dual left turn lane on Daimler Drive approaching Highway 97
- Roundabout or signals at Daimler Drive & Old Okanagan Highway / Byland Road

Ministry of Transportation and Infrastructure Comments

It is important to note that 2 the intersections identified are controlled by the Ministry of Transportation and Infrastructure (Hwy 97 & Daimler Dr, Daimler Dr & Old Okanagan/Byland Road). MoTI was involved in the scoping of the TIA and its review and have advised they do not require any improvements as a condition of this development.

Additional Traffic Review

Given the noted intersection performance issues, staff have engaged the City's traffic engineer⁵ to consider additional options to address operational and safety concerns in the immediate area. The preliminary comments received indicate traffic conditions deteriorate with background growth⁶, and with the addition of development traffic conditions will be impacted at the time of development (in 2024) as follows:

- *The southbound stop-controlled approach of Auburn Road at Daimler Drive deteriorates to LOS F.*
- *Eastbound queue spillback from Highway 97 is expected to increase to 205 m in the PM peak with development compared to 128m in the PM peak background condition. This would extend beyond both the Constable Neil Bruce Middle School and Alliance Church accesses. The eastbound queue on Daimler Drive from Highway 97 has only 60 m of vehicle storage to Old Okanagan Highway / Byland Road.*
- *Northbound (Old Okanagan Highway) and southbound (Byland Road) movements fail, and queues have major spillback over 300 m in the simulation model accounting for intersection spillover⁷*

In reviewing the improvements presented by the applicant, the City's traffic consultant notes:

- *All-way stop control at Auburn Road & Daimler Drive improving operations from LOS E to LOS C. It is noted that in the simulation, this intersection operates well.*
- *Dual left turn lane on Daimler Drive approaching Highway 97 however this will impact current signal timings and may not fully relieve traffic queues.*

⁵ Align Engineering, April 2022

⁶ Excluding projected development traffic

⁷ Align Engineering updated memo, February 6, 2023

The preliminary transportation review concludes that *the existing closely spaced intersections on Daimler Drive from Highway 97 limits operations of the adjacent intersections with Old Okanagan Highway / Byland Road and Ross Road. A roundabout would be placed too close to the intersection to be functional without backing up the roundabout. Signals would need to communicate with the Highway 97 signal controller and could improve cross Daimler Drive movements but may not improve Daimler Drive queues. Other mitigation could consider turning restrictions and previous plans to recirculate traffic at Daimler Drive & Auburn Road with a roundabout, but this needs to be further explored.*

It is recommended⁷ that:

- *The City pursues resolving existing safety issues at Daimler Drive & Old Okanagan Highway / Byland Road and network options in partnership with WFN and MoTI in step with the City's Transportation Plan.*
- *Any additional measures that could be taken by the City of West Kelowna and/or MoTI to resolve operational and safety challenges.*

Based on this information, while staff are in general support of the proposed lands use, there are significant concerns with the impact of the development on the functionality of the existing intersections. As a result, staff are completing additional measures to resolve operational and safety challenges as recommended by the City's traffic consultant. Additional traffic review that will be presented at time of Second Reading.



Figure 2: Related Traffic Intersections

Density Covenant (LB138594)

A density covenant was registered on the subject property as a condition of rezoning under File Z 07-12 that restricts the total number of units to 220. Council would therefore need to discharge the covenant to move the application forward to adoption due to the proposed 401-unit density being proposed under the CD zone. If Council determines the covenant should remain, the applicant will have the option to develop the site under the maximum density and existing R3 zone, subject to compliance with Development Permit and other regulations, or modify their CD zone proposal.

Parkland Dedication

A parkland dedication covenant is registered on the property for the dedication of a 7.6-ha portion of land northwest of the development area, an area consistent with the proposed Parks and Open Space (P1) boundary (*Attachment 1*). The trigger for dedication noted in the covenant is at time of building permit. The parkland dedication was determined as a condition under File Z 07-12 following environmental review at the time the adjacent site, now containing the Terravita development, was subdivided from the parent parcel.

The City's Parks Department has confirmed that dedication of these lands is generally aligned with the City's Parkland Acceptance Policy and would provide an amenity opportunity for neighbourhood residents. More information on suitability of these lands for dedication in relation to the Parkland Acceptance Policy will be presented at time of Second Reading.

Referrals

Advisory Planning Commission

The Advisory Planning Commission (APC) was presented the application on July 20, 2022. The APC passed a motion recommending support for the proposal. Highlights from the discussion included the public benefit from the parkland dedication and future hiking trails, opportunities for traffic improvements, that the site was in a suitable location, as well as concerns related to increased traffic and potential on-street parking within the surrounding neighbourhood and accessibility through a school zone.

School District

The Central Okanagan Public Schools Operations Department noted the proximity of the subject property to the Constable Neil Bruce Middle School, which has a current enrollment of 961 students. Enrollment at the school is anticipated to peak at 1031 in 2025 and maintain over 900 students through to 2027. Concerns were noted in relation to traffic, given increased residential development in the area together with enrollment, particularly on Daimler Drive, and the safety and convenience for students, staff, and visitors to the school. Improvements in 2018 to Daimler Drive are described to have improved traffic flows and pedestrian safety. Also, to help improve drop-off / pick-up for the school, the School District has entered into an agreement with the Westside Alliance Church to utilize the church's parking lot. The School District suggests that the recent improvements may need to be considered again due to the anticipated increase of traffic volumes and access to the school site with the proposed development.

Public Notification

On-site Notice of Application signage has been installed on the subject property in accordance with the City's Applications Procedures Bylaw No. 0260. Should the application proceed to Public Hearing, a notice will be mailed to owners and tenants living within 100 m of the subject property and advertised in the local newspapers in accordance with the *Local Government Act*.

CONCLUSION

Staff recommend that Council give First Reading based on the following rationale:

- The applicant and City will engage in further traffic review and analysis to determine if there is potential to resolve noted intersection issues via off-site improvements. More information will be shared about potential improvements and their ability to address traffic issues, as well as the steps required to initiate those improvements, at time of Second Reading.
- The proposed use is consistent with the existing Land Use designation of Low Density Residential.
- The proposed multi-family use of the site is determined to be generally suitable for the neighbourhood, notwithstanding potential traffic issues.

Alternate Resolution:

1. That Council postpone consideration of the amendment bylaw.

THAT Council consider and resolve to postpone First Reading to the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08).

Should Council postpone consideration of the proposed amendment bylaw, further direction to staff on how to proceed is requested.

2. That Council deny the amendment bylaw.

THAT Council consider and resolve to deny First Reading to the City of West Kelowna Zoning Amendment Bylaw No. 0265.12, 2023 (File: Z 22-08) and abandon the bylaw; and

THAT Council consider and resolve to direct staff to close the file.

Should Council deny First Reading to the amendment bylaw, the file will be closed in accordance with the Application Procedures Bylaw No. 0260.

REVIEWED BY

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Zoning Amendment Bylaw No. 0265.12, 2023
2. Delegation Request



CITY OF WEST KELOWNA

BYLAW NO. 0265.12

A BYLAW TO AMEND "ZONING BYLAW NO. 0265"

WHEREAS the Council of the City of West Kelowna desires to amend "CITY OF WEST KELOWNA ZONING BYLAW NO. 0265" under the provisions of the *Local Government Act*.

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna, in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as "CITY OF WEST KELOWNA ZONING AMENDMENT BYLAW NO. 0265.12, 2023".

2. Amendments

"Zoning Bylaw No. 0265" is hereby amended as follows:

2.1 By adding the attached zoning regulations for the CD 10 – Comprehensive Development Zone (Auburn Road) to Part 14 – Comprehensive Development Zones.

2.2 By adding the zone "Comprehensive Development Zone (Auburn Road)" and zone abbreviation "CD10" to Part 5 – Establishment of Zones, Table 5.1.

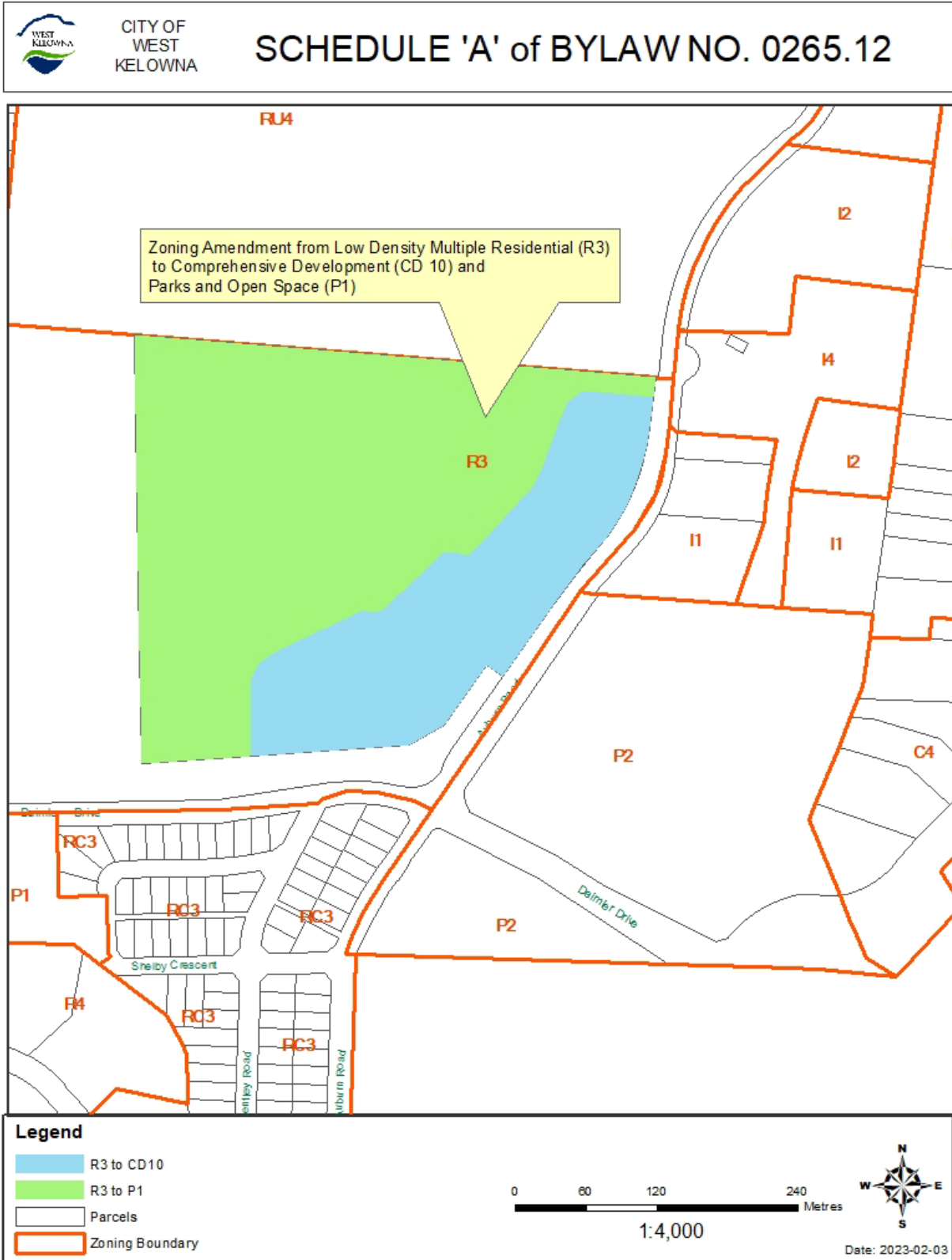
2.3 By adding "CD10 – Comprehensive Development Zone (Auburn Road)" to the Table of Contents.

2.4 By changing the zoning of Lot 2 District Lot 2601 Osoyoos Division Yale District Plan KAP77227, as shown on Schedule 'A' attached to and forming part of the bylaw from Low Density Multiple Family (R3) to Comprehensive Development (CD 10) and Parks and Open Space (P1).

2.5 By depicting the change on "Zoning Bylaw No. 0265 Schedule B" (Zoning Bylaw map).

MAYOR

CORPORATE OFFICER



CD10 – Comprehensive Development Zone (Auburn Road)

.1 Purpose

To accommodate multiple residential units the Shannon Lake neighbourhood.

.2 Principal Uses, Buildings and Structures

(a) Apartment

.3 Secondary Uses, Buildings and Structures

(a) Accessory uses, buildings, and structures

(b) Home based business, minor

.4 Regulations Table

SUBDIVISION REGULATIONS		
(a)	Minimum parcel area	48,349 m ²
(b)	Minimum parcel frontage	291 m
DEVELOPMENT REGULATIONS		
(c)	Maximum density	316,930 ft ² (29,443 m ²) FAR 0.236
(d)	Maximum coverage	98,855 ft ² (9,184 m ²) 7.36%
(e)	Maximum gross floor area and maximum units by building	
.1	Building 1	5,537 m ² (59,597 ft ²) up to 74 Units
	Building 2	5,906 m ² (63,572 ft ²) up to 78 Units
	Building 3	6,000 m ² (64,587 ft ²) up to 83 Units
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(f)	Maximum building height	
	<ul style="list-style-type: none"> • Building 1. <i>(all floor levels to be considered as nominal 3.15m). (See Schedule B for schematic building sections)</i> • Building 2 • Building 3 • Building 4 • Building 5 	<ul style="list-style-type: none"> • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 3 storeys (9 m) exposed on-grade at the west facing internal street, 4 storeys (12 m) exposed at the east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade.

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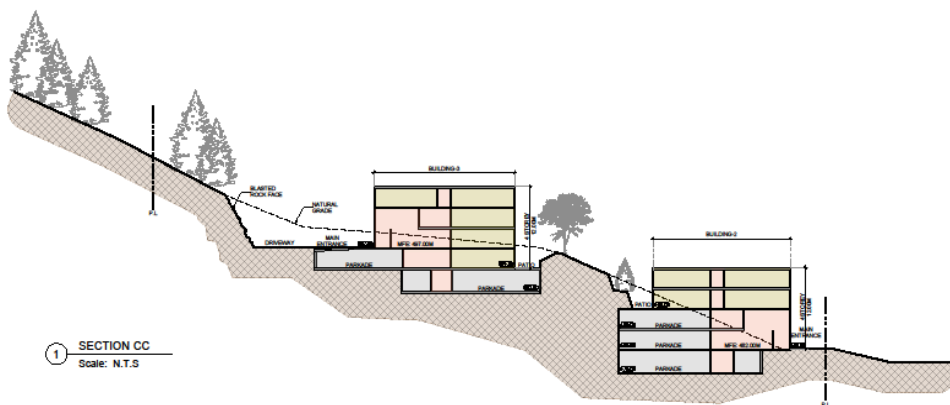
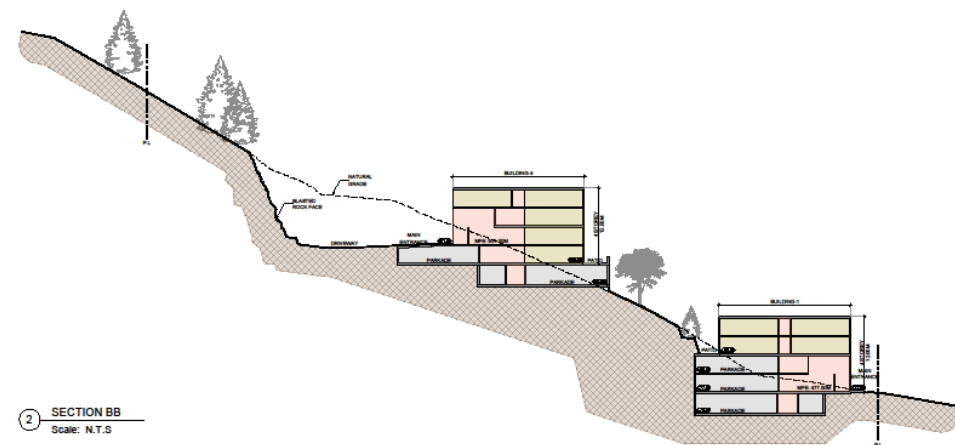
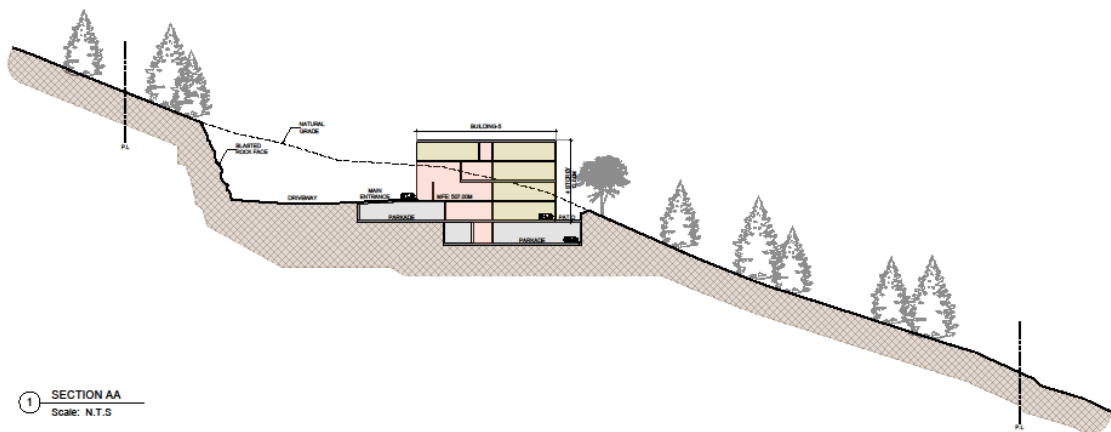
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.2	Outdoor Amenity-2	7,850 ft ² (729 m ²)
.3	Indoor Amenity	4,850 ft ² (451 m ²)

Schedule A



Schedule B



From: noreply@esolutionsgroup.ca
To: [Delegation Requests](#)
Subject: New Response Completed for Request to Appear As a Delegation
Date: February 9, 2023 3:27:21 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe. **The City will never ask for personal or account information or account password through email.** If you feel this email is malicious or a scam, please forward to westkelowna@phishforward.beaucheronsecurity.com.

Hello, Please note the following response to Request to Appear As a Delegation has been submitted at Thursday February 9th 2023 3:26 PM with reference number 2023-02-09-003.

- **Request to appear as a delegation on:**
2/14/2023
- **Name of person making the presentation:**
Kiran Nagra and Rachael Sansom
- **Name of the group or organization that the person is representing:**
LandVision Group
- **Daytime phone number:**
[REDACTED] (Kiran) [REDACTED] (Rachael)
- **Email address:**
kiran@landvisiongroup.com, [REDACTED]
- **Presentation title:**
Zoning Bylaw Amendment for 2741 Auburn Road
- **What is your request to Council?**
Introduction of the LandVision team and overview of the Application
the Zoning Bylaw Amendment for 2741 Auburn Road
- **Presentation points:**
Introduction of the team
Reason for the rezoning amendment request
Project background and overview
Next steps

September 7th , 2023

Mayor and Council
City of West Kelowna
2760 Cameron Road
West Kelowna B.C. V1Z 2T6

Dear Mayor and Council,

Re: Daimler Road Safety Improvements

Introduction

1360948 BC Ltd., represented by the LandVision Group, (the “developer”) has made an application to increase the density of the property at 2741 Auburn Drive to accommodate 401 condominium homes, from the currently approved 220 units.

The project is adjacent to a road network that is experiencing pressures from the existing traffic and parking demands. Daimler Road lies adjacent to Constable Bruce Middle School, which contributes to congestion and safety concerns at school drop-off and pick-up hours. The intersection at Old Okanagan Highway and Byland Road is already operating at failing levels of service during the school peak hours. Additionally, staff have noted that there is a parking issue on Auburn Drive at the school fields during soccer season, that causes conflict with industrial traffic travelling to and from the industrial lands to the north. These problems have been identified for quite some time, and were previously discussed by West Kelowna, the School District and the Ministry of Transportation and Infrastructure (MoTI).

As a part of the analysis of the development proposal for Auburn Drive, Align Engineering was commissioned by the City of West Kelowna to review current and future traffic safety concerns and propose solutions. Several options have been presented, and after discussions with City Engineering staff, Option 1 was selected as the preferred improvement approach. This option includes dual left turns from Daimler northbound to Highway 97, restricting the through and left turn movements from Old Okanagan Highway and Byland Road, and a roundabout at Auburn Road and Daimler Drive.

Cost Share and Phasing Model

The developers of the Auburn Project wish to be part of the solution. Because the proposed road works will alleviate existing traffic problems, regardless of whether the Auburn project proceeds, the developers respectfully request that Council consider a cost share model to assist with the expense of these road improvements as follows:

Phase	Item	Building Phase	Cost Share
1	• Extra Parking & Frontage works Auburn Road	Building Permit for 1 st unit	100% LandVision
	• Daimler Dual Left Turn Hwy 97		50% Landvision 50% West Kelowna
2	OOR/Byland/Daimler Intersection & Auburn Roundabout	Building Permit for 236 th unit	50% Landvision 50% West Kelowna

The cost of the required works would be secured at building permit, and constructed concurrently with that phase of development so that the improvements are complete by the time each building is occupied.

1. As a condition of the 1st building permit, Landvision will commit to the frontage improvements along Auburn Drive including extra parking for the soccer fields and will provide 50% of the cost of the dual left turn improvements at Daimler Road and Highway 97.
2. As a condition of the building permit for the 236th unit, Landvision will provide 50% of the cost of the roundabout at Auburn and Daimler Road, and the intersection improvements at Old Okanagan Highway and Byland Roads.

Conclusion

A phased cost share model between the developer and the City of West Kelowna will guarantee that the works are constructed in a timely manner to offset the impacts of the development as it builds out. We look forward to working with the City to improve safety and traffic efficiency in this neighbourhood.

Respectfully,

LandVision Group

CITY OF WEST KELOWNA

BYLAW NO. 0265.12

A BYLAW TO AMEND "ZONING BYLAW NO. 0265"

WHEREAS the Council of the City of West Kelowna desires to amend "CITY OF WEST KELOWNA ZONING BYLAW NO. 0265" under the provisions of the *Local Government Act*.

THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna, in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw may be cited as "CITY OF WEST KELOWNA ZONING AMENDMENT BYLAW NO. 0265.12, 2023".

2. Amendments

"Zoning Bylaw No. 0265" is hereby amended as follows:

2.1 By adding the attached zoning regulations for the CD 10 – Comprehensive Development Zone (Auburn Road) to Part 14 – Comprehensive Development Zones.

2.2 By adding the zone "Comprehensive Development Zone (Auburn Road)" and zone abbreviation "CD10" to Part 5 – Establishment of Zones, Table 5.1.

2.3 By adding "CD10 – Comprehensive Development Zone (Auburn Road)" to the Table of Contents.

2.4 By changing the zoning of Lot 2 District Lot 2601 Osoyoos Division Yale District Plan KAP77227, as shown on Schedule 'A' attached to and forming part of the bylaw from Low Density Multiple Family (R3) to Comprehensive Development (CD 10) and Parks and Open Space (P1).

2.5 By depicting the change on "Zoning Bylaw No. 0265 Schedule B" (Zoning Bylaw map).

READ FOR A FIRST TIME THIS 14TH DAY OF FEBRUARY, 2023

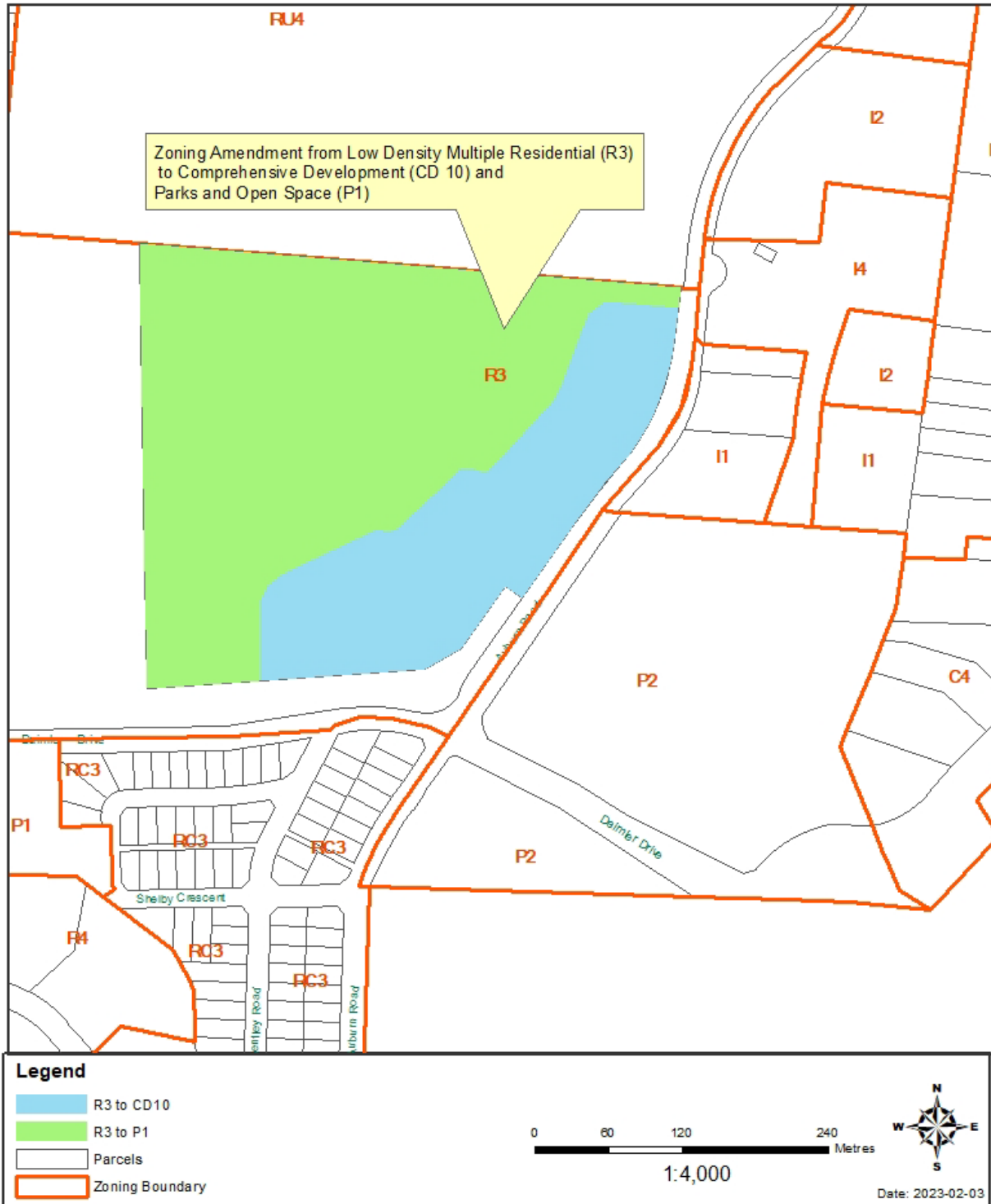
MAYOR

CORPORATE OFFICER



CITY OF
WEST
KELOWNA

SCHEDULE 'A' of BYLAW NO. 0265.12



CD10 – Comprehensive Development Zone (Auburn Road)

.1 Purpose

To accommodate multiple residential units the Shannon Lake neighbourhood.

.2 Principal Uses, Buildings and Structures

- (a) Apartment

.3 Secondary Uses, Buildings and Structures

- (a) Accessory uses, buildings, and structures
(b) Home based business, minor

.4 Regulations Table

SUBDIVISION REGULATIONS		
(a)	Minimum parcel area	48,349 m ²
(b)	Minimum parcel frontage	291 m
DEVELOPMENT REGULATIONS		
(c)	Maximum density	316,930 ft ² (29,443 m ²) FAR 0.236
(d)	Maximum coverage	98,855 ft ² (9,184 m ²) 7.36%
(e)	Maximum gross floor area and maximum units by building	
.1	Building 1 Building 2 Building 3 Building 4 Building 5	5,537 m ² (59,597 ft ²) up to 74 Units 5,906 m ² (63,572 ft ²) up to 78 Units 6,000 m ² (64,587 ft ²) up to 83 Units 6,000 m ² (64,587 ft ²) up to 83 Units 6,000 m ² (64,587 ft ²) up to 83 Units
	Total Gross Floor Area and Unit Count	29,443 m² (316,930 ft²) up to 401 Units
(f)	Maximum building height <ul style="list-style-type: none"> • Building 1. <i>(all floor levels to be considered as nominal 3.15m). (See Schedule B for schematic building sections)</i> • Building 2 • Building 3 • Building 4 • Building 5 	<ul style="list-style-type: none"> • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 2 storeys (6 m) exposed at the west façade; 4 storeys (12 m) exposed at the east street front. • 4 storeys (12 m): 3 storeys (9 m) exposed on-grade at the west facing internal street, 4 storeys (12 m) exposed at the east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade. • 4-Storeys (12 m): 3 storeys (9 m) exposed on-grade at the north-west facing internal street, 4 storeys (12 m) exposed at the south-east façade.

SITING REGULATIONS		
(h)	Buildings and structures shall be sited at least the distance from the feature indicated in the middle column below, that is indicated in the right-hand column opposite that feature:	
.1	Minimum front parcel boundary	4.50m (14'-6" ft)
.2	Rear parcel boundary	32.45m (106'-6")
.3	Interior side parcel boundary	44.50m (146'-0")
.4	Exterior side parcel boundary	4.50m (14'-6" ft)

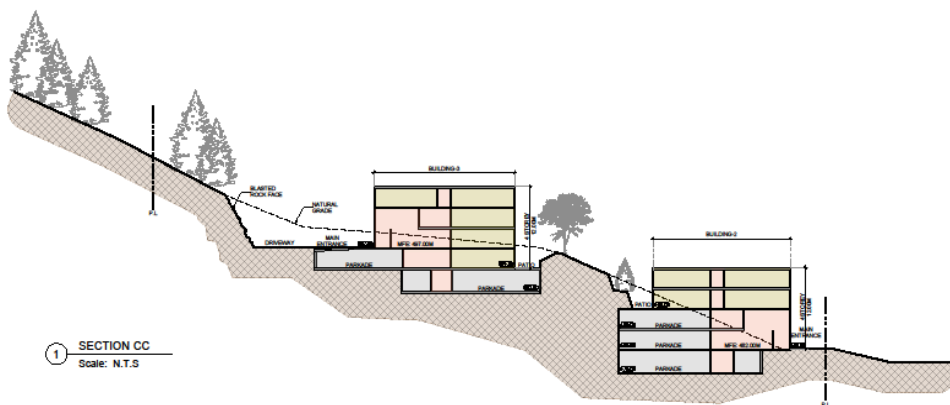
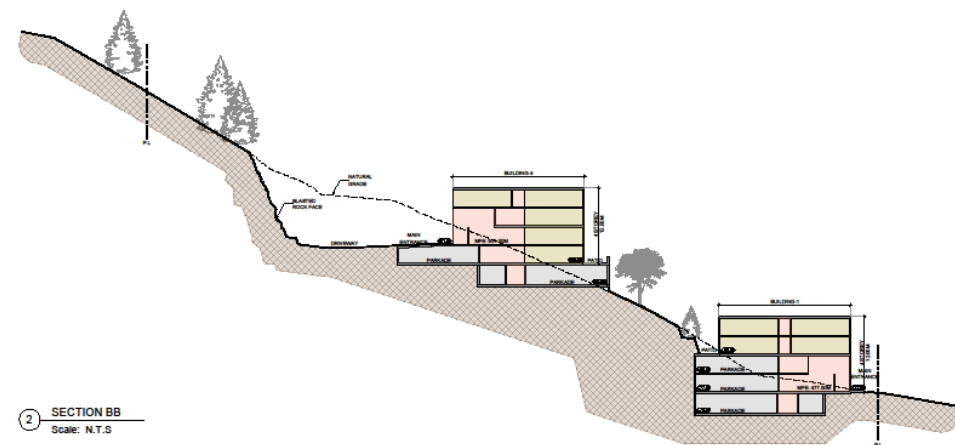
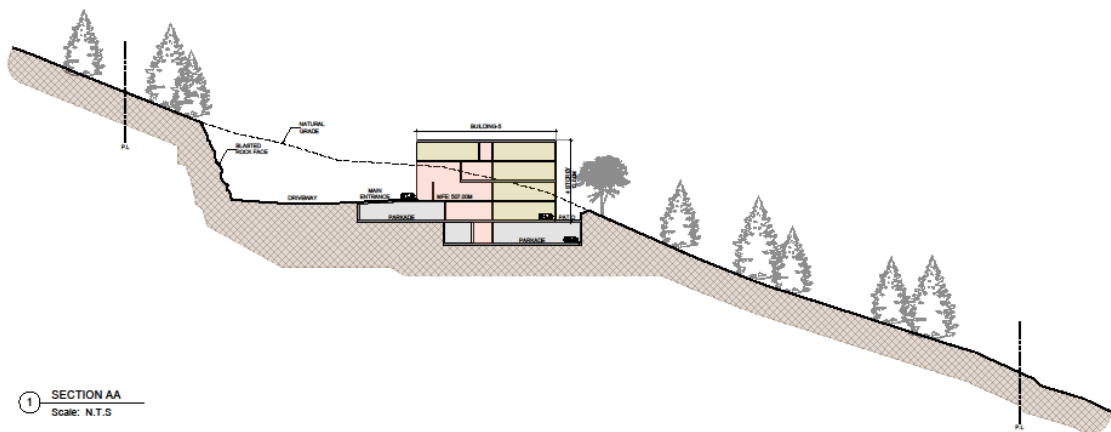
.5 Other Regulations:

AMENITIES		
(j)	The required areas of amenities are as follows: (See Schedule A)	
.1	Outdoor Amenity-1	3,925 ft ² (365 m ²)
.2	Outdoor Amenity-2	7,850 ft ² (729 m ²)
.3	Indoor Amenity	4,850 ft ² (451 m ²)

Schedule A



Schedule B





DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: July 11, 2023

From: Paul Gipps, CAO

Subject: **ALC Standing Resolution; 1221 Hudson Road, 3044 Sandstone Drive, and 2829 Inverness Road**

Report Prepared By: Yvonne Mitchell, Planner II

RECOMMENDATION to Consider and Resolve:

THAT Council, pursuant to the *Agricultural Land Commission Act*, authorize the Director of Development Approvals, to directly forward to the Agricultural Land Commission with support, the following Non-Farm Use Applications:

For all school and childcare related uses at Hudson Road Elementary (1221 Hudson Road), Shannon Lake Elementary (3044 Sandstone Drive), and Webber Road Community Centre (2829 Inverness Road) where:

- a. the proposal complies with the City of West Kelowna's Zoning Bylaw as amended or replaced from time to time;
- b. the proposal complies with the City of West Kelowna's Official Community Plan as amended or replaced from time to time; and
- c. the property is owned by the Board of School Trustees of School District No. 23 (Central Okanagan)

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

1221 Hudson Road, is within the Lakeview Heights neighbourhood, and is occupied by Hudson Road Elementary School and an early learning facility currently under construction. 3044 Sandstone Drive, is within the Shannon Lake neighbourhood, and is occupied by Shannon Lake Elementary School. 2829 Inverness Road, is within the Glenrosa neighbourhood, and is occupied by Webber Road Community Centre. All three

properties have a long history of institutional use with Hudson Road Elementary constructed in 1974, Shannon Lake Elementary in 1993, and the Weber Road Community Centre in 1983. All three properties are surrounded by residential neighbourhoods and are located within the Agricultural Land Reserve (ALR).

Given their long-term institutional use, an application was made to exclude these properties from the ALR in 2019. The application was supported by Council, but denied by the Agricultural Land Commission (ALC). As the exclusion request was denied, any school or childcare-related uses on the properties must continue to be approved by means of a non-farm use application considered by Council and then forwarded to the ALC.

In addition to the previous exclusion, Council has supported all previous non-farm use applications for these properties. These applications include an application in 2021 to construct an early learning facility at Hudson Road Elementary (File No. 21-04), and more recently to permit school related activities in dedicated school footprint areas at Hudson Road and Shannon Lake Elementary (File No. A 23-02).

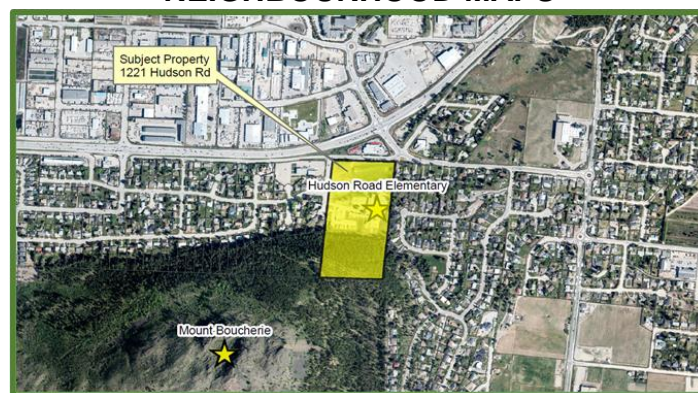
PROPERTY DETAILS			
Address		1221 Hudson Road, 3044 Sandstone Drive, 2829 Inverness Road	
PID		010-346-961 and 004-771-133 and 007-928-190	
Folio		36412732.000 and 36414114.442 and 36414732.099	
Lot Size		19.5 acres (78,954m2) and 7.1 acres (28,895m2) and 13.8 acres (56,041m2)	
Owner	The Board of School Trustees of School District No 23 (Central Okanagan)		
Current Zoning	Institutional and Assembly Zone (P2)	Proposed Zoning	N/A
Current OCP	Educational/Institutional	Proposed OCP	N/A
Current Use	Education facility, care facility, major, and community hall	Proposed Use	N/A
Development Permit Areas	N/A		
Hazards	None		
Agricultural Land Reserve	Yes		
ADJACENT ZONING & LAND USES - 1221 Hudson Road			
North	^	Gasoline Service Station Commercial Zone (C3)	
East	>	Single Detached Residential Zone (R1)	
West	<	Single Detached Residential Zone (R1), Institutional and Assembly Zone (P2), and Parks and Open Space Zone (P1)	
South	v	Parks And Open Space Zone (P1)	
ADJACENT ZONING & LAND USES - 3044 Sandstone Drive			

North	^	Single Detached Residential Zone (R1)
East	>	Single Detached Residential Zone (R1)
West	<	Single Detached Residential Zone (R1)
South	v	Single Detached Residential Zone (R1)

ADJACENT ZONING & LAND USES – 2829 Inverness Road

North	^	Single Detached Residential Zone (R1)
East	>	Single Detached Residential Zone (R1)
West	<	Single Detached Residential Zone (R1)
South	v	Single Detached Residential Zone (R1) and Rural Residential Small Parcel Zone (RU2)

NEIGHBOURHOOD MAPS



DISCUSSION

Under Section 20 of the *Agricultural Land Commission Act (ALCA)* an owner may apply to the ALC for a non-farm use application. Under Section 34.1 of the *ALCA* such application must not proceed to the ALC for consideration unless authorized by a resolution of the local government. By passing a standing resolution, applications would be automatically forwarded to the ALC for consideration. A new standing resolution is required after each municipal election or if there are significant changes to the ALC regulations.

Given the consistency of Council decisions to forward previous non-farm use applications to the ALC with support, staff recommend Council consider a standing resolution for these three school district properties. If the standing resolution is passed, non-farm use applications for school and childcare related uses will be automatically forwarded with support to the ALC if the following requirements are met:

- the proposal complies with the City of West Kelowna's Zoning Bylaw as amended or replaced from time to time;
- the proposal complies with the City of West Kelowna's Official Community Plan as amended or replaced from time to time; and
- the property is owned by the Board of School Trustees of School District No. 23 (Central Okanagan)

The City of West Kelowna continues to grow and with it, the demand for school and childcare facilities. Westside schools are facing significant capacity challenges and childcare spaces are hard to find. Any School District capital project is contingent on receiving funding from the Ministry of Education and Child Care. To complete capital projects on the three properties, the School District must also submit non-farm use applications to the ALC, no matter the size of the project or the possibility of receiving funding. This standing resolution for non-farm use applications on the three properties will allow school and childcare facility challenges in the City to be addressed more quickly.

Impact on Application Timelines

Current application process requires all non-farm use applications to be prepared for and approved by Council; approval of a blanket resolution for these 3 sites will reduce non-farm use application timelines AND reduce the additional burden on planning staff workload, which results in increased application timelines in other priority areas.

Policy and Bylaw Review

Official Community Plan Bylaw No. 0300

The land use designation in the Official Community Plan for the three properties is Educational/Institutional. The Educational/Institutional designation recognizes the importance of providing land for these uses in a growing City, and the areas under the

designation are selected in co-ordination with relevant jurisdictions to foster locations that are suitable to effective service delivery across the entire City.

Zoning Bylaw No. 0265

The three properties are Institutional and Assembly Zone (P2) in the Zoning Bylaw. The P2 Zone permits the proposed school and childcare related uses. If a non-farm use application is approved, the use would be required to meet other regulations in the Zoning Bylaw such as setbacks, height, and parking.

Agricultural Plan

The City's Agricultural Plan includes Recommendation 11: Protection of Agricultural Land. The Recommendation includes the following objective: To protect agricultural land in the City of West Kelowna for agricultural purposes.

The Plan does not speak to ALR Lands with existing school or community centre operations on them.

CONCLUSION

If passed, the standing resolution will support Council's commitment to institutional and childcare related uses on SD 23 lands and increase application timelines.

Alternate Recommendation to Consider and Resolve:

1. **THAT** Council postpone consideration of the standing resolution.

If Council chooses to postpone, further direction is requested by staff.

2. **THAT** Council deny consideration of the standing resolution.

REVIEWED BY

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☒ No ☐



COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

Subject: **Adoption of Local Area Service Inclusion – 1135 Westside Road**

Report Prepared by: Shaun Courtney, Senior Development Engineering Technologist

RECOMMENDATION to Consider and Resolve:

THAT Council adopt Bylaw No. 0280.02 being “City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Bylaw No. 0280 2020”.

STRATEGIC AREA(S) OF FOCUS

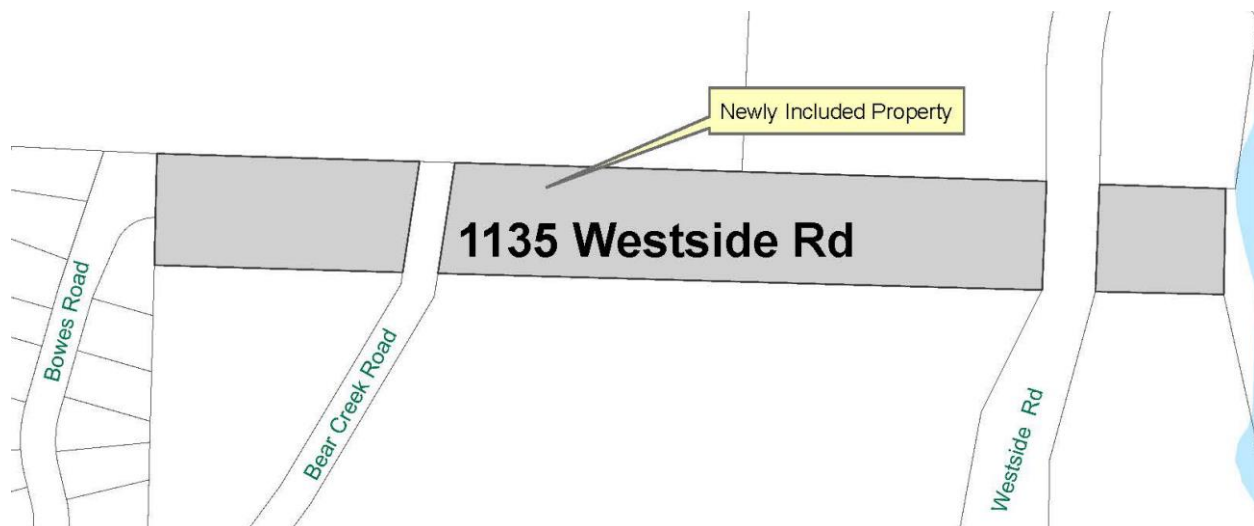
Invest in Infrastructure – We will invest in building, improving, and maintaining infrastructure to meet the needs of, and to provide a high quality of life for, current and future generations.

BACKGROUND

Under the *Local Government Act*, Division 5 – Local Service Taxes, a local area service is a municipal service that is to be paid for in whole or in part by a local service tax imposed only on the benefiting properties. A municipality must adopt a bylaw to establish a local area service. When a property located outside the local area service boundary wishes to be added or included in the service area, the property owner must petition the municipality to amend the boundary of the service area.

A petition to amend the boundary of the City of West Kelowna Rose Valley Water Treatment Plant Local Area Service was received from the owners of DL668 ODYD, Portion N 210 Ft, Except Plan 43386, and was certified by the Corporate Officer at the July 25, 2023, meeting of Council.

The petition for inclusion into the local service area is requested to provide approximately a 19-lot single family residential strata development. The current re-zoning application is for the westerly portion of the property to Compact Single Detached Residential Zone (Strata) – RC4 to facilitate an approximately 19 lot single family residential strata development. These services would be installed under subdivision servicing and would connect to the City’s water infrastructure.



FINANCIAL IMPLICATIONS

The costs associated with the connection of the subject property to the community water system will be recovered in accordance with the cost recovery mechanism authorized under District of West Kelowna Local Area Water Bylaw No. 0080.

Water connection costs and fees will be paid at time of Building Permit in accordance with the City's Fees and Charges Bylaw No. 0028.

The water Development Cost Charges (DCC's), to be collected under Bylaw 0190 at time of final subdivision for the approximately 19 single family units, are estimated to be \$178,320.

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
July 25, 2023	<p>THAT Council receives the Certificate of Sufficiency from the Corporate Officer pertaining to the owner initiated Local Area Service (LAS) petition to provide water service to District Lot 668, ODYD, Portion N 210 Ft, Except Plan 43386 (PID 011-555-190); and</p> <p>THAT Amendment Bylaw No. 0280.02 being "City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Bylaw No. 0280 2020" be given first, second and third reading.</p> <p><u>CARRIED UNANIMOUSLY</u></p>	C295/23

REVIEWED BY

Bob Dargatz, Development Engineer Manager

Allen Fillion, Director of Engineering & Operations

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

Powerpoint: Yes ☐ No ☒

Attachments:

1. Bylaw No. 0280.02

CITY OF WEST KELOWNA

BYLAW NO. 0280.02

A BYLAW TO AMEND THE ROSE VALLEY WATER TREATMENT PLANT
LOCAL AREA SERVICE ESTABLISHMENT BYLAW

WHEREAS the Council of the City of West Kelowna wishes to amend, in accordance with Section 218 of the *Community Charter*, the established local area service to include District Lot 668, ODYD, Portion N 210 Ft, Except Plan 43386 located on Westside Road; and

WHEREAS the expansion of the service and its cost recovery methods have been proposed by a petition process in accordance with Section 212 of the *Community Charter*;

NOW THEREFORE BE IT RESOLVED that the Council of the City of West Kelowna in open meeting assembled, hereby enacts as follows:

1. Title

This Bylaw shall be cited for all purposes as the “City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Amendment Bylaw No. 0280.02, 2023”.

2. Amendments

“City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Bylaw No. 0280” is hereby amended as follows:

2.1 Boundaries

The boundaries of the local area service created under Bylaw No. 0280 cited as the “City of West Kelowna Rose Valley Water Treatment Plant Local Area Service Establishment Bylaw No. 0280” are hereby extended to include District Lot 668, ODYD, Portion N 210 Ft, Except Plan 43386, as shown as the subject property identified in Schedule A, attached hereto and forming part of this Bylaw.

READ A FIRST, SECOND AND THIRD TIME THIS 25TH DAY OF JULY, 2023
ADOPTED

MAYOR

CORPORATE OFFICER



SCHEDULE A

Date: 2023-07-14



File Bylaw No. 0280.02

1135 Westside Rd Petition

Legal Description: District Lot 668, ODYD, Portion N 210 Ft, Except Plan 43386

0 50 100 200 Metres





INFORMATION ONLY COUNCIL REPORT

To: Mayor and Council

Date: September 26, 2023

From: Paul Gipps, CAO

File No: 5330-20-56

Subject: **Shannon Lake Road Active Transportation Corridor**

Report Prepared by: Evan Sun, Engineering Technologist II

STRATEGIC AREA(S) OF FOCUS

Invest in Infrastructure – We will invest in building, improving and maintaining infrastructure to meet the needs of, and to provide a high quality of life for, current and future generations.

BACKGROUND

Conceptual Design Background

Shannon Lake Road is classified as an Urban Arterial road that provides essential connectivity and accessibility to Old Okanagan Highway and Stevens Road. Shannon Lake Road from the Westbank First Nation Boundary to Swite Road lacks cycling and pedestrian facilities to provide a safe means of transportation for all users. Shannon Lake Elementary School is in the vicinity of the project area and the Shannon Lake Active Transportation Corridor project will provide connectivity to the existing sidewalk on both ends of the project. Accesses along the project corridor include the Kinsmen Park baseball and soccer fields, the commercial complex south of the Shannon Lake Road/Shannon Way intersection, the Westside Transfer Station at Saddleback Way/Asquith Road intersection and the parking lot at Swite Road to access the Shannon Lake Regional Park.

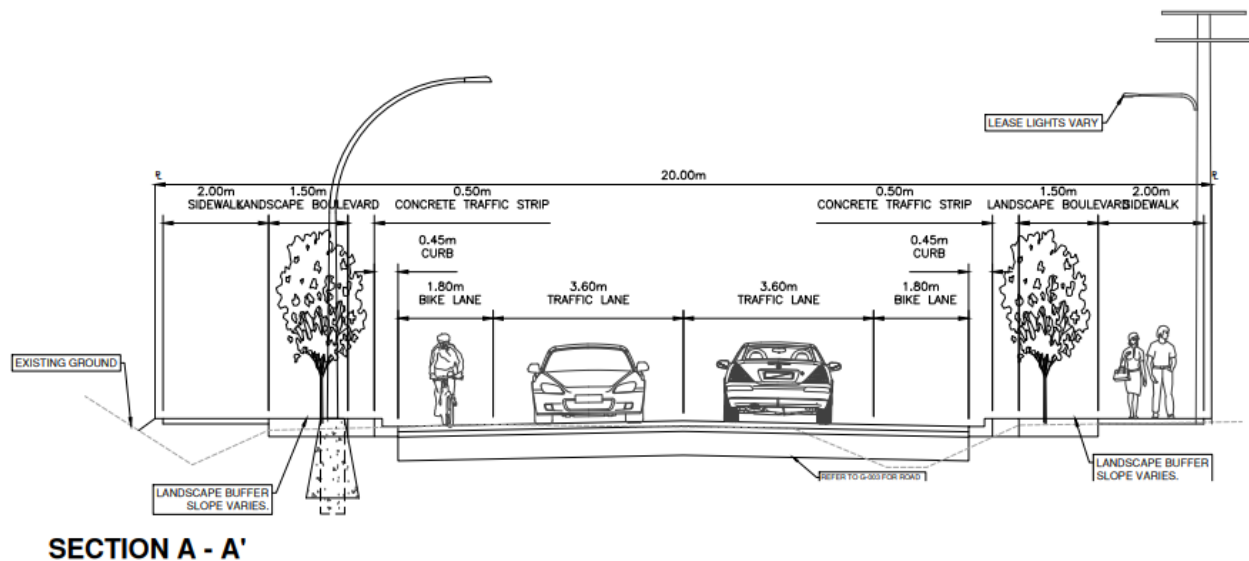
In April 2022, the City of West Kelowna awarded the Shannon Lake Road Active Transportation Corridor project between the Westbank First Nation boundary and Swite Road to Stantec Consulting. The design for the Shannon Lake Active Transportation Corridor project will include 1.1-kilometre sidewalk, road upgrades, bike lanes, drainage improvements, landscape improvements, and streetlights.

Detailed Design

The design utilizes the City's Urban Arterial Standard Detail Drawing in the Works and Services Bylaw No. 0249 where practical (see Figure 1). There are areas where the design deviates from the City's standard for an Urban Arterial due to existing site

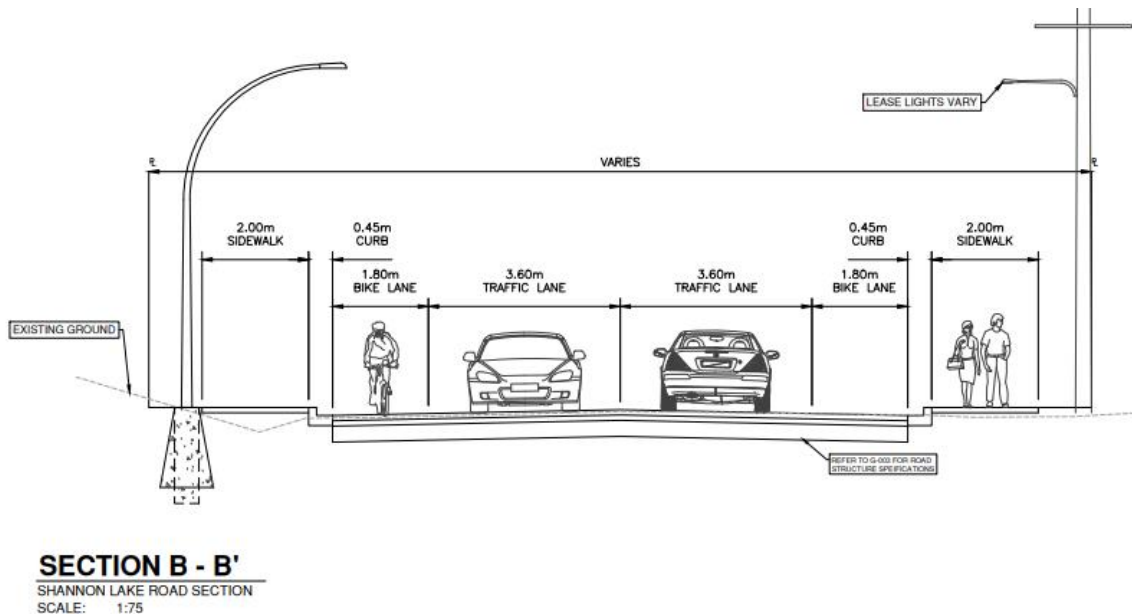
constraints such as topography, available right of way, and existing utilities. Deviating the design standards followed a similar process as other major roadway improvements, such as the designs for Glenrosa Road, Boucherie Road, and Stevens Road/Shannon Lake Road. There are four different cross sections proposed, depending on the constraints encountered. Please refer to Appendix A for full set concept streetscape plans and cross-section view of Shannon Lake Road.

Figure 1 – Shannon Lake Road Section A-A



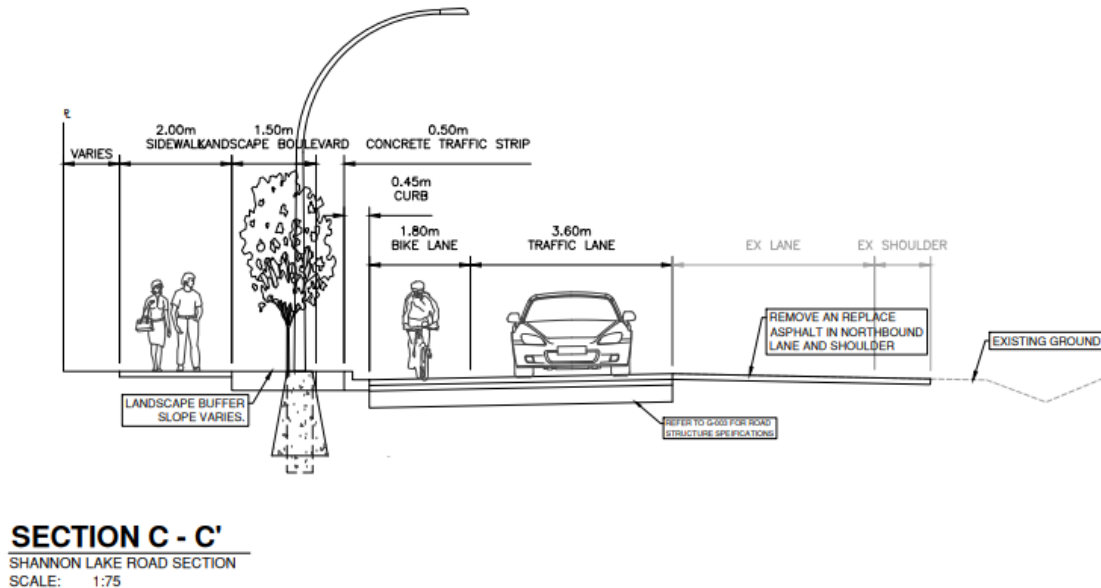
Due to limited existing right-of-way width (approximately 18.0m) along the Shannon Lake Road corridor, the landscape boulevards are proposed to be eliminated and designed similar to the City's Urban Collector cross-section (see Figure 2). Section B-B was used to tie into the existing sidewalk at the commercial complex and fronting the bulk water station.

Figure 2 – Shannon Lake Road Section B-B



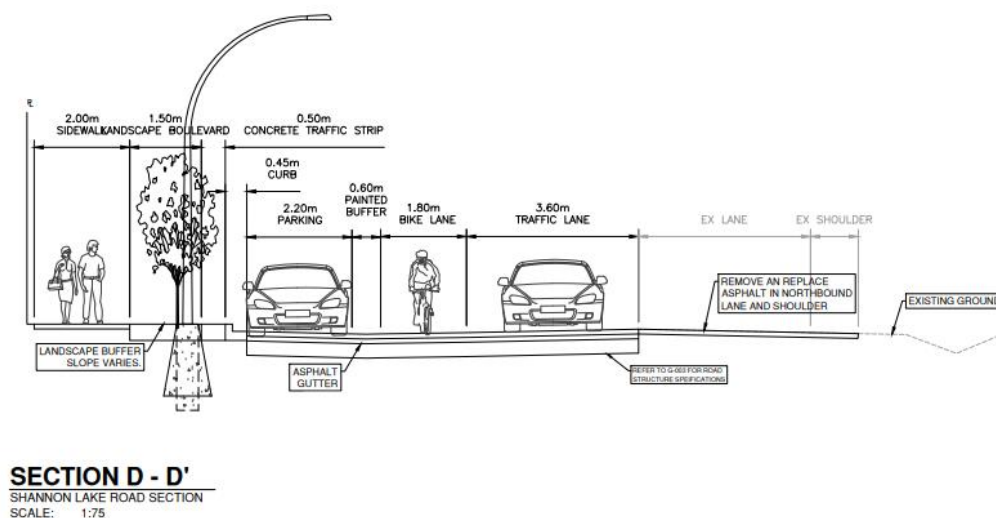
Section C-C was used north of the Shannon Lake Road and Asquith Road/Asquith Court intersection, refer to Figure 3. The topography of the east side of the road made it impractical to include a sidewalk.

Figure 3 – Shannon Lake Road Section C-C



Section D-D was included from north mid-block of Shannon Ridge Drive to Swite Road. The section includes approximately 100m additional street parking (see Figure 4) for Shannon Lake Regional Park. A pedestrian controlled flashing crossing will be installed to provide a crossing for those accessing the park.

Figure 4 – Shannon Lake Road Section D-D



Utility conflicts were identified within the project limits. One Telus pole will need to be relocated and replaced with a new pole and anchor at 3170 Shannon Lake Road to accommodate the Shannon Lake Road widening. Design permitting for this proposed

pole and anchor is in progress. A northbound right-turn only lane at intersection of Saddleback Way and Asquith Road has been incorporated into the design drawings to provide access to the Westside Transfer Station. City staff receives traffic complaints on a yearly basis regarding traffic congestion to access the Westside Transfer station and the addition of a right-turn only lane to the Westside Transfer Station will create fewer delays for motorists travelling northbound on Asquith Road.

Concept design landscape drawings have been reviewed by City Parks staff and their recommendations have been incorporated. Future maintenance has been reduced where practical, while still providing a pleasantly landscaped road corridor. BC Transit design guidelines were followed for the design and Stantec Consulting has also reached out to BC Transit for any additional input. Also, an ICBC Road Safety Audit has been completed and the recommendations have been included in the design. The ICBC Road Safety Audit suggestions included items such as cycling pavement markings, signage, and turning movement improvements.

Intersection Analysis

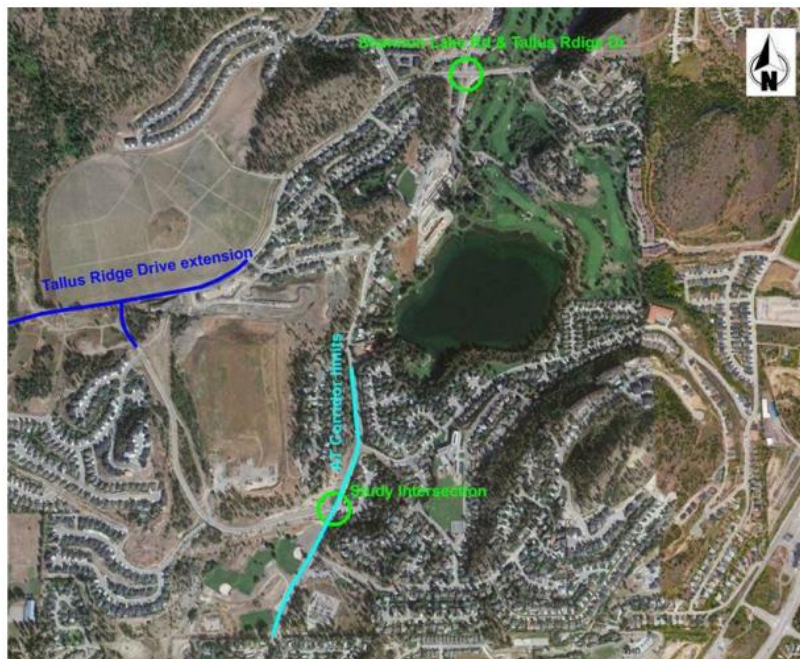
Stantec Consulting performed an intersection capacity analysis to examine the existing and future operation of the unsignalized intersection of Shannon Lake Road and Asquith Road/Asquith Court. In addition to growth, analysis included the change in travel patterns from a future connection between Tallus Ridge and Asquith Road. The traffic analysis used a combination of existing traffic volume and forecasted volumes at the Shannon Lake Road and Asquith intersection. As a result of the study, three geometric intersection options were examined.

- Existing Geometry
- Existing Geometry plus a northbound left turn lane
- Single lane roundabout

Based on a ten-year horizon, all three geometric improvement options for the intersection perform acceptably. As the ten-year horizon approaches, some turning movements will become challenging, such as the eastbound left turn movement onto Shannon Lake Road from Asquith Road. It is recommended that the existing geometry remain, and staff continue to perform ongoing monitoring of the intersection's traffic volumes. The design has considered adding a roundabout in the future when it is required.

Transportation modelling from the Transportation Master Plan was used to determine whether there was a benefit to focusing access to the Tallus neighbourhood via either the Shannon Lake Road and Tallus Ridge Drive intersection or the Shannon Lake Road and Asquith Road/Asquith Court intersection. Both intersections are expected to perform acceptably over the 10-year horizon and the ICBC Crash Data did not show a large amount of collisions at either intersection. The two intersections are recommended to be reviewed in a future update to the Transportation Master Plan.

Figure 6 – Intersection Locations



Stormwater Improvements

The design included a review of the contributing stormwater drainage area, the catchment area is shown in Figure 7. The need for a 1,100 m³ stormwater detention tank has been identified to be installed within Kinsmen Park under the existing gravel parking area. Stormwater improvements will be installed within the roadway with a storm main which connects to the detention tank. Drainage discharge onto Westbank First Nation jurisdiction and has been coordination with their Engineering team.

Figure 7 – Storm Drainage Catchment Area



FINANCIAL IMPLICATIONS

The Shannon Lake Road Active Transportation Corridor project is estimated to cost \$4,750,000, including construction, contingency, contract administration, inspections, utilities, and consulting. The project is eligible for Grant Funding from both BC Active Transportation Program (up to \$500,000) and ICBC Road Safety funding. The grant application for the BC Active Transportation Program is in progress. A funding request is planned in the 2024 Budget.

REVIEWED BY

Allen Fillion, Director of Engineering & Operations

Corinne Boback, Legislative Services Manager / Corporate Officer

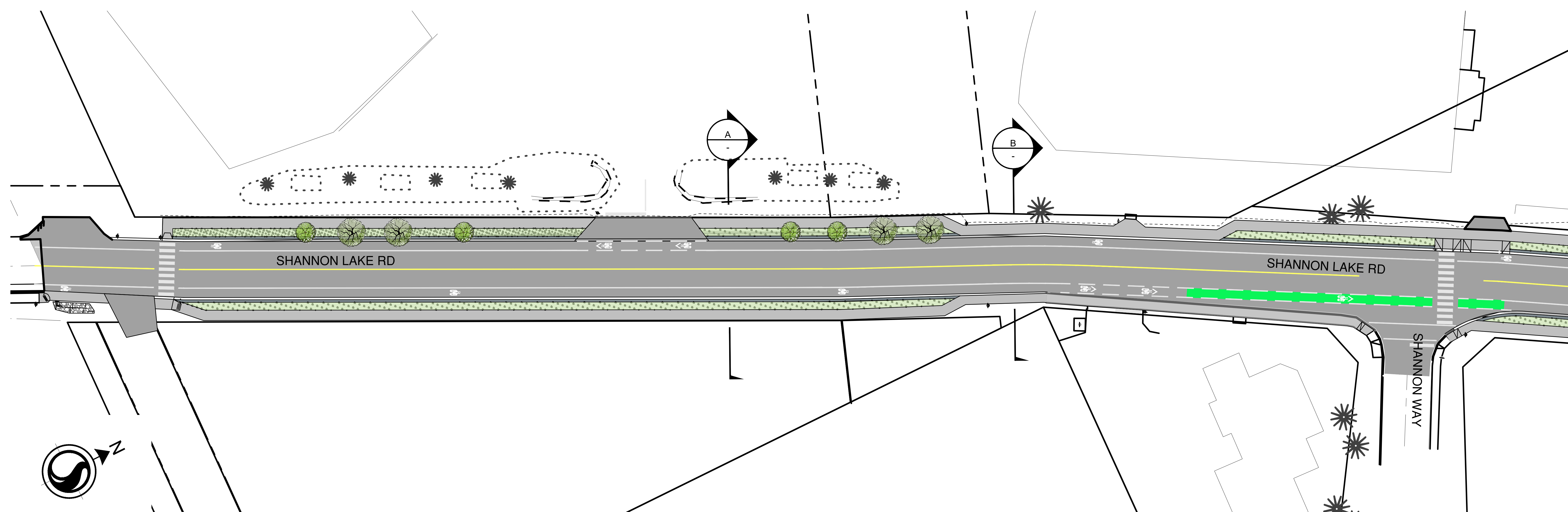
APPROVED FOR THE AGENDA BY

Trevor Seibel, Deputy CAO

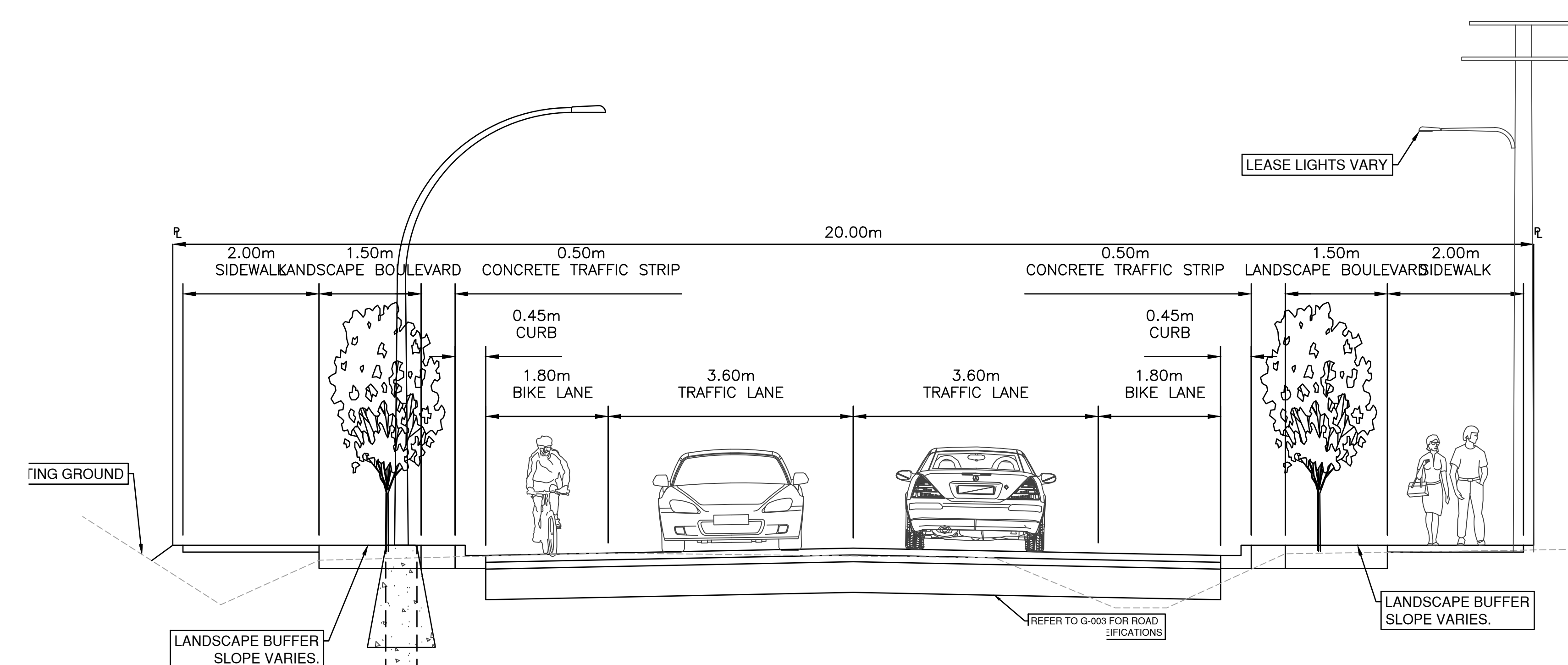
Powerpoint: Yes ☒ No ☐

Attachments:

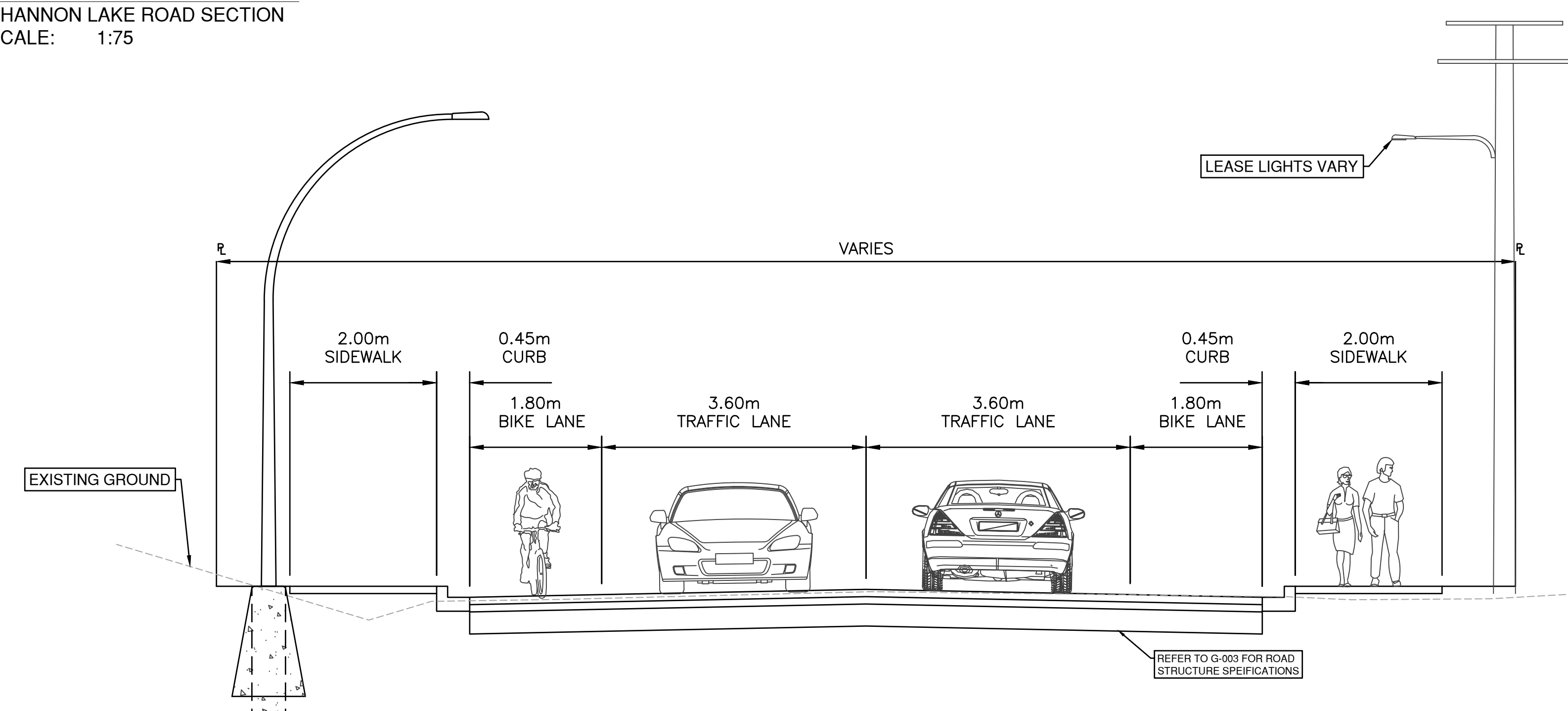
(Appendix A – Conceptual Streetscape Plan and Cross Section Design)



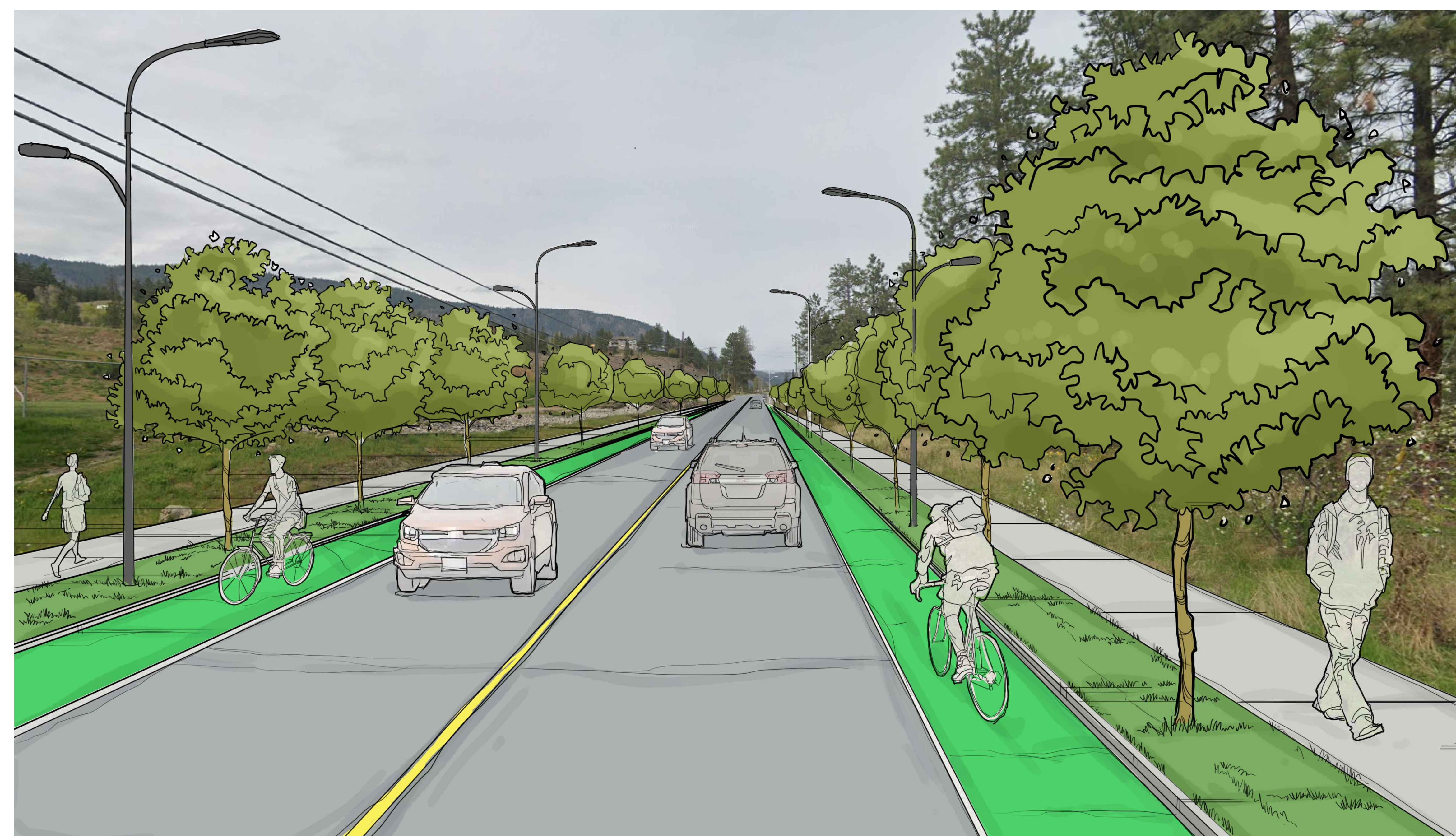
STREETSCAPE CONCEPT - SOUTH OF SHANNON LAKE ROAD
SCALE: 1:500



SECTION A - A'
SHANNON LAKE ROAD SECTION
SCALE: 1:75



SECTION B - B'
SHANNON LAKE ROAD SECTION
SCALE: 1:75

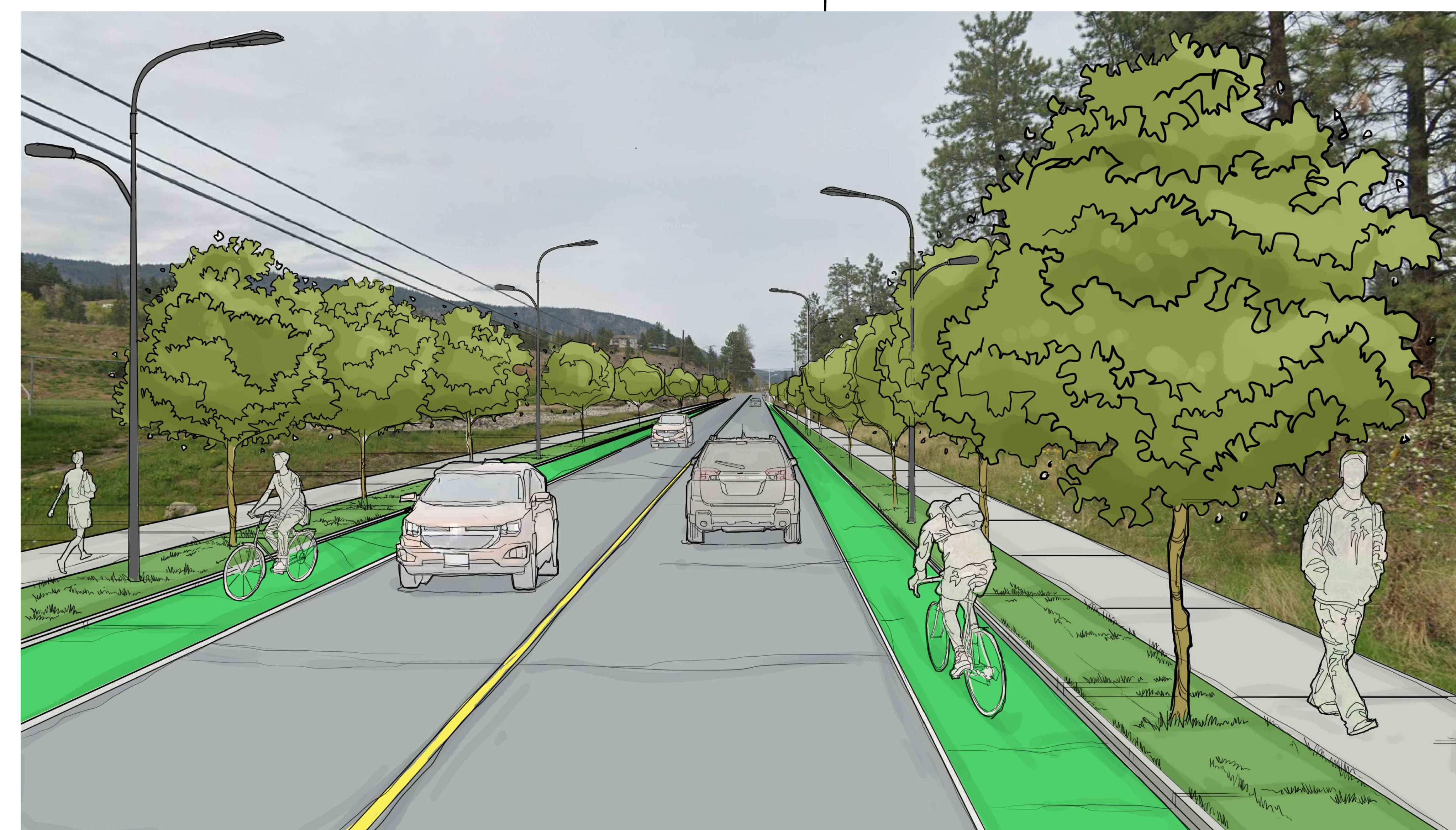
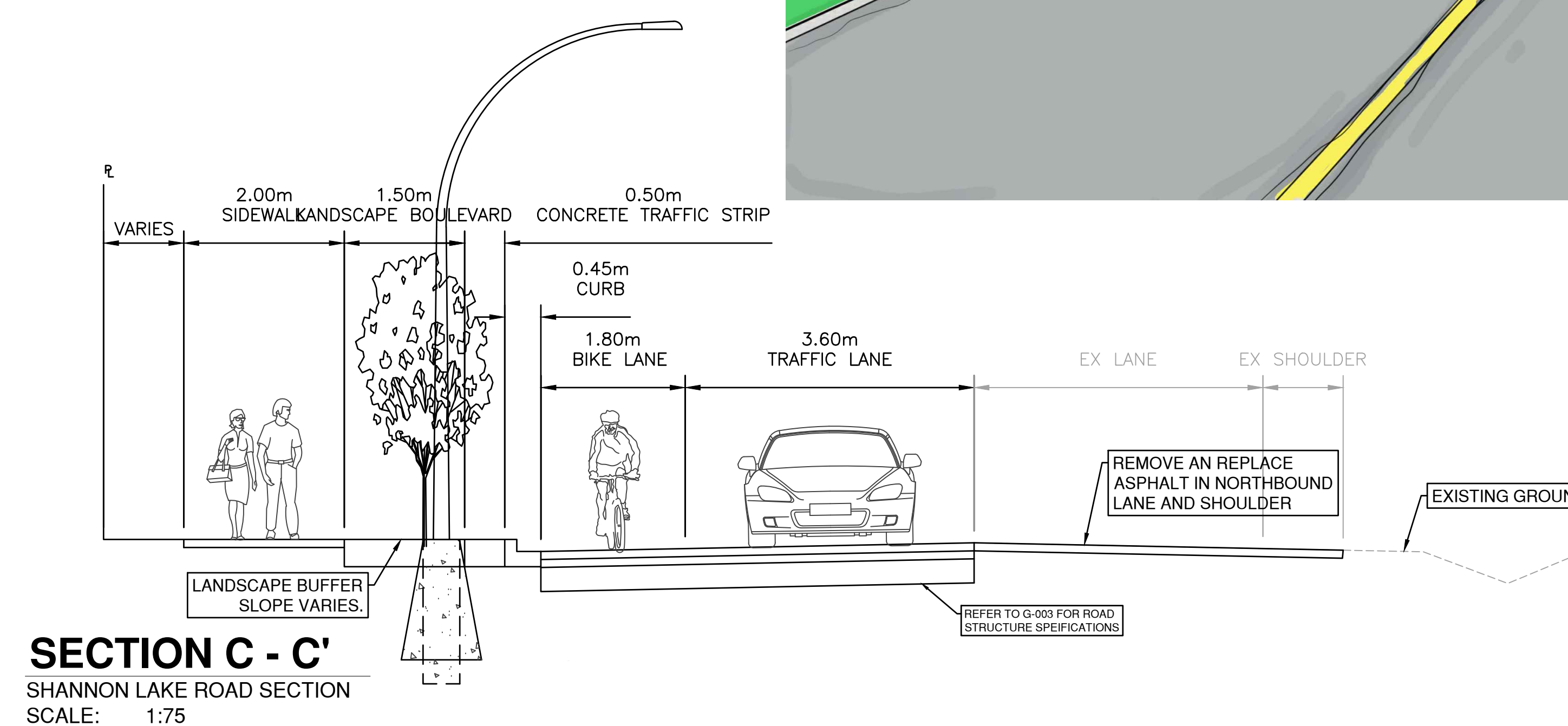
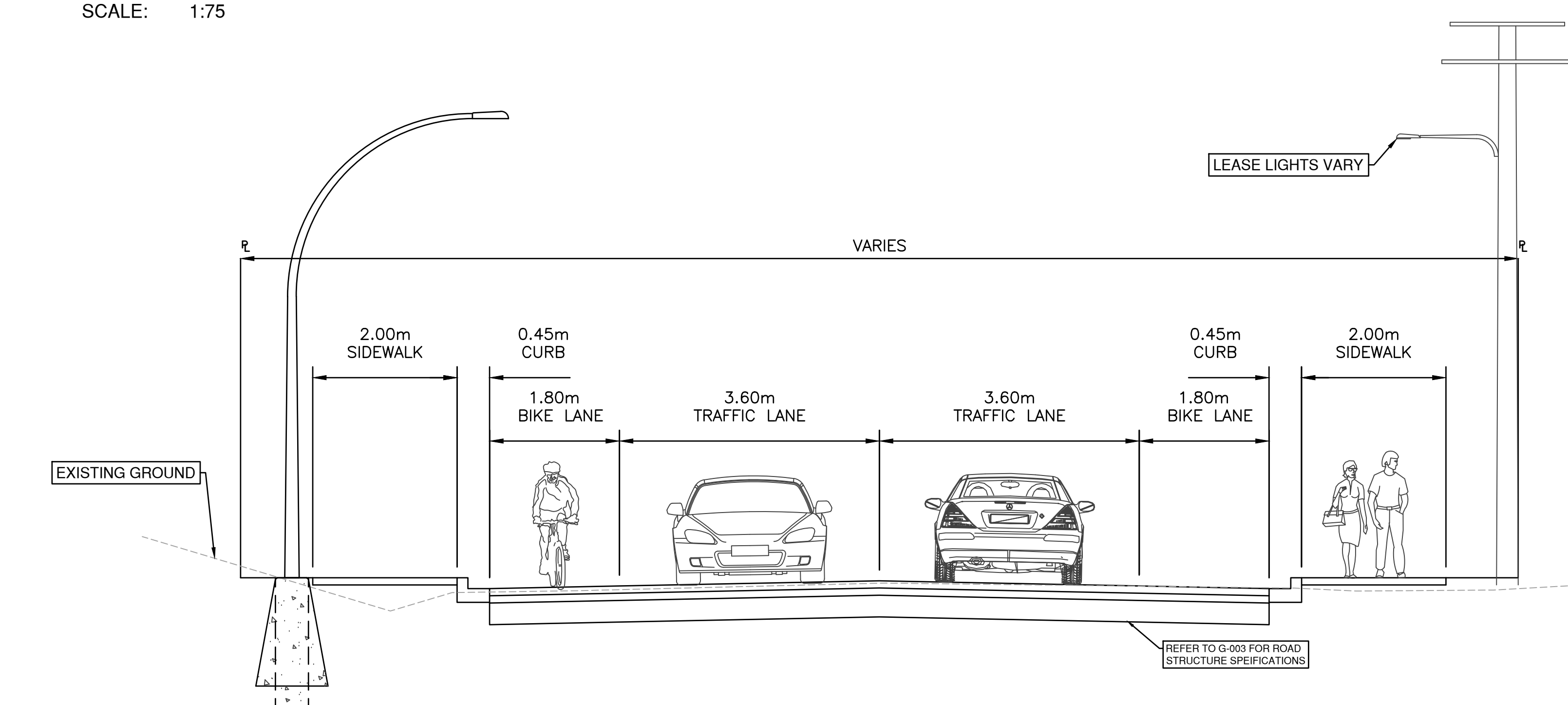
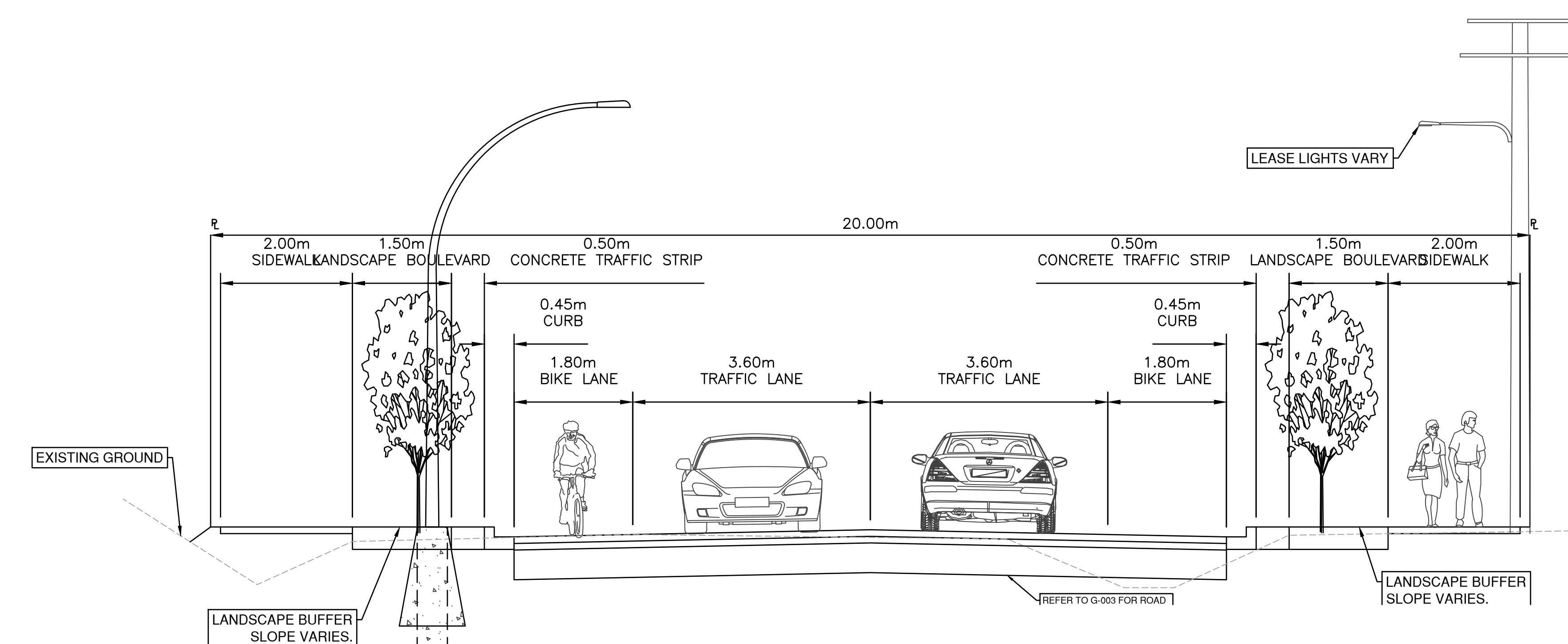
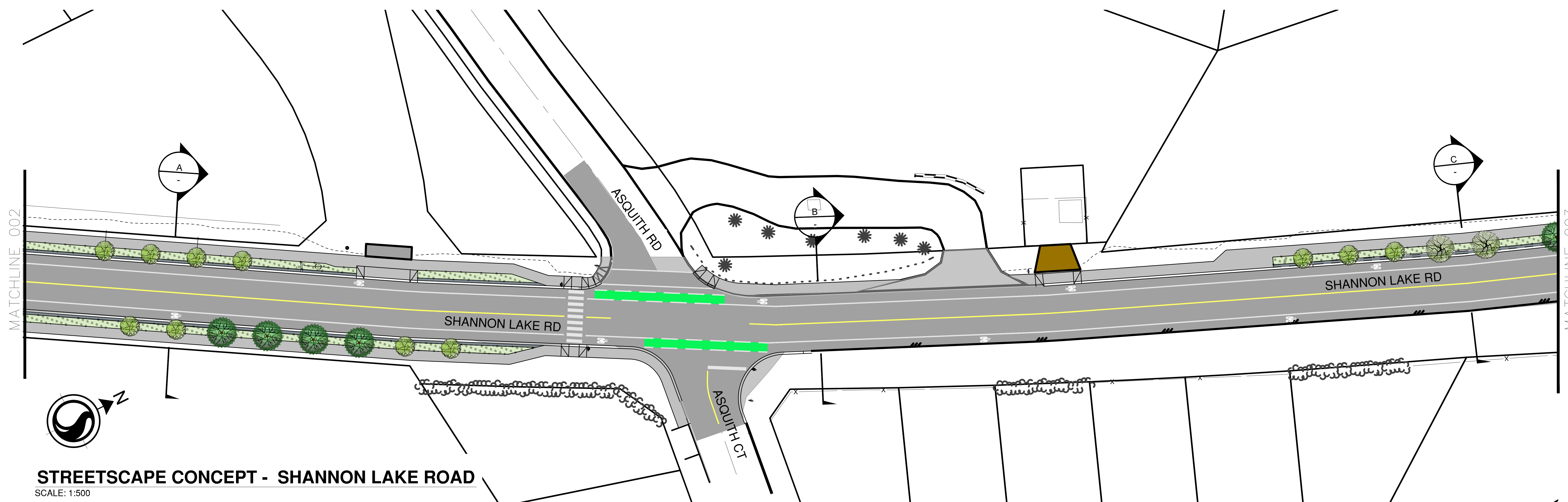


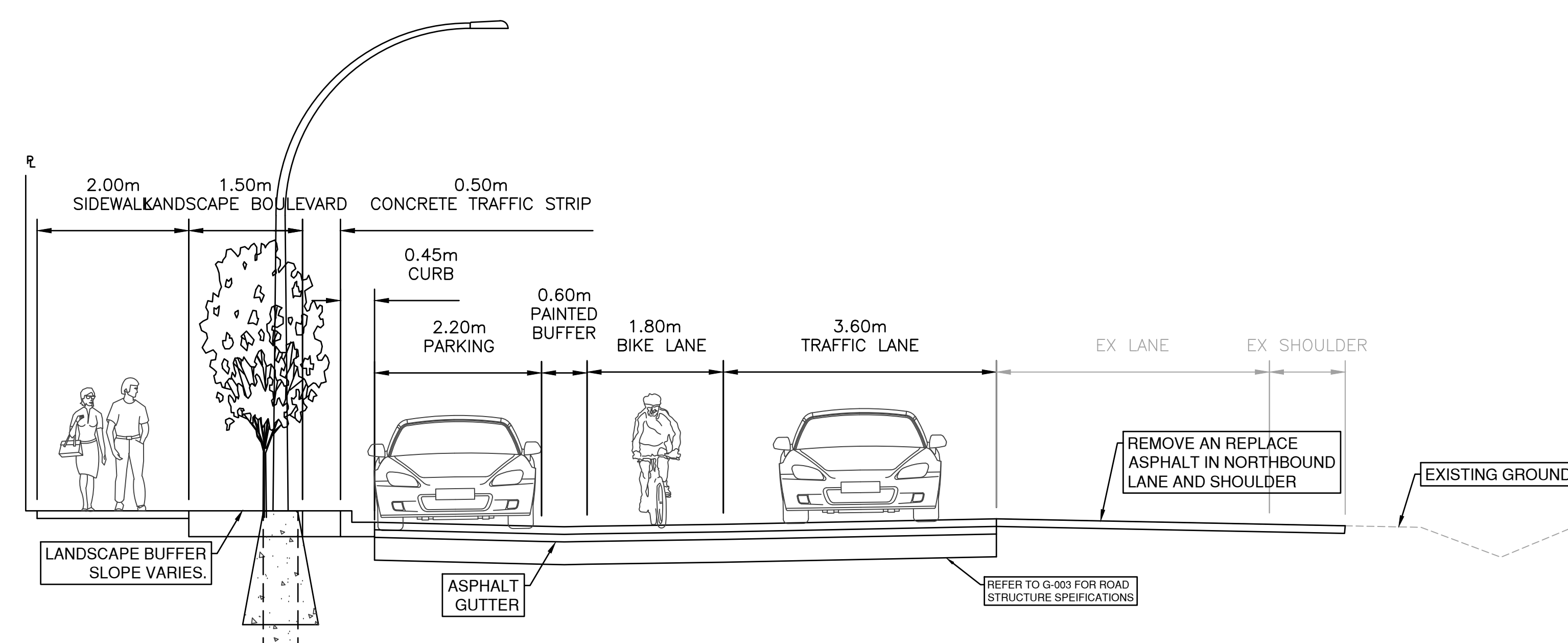
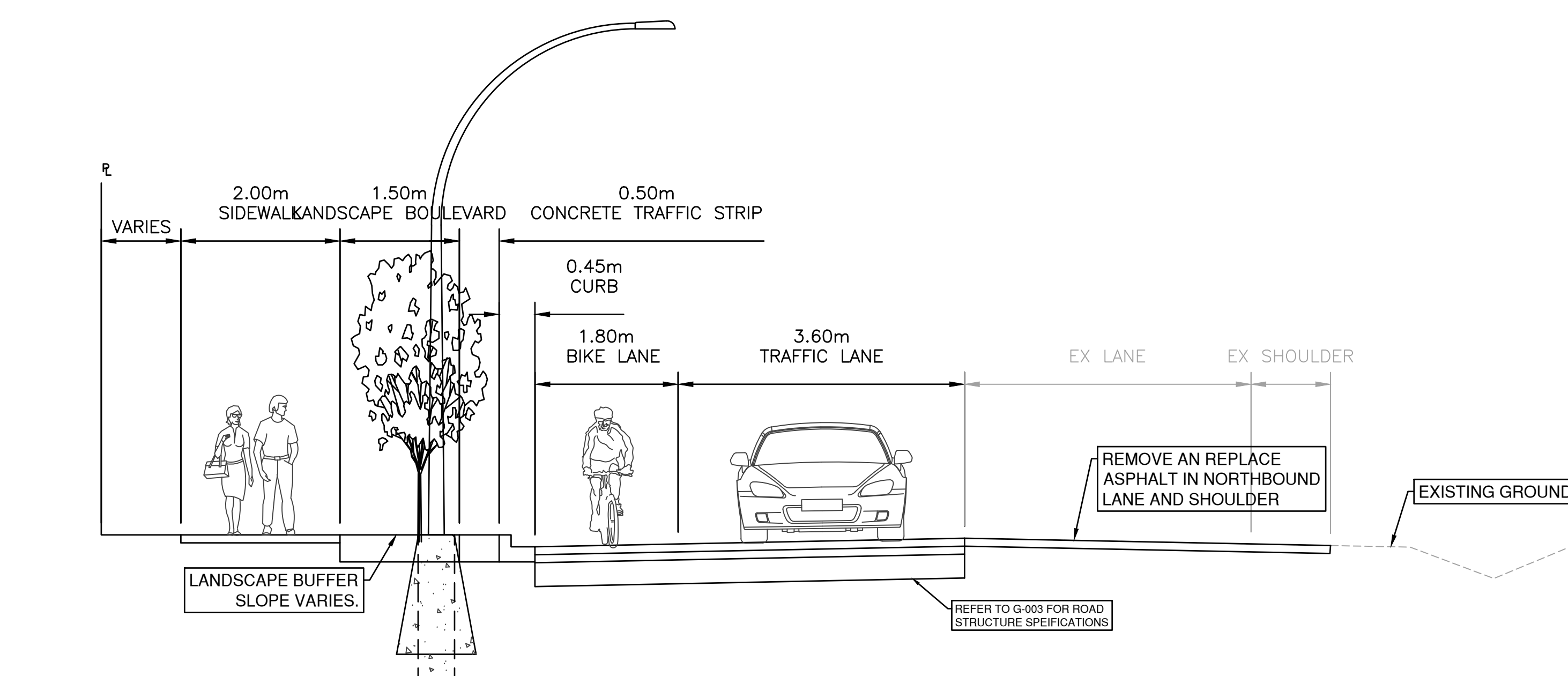
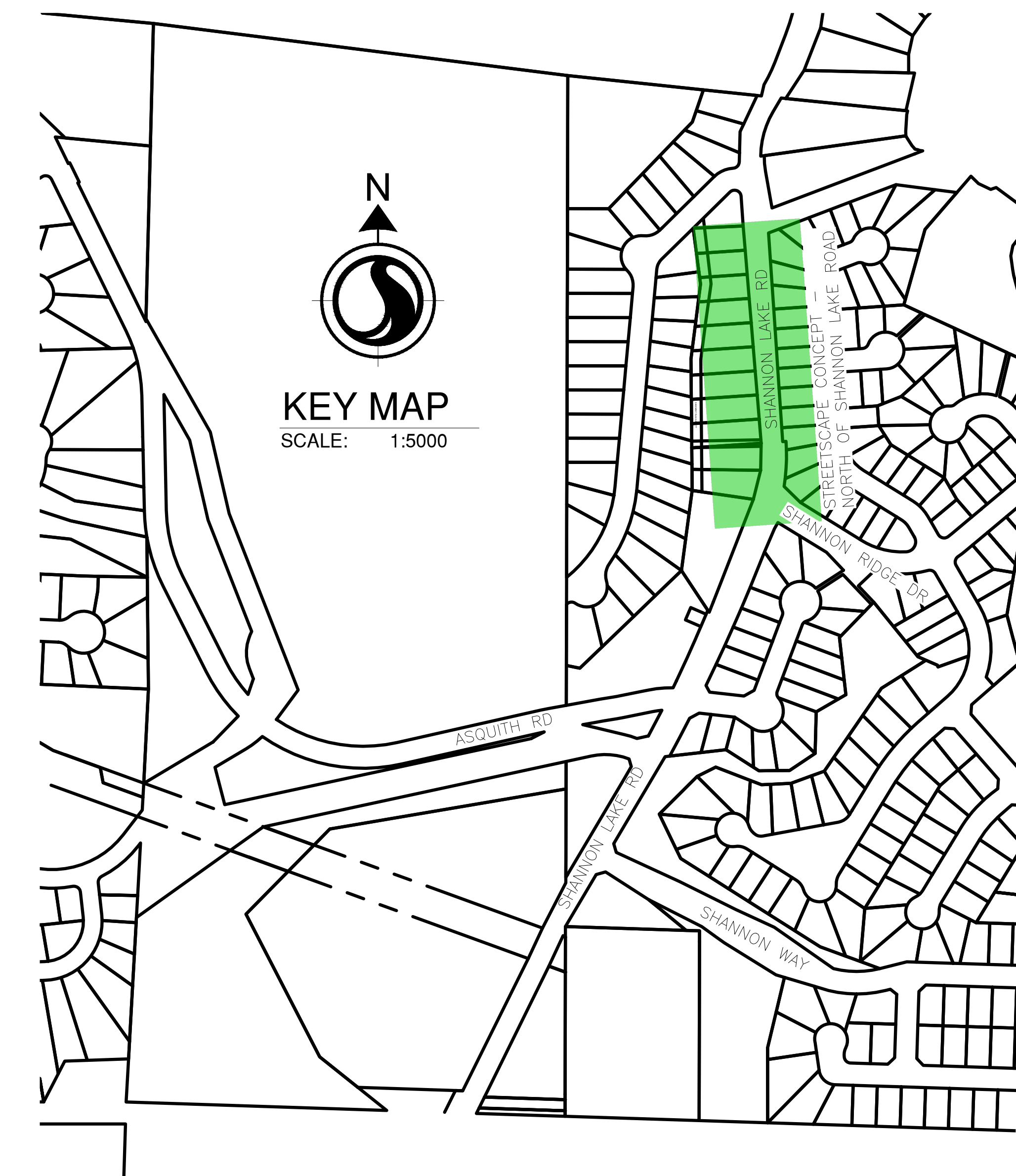
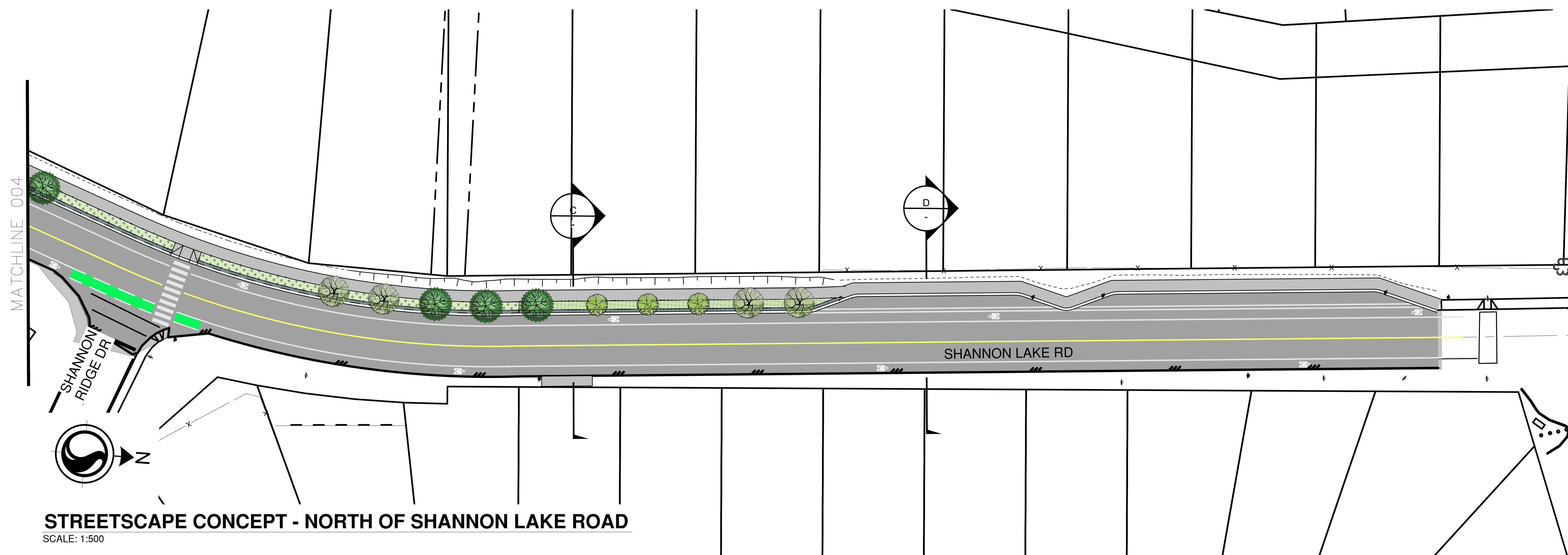
RENDERING
SCALE: N.T.S.

1 OF 4

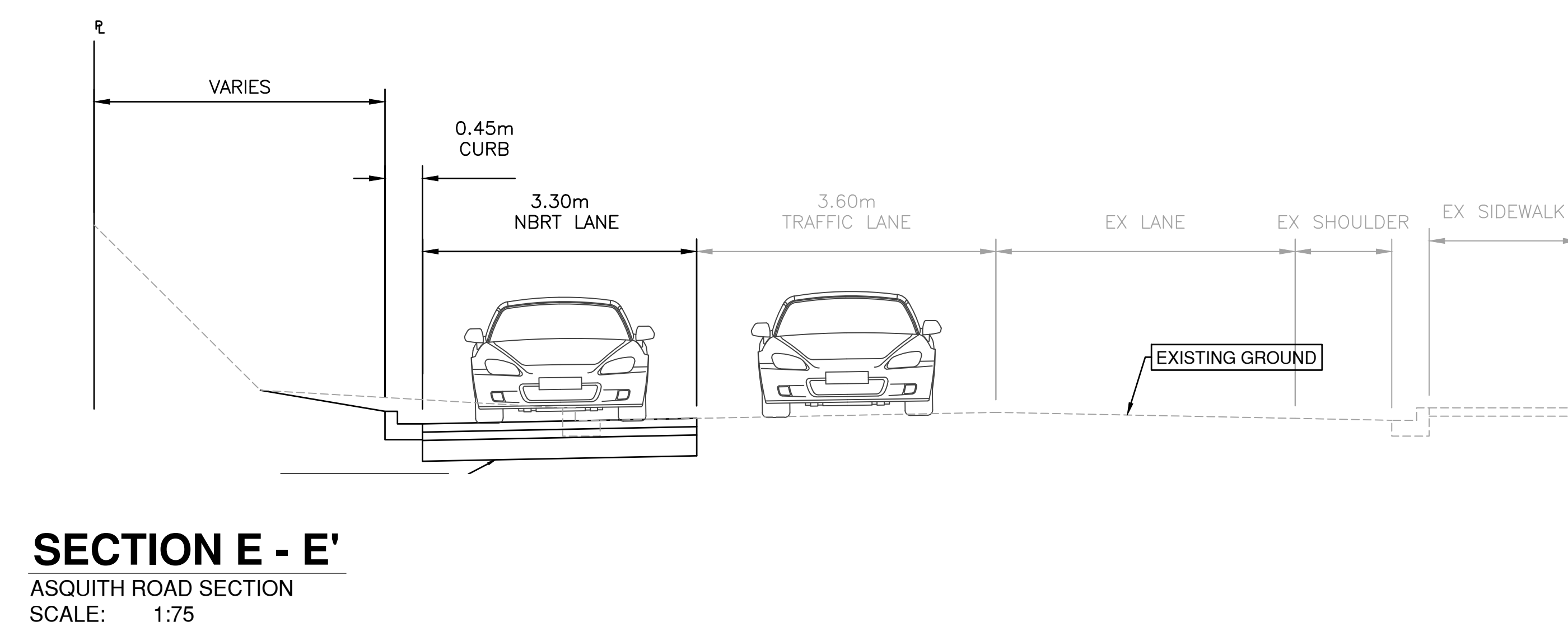
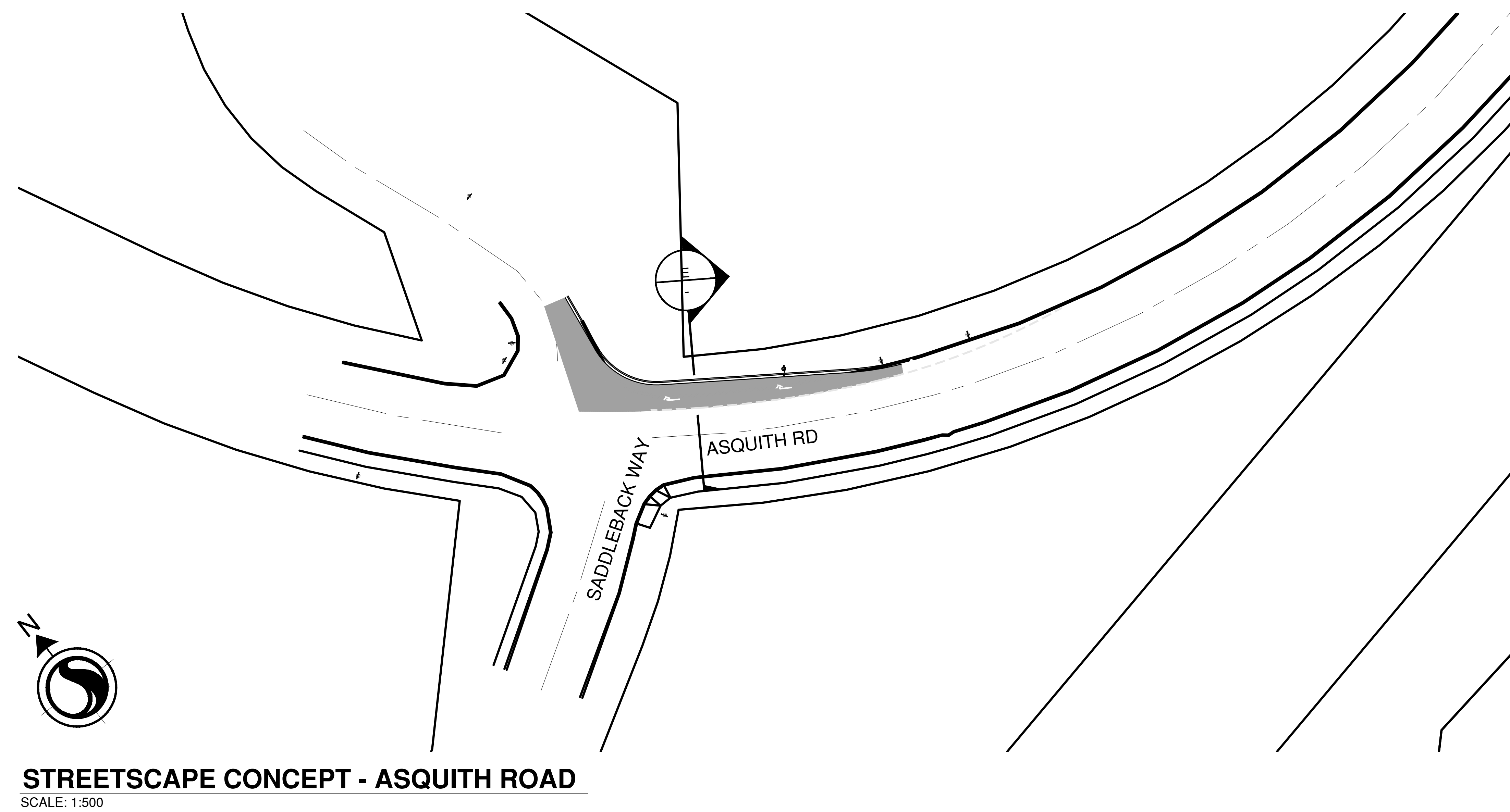


SHANNON LAKE ROAD ACTIVE TRANSPORTATION CORRIDOR





RENDERING
SCALE: N.T.S.



4 OF 4



SHANNON LAKE ROAD ACTIVE TRANSPORTATION CORRIDOR