



CITY OF WEST KELOWNA REGULAR COUNCIL AGENDA

Tuesday, May 16, 2023, 1:30 P.M.

COUNCIL CHAMBERS

2760 CAMERON ROAD, WEST KELOWNA, BC

Pages

1. CALL THE REGULAR COUNCIL MEETING TO ORDER

It is acknowledged that this meeting is being held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting is open to the public. All representations to Council form part of the public record. This meeting is being webcast live and will be archived on the City's website.

2. INTRODUCTION OF LATE ITEMS

3. ADOPTION OF AGENDA

4. ADOPTION OF MINUTES

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5. MAYOR AND COUNCILLOR'S REPORTS

5.1	Regional District of Central Okanagan Highlights from the Thursday, May 4, 2023 Regional Board Meeting	21
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6. DELEGATIONS

7. UNFINISHED BUSINESS

8. DIVISION REPORTS

8.1 CHIEF ADMINISTRATIVE OFFICER

8.2 FINANCIAL SERVICES

8.3 DEVELOPMENT SERVICES

8.3.1 DP 22-26; Development Permit with Variances; 3401 Sundance Drive

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Recommendation to Consider and Resolve:

THAT Council postpone consideration of the proposed multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) and direct the applicant to revise the following elements of their proposal:

- Include a turnaround and loading space on Ensign Lane;
- Secure legal access for units 53-56 (approx.) on Ensign Lane;
- Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
- Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
- Clearly provide a location for snow deposition on Ensign Lane.

8.3.2 P 21-01; Official Community Plan Update; Stakeholder and Public Feedback

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Information Report from the Director of Development Approvals

8.4 RECREATION AND CULTURE

8.5 ENGINEERING / PUBLIC WORKS / PARKS

8.6 CORPORATE INITIATIVES

8.7 FIRE RESCUE SERVICES

9. CORRESPONDENCE AND INFORMATION ITEMS

10. NOTICE OF MOTION

11. ADJOURNMENT OF THE REGULAR MEETING

The next Council meeting is scheduled for Tuesday, May 23, 2023.



CITY OF WEST KELOWNA
MINUTES OF THE SPECIAL MEETING OF COUNCIL

Monday, May 8, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen (arrived at 3:05 p.m.)
Councillor Stephen Johnston
Councillor Garrett Millsap

MEMBER ABSENT: Councillor Carol Zanon

Staff Present: Paul Gipps, CAO
Brent Magnan, Director of Development Approvals
Corinne Boback, Legislative Services Manager/Corporate Officer
Jayden Riley, Planner III
Melissa Hunt-Anderson, Community & Social Development
Coordinator

Other Present: Marina Jozipovic, Consultant from Urban Matters

1. CALL THE SPECIAL COUNCIL MEETING TO ORDER

The Special Council meeting was called to order at 3:02 p.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting was open to the public. All representations to Council form part of the public record.

2. ADDITIONS OR CHANGES TO PROCEDURAL MOTION

3. ADOPTION OF AGENDA

It was moved and seconded

Resolution No. C185/23

THAT the agenda be adopted as presented.

CARRIED UNANIMOUSLY

4. PROCEDURAL MOTION

It was moved and seconded

Resolution No. C186/23

THAT Council close the meeting in accordance with Section 90(1) of the *Community Charter* for:

(e) the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality.

CARRIED UNANIMOUSLY

5. ADJOURNMENT OF THE SPECIAL COUNCIL MEETING

The Special Council meeting adjourned at 6:03 p.m.

MAYOR

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER



CITY OF WEST KELOWNA
MINUTES OF THE SPECIAL MEETING OF COUNCIL

Tuesday, May 9, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen
Councillor Stephen Johnston
Councillor Garrett Millsap
Councillor Carol Zanon (virtual)

Staff Present: Paul Gipps, CAO
Corinne Boback, Legislative Services Manager/Corporate Officer

1. CALL THE SPECIAL COUNCIL MEETING TO ORDER

The Special Council meeting was called to order at 10:34 a.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting was open to the public. All representations to Council form part of the public record.

2. ADDITIONS OR CHANGES TO PROCEDURAL MOTION

3. ADOPTION OF AGENDA

It was moved and seconded

Resolution No. C187/23

THAT the agenda be adopted as presented.

CARRIED UNANIMOUSLY

4. PROCEDURAL MOTION

It was moved and seconded

Resolution No. C188/23

THAT Council close the meeting in accordance with Section 90(1) of the *Community Charter* for:

(a) personal information about an identifiable individual who holds or is being considered for a position as an officer, employee or agent of the municipality or another position appointed by the municipality;

(c) labour relations or other employee relations;

(e) the acquisition, disposition or expropriation of land or improvements, if the council considers that disclosure could reasonably be expected to harm the interests of the municipality;

(g) litigation or potential litigation affecting the municipality; and

(l) discussions with municipal officers and employees respecting municipal objectives, measures and progress reports for the purposes of preparing an annual report under section 98 [*annual municipal report*].

CARRIED UNANIMOUSLY

5. ADJOURNMENT OF THE SPECIAL COUNCIL MEETING

The Special Council meeting adjourned at 1:10 p.m.

MAYOR

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER



CITY OF WEST KELOWNA
MINUTES OF THE PUBLIC HEARING

Tuesday, May 9, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen
Councillor Stephen Johnston
Councillor Garrett Millsap

MEMBER ABSENT: Councillor Carol Zanon

Staff Present: Paul Gipps, CAO
Allen Fillion, Director of Engineering / Operations
Warren Everton, Director of Finance / CFO
Sandy Webster, Director of Corporate Initiatives
Jason Brolund, Fire Chief
Brad Savoury, Director of Legal Services
Brent Magnan, Director of Development Approvals
Corinne Boback, Legislative Services Manager/Corporate Officer
Bob Dargatz, Development Manager/Approving Officer
Lisa Siavashi, Financial Services Manager/Deputy CFO
Kassidie Cornell, Communications and Engagement Specialist
Mike Cain, Bylaw Services Manager
Jeremy Frick, Building and Licensing Manager
Kandice Brzezynski, Senior Business Licensing and Building Official

1. CALL THE PUBLIC HEARING TO ORDER

The Public Hearing was called to order at 1:30 p.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

2. INTRODUCTION OF LATE ITEMS

2.1 Submissions for Z 22-09; Zoning Amendment Bylaw No. 0265.02 (Public Hearing); 3349 Glenrosa Road

Correspondence received from:

- Corbett McCabe
- Liam McCabe

3. ADOPTION OF AGENDA

It was moved and seconded

THAT the agenda be adopted as amended.

CARRIED UNANIMOUSLY

4. OPENING STATEMENT

This meeting was open to the public and public participation was available in person, by phone, and by written submission. All representations to Council form part of the public record. This meeting was webcast live and archived on the City's website.

The Mayor read the Public Hearing Opening Statement, advising that the Public Hearing has been advertised, the bylaws and supporting documentation have been made available for inspection at City of West Kelowna offices and on the City's website, and outlined the process for the public hearing.

5. PUBLIC HEARING

The Mayor explained the process of this public hearing being held pursuant to Division 3, Part 14 of the *Local Government Act* for the purpose of hearing representation from those persons who believe their interests may be affected by:

- Zoning Amendment Bylaw No. 0265.02, 3349 Glenrosa Road

The Mayor noted that information has been available for inspection, which includes any written comments received to date for the applications, and that Notice of the Public Hearing was duly advertised in accordance with the requirements of the *Local Government Act*.

**5.1 Z 22-09; Zoning Amendment Bylaw No. 0265.02 (Public Hearing);
3349 Glenrosa Road**

The Director of Development Approvals introduced Zoning Amendment Bylaw No. 0265.02 to rezone from Rural Residential Large Parcel (RU4) to Rural Residential Small Parcel (RU2) to accommodate a 2-lot subdivision.

The Mayor asked if the owner/applicant wished to address Council regarding this file.

Desmond O'Brien - Property Owner

- Has lived on this property for 34 years;
- Realized property rezoned when City of West Kelowna took over from Regional District;
- Property was originally zoned RU2 then removed from the Community Plan and rezoned to RU4;
- Was approached by City of West Kelowna for reservoir at the corner of the road;
- For 17 months property was used as a laydown yard, material from property used to finish reservoir;
- Requested separate entrance to property as it was a used to access other properties;
- Wanted to rezone back to original zone;
- Requested City water which was provided;
- Engineering Development and Planning didn't communicate with owners request;
- Completed application with Planning to proceed with rezoning to RU2;
- Mr. O'Brien provided photos and map to Council showing different stages of construction;
- Lost more property than City of West Kelowna originally said;
- Stated immediate neighbours are okay with rezoning;
- Existing house is at the far end of the property and accessed through the new City of West Kelowna road by the reservoir;

- D.E. Pilling is assisting Mr. O'Brien.

The Mayor asked for a first time if there were any members of the public who wished to address Council regarding this file.

Corbett McCabe

- Spoke to his understanding of reservoir when he purchased land;
- Supports rezoning from RU4 to RU2;
- Rezoning will allow another family to enjoy country style living;
- Neighbours all share driveway;
- Water through City and wants to add sewer.

Zoltan Majoros

- Owns property below subject property;
- Supports rezoning it would add more dwellings to area;
- 6 years ago properties across Glenrosa road were approved for development, contingent on lift station;
- Support future growth;
- In the future, reservoirs and pump stations should be built to support upper Glenrosa;
- Gates to Turnbull should be included in the Transportation Master Plan;
- Recommendation to move future growth boundary to accommodate future growth in the area;
- Supports this rezoning but a lot more needs to be done in the area.

The Mayor asked a second time if there were any members of the public who wished to address Council regarding this file. There were no comments from the public.

The Mayor asked Council if they had any questions of staff or any specific information needed from staff in anticipation of this coming back to Council for consideration. There were no questions from Council.

The Mayor asked for a third and final time if there were any members of the public who wished to address Council regarding this file. There were no comments from the public.

The Mayor declared the public hearing closed at 1:54 p.m. and Council cannot accept any further information regarding this application.

6. ADJOURNMENT OF THE PUBLIC HEARING

The Public Hearing adjourned at 1:54 p.m.

MAYOR

I hereby certify this to be a fair and accurate summary of the nature of the representations made by the public at the Public Hearing with regard to Zoning Bylaw No. 0265.02, 3349 Glenrosa Road, held on May 9, 2023.

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER



CITY OF WEST KELOWNA
MINUTES OF THE REGULAR MEETING OF COUNCIL

Tuesday, May 9, 2023
COUNCIL CHAMBERS
2760 CAMERON ROAD, WEST KELOWNA, BC

MEMBERS PRESENT: Mayor Gord Milsom
Councillor Tasha Da Silva
Councillor Rick de Jong
Councillor Jason Friesen
Councillor Stephen Johnston
Councillor Garrett Millsap

MEMBER ABSENT: Councillor Carol Zanon

Staff Present: Paul Gipps, CAO
Allen Fillion, Director of Engineering / Operations
Warren Everton, Director of Finance / CFO
Sandy Webster, Director of Corporate Initiatives
Jason Brolund, Fire Chief
Brad Savoury, Director of Legal Services
Brent Magnan, Director of Development Approvals
Corinne Boback, Legislative Services Manager/Corporate Officer
Bob Dargatz, Development Manager/Approving Officer
Lisa Siavashi, Financial Services Manager/Deputy CFO
Stacey Harding, Fleet Operations and Project Manager
Mike Cain, Bylaw Services Manager
Jeremy Frick, Building and Licensing Manager
Rob Hillis, Engineering Manager
Kassidie Cornell, Communications and Engagement Specialist
Kandice Brzezynski, Senior Business Licensing and Building Official
Yvonne Mitchell, Planner II

1. CALL THE REGULAR COUNCIL MEETING TO ORDER

The meeting was called to order at 1:54 p.m.

It was acknowledged that this meeting was held on the traditional territory of the Syilx/Okanagan Peoples.

This meeting was open to the public. All representations to Council form part of the public record. This meeting was webcast live and archived on the City's website.

2. INTRODUCTION OF LATE ITEMS

3. ADOPTION OF AGENDA

It was moved and seconded

Resolution No. C189/23

THAT the agenda be adopted as presented.

CARRIED UNANIMOUSLY

4. ADOPTION OF MINUTES

4.1 Minutes of the Special Council Meeting held Tuesday, April 18, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C190/23

THAT the minutes of the Special Council meeting held Tuesday, April 18, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

4.2 Minutes of the Regular Council Meeting held Tuesday, April 18, 2023 in the City of West Kelowna Council Chambers

It was moved and seconded

Resolution No. C191/23

THAT the minutes of the Regular Council meeting held Tuesday, April 18, 2023 in the City of West Kelowna Council Chambers be adopted.

CARRIED UNANIMOUSLY

5. MAYOR AND COUNCILLOR'S REPORTS

5.1 Regional District of Central Okanagan Highlights from the April 20, 2023 Regional Board Meeting

5.2 Southern Interior Local Government Association 2023 Conference

Councillor Da Silva spoke about her experience at the Southern Interior Local Government Association (SILGA) conference. Councillor Da Silva was elected to the board as a Director-at-Large. SILGA endorsed the Firesmart and Wildfire Mitigation funding resolution to advance to the Union of BC Municipalities Conference in September 2023.

6. DELEGATIONS

6.1 Keith Funk, New Town

Keith Funk, New Town Architecture & Engineering Inc. provided a PowerPoint presentation on the Raymer Comprehensive Development Plan with proposed suggestions and was available for questions and comments from Council.

7. UNFINISHED BUSINESS

7.1 Mayor Milsom

It was moved and seconded

Resolution No. C192/23

THAT Council direct staff to bring forward a report with a recommendation on how to fund the emergency only egress/multi-use path leading from Casa Loma neighbourhood to the Lakeview neighbourhood in the 2023 budget for consideration at the May 9, 2023, meeting of Council.

CARRIED UNANIMOUSLY

8. DIVISION REPORTS

8.1 CHIEF ADMINISTRATIVE OFFICER

8.1.1 Council's 2022 – 2026 Strategic Priorities

It was moved and seconded

Resolution No. C193/23

THAT Council adopt the City of West Kelowna 2022 – 2026 Strategic Priorities.

CARRIED UNANIMOUSLY

8.1.2 Casa Loma Multi-Use Path and Emergency Egress – Update

It was moved and seconded

Resolution No. C194/23

THAT Council direct staff to amend the 2023 Annual Budget to include the Casa Loma Multi-Use Path and Emergency Egress; and

THAT Council direct staff to report back on the timing of the project; and further

THAT the funds for the Casa Loma Multi-Use Path and Emergency Egress be allocated from the Infrastructure Reserve Fund.

CARRIED UNANIMOUSLY

8.2 FINANCIAL SERVICES

8.2.1 2023 Tax Rate Bylaw No. 0305, 2023

It was moved and seconded

Resolution No. C195/23

THAT Council adopt the “City of West Kelowna Tax Rates Bylaw No. 0305, 2023”.

CARRIED UNANIMOUSLY

8.2.2 2023 Transfer Station Parcel Tax Bylaw Adoption

It was moved and seconded

Resolution No. C196/23

THAT Council adopt the “City of West Kelowna Transfer Station Parcel Tax Bylaw No. 0307, 2023”.

CARRIED UNANIMOUSLY

8.2.3 2023 Sterile Insect Release Parcel Tax Bylaw Adoption

It was moved and seconded

Resolution No. C197/23

THAT Council adopt the “City of West Kelowna Sterile Insect Release Parcel Tax Bylaw No. 0306, 2023”.

CARRIED UNANIMOUSLY

8.2.4 2023 RDCO Solid Waste and Recycling Program Parcel Tax Bylaw Adoption

It was moved and seconded

Resolution No. C198/23

THAT Council adopt the “City of West Kelowna RDCO Solid Waste and Recycling Program Parcel Tax Bylaw No. 0308, 2023”.

CARRIED UNANIMOUSLY

8.2.5 2022 Financial Statements - Audited

It was moved and seconded

Resolution No. C199/23

THAT Council accept the 2022 Financial Statements for the City of West Kelowna as attached in “Attachment 1”.

CARRIED UNANIMOUSLY

The meeting recessed at 2:52 p.m.

The meeting reconvened at 3:10 p.m.

8.3 DEVELOPMENT SERVICES

8.3.1 Z 22-09; Zoning Amendment Bylaw No. 0265.02, 2022 (Post Public Hearing); 3349 Glenrosa Road

It was moved and seconded

Resolution No. C200/23

THAT Council give third reading to City of West Kelowna Zoning Amendment Bylaw No. 0265.02, 2022 (File: Z 22-09) and schedule the bylaw for adoption subject to:

- Registration of a S.219 Covenant to require any new dwelling constructed on the subject property to include a sprinkler system, and that any additional infrastructure required to support the sprinkler system is operated, maintained, and located on private property.

CARRIED UNANIMOUSLY

8.3.2 P22-03; Development Application Process Review Streamlining – Bill 26 Phase 2 (Rescind 3rd Reading and 3rd Reading as Amended)

It was moved and seconded

Resolution No. C201/23

THAT Council rescind third reading of City of West Kelowna Development Applications Procedures Amendment Bylaw No. 0260.02, 2023; and

THAT Council give third reading as amended to City of West Kelowna Development Applications Procedures Amendment Bylaw No. 0260.02, 2023.

CARRIED UNANIMOUSLY

8.3.3 Short Term Accommodation - 1 Year Review

The Director of Development Approvals provided an updated on the Short Term Accommodation program. Council provided feedback and suggestions pertaining to compliance matters, spoke to supporting the Home Owner Grant verification and unanimously directed staff to do further investigation on Short Term Accommodations and report back at a future meeting of Council.

8.3.4 Liquor License Amendment for Niche Winery

It was moved and seconded

Resolution No. C202/23

THAT Council support the proposed Liquor and Cannabis Regulations Branch Lounge Endorsement application (LL 23-01) for Niche Winery, at 1895 Bartley Road, in accordance with the information contained in this report.

CARRIED UNANIMOUSLY

8.3.5 Z 22-06; Zoning Amendment Bylaw No.0265.08, 2022 (Adoption); Smith Creek, Stage 4

It was moved and seconded

Resolution No. C203/23

THAT Council adopt City of West Kelowna Zoning Amendment Bylaw No. 0265.08, 2022.

CARRIED UNANIMOUSLY

8.3.6 Z 21-02; OCP and Zoning Amendment Bylaw No. 0100.64 and 0265.04, 2022 (Rescind Third Reading, Re-read as Amended and Adopt); Smith Creek, Phase 1

It was moved and seconded

Resolution No. C204/23

THAT Council adopt City of West Kelowna Official Community Plan Amendment Bylaw No. 0100.64, 2022 (File: Z 21-02);

THAT Council rescind third reading to City of West Kelowna Zoning Amendment Bylaw No. 0154.104, 2022 (File: Z 21-02);

THAT Council give third reading as amended to City of West Kelowna Zoning Amendment Bylaw No. 0265.04, 2022 (File: Z 21-02); and

THAT Council adopt City of West Kelowna Zoning Amendment Bylaw No. 0265.04, 2022 (File: Z 21-02).

CARRIED UNANIMOUSLY

8.4 RECREATION AND CULTURE

8.5 ENGINEERING / PUBLIC WORKS / PARKS

8.5.1 Horizon Drive Pedestrian Improvements Tender Results

The Engineering Manager provided a PowerPoint presentation on the Horizon Drive Pedestrian Improvement Tender Results and was available for questions and comments from Council.

8.5.2 Asset Management Planning Grant Program Application

It was moved and seconded

Resolution No. C205/23

THAT Council direct staff to apply to the 2023 Asset Management Planning Grant Program to support the ongoing advancement of the City of West Kelowna's Asset Management Program;

THAT Council approve a matching City contribution, of up to \$25,000, to be allocated from Future Expenditures per Item C2023-31 in the 2023 to 2027 Financial Plan; and

THAT Council authorize the Mayor and/or Chief Administrative Officer to execute the contribution agreement as required for a successful grant application.

CARRIED UNANIMOUSLY

8.6 CORPORATE INITIATIVES

8.7 FIRE RESCUE SERVICES

9. CORRESPONDENCE AND INFORMATION ITEMS

10. NOTICE OF MOTION

11. ADJOURNMENT OF THE REGULAR MEETING

The meeting adjourned at 4:26 p.m.

MAYOR

LEGISLATIVE SERVICES MANAGER/CORPORATE OFFICER

Highlights of the Regional Board meeting – May 4, 2023

2022 Financial Statements

The Regional Board and Regional Hospital District Board approved the financial statements for the year ended December 31, 2022. Approval of the 2022 financial statements is required to be completed no later than May 15th of the subsequent year to comply with Provincial legislation — Section 167 of the Community Charter.

Updated Development Bylaws

The Regional Board approved a new Development Procedures Bylaw, Fees and Charges Bylaw and Building Bylaw. These bylaws were updated following changes to provincial legislation and are intended to make the development of housing more efficient.

Rockchild Landing Amendments

The Regional Board approved the Rural Westside Official Community Plan Amendment Bylaw and Zoning Amendment for the Rockchild Landing property on Westside Road to amend the future land use designation, rezone the property and discharge three covenants.

Development Variance Permit (VP-22-08) and LGA Exemption - 1711, 1725 and 1733 Huckleberry Road

The Electoral Area Directors supported the issuance of a Development Variance Permit to allow an increase in the maximum grade for a driveway of a panhandle lot and to exempt the subject properties from the 10% frontage requirement of Section 512 of the Local Government Act.

EECO Renaming – Mission Creek

The Regional Board approved the renaming of the

Environmental Education Centre for the Okanagan (EECO) to the "Robert Hobson Environmental Education Centre for the Okanagan". During his two-decade career as the Chair of the Regional District, Robert was a dedicated advocate for the establishment of park land and protecting the environment. A public renaming event is scheduled for June 9, 2023. Information about the event will be available on calendar.rdco.com in the coming weeks.

Rutland Urgent Primary Care Centre

The Regional Hospital District Board adopted a bylaw to amend the 2023-2027 Five-Year Financial Plan to incorporate the previously approved request from Interior Health Authority to fund 40% of the Rutland Urgent Primary Care Centre.

Regional Board meetings

Regional District office – 1450 KLO Road, Kelowna (Woodhaven Boardroom)

- Thursday, May 18 – 6 p.m.
- Thursday, June 1 – 8:30 a.m.
- Thursday, June 8 – Committee of the Whole 8:30 a.m.

A live stream video link will be available for each meeting in the Upcoming Meeting agenda section of rdco.com/agenda. When available, a link to the Video recording will be posted in the Past Meetings section for the individual meeting date.

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DEVELOPMENT SERVICES COUNCIL REPORT

To: Mayor and Council

Date: May 16, 2023

From: Paul Gipps, CAO

File No: DP 22-26

Subject: **DP 22-26; Development Permit with Variances; 3401 Sundance Drive**

Report Prepared By: Jayden Riley, Planner III

RECOMMENDATION to Consider and Resolve:

THAT Council postpone consideration of the proposed multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) and direct the applicant to revise the following elements of their proposal:

- Include a turnaround and loading space on Ensign Lane;
- Secure legal access for units 53-56 (approx.) on Ensign Lane;
- Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
- Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
- Clearly provide a location for snow deposition on Ensign Lane.

STRATEGIC AREA(S) OF FOCUS

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

BACKGROUND

The subject property is currently vacant, located in the Shannon Lake neighbourhood between Ensign Quay Lane and Ensign Lane to the south-east and Sundance Drive to the north-west. The property includes steep hillsides, sloping downward from Sundance Drive toward the private laneways, which are currently shared by nine existing residences and the subject property via an access easement.

PROPERTY DETAILS			
Address	3401 Sundance Drive		
PID	028-172-001		
Folio	3641391.497		
Lot Size (m²)	1.29 ha (12,990 m ²)		
Owner	Gatehouse Developments Inc.	Agent	Carlo DiStefano, DiStefano Jaud Architecture
Current Zoning	R4 (Medium Density Multiple Family)	Proposed Zoning	N/A
Current OCP	Low Density Multiple Family	Proposed OCP	N/A
Current Use	Vacant	Proposed Use	58-unit townhouse development
Development Permit Areas	Hillside, Sensitive Terrestrial Ecosystem, Form and Character		
Hazards	Hillside		
Agricultural Land Reserve	No		

ADJACENT ZONING & LAND USES		
North	^	P1 – Parks and Open Space
East	>	R1 – Single Detached Residential
West	<	R4 – Medium Density Residential
South	v	R1 – Single Detached Residential

NEIGHBOURHOOD MAP



PROPERTY MAP



DISCUSSION

Legislative Requirements

Council has the authority under S.490 of the *Local Government Act* to issue a Development Permit. Section 498 of the *Local Government Act* gives Council the authority to issue a Development Variance Permit that varies, in respect of the land covered in the permit, the provision of the Zoning Bylaw.

Proposal

The applicant is seeking a Form and Character, Hillside, and Sensitive Terrestrial Development Permit for a 58-unit townhouse development comprised of 20 units fronting Sundance Drive and 38 units fronting Ensign Quay Lane and Ensign Lane (Figure 1, Attachment 2).

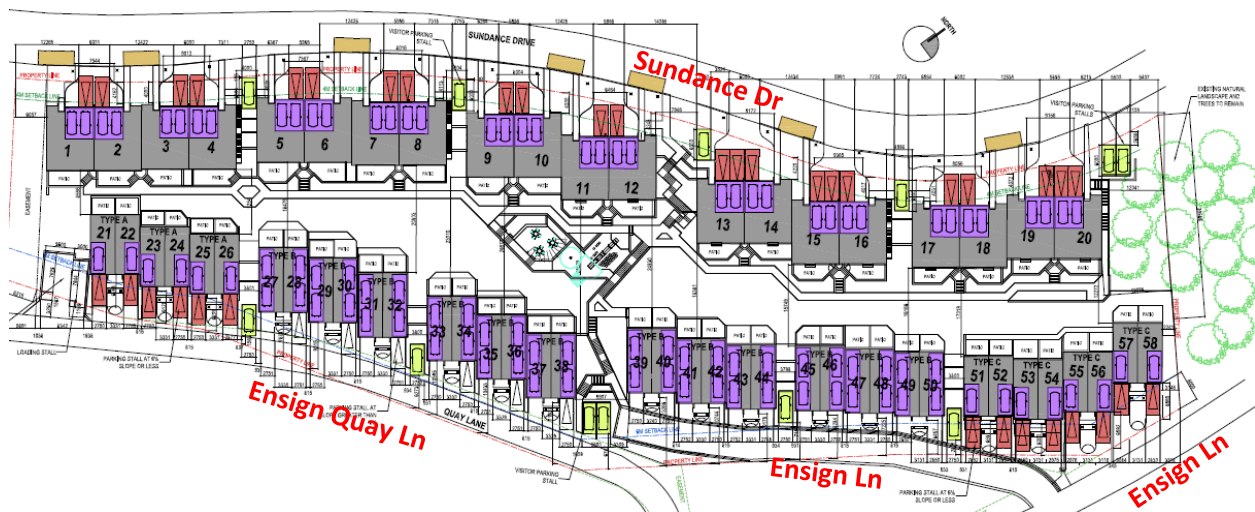


Figure 1: Site Plan

Due to topographical conditions combined with the proposed density, this application contains several variances to accommodate the proposed number of units related to siting, off-street parking, and site circulation. The following variances (7) are included in this proposal:

1. Reduction to the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies);
2. Reduction to the minimum required setback of loading and visitor parking spaces from the front or exterior parcel boundary from 3.0 m from 0.0 m (varies);
3. Reduction to the minimum required number of loading spaces from 4 to 1 space(s);
4. Reduction to the minimum dimensions of a loading space from 3.0 m x 9.0 m to the dimensions noted on site plan;
5. Increase the maximum height of two retaining walls from 2.5 m to 3.5 m;
6. Increase the width of driveway crossings at the property line from 7.0 m to 9.4 m (varies); and
7. Forgo the requirement to install a turnaround at or near the terminus of a private hillside lane.

Site Specific Considerations

The subject property is located upland from existing residences on Ensign Lane and Ensign Quay Lane. Both laneways include a series of private easements for access between the subject and adjacent properties, as well as Statutory Right of Ways for public utilities and emergency access. Subject to Council approval, the laneways would each be shared between the (9) existing residents and the proposed 38 townhouse units by a six-metre-wide drive aisle that does not permit on-street parking. Due to the topography of the site, retaining walls with height variances are required to accommodate a turnaround and loading space on the segment of Ensign Quay Lane and an extension of the physical laneway on Ensign Lane to access proposed units 39-44 (Attachment 2, Figure 1).

Bylaw and Policy Review

Official Community Plan Bylaw No. 0100

Development Permit Areas

The proposed development is subject to three of the City's DPAs: 1) Multiple Family and Intensive Residential, 2) Hillside, and 3) Sensitive Terrestrial Ecosystem.

1. Multiple Family and Intensive Residential DPA guidelines ensure that residential development is well designed and appropriately integrated into the community through use of good urban design principles. This includes strong design, amenity space contributions, and architectural focal points. The proposal is found to be generally consistent with the form and character, and building materials guidelines of this DPA; however, the proposal is less consistent with the guidelines in terms of adjacent use considerations, and potential hazards for

vehicle and pedestrian circulation due to disproportionate allocation of visitor parking. More detail is provided in the following sections of this report.

2. The **Hillside DPA** guidelines are applied to address steep slopes to ensure public safety and the protection of environmentally sensitive areas. This includes addressing rockfall hazard mitigation, location of roads and laneways, and setbacks to disturbance zones. The applicant has submitted a geotechnical report that has identified existing site conditions and provides recommendations related to site preparation and foundation design, site grading, rock slopes, drainage, pavement design and utility installation, and has confirmed the development, as proposed, is safe for the intended use. The recommendations of the geotechnical report and memo are included in the attached permit (Attachment 1). The proposal is found to be in general accordance with the DPA guidelines; however, the proposed laneway extension (Ensign Lane) it is not due to the adjacent steep slope, creating a safety hazard. More information is provided in the following sections of the report.

3. The **Sensitive Terrestrial Ecosystem DPA** guidelines are applied to all land as having environmental values. These guidelines ensure that development considers relevant provincial legislation, that site design is consistent with the environmental reporting, and to minimize disturbance to highly sensitive environmental areas. As part of this application, an Environmental Assessment was submitted, noting the area to contain Moderate (ESA 2) and Low (ESA 3) Environmentally Sensitive Areas. Most of the disturbance is proposed within the ESA 3 area. A portion of the ESA 2 area to the north-east is being preserved. Recommendations of the environmental report would be included as part of the permit conditions.

Form and Character Review

The proposal contains a total of 58 townhome units within 11 buildings; this includes 20 units on Sundance Drive (units 1-20), 18 units on Ensign Quay Lane (units 21-38), and 20 units on Ensign Lane (units 39-58). Three colour variations (i.e., dark, medium, light) are proposed to be applied to each grouping of buildings.

Building materials for all buildings include Hardie lap siding, brick – pacific art stone, Hardie plank, Hardie board trim lintel, painted fascia, dual black asphalt shingle roofing, and aluminum railing with glass panels and pickets – full architectural drawings are provided in Attachment 2. A list of additional features unique to each unit type is include below:

- Sundance Drive (Units 1-20):
 - Three storeys, three bedrooms, and double (parallel) garages.
 - Each unit contains an upper (entry) floor with garage, living area and master bedroom, mid-level floor with two bedrooms, living space and balcony, and a lower-level basement with patio.
 - Each unit is accessed by a paired driveway.



Figure 2: Street elevation (Sundance Dr., Units 1-20)



Figure 3: Rear elevation (Sundance Dr., Units 1-20)



Figure 4: Rendering of Sundance Dr. units with colour variation between two buildings, Units 1-20

- Ensign Quay Lane (Units 21-38)
 - Contains both “Type A” and “Type B” units.
 - Type A units are contained in one six-unit building (units 21-26) and include single garages with one required tandem parking space on the driveway in front of each (6) garage.
 - Type B units (units 27-38) are contained in two six-unit buildings and include double (tandem) garages with no driveway parking.
 - Each unit contains a lower (entry)-level with garage and flex room, mid-level living space with rear balcony, and three bedrooms at the top level.



Figure 5: Front / lane elevation (units 21-38 and 39-58)



Figure 6: Rear elevation (units 21-38 and 39-58)



Figure 7: Rendering of lane units with colour variation (units 21-38 and 39-58)

- Ensign Lane (Units 39-58)
 - Contains both “Type B” and “Type C” units.
 - Type B units (units 39-50) are contained in two six-unit buildings and include double (tandem) garages with no driveway.
 - Type C units are contained in one eight-unit building (units 51-58) and include single garages with one required tandem parking space on the driveway in front of each (8) garage.
 - Identical to the units on Ensign Quay Lane, each unit contains an entry (lower)-level with garage and flex room, mid-level living space with rear balcony, and three bedrooms at the top level.



Figure 8: Laneway units (combined) streetscape with colour variation (units 21-58)

Landscaping / Amenity Space

A landscape plan was submitted with the application that proposes a common amenity space between the upper (Sundance Dr.) and lower (laneway) units accessible to all owners, providing connectivity through the property (Attachment 3, Figure 9).

The proposed landscaping consists of a mixture of concrete pavers, decorative rocks, dry meadow slope retention hydro-seed, coarse boulder slope retention, composite wood decking, xeriscape shrub beds, as well as a mixture of native trees, shrubs, dwarf conifers, grasses, perennials, and ground covers. The interior amenity area also includes a series of landscape walls and stairs leading to a main boardwalk with handrails, as well as a seating area with trellis and bar tables and a playground area (Figure 9). See Attachment 3 for full details.

The proposed landscaping estimate is \$320,064.72. Subject to Council approval, and in accordance with the City's Development Applications Procedures Bylaw No. 0260, 125% (\$400,080.90) of this amount would be required as a condition of permit issuance – see Alternate Motion and Attachment 1.



Figure 9: Landscape plan / site plan, rendering, and conceptual drawing.



Figure 10: Playground area (proposed for centre of subject property), rendering and sample image.

Zoning Bylaw No. 0265

The subject property is zoned Medium Density Multiple Residential (R4). The proposed development is generally consistent with the siting regulations of the R4 zone except for the front parcel boundary setback (Sundance Drive), but otherwise meets maximum coverage (50%) and height (12.0 m up to max. three storeys). However, the proposal does contain seven variances, with the majority related to the Bylaw's off-street parking regulations. An overview of the proposed variances is provided below.

Proposed Zoning Bylaw Variances:

1. S.10.11.4(g)(i) – Reduce the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies), in accordance with the site plan (Attachment 2, Figure 1 and 11). This variance affects all five buildings located on Sundance Drive. Recent amendments to the Bylaw allowing for setback measurements to be taken from the back of a curb or sidewalk were not added to the R4 Zone.

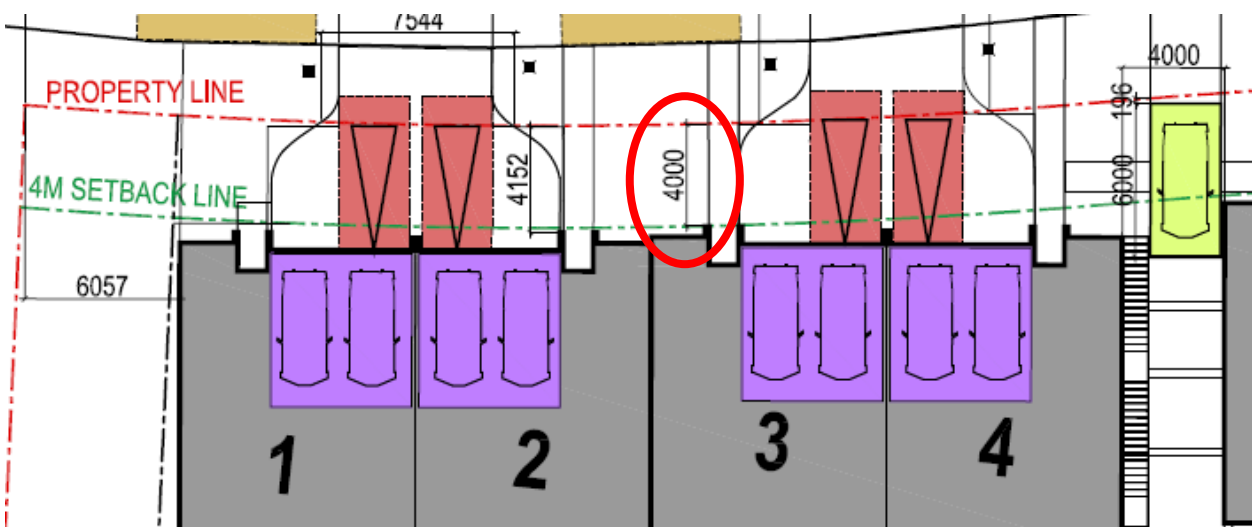


Figure 11: Setback variance for Sundance Units (1-20) – example of Building 1, units 1-4.

Since the Sundance Drive Right of Way will accommodate 3-4 metres of driveway from the curb to the property line, plus the additional 4 m setback (minimum) from

the property line to the buildings, vehicle overhang into the physical roadway is not anticipated. Staff support this variance.

2. S.4.3.2(b) – Reduce the minimum required distance between the required parking, loading and visitor parking spaces to the front or exterior parcel boundary from 3.0 m from 0.0 m (varies), in accordance with the site plan (Attachment 2, Figure 12).



Figure 12: Site plan, parking setback variances with insert.

This proposed variance affects nine of 12 visitor spaces, one (and sole) loading space, and 13 required townhouse parking spaces proposed in front of the garages for the Type A and C units located on the laneways (i.e., units 21-26, 51-56, and 58).

Staff have concerns about the potential impact of these variances, specifically in regard to the laneway and site circulation.

The intent of this bylaw regulation is to provide a buffer between more intensive multiple-family surface parking areas and public or private roadways, typically via landscaping. In this circumstance, the regulation is applied due to required parking being proposed in tandem with, and in front, of the single garages adjacent to the laneway. While this particular application of the parking regulation was not specifically considered with its original intent, it does reflect overall site design

challenges to accommodate the overall density and does have the potential to contribute toward site circulation challenges and periodic obstruction of the laneway.

3. S.4.10.1(a) – Reduce the minimum required number of loading spaces from four to one space(s), subject to a variance in the dimensions of the loading space (see Variance No. 4, Figure 13).

The number of loading spaces is proposed to be reduced from four to one space(s) to maximize the number of units on the site. The proposal includes a single loading space near the terminus of Ensign Quay Lane, adjacent to a proposed emergency vehicle turnaround. No loading space is proposed on the segment of Ensign Lane, and no loading spaces are proposed from Sundance Drive.



Figure 13: Site plan with loading space

Staff have concerns with a variance to reduce the required number of loading spaces, due to potential circulation and safety impacts to Ensign Lane. Ideally, the proposal would include a loading space on each segment of laneway for each grouping of townhome buildings (for example, units 21-38 and 39-58). In this case, only units 21-38 are served by a loading space. Without a loading space on Ensign Lane, it is anticipated that moving vehicles would temporarily block the laneway and would need to reverse in or out. Staff do not support this variance and recommend that a loading space is provided on Ensign Lane.

The lack of a loading space on Sundance Drive is not anticipated to create significant impacts due to the additional driveway depth between the property line and the curb and availability of on-street parking.

4. S.4.10.2(a) – Reduce the minimum required dimensions of a loading space from 3.0 m x 9.0 m to 7.8 m x 3.0 m (Attachment 2, Figure 14).

Related to Variance No. 3 above, the single loading space proposed on Ensign Quay Lane cannot be adequately accommodated on the subject property (Figure 14).

The construction of the loading space must also occur within the easement boundary contained on the adjacent property. The access easement permits maintenance of the physical laneway. The easement boundary also widens at this point of the laneway, so vehicles using the loading space are not anticipated to obstruct the pass or repass of vehicles. Staff support the proposed variance.

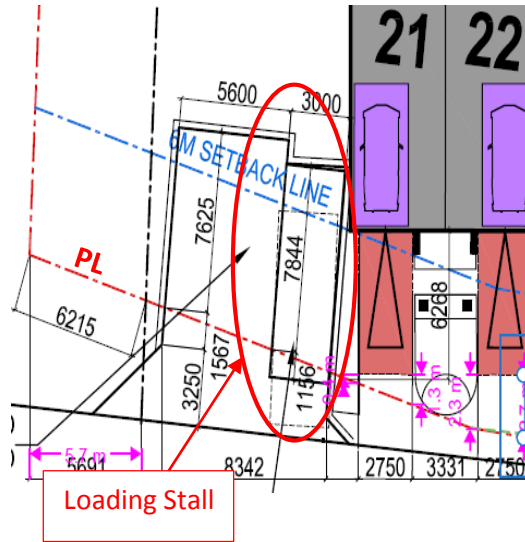


Figure 14: Loading space dimensions.

5. S.3.14.1 – Increase the maximum height of two retaining walls from 2.5 m to 3.5 m (Figures 15-18).

The proposal includes two over-height retaining walls up to a maximum 3.5 m. The first retaining wall (Figure 15) is proposed to accommodate the emergency turnaround at the terminus of Ensign Quay Lane. Staff are supportive of this variance, given the challenging topography of the site, the critical purpose of the turnaround, and the limited impacts to adjacent properties.

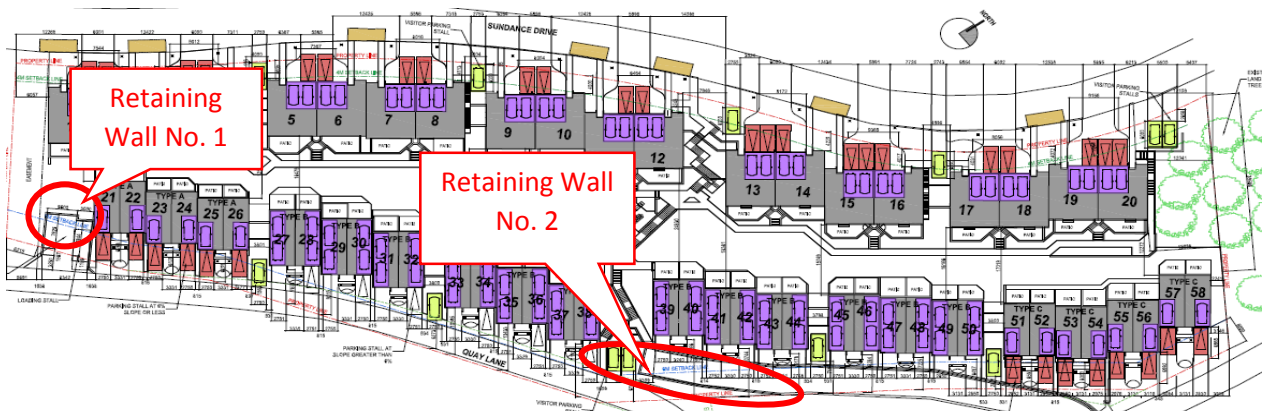


Figure 15: Over-height retaining walls (2)

The second over-height retaining wall (Figure 15-18) is proposed to extend the existing physical laneway on Ensign Lane to accommodate vehicle access/egress to units 39-44, as the existing site topography is steep at this location. The wall is

proposed to be located adjacent to the easement boundary but not encroach within it or the adjacent property.

Staff have do not have concerns with the height of the wall, but do have concerns due to its location and the proximity of the extended portion of laneway to the units near its terminus as it relates to on-site circulation and safety. Proposed units 39 and 40 are likely to encounter challenges with egress from their tandem garages given the minimal space available for turning. Where turning is possible, the wall results in an immediate 3.5 m drop in elevation to surrounding steep slopes and visitor parking spaces. Winter conditions and snow storage at the terminus of the lane is also likely to further exacerbate this hazard.

Staff are recommending that Council direct the applicant to revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus.

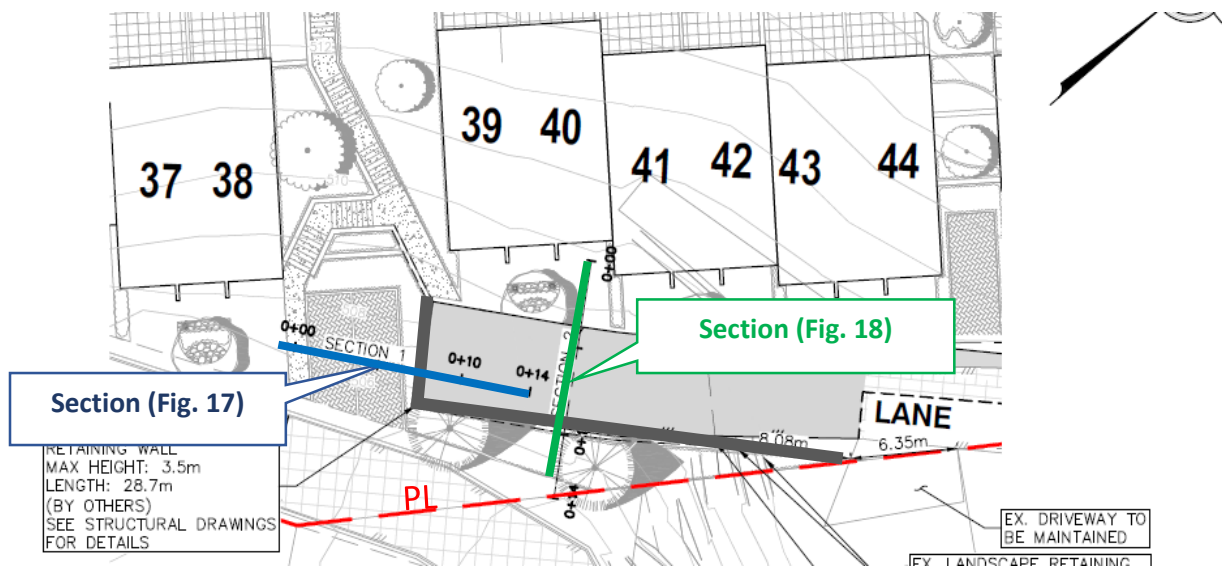


Figure 16: Over-height retaining wall No. 2.

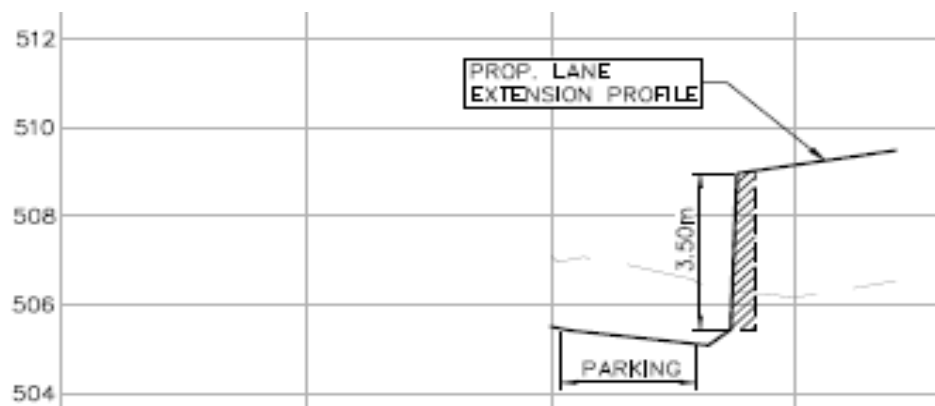


Figure 17: Over-height retaining wall No. 2, section drawing (blue).

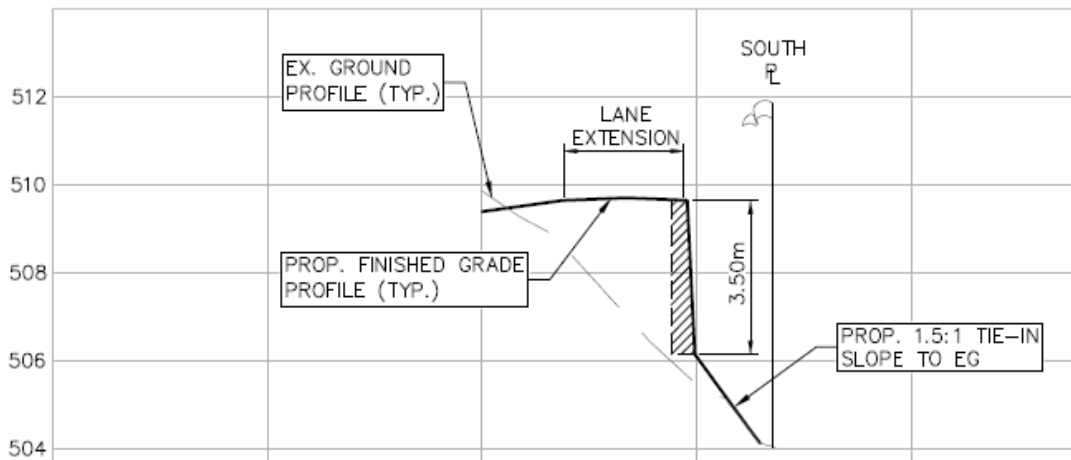


Figure 18: Over-height retaining wall No. 2, section drawing (green).

6. S.4.4.3(ii)(b) – Increase the width of driveway crossings at the property line (Sundance Drive) from a maximum 7.0 m to 9.4 m (varies), in accordance with the site plan (Attachment 2, Figure 19).

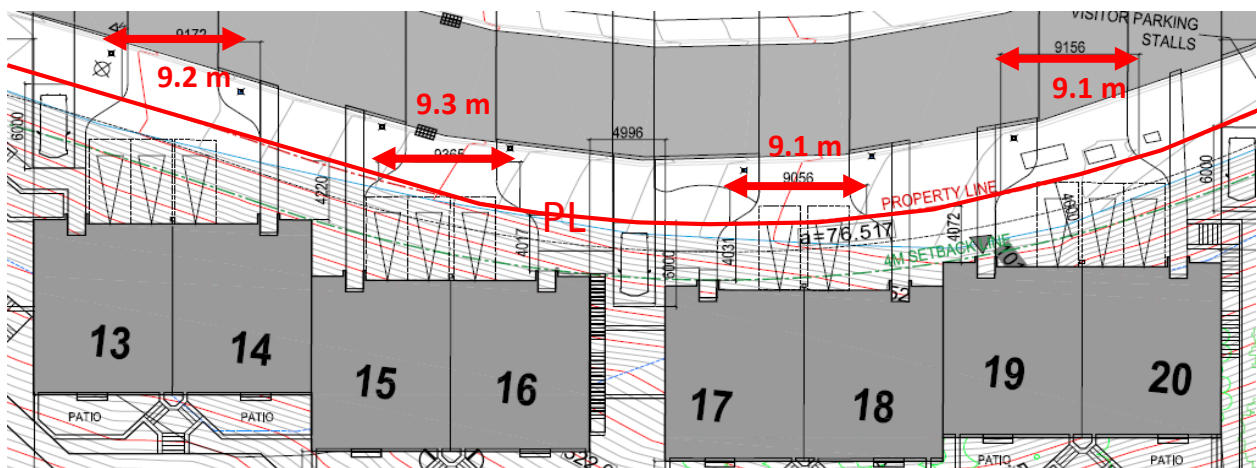


Figure 19: variance to the width of driveway crossings (4 of 8 crossing over 7.0 m shown here).

A variance to the maximum width of driveway crossings is to accommodate double driveways for the units on Sundance Drive. This variance applies to eight of 10 paired driveway crossings. Staff do not anticipate impacts from the paired driveways and support the variance.

Works and Services Bylaw No. 0249 Variance

7. S.8.2.2: To forgo the requirement to install a turnaround at or near the terminus of private hillside lane (Figure 20).

The City's Works and Services Bylaw requires private hillside lanes to include a turnaround at or near their terminus. This is for the purpose of improving vehicular

circulation, particularly with emergency or service vehicles (snow clearing, delivery, waste management). In the absence of a turnaround, it is anticipated that larger vehicles would need to turnaround on private driveways or reverse in or out of the laneway. This variance applies to Ensign Lane only, as a turnaround is proposed on Ensign Quay Lane.

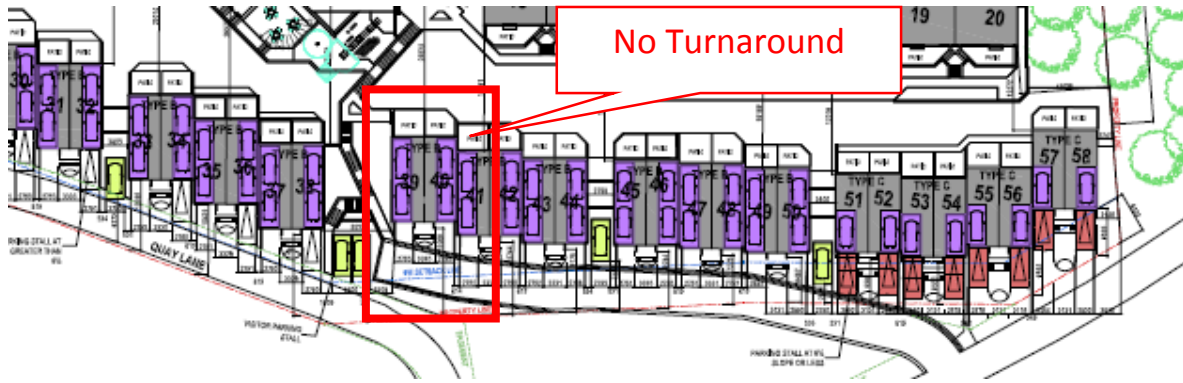


Figure 20: Variance to forgo turnaround on Ensign Lane

Although the City's Fire Department has confirmed they do not oppose the variance, the City's Engineering Department has stated that the turnaround is important for vehicle circulation and safety. In the absence of a turnaround, service and emergency vehicles would potentially be obstructed or obstruct the laneway. Passenger vehicles are also anticipated to have challenges turning around, given that the laneway contains only two visitor spaces and short driveway apron depths for units 39-50. Also, units 51-58 include only single garages; while consistent with the Zoning Bylaw regulations for off-street parking, including required parking in front of the garages adjacent to the laneway may exacerbate anticipated access and circulation challenges.

Technical Review

Servicing

A Functional Servicing Report (FSR) has been provided that concludes the property is well suited for this development and can be developed for the intended use. The property is noted to be connected to community water, storm, and sanitary sewer services. Access is accommodated by the public road, Sundance Drive, and two private laneways – one of which requires a variance to forgo a turnaround (see variance No. 7, Figure 20).

Construction Management Plan

A Construction Management Plan (CMP) was submitted with the application (Attachment 4). The CMP outlines the phasing of the proposed development and steps to mitigate impacts to adjacent properties. Primary site access is proposed from Sundance Drive, with the lanes to the south used only in case of emergency and as needed to conduct construction activities otherwise not feasible from Sundance Drive. Construction is proposed to occur in five phases in the sequence noted in Figure 21.



Figure 21: Construction phasing (Construction Management Plan)

Work is anticipated to occur on the laneways to extend and tie-in utilities to existing city infrastructure. A variety of mitigation measures are noted in the CMP, including those that address erosion and sediment, dust, traffic, trade parking, and noise. The CMP is also intended to be attached as a schedule to the permit to ensure construction follows the noted phasing and mitigation measures.

Snow-Clearing

Ensign Lane and Ensign Quay Lane are both private lanes. Snow-clearing and maintenance of the physical laneways are managed by the subject property and those existing residences/properties located to the south-east of the laneway. Snow storage locations for the site have not been confirmed and have the potential to cause circulation challenges, specifically on Ensign Lane due to the retaining wall and lack of turnaround or loading space. Based on this, it is recommended that additional consideration be put into snow storage.

Garbage / Recycling Collection

Due to the proposed variances to the turnaround requirement on Ensign Lane, staff have been in contact with the RDCO to confirm serviceability related to garbage and recycling collection. The RDCO confirmed that due to the size of the large collection vehicles, narrow width of the laneway, and lack of turnaround, the townhouse units (21-58) are not serviceable. In providing this information to the applicant, they have stated that private

garbage/recycling collection has been confirmed possible through the use of smaller trucks for ease of access in the area.

Referrals

The application was referred to multiple external agencies and internal departments. Staff have been working with the applicant to reduce the number of variances as much as possible, with the priority being the mitigation of potential impacts to existing and proposed residences along the laneways and related circulation and safety. Detailed review comments were provided to the applicant for their consideration in advance of their request to move the application forward to Council as presented.

Public Notification

In accordance with the *Local Government Act*, a notice has been mailed to residents and tenants within 100 m of the subject property advising them how to make a submission for Council's consideration, as it relates to the proposed variances. Notice of Application signage has also been installed on the subject property in accordance with the City's Application Procedures Bylaw No. 0260. At the time of writing this report, one submission has been received.

During the notification period, staff were advised of a small area of land that would require an access easement to support access for approximately four units (53-56) proposed on Ensign Lane. This issue has been included in the recommended motion to be addressed prior to the application returning to Council for consideration. Alternatively, should Council otherwise be supportive of the variances and the proposal as presented, registration of an access easement will be included as a condition of permit issuance – see Alternate Motion.

CONCLUSION

While staff are generally supportive of the additional housing units, form and character, building materials and many of the variances, individually, there are concerns with the collective impacts of the variances related to off-street parking and circulation. When considered collectively, their anticipated impact creates a situation that may compromise the safety and circulation of site traffic and visitors to the site, as well as service and emergency vehicles, particularly on Ensign Lane.

Staff recommend that Council postpone consideration of this application and direct the applicant to revise the following elements of their proposal without resulting in further variances:

- Include a turnaround and loading space on Ensign Lane;
- Secure legal access for units 53-56 (approx.) on Ensign Lane;

- Reallocate the number of visitor spaces for each laneway to be proportional to the number of units;
- Revise the design of the extended portion of Ensign Lane to include increased safety elements consistent with best practices and the City's Hillside DPA guidelines and demonstrate that turning movements can be reasonably accommodated to any units near its terminus; and
- Clearly provide a location for snow deposition on Ensign Lane.

Alternate Recommendation to Consider and Resolve:

1. Authorize Issuance of Development Permit (DP 22-26)

THAT Council authorize the issuance of a multiple family and intensive residential, hillside, and sensitive terrestrial ecosystem development permit with variances (File: DP 22-26) to accommodate a 58-unit townhome development, in accordance with the attached permit (Attachment 1), subject to:

- submission of a landscape security in the amount of \$400,080.90; and
- registration of an easement for legal access to units 53-56 (approx.) proposed on Ensign Lane.

2. Deny Application

THAT Council deny Development Permit (DP 22-26).

Council may wish to deny the application and have the applicant redesign the proposal in accordance with the City's Official Community Plan Development Permit guidelines. If the proposal was revised, the Development Permit would require further consideration by Council.

REVIEWED BY

Chris Oliver, Planning Manager

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

Powerpoint: Yes ☒ No ☐

Attachments:

1. Draft Development Permit (DP 22-26)
2. Architectural Submission (Site Plan, Elevations, Floor Plan, Renderings)
3. Landscape Plan
4. Construction Management Plan
5. Submission
6. Delegation Request – Ryan Jones



CITY OF WEST KELOWNA
DEVELOPMENT PERMIT WITH VARIANCES
DP 22-26

To: Gatehouse Developments (Providence) Inc., BC1304810
 c/o Mike Sheppard
 PO Box 2585 STN Sardis Main
 Sardis, BC, VR2 1A8

CC: Distefano Jaud Architecture
 c/o Carlo Distefano
 3-1331 Ellis Street
 Kelowna, BC, V1Y 1Z9

1. This Permit is issued subject to compliance with all of the Bylaws of the City of West Kelowna applicable thereto, except as specifically varied or supplemented by this Permit.
2. This Permit applies to and only to those lands within the City of West Kelowna described below, and any and all buildings, structures and other developments thereon:

Lot 37 District Lot 2044 ODYD Plan KAP90501
(3401 Sundance Drive)

3. This **Multiple Family and Intensive Residential (DPA 3), Hillside (DPA 4), and Sensitive Terrestrial Ecosystem (DPA 5) Development Permit Area** allows the construction of 58 townhouses contained in 11 buildings, subject to the following conditions and related Schedules:
 - The siting and phasing of buildings are to be in accordance the **Site Plan Drawings A1.0-A1.2**, prepared by DiStefano Jaud Architecture, dated February 24, attached as Schedule "A";
 - The exterior design and finish of buildings are to be in accordance with the **Architectural submission** (elevations, material and colour schedule, renderings) prepared by DiStefano Jaud Architecture, dated February 24, attached as Schedule "A";
 - The dimensions and siting of retaining walls and servicing to be constructed on the land shall be in accordance with **Civil Engineering Drawings** (site servicing and grading plans, pre- and post-contours, hammerhead cross sections, lane extension cross sections, stormwater management, erosion and sediment control plan, turning movement plan), package prepared by Aplin & Martin Consultants Ltd., prepared April 18, 2023, Rev. 2, "Issued for Development Permit", Project No. 20-3099, attached as Schedule "B";
 - Landscaping to be provided on the land in general accordance with **Landscape Drawings**, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-L01C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, dated April 18, 2023, attached as Schedule "C";

- Construction activities to be in accordance with the **Environmental Assessment Reports**, prepared by Makonis Consulting Ltd., dated June 2021, attached as Schedule “E”, including but not limited to:
 - i. Bird nest surveys must be done in advance of clearing of trees between March 15 and August 15, and in accordance with the *Migratory Birds Act* and any other provincial and federal regulations;
 - ii. Spoil piles left for longer than two weeks are to be tarped;
 - iii. Spill-containment kits will be on site and on each equipment/machine; and
 - iv. Work site will be maintained in clean state. Oil containers, cans, grease tubes, rags, etc. and any other materials packaging will be removed from the work area to an approved disposal location immediately on completion of the servicing job.
- All construction activities and associated mitigation measures to be conducted on the land be in general accordance with the **Geotechnical Report**, dated February 8, 2021, and Geotechnical Memo, dated July 8, 2021, prepared by Interior Testing Services Ltd, attached as Schedule “F”, and the **Construction Management Plan**, prepared by Gatehouse Developments Inc., dated September 2, 2020, attached as Schedule “G”, including but not limited to:
 - i. Rock slopes to be finished no steeper than 0.5(H):1(V) and scaled to remove loose particles at the rock face.
 - ii. 3 m clearing of overburden above the top of the rock cuts with chain link fencing installed above the rock cut.
 - iii. With rock slopes less than 2.5 m in height, a catchment buffer is not required, however where rock cuts are 2.5 m to 6 m in height, provide a 1.5 m wide catchment buffer area.
 - iv. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - v. Finished grades should be sloped away from the building in order to minimize infiltration of water into the backfill zone. Foundations to be set below and behind a conventional 2H:1V plane projected up from the toe of a downhill slope.
 - vi. ITSL should be called to review the stripped subgrade prior to placing new grading fills, as well as during fill placement to carry out periodic field density testing.
- The following **variances to Zoning Bylaw No. 0265** are included as part of this Development Permit, in accordance with the Site Plan attached as Schedule “A”, and Civil Package, attached as Schedule “B”:
 - i. S.10.11.4(g)(i) – Reduce the minimum required front parcel boundary setback (Sundance Drive) from 6.0 m to 4.0 m (varies);
 - ii. S.4.3.2(b) – Reduce minimum required distance between the parking and loading spaces from the front or exterior parcel boundary from 3.0 m to 0.0 m (varies);
 - iii. S.4.10.1(a) – Reduce the minimum required number of loading spaces from four to one space(s);

- iv. S.4.10.2(a) – Reduce the minimum required dimensions of a loading space from 3.0 m x 9.0 m; and
 - v. S.3.14.1 – Increase the maximum height of two retaining walls from 2.5 m to 3.5 m.
 - vi. S.4.4.3(ii)(b) – increase the width of driveway crossing at the property line from a maximum 7.0 m to 9.4 m (varies).
- The following **variances to the Works and Services Bylaw No.0249** are included as part of this Development Permit, in accordance with the Site Plan attached as Schedule “A”:
 - i. S.8.2.2: Forgo the requirement to install a turnaround at or near the terminus of private hillside lane.
4. As a condition of the issuance of this Permit, the property owner shall deposit 125% of the cost estimate for the **Landscaping (\$400,080.90)** as performance security to ensure installation of the required landscape plantings, in accordance with the City’s Development Application Procedures Bylaw No. 0260 and Schedule “D”.
 5. Should any interest be earned upon the security, it shall accrue to the Permittee and be paid to the Permittee if the security is returned. The condition of the posting of the security is that should the Permittee fail to carry out the development hereby authorized, according to the terms and conditions of the Permit within the time provided, the City of West Kelowna may use the security to carry out the work by its servants, agents or contractors, and any surplus shall be paid over to the Permittee, or should the Permittee carry out the development permitted by this Permit within the time set out below, the security shall be returned to the Permittee. There is filed accordingly:
 6. The land described herein shall be developed strictly in accordance with the terms and conditions of this Permit and any plans and specifications attached to this Permit, which shall form a part hereof. Should any changes be required to this permit, please ensure that you obtain written approval from City of West Kelowna prior to making any changes.
 7. If this Development Permit has not been issued within one year from approval, Development Permit DP 22-26 shall be deemed to have been refused and the file will be closed.
 8. **This Permit is not a Building Permit.**
 9. **This is not a Municipal Highways Permit.**
 10. All archaeological sites in B.C. are protected under the Heritage Conservation Act. This applies to whether sites are located on public or private land and whether the site is known or unknown. If you think you have uncovered an archaeological site during a building project or renovation, please do not disturb the site further and call B.C.’s Archaeology Branch immediately at (250) 953-3334. Branch archaeologists will review your project plans and make recommendations to manage site impacts and secure the required permitting.
 11. Subject to the terms of the permit, where the holder of a permit issued under the *Local Government Act* does not substantially commence any construction with respect to which

the permit was issued within one year after the date it is issued, the permit lapses.

AUTHORIZING RESOLUTION NO. CXXX/XX PASSED BY THE MUNICIPAL COUNCIL ON _____, 2023.

Signed on _____

Corporate Officer

I hereby confirm that I have read and agree with the conditions of Development Permit DP 22-26 with Variances and will ensure that copies of the Permit will be provided to onsite personnel at time of construction.

Signed on _____

Property Owner or Agent

ISSUED on _____

Schedules:

- A. Architectural submission by DiStefano Jaud Architecture, Drawing No. A1.0-1.2, A2.1-2.7, A3.1-3.7, dated February 24, 2023.
- B. Civil drawings, prepared by Aplin & Martin Consultants Ltd., Drawing Package prepared April 18, 2023, Rev. 2, Issued for Development Permit, Project No. 20-3099.
- C. Landscape Plan, prepared by van der Zal + Associates, Drawing No. L-00, L-01A-Lo1C, L-02, L-03A-L03C, LS-01-LS-03, LD-01-LD-04, dated April 18, 2023.
- D. Landscape Estimate, prepared van der Zal + Associates, dated May 5, 2023, Project File Reference 2021-128.
- E. Environmental Reports, prepared by Makonis Consulting Ltd., dated June 2021.
- F. Geotechnical report, dated February 8, 2021, and Geotechnical Memo dated, July 8, 2021, prepared by Interior Testing Services Ltd.
- G. Construction management plan, prepared by Kyle Weiher, Senior Project Manager, Gatehouse Construction Inc., no date.

Attachment 2

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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PROJECT
PROVIDENCE
RESIDENTIAL DEVELOPMENT

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
FULL SITE PLAN
PROJECT DETAILS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A1.0

SCALE
1:300



PARCEL ZONING	R4
PARCEL AREA	13,043m2
TOTAL FOOTPRINT AREA OF HOMES	4,534m2
LOT COVERAGE	35%

FOR MORE THAN 1 BEDROOM UNIT	25.0m2 (269.1 ft2)
TOTAL NUMBER OF UNITS	58
REQUIRED AMENITY AREA	25 X 60 = 1,500m2 OF AMENITY SPACE
AMENITY AREA PROVIDED	4,012 m2 for central court area and patios

LOADING	
REQUIRED	PROVIDED
1 PER 15 UNITS TOTAL UNITS = 58 REQUIRED = 3.9 = 4	PROVIDE 1 DEDICATED LOADING ZONE ON QUAY LANE ON SUNDANCE 8 HOMES HAVE ADEQUATE DRIVEWAY AREA TO ALLOW FOR LOADING ON QUAY LANE 3 HOMES HAVE ADEQUATE DRIVEWAY AREA TO ALLOW FOR LOADING

TANDEM UNIT TYPE A: SHORT UNIT WITH ONE DEDICATED STALL IN GARAGE	TANDEM UNIT TYPE B: DEEP UNIT WITH 2 CAR TANDEM GARAGE	TANDEM UNIT TYPE C: SHORT UNIT WITH ONE DEDICATED STALL IN GARAGE. SUBSTANTIAL GRADE CHANGE RESULTS IN PATIO ONLY ACCESSIBLE AT 3RD LEVEL. FRONT DECK ALSO EXTENDED AT 2ND LEVEL ON FRONT OF UNIT.
--	---	---

SUNDANCE DRIVE UNITS - SIDE BY SIDE		QUAY LANE UNITS - TANDEM		
TYPICAL UNIT AREA = 112.4m2		TYPICAL UNIT AREA TYPE A = 59.92m2	TYPICAL UNIT AREA TYPE B = 60.29m2	TYPICAL UNIT AREA TYPE C = 59.92m2
TOTAL NUMBER OF UNITS = 20		TOTAL NUMBER OF UNITS TYPE A = 6	TOTAL NUMBER OF UNITS TYPE B = 24	TOTAL NUMBER OF UNITS TYPE C = 8
TOTAL AREA FOR ALL UNIT 1 TO 20 = 2,248 m2		TOTAL AREA FOR ALL UNITS = 360 m2	TOTAL AREA FOR ALL UNITS = 1,447 m2	TOTAL AREA FOR ALL UNITS = 479 m2

UNIT PARKING						
REQUIRED	PROVIDED SUNDANCE		REQUIRED	PROVIDED QUAY	ALL REQUIRED PARKING STALL SIZES SHOWN ARE DESIGNED AS PER BYLAW STALL SIZE REQUIREMENTS	
PER UNIT = 2 TOTAL UNITS = 20 REQUIRED = 40	GARAGE PARKING = 40		PER UNIT = 2 TOTAL UNITS = 38 REQUIRED = 76	GARAGE PARKING = 62		
	DRIVEWAY STALLS = 23			DRIVEWAY STALLS = 14		
		TOTAL = 63		TOTAL = 76		
VISITOR PARKING						
DEVELOPMENT VISITOR PARKING REQUIREMENT = 10% OF REQ. TOTAL REQUIRED = 40 + 76 = 116 x 10% = 11.6 OR 12			TOTAL REQUIRED PARKING = 40+76 = 116 x 10% = 11.6 OR 12 PROVIDED 12			
ALSO AVAILABLE IS STREET PARKING ALONG SUNDANCE ROAD			STALLS AVAILABLE = 7			

2750
6000

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PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

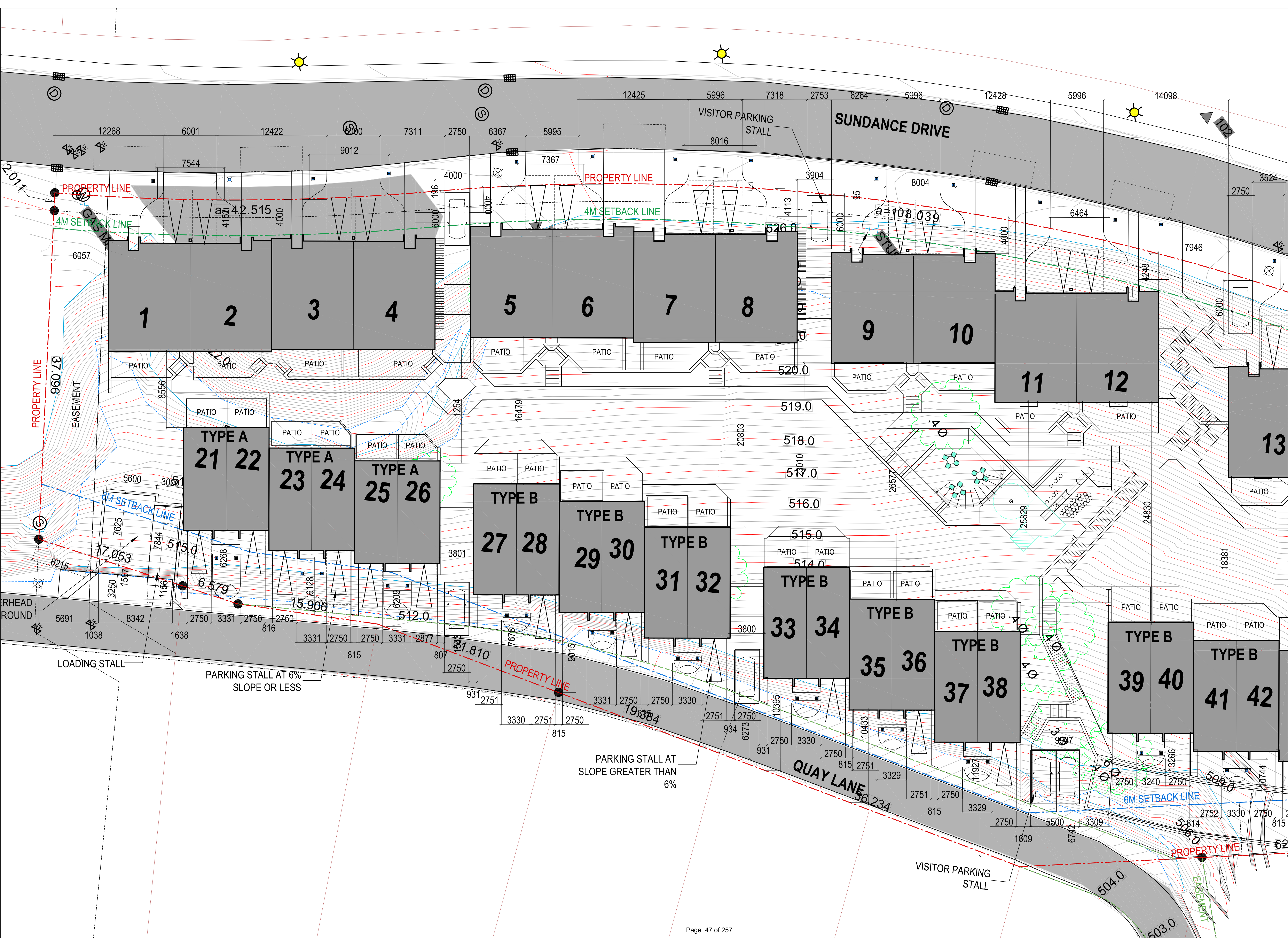
SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

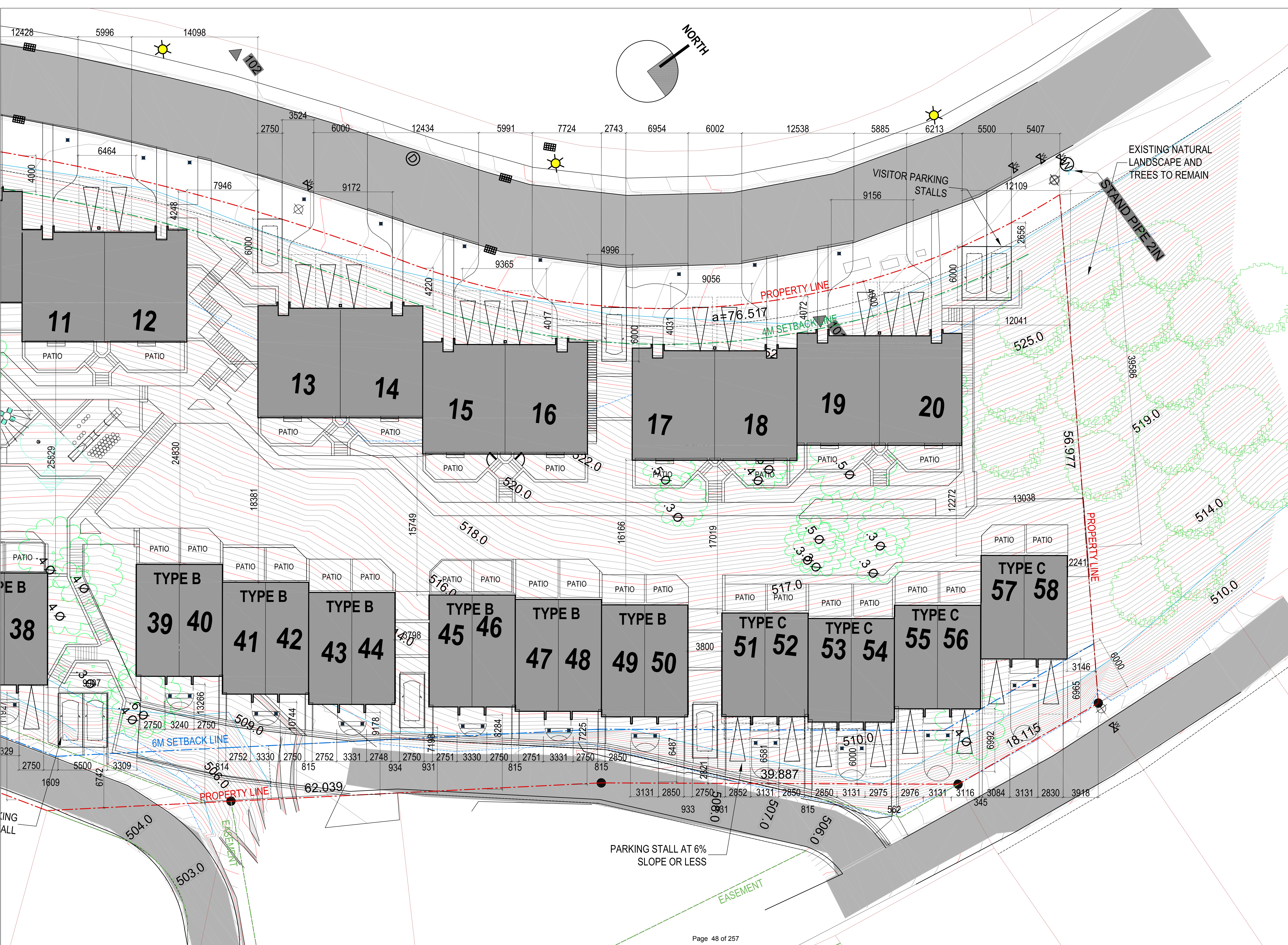
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A1.1

SCALE
1:175





ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
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PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
3401 SUNDANCE DRIVE

SHEET TITLE
PARTIAL SITE PLAN
SETBACKS AND DIMENSIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A1.2

SCALE
1:175

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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UNIT AREAS:

MAIN FLOOR (ENTRY LEVEL) = 1,210 sq.ft.
MID-LOWER LEVEL = 738 sq.ft.
BOTTOM LEVEL = 738 sq.ft.

GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED IN 2 GROUPS OF 2 FOR A TOTAL OF 4 UNITS AS PER THE SITE PLAN.
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
ENTRY LEVEL PLAN
SUNDANCE ROAD

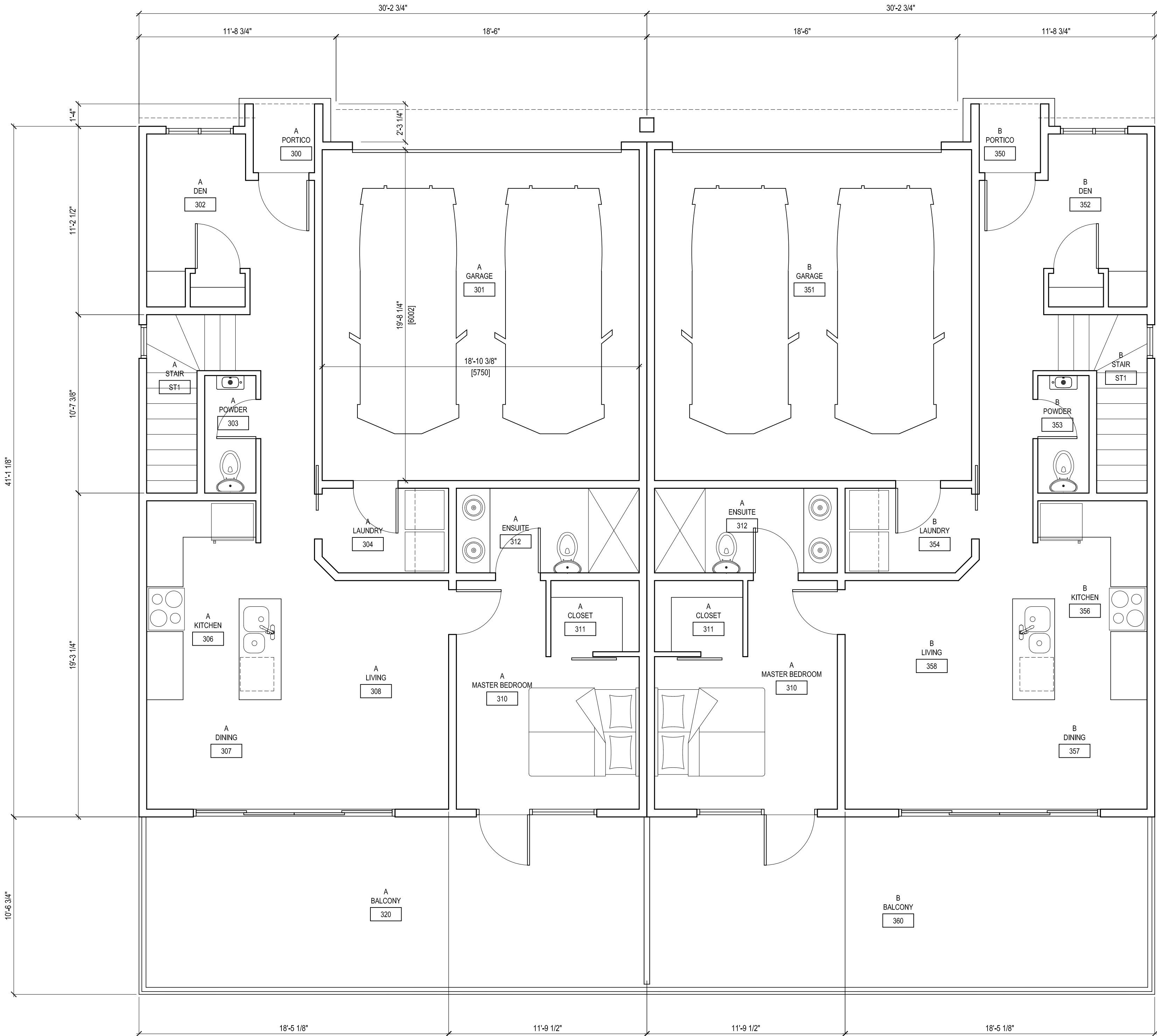
DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.1

SCALE
1/4"=1'-0"

SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE



1
A2.1
ENTRY LEVEL
1/4"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
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DP REVISIONS	03	APRIL 17, 2023

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PROJECT LOCATION
SUNDANCE ROAD

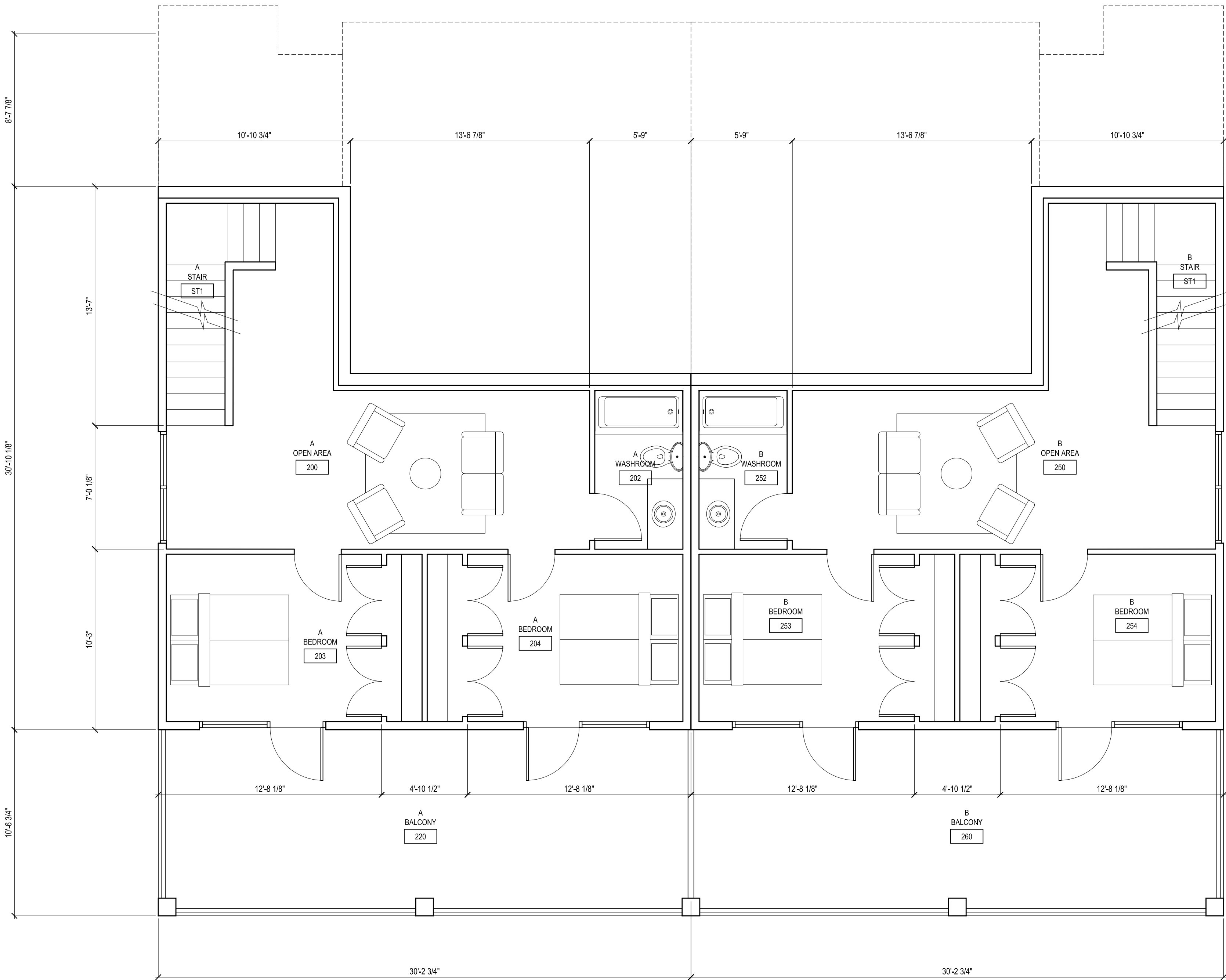
SHEET TITLE
SIDE BY SIDE UNITS
LOWER MID-LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.2

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.2
MID-LOWER LEVEL
1/4"=1'-0"

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PROJECT NUMBER
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SUNDANCE ROAD

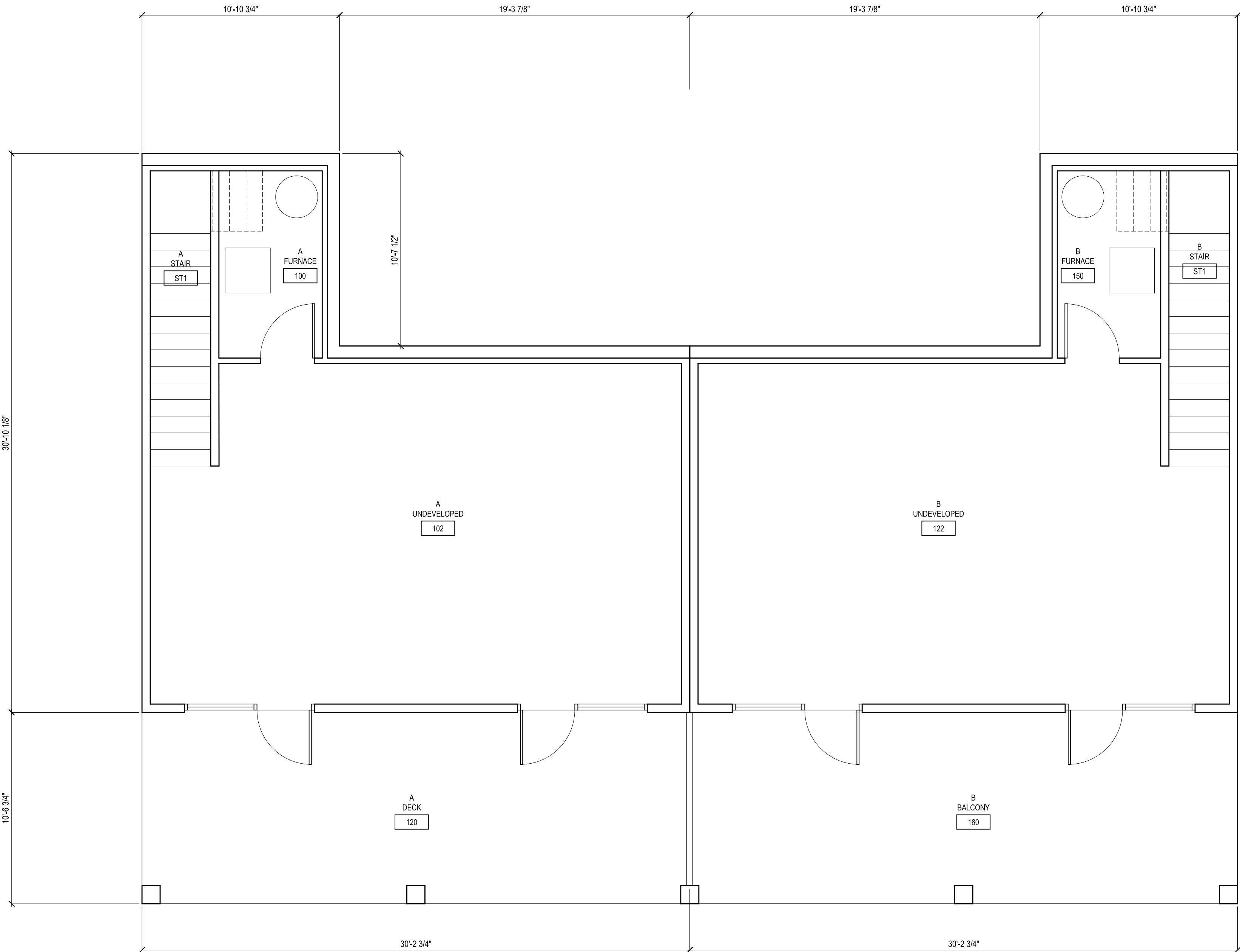
SHEET TITLE
SIDE BY SIDE UNITS
BOTTOM LEVEL PLAN
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.3

SCALE
1/4"=1'-0"



SIDE BY SIDE GARAGE UNIT
TYPICAL PLAN
SUNDANCE ROAD FRONTAGE

1
A2.3 LOW LEVEL
1/4"=1'-0"

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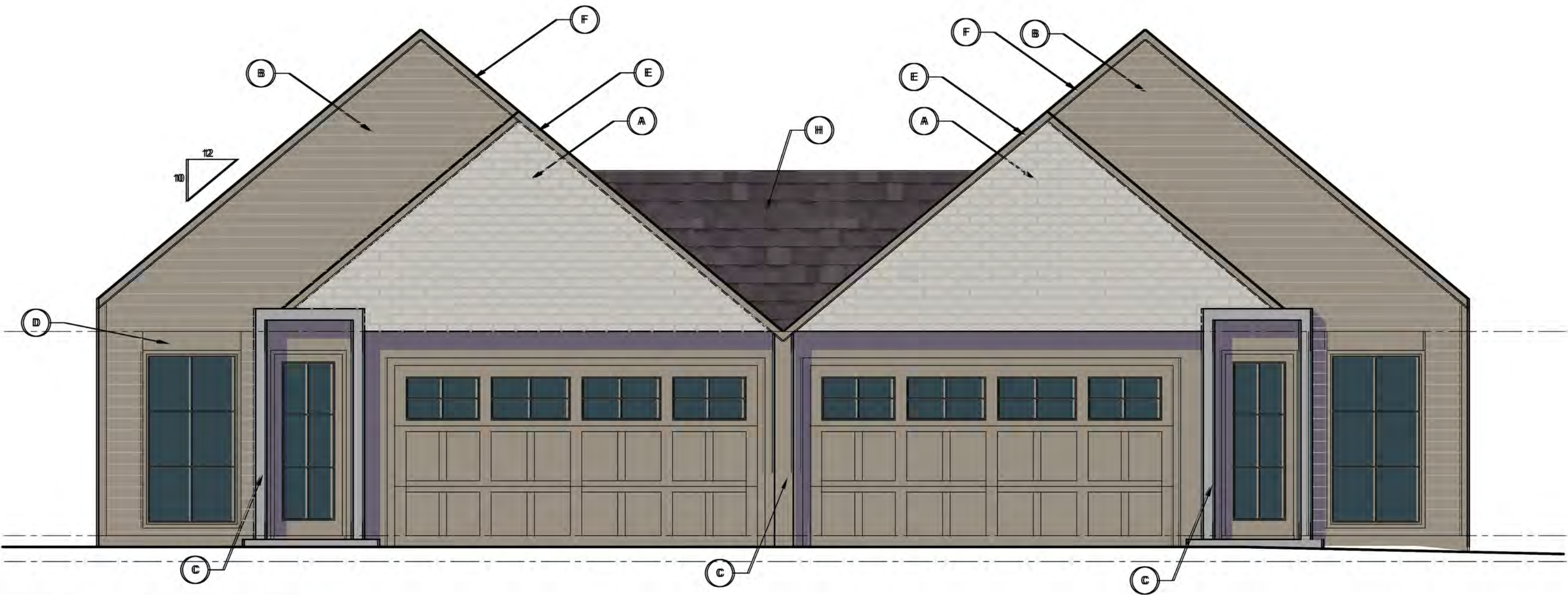
SHEET TITLE
SIDE BY SIDE UNITS
ELEVATIONS

DATE
FEBRUARY 24, 2023

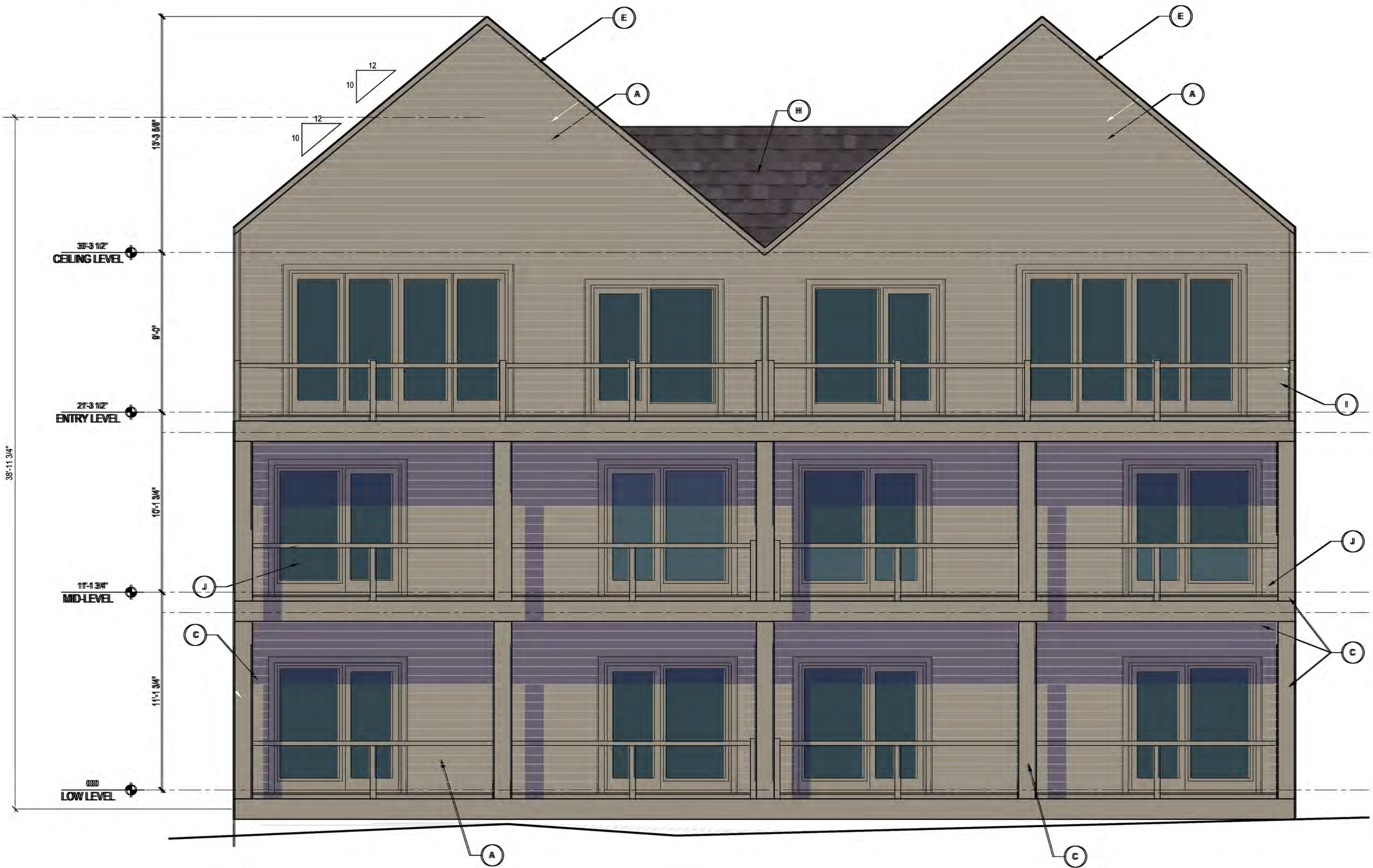
SHEET NUMBER

A2.4

SCALE
1/4"=1'-0"



1
A2.4
STREET ELEVATION - COMBINED
1/4"=1'-0"



2
A2.4
REAR ELEVATION - DOUBLE UNIT
1/4"=1'-0"

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION		
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS		GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUP OF UNITS USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE IN COLOUR TONES FROM LIGHT TO MID AND THEN TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS. THE COLOURS SHOWN IN THIS ELEVATION REPRESENT THE MID-TONE COLOUR OPTION.

ISSUE	NO.	DATE
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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
ELEVATIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.5

SCALE
1/4"=1'-0"



GENERAL NOTES:

- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
- REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
- DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAP ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION		
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS	F	PAINTED FASCIA COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS	H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
C	HARDIE PLANK COLOUR: 3 VARIATIONS	I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS	J	ALUMINUM RAILING WITH PICKETS COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS		GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
STREET SCAPE
ELEVATIONS

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A2.6

SCALE
1/8"=1'-0"

1
A2.6
STREET SCAPE
1/8" = 1'-0"



ISSUE	NO.	DATE
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DP REVISIONS	02	FEBRUARY 24, 2023

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FINAL RENDERINGS BEING COMPLETED AND WILL BE SUBMITTED AS SOON AS DONE. MINOR CHANGES ONLY TO OCCUR TO IMAGES

PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPINGS OF LIGHT TONE UNITS AND MID TONE UNITS IN DISTANCE



PERSPECTIVE STREET VIEW OF SUNDANCE DRIVE SIDE-BY-SIDE UNITS SHOWING GROUPING OF DARK TONE UNITS

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PROJECT NUMBER
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PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
SIDE BY SIDE UNITS
RENDERED PERSPECTIVES

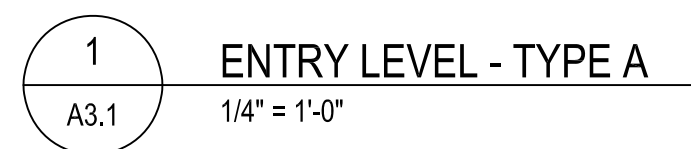
DATE
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SHEET NUMBER
A2.7

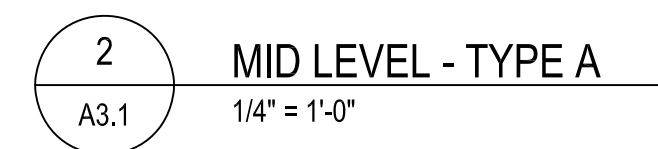
SCALE
N.T.S.

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- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING. FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS



SIDE A	SIDE B
ENTRY LEVEL (GARAGE) 645 sq.ft.	ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 668 sq.ft.	MID LEVEL = 645 sq.ft.
TOP FLOOR = 679 sq.ft.	TOP FLOOR = 678 sq.ft.

DiStefano Jaud
ArchitecturePROJECT
PROVIDENCE

PROJECT LOCATION
SUNDANCE ROAD

DATE
FEBRUARY 24, 2023

A3.1

SCALE
1/4"=1'-0"

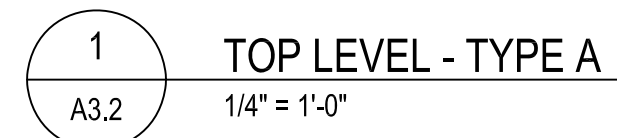
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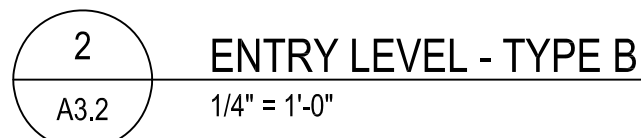
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SCALE
1/4"=1'-0"



**TANDEM GARAGE UNIT
UNIT TYPE B - ELONGATED PLAN
QUAY LANE FRONTAGE**



SIDE B
ENTRY LEVEL (GARAGE) 649 sq.ft.
MID LEVEL = 663 sq.ft.
TOP FLOOR = 711 sq.ft.

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DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

GENERAL NOTES:

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- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

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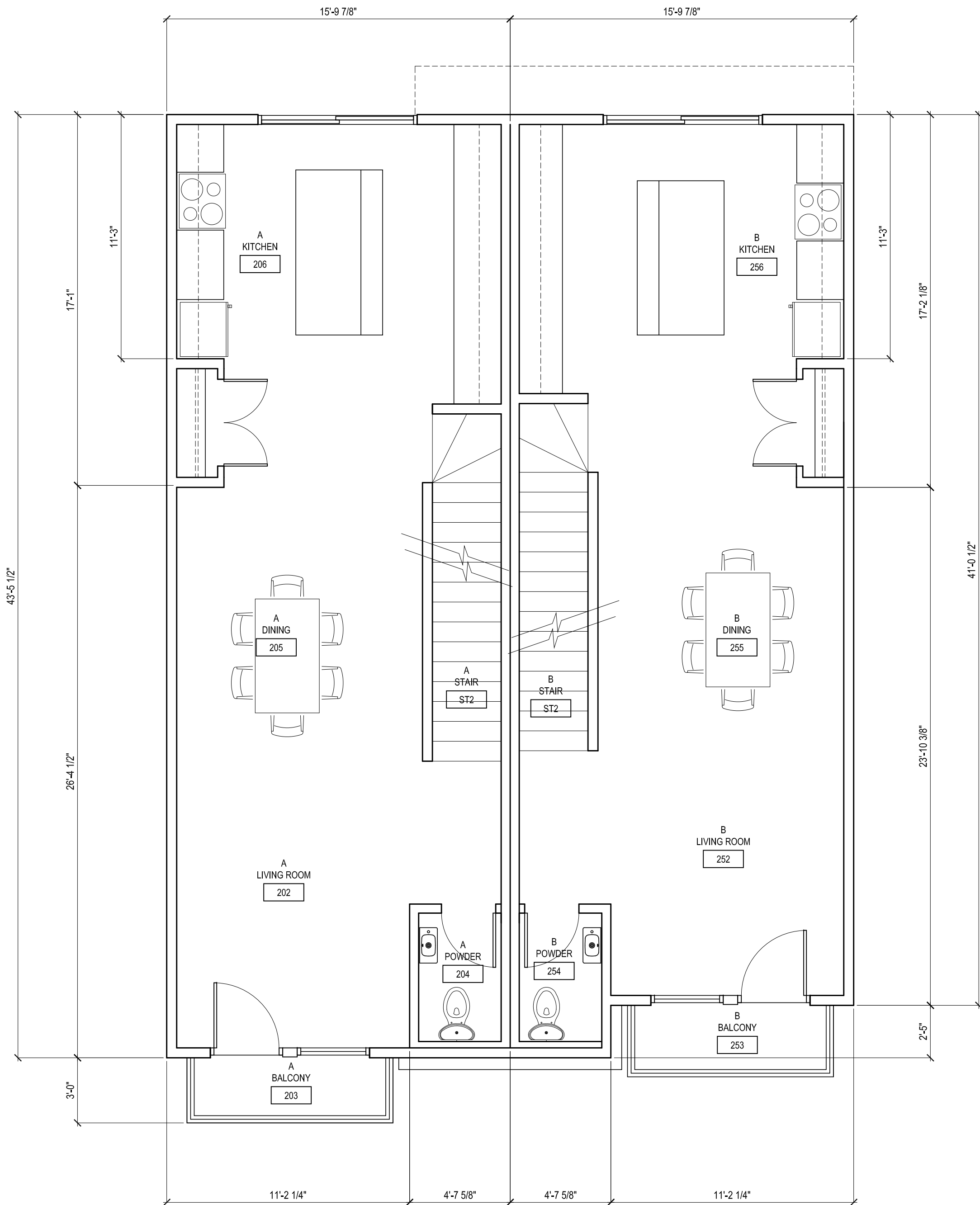
SHEET TITLE
**TANDEM UNITS - TYPE B
FLOOR PLANS
QUAY LANE**

DATE
FEBRUARY 24, 2023

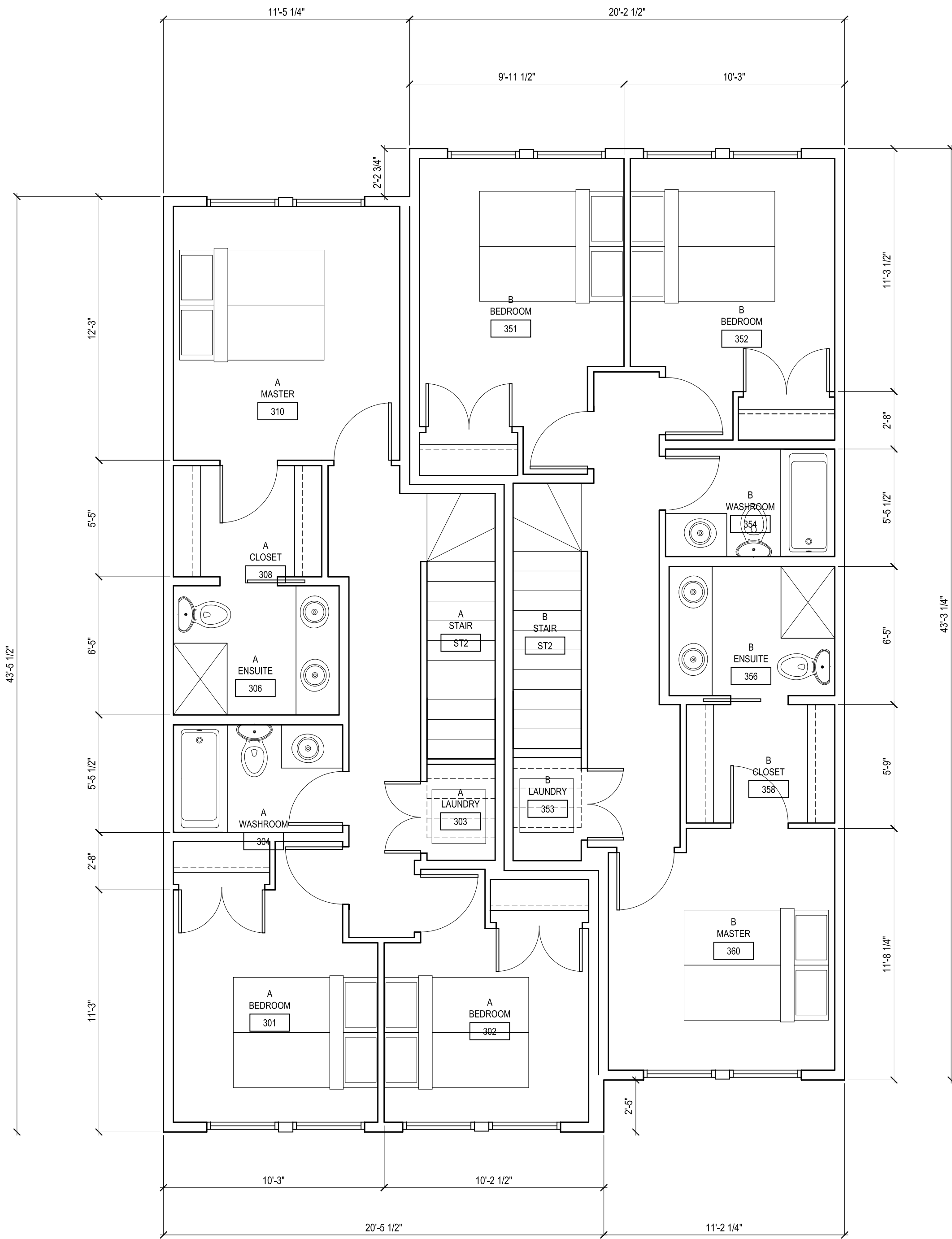
SHEET NUMBER

A3.3

SCALE
1/4"=1'-0"



1
A3.3 **MID LEVEL - TYPE B**
1/4" = 1'-0"



2
A3.3 **TOP LEVEL - TYPE B**
1/4" = 1'-0"

TANDEM GARAGE UNIT
UNIT TYPE C - SHORTENED PLAN,
EXTENDED FRONT DECK
QUAY LANE FRONTAGE

UNIT AREAS TYPE C:

SIDE C
ENTRY LEVEL (GARAGE) 645 sq.ft.
MID LEVEL = 668 sq.ft.
TOP FLOOR = 679 sq.ft.

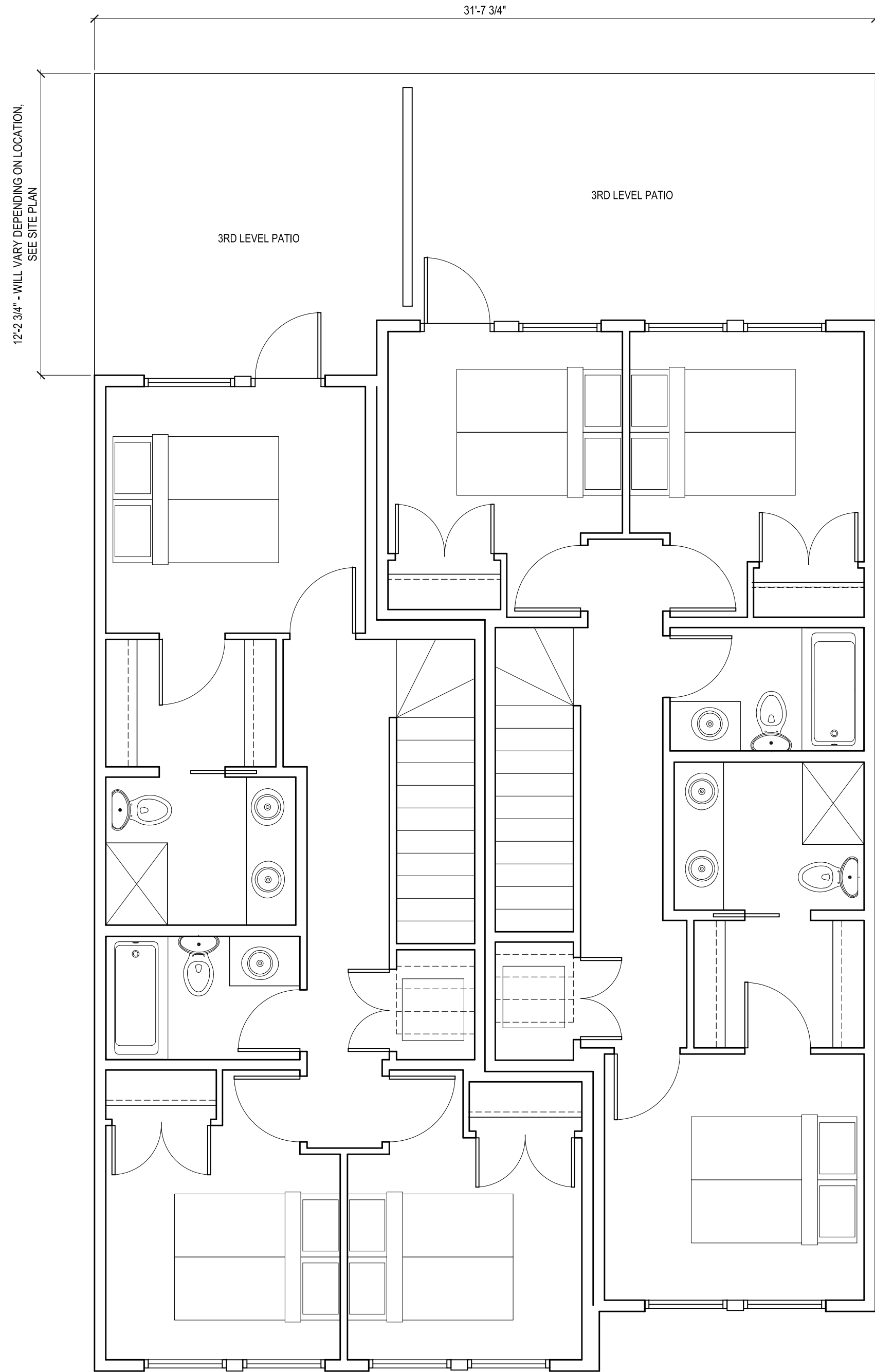
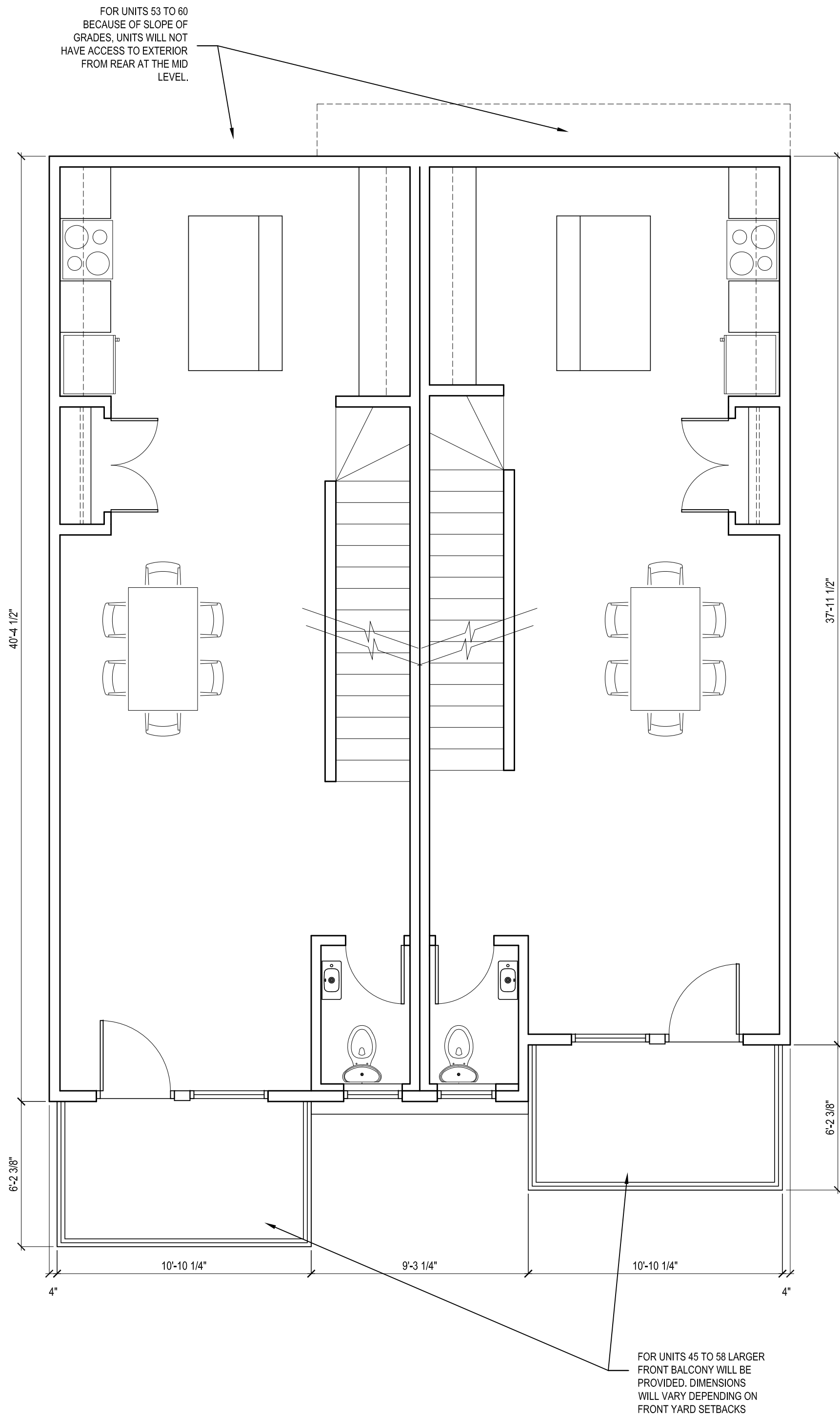
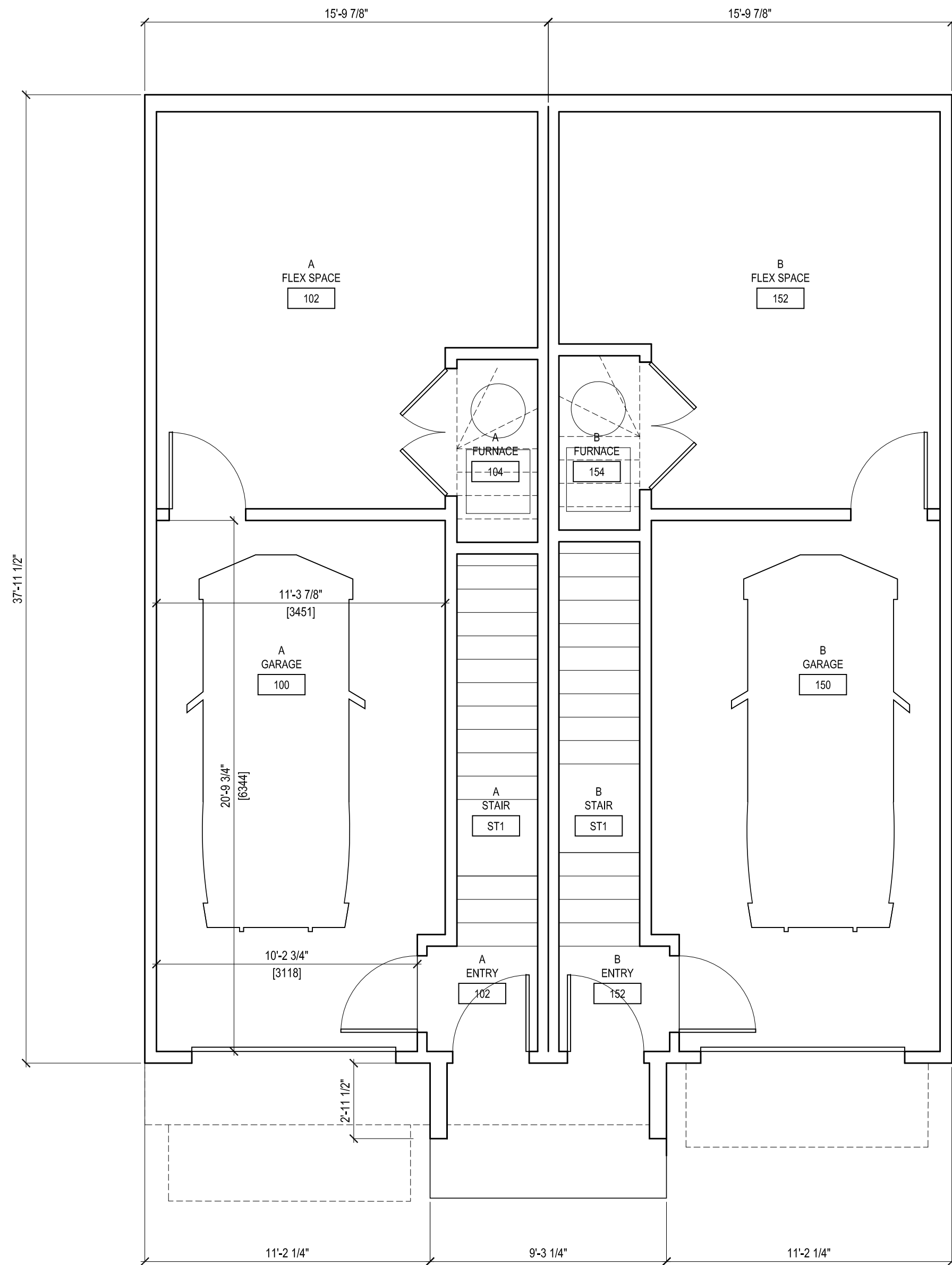
GENERAL NOTES:

- PLANS SHOWN ARE THE TYPICAL TWO UNIT GROUPING, FOR THESE UNIT TYPES THEY WILL BE ASSEMBLED AS PER THE SITE PLAN
- SEE SITE PLAN FOR LOCATIONS AND SETBACKS ON LOTS

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023
DP REVISIONS	03	APRIL 17, 2023

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TANDEM UNITS 51 TO 58



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WWW.DISTEFANOJAUD.COM

PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS - TYPE C
FLOOR PLANS
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER

A3.4

SCALE
1/4"=1'-0"

1 ENTRY LEVEL - TYPE C
A3.4 1/4" = 1'-0"

2 MID LEVEL - TYPE C
A3.4 1/4" = 1'-0"

3 TOP LEVEL - TYPE C
A3.4 1/4" = 1'-0"

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EXTERIOR CLADDING LEGEND

TYPE	DESCRIPTION
A	HARDIE PLANK LAP SIDING COLOUR: 3 VARIATIONS
B	BRICK - PACIFIC ART STONE COLOUR: 3 VARIATIONS
C	HARDIE PLANK CLAD ELEMENT COLOUR: 3 VARIATIONS
D	HARDIE BOARD TRIM LINTEL COLOUR: 3 VARIATIONS
E	PAINTED FASCIA COLOUR: 3 VARIATIONS
F	PAINTED FASCIA COLOUR: 3 VARIATIONS
H	ASPHALT SHINGLE ROOFING COLOUR: DUAL BLACK
I	ALUMINUM RAILING WITH GLASS PANELS COLOUR: 3 VARIATIONS
	GLASS

BUILDING COLOUR VARIATIONS

3 COLOUR VARIATIONS WILL BE USED FOR THE DEVELOPMENT. EACH GROUPING OF BUILDING WILL USE ONE COLOUR OPTION WITH THE NEXT GROUPING USING COLOUR VARIATION 2 AND THEN 3. THE GENERAL DIFFERENCE IS A CHANGE FROM LIGHT TO MID TO DARK TONES. SEE ATTACHED CLADDING SPECIFICATION SHEETS AND RENDERINGS FOR FURTHER DETAILS

GENERAL NOTES:

- STREET ELEVATION SHOWS TYPICAL GROUPING OF UNITS ALONG SUNDANCE ROAD. MATERIAL USAGE AND MODULATION WILL BE REPEATED IN THIS MANNER FOR FULL DEVELOPMENT AS PER RENDERINGS
- REAR ELEVATIONS WILL USE ONLY THE SIDING COLOUR USED ON THE FRONT ELEVATION WITH NO MODULAR PER 4 UNIT GROUPINGS.
- DEVELOPMENT TO HAVE 3 DIFFERENT COLOUR THEMES USED THROUGHOUT THE DEVELOPMENT. SEE THEMES OR VARIATIONS IN LEGEND AND IN STREET SCAPE ELEVATIONS FOR HOW COLOURS WILL BE ASSIGNED

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS - TYPE C
TYPICAL ELEVATIONS
QUAY LANE

DATE
FEBRUARY 24, 2023

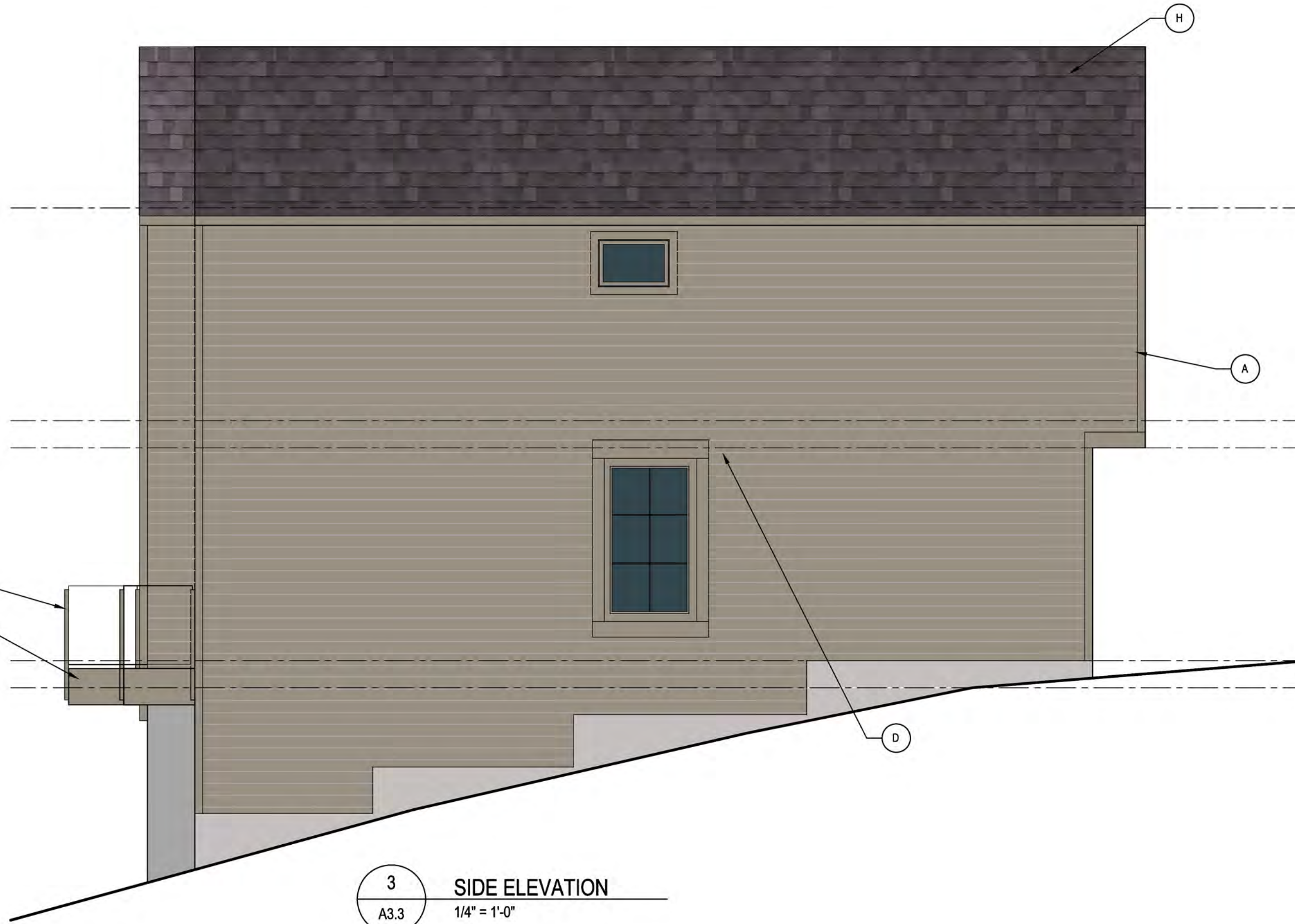
SHEET NUMBER

A3.5

SCALE
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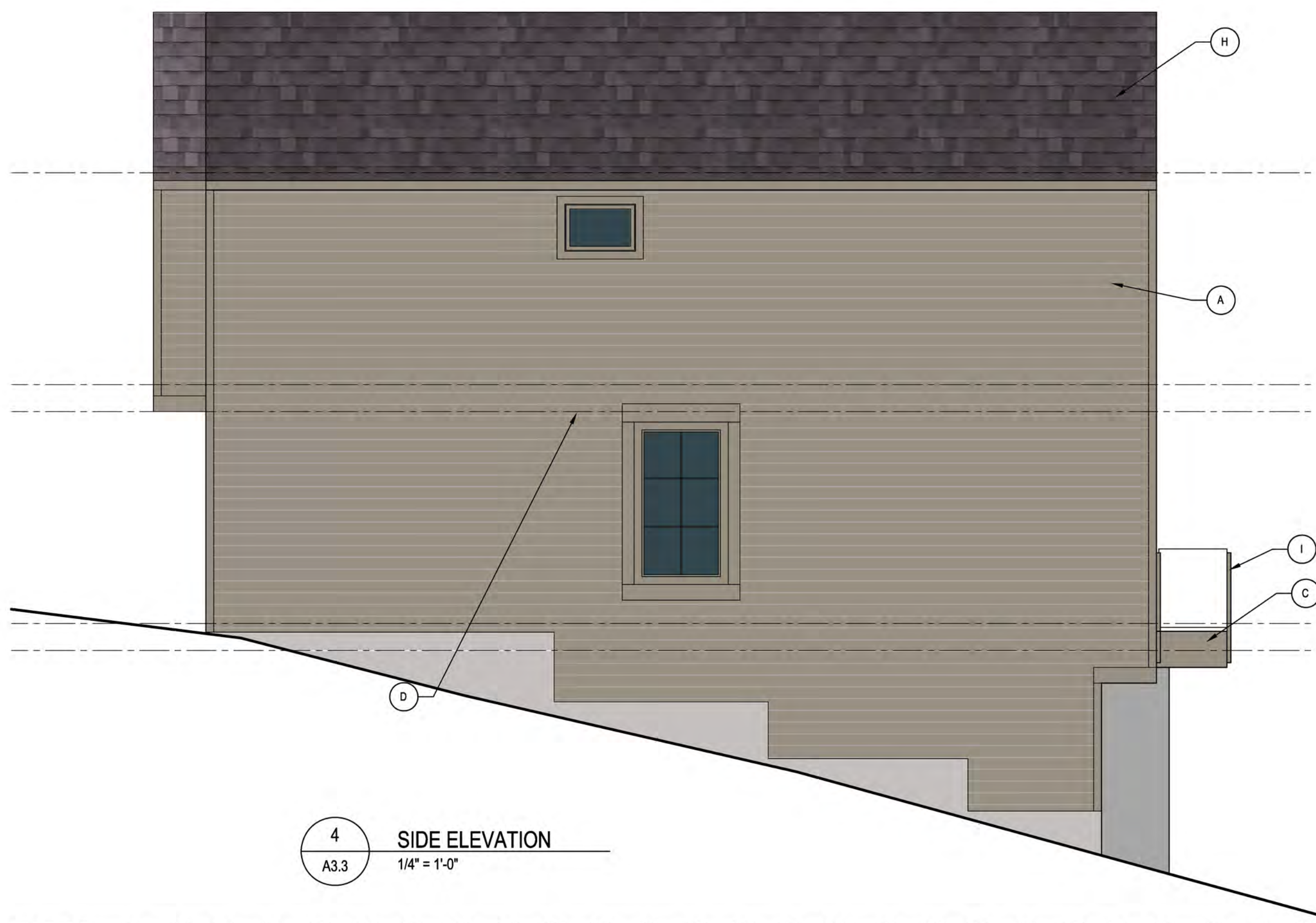
1 STREET ELEVATION
A3.3 1/4" = 1'-0"



3 SIDE ELEVATION
A3.3 1/4" = 1'-0"



2 REAR ELEVATION
A3.3 1/4" = 1'-0"



4 SIDE ELEVATION
A3.3 1/4" = 1'-0"



ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58
PROJECT LOCATION
SUNDANCE ROAD
SHEET TITLE
TANDEM UNITS - TYPE C
STREETSCAPE ELEVATION
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.6

SCALE
1/8"=1'-0"

ISSUE	NO.	DATE
DP SUBMISSION	01	MAY 21, 2022
DP REVISIONS	02	FEBRUARY 24, 2023

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PERSPECTIVE STREET VIEW OF
SUNDANCE DRIVE SIDE-BY-SIDE
UNITS SHOWING GROUPINGS OF
LIGHT TONE UNITS AND MID
TONE UNITS IN DISTANCE

FINAL RENDERINGS BEING
COMPLETED AND WILL BE
SUBMITTED AS SOON AS DONE.
MINOR CHANGES ONLY TO
OCCUR TO IMAGES

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DiStefano Jaud
Architecture

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WWW.DISTEFANOJAUD.COM

PROJECT
PROVIDENCE

PROJECT NUMBER
DJA 2021-58

PROJECT LOCATION
SUNDANCE ROAD

SHEET TITLE
TANDEM UNITS
RENDERED VIEW
QUAY LANE

DATE
FEBRUARY 24, 2023

SHEET NUMBER
A3.7

SCALE
N.T.S.

GATEHOUSE SUNDANCE DEVELOPMENT

Issued for Development Permit

Contact Information

VDZ+A

Project Landscape Architecture

Kelowna Studio
302 - 1150 Sunset Drive
Kelowna, British Columbia, V1Y 9Z5

Fort Langley Studio
102 - 9181 Church Street
Fort Langley, British Columbia, V1M 2R8

Mount Pleasant Studio
102-355 Kingsway
Vancouver, British Columbia, V5T 3J7

Primary project contact:
Kim McNamee
Landscape Architect
kim@vdz.ca
778 298 9738

Alternate contacts (incase away):
Stephen Heller
Associate Landscape Architect
stephen@vdz.ca
604 546 0925

Key Project Contacts

GATEHOUSE

Project Owner

PO BOX 2585 Stn Sardis
Chilliwack, BC V2R 1A8

Mike Shepherd
mike@madebygatehouse.com
604 819 2192

Distefano Jaud Architecture

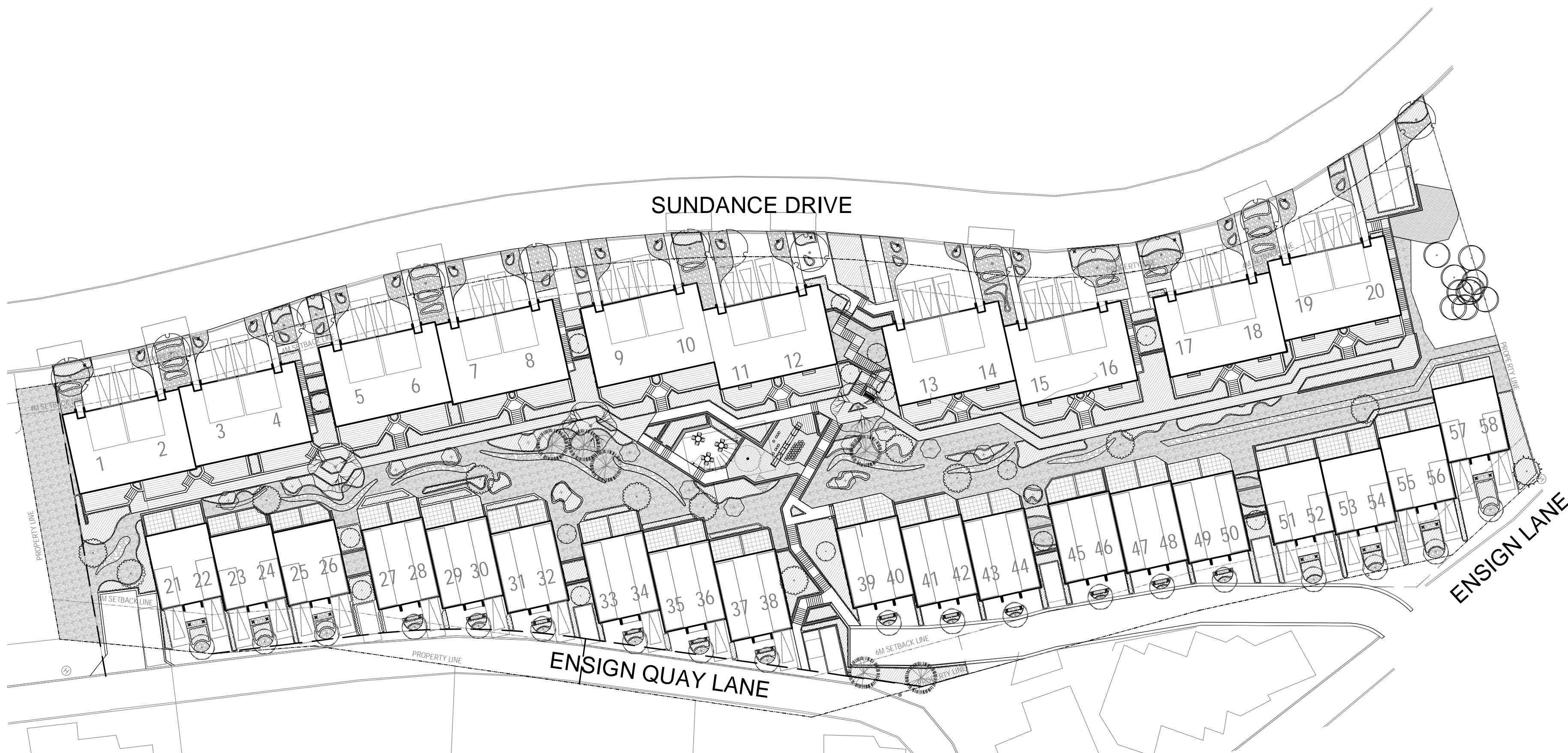
Project Architect

3, 1331 Ellis Street
Kelowna, BC V1Y 1Z9

Carlo Distefano
carlo@distefanojud.com
250 868 9278

Sheet List Table

Sheet Number	Sheet Title
L-00	COVER PAGE
L-01A	LANDSCAPE SITE PLAN WEST
L-01B	LANDSCAPE SITE PLAN CENTER
L-01C	LANDSCAPE SITE PLAN EAST
L-02	PLANT PALETTE
L-03A	FENCE AND GRADING PLAN WEST
L-03B	FENCE AND GRADING PLAN CENTER
L-03C	FENCE AND GRADING PLAN EAST
LS-01	SECTION A & B
LS-02	SECTION C & D
LS-03	SECTION E & F
LD-01	DETAILS
LD-02	DETAILS
LD-03	DETAILS
LD-04	DETAILS



1 SITE PLAN OVERVIEW
Scale 1:500



2 LOCATION MAP
NTS

VDZ+A

LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO
100-9181 Church St | 102-355 Kingsway
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V1M 2R8 | V5T 3J7
www.vdz.ca 604-882-0024

8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
No.	By:	Description	Date
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No.	By:	Description	Date
REVISIONS TABLE FOR SHEET			
Project: PROVIDENCE / SUNDANCE			
Location: 3401 Sundance Drive West Kelowna, B.C.			
Drawn:	Stamp:		
AH ET			
Checked:			
SH			
Approved:	Original Sheet Size:		
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Scale:	CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS TO DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.		
AS SHOWN			

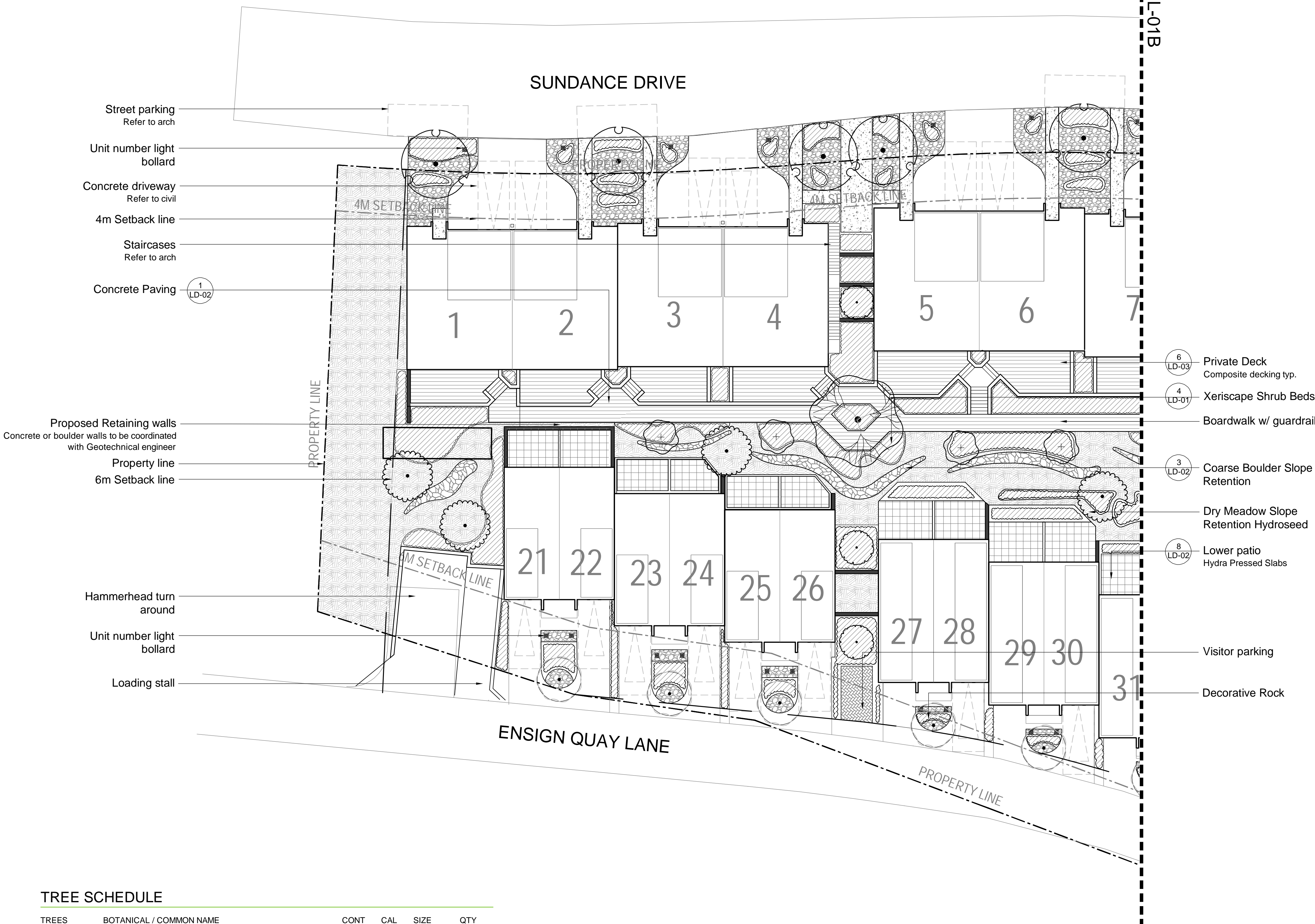
Drawing Title:
COVER PAGE



VDZ Project #:
DP2022-16

Drawing #:
L-00

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MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 3" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

VDZ+A

LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO

100-9181 Church St | 102-355 Kingsway

Fort Langley, BC | Vancouver, BC

VIM 2881 VST 317

www.vdz.ca

604-882-0024

Key Map (NTS)



No.	By:	Description	Date
8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
5	KM	Re-Issued for Coordination	2023-02-10
4	KM	Issued for Coordination	2023-02-08
3	ET	Issued for Development Permit	2022-06-10
2	ET	Issued for Review	2022-05-31
1	SH	Issued for Review	2022-05-18
REVISIONS TABLE FOR DRAWINGS			
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No.	By:	Description	Date
REVISIONS TABLE FOR SHEET			

Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn:
ET

Checked:
SH

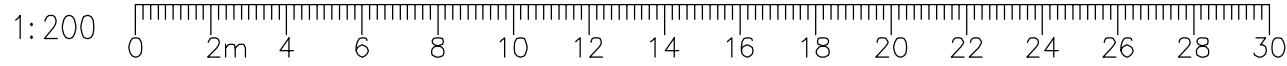
Approved:
APPROVED

Scale:
1:200

Stamp:

Original Sheet Size:
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MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 2" Basaltic River Stone
		DRY MEADOW SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

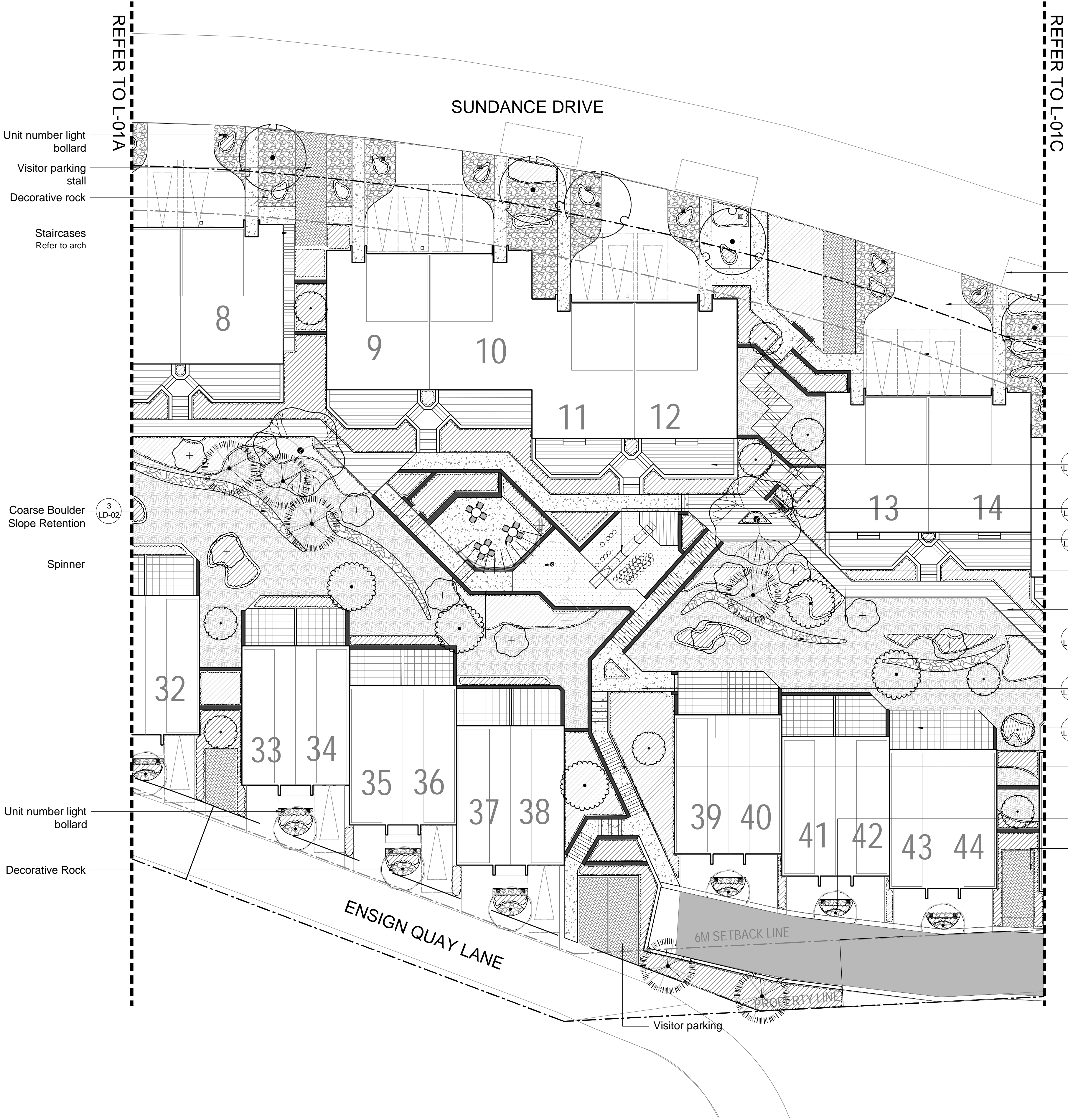
RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL: CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL: REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

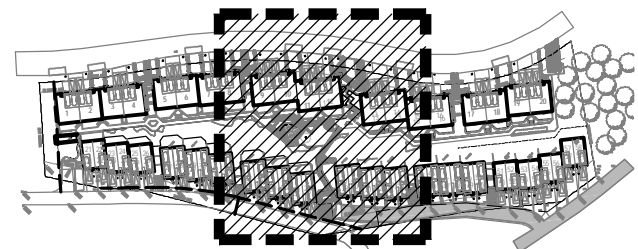
TREE SCHEDULE

TREES	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum 'Schmidt' / Rocky Mountain Glow Maple	B&B		6cm cal.	19
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos 'Skyline' / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta 'Contorta' / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

1:200 0 2m 4 6 8 10 12 14 16 18 20 22 24 26 28 30



Key Map (NTS)



No.	By:	Description	Date
8	KM	Issued for Development Permit	2023-04-18
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No.	By:	Description	Date
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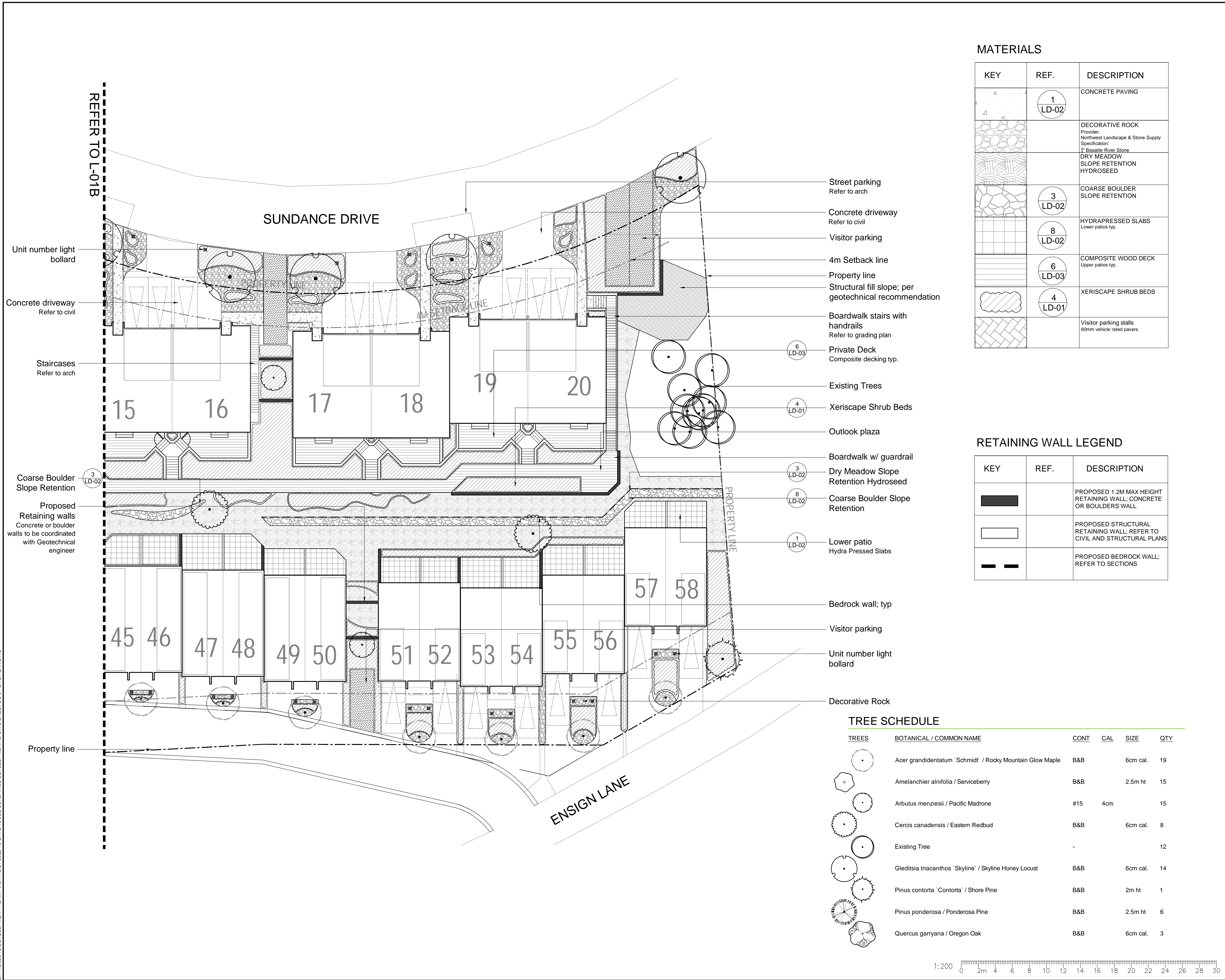
REVISIONS TABLE FOR SHEET

Project:
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Location:
3401 Sundance Drive
West Kelowna, B.C.

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Checked: SH	
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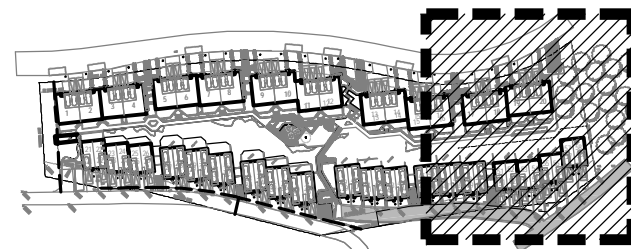
MATERIALS

KEY	REF.	DESCRIPTION
	1 LD-02	CONCRETE PAVING
		DECORATIVE ROCK Provider: Northwest Landscape & Stone Supply Specification: 3" Basaltic River Stone
		DRY MEADOW/ SLOPE RETENTION HYDROSEED
	3 LD-02	COARSE BOULDER SLOPE RETENTION
	8 LD-02	HYDRAPRESSED SLABS Lower patios typ.
	6 LD-03	COMPOSITE WOOD DECK Upper patios typ.
	4 LD-01	XERISCAPE SHRUB BEDS
		Visitor parking stalls 80mm vehicle rated pavers

RETAINING WALL LEGEND

KEY	REF.	DESCRIPTION
		PROPOSED 1.2M MAX HEIGHT RETAINING WALL; CONCRETE OR BOULDERS WALL
		PROPOSED STRUCTURAL RETAINING WALL; REFER TO CIVIL AND STRUCTURAL PLANS
		PROPOSED BEDROCK WALL; REFER TO SECTIONS

Key Map (NTS)



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No.	By:	Description	Date

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Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
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Approved: APPROVED	Original Sheet Size: 24"x36"
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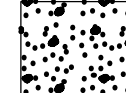




Z:\PROJECTS\DEVELOPMENT PERMIT\ACTIVEDP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWGS\SHEETS\L-02 PLANTING PLAN.DWG

Tree Schedule

Trees	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	Acer grandidentatum `Schmidt` / Rocky Mountain Glow Maple	B&B		6cm cal.	19
	Amelanchier alnifolia / Serviceberry	B&B		2.5m ht	15
	Arbutus menziesii / Pacific Madrone	#15	4cm		15
	Cercis canadensis / Eastern Redbud	B&B		6cm cal.	8
	Existing Tree	-			12
	Gleditsia triacanthos `Skyline` / Skyline Honey Locust	B&B		6cm cal.	14
	Pinus contorta `Contorta` / Shore Pine	B&B		2m ht	1
	Pinus ponderosa / Ponderosa Pine	B&B		2.5m ht	6
	Quercus garryana / Oregon Oak	B&B		6cm cal.	3

Plant Schedule

SHRUBS	BOTANICAL / COMMON NAME	CONT	
	Arbutus unedo `Compacta` / Dwarf Strawberry Tree	#3	
	Artemisia tridentata / Big Sagebrush	#2	
	Berberis thunbergii `Concorde` / Concorde Barberry	#2	
	Caryopteris x clandonensis / Bluebeard	#2	
	Ericameria nauseosa / Rubber Rabbitbrush	#2	
	Potentilla fruticosa `Tangerine` / Tangerine Potentilla	#2	
	Rhus glabra / Smooth Sumac	#2	
DWARF CONIFERS	BOTANICAL / COMMON NAME	CONT	
	Juniperus chinensis `Torulosa` / Hollywood Juniper	#3	
	Juniperus sabina / Savin Juniper	#2	
	Pinus mugo `Pumilio` / Mugo Pine	#2	
GRASSES	BOTANICAL / COMMON NAME	CONT	
	Chasmanthium latifolium / Wood Oats	#1	
	Elymus mollis / American Dunegrass	#1	
PERENNIALS	BOTANICAL / COMMON NAME	CONT	
	Balsamorhiza sagittata / Arrowleaf Balsamroot	#1	
	Centranthus ruber / Red Valerian	#1	
	Echinacea purpurea `Firebird` / Firebird Coneflower	#1	
	Eryngium alpinum `Blue Star` / Sea Holly	#1	
	Perovskia x `Little Spire` / Russian Sage	#1	
	Sedum x `Thunderhead` / Thunderhead Sedum	#1	
	Yucca glauca / Narrowleaf Yucca	#2	
GROUND COVERS	BOTANICAL / COMMON NAME	CONT	SPACING
	Arctostaphylos uva-ursi / Kinnikinnick	#1	300mm
	Juniperus horizontalis `Andorra` / Andorra Juniper	#1	600mm
	Thymus pseudolanuginosus / Woolly Thyme	#1	300mm



Ericameria nauseosa



Balsamorhiza sagittata



Potentilla fruticosa 'Tangerine'



Arbutus unedo 'nana'



Rhus Glabra



Echinacea 'Firebird'



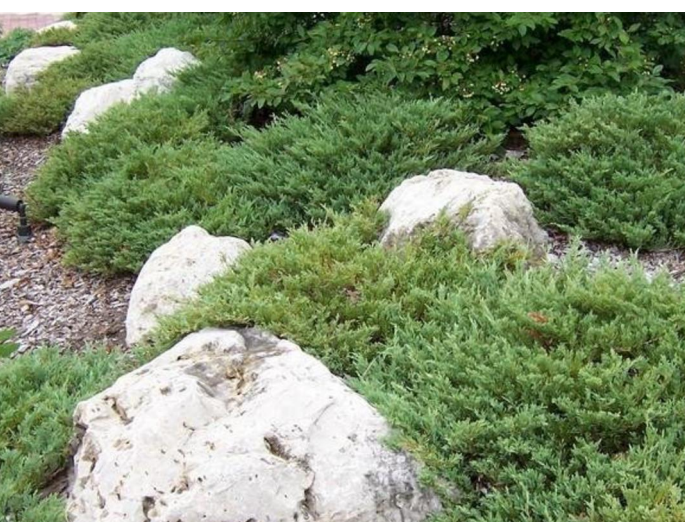
Perovskia Atriplicifolia



Centranthus Ruber



Eryngium Alpinum 'Blue star'



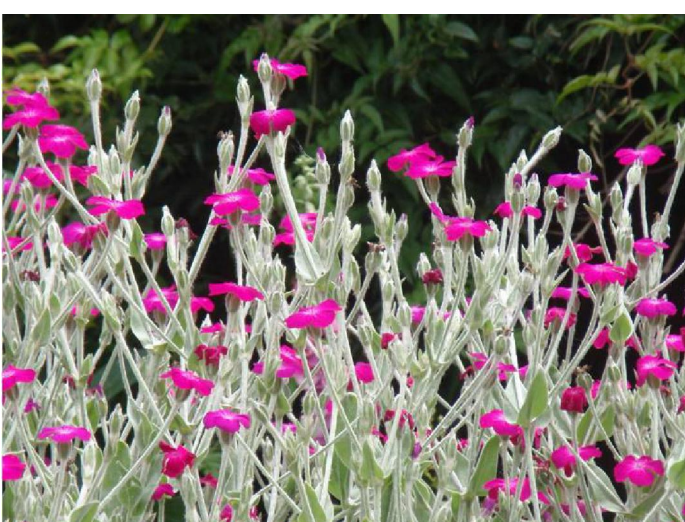
Juniperous Horizontalis 'Andorra'



Juniperous Sabina



Caryopteris x Clandonensis



Lychnis



Sedum x `Thunderhead`



Berberis Thunbergii 'Concorde'



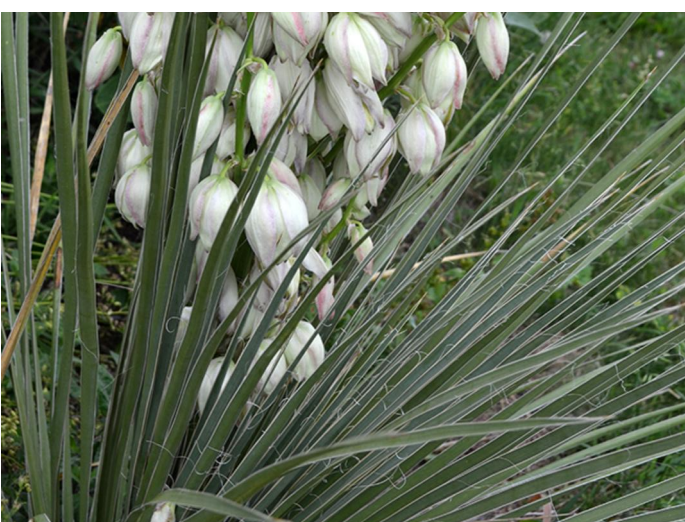
Artemisia Tridentata



Thymus Pseudolanuginosus



Arctostaphylos Uva-Ursi



Yucca Glauca



Dry meadow seed mix

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LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY


FORT LANGLEY STUDIO | MOUNT PLEASANT STUDIO

100-9181 Church St | 102-355 Kingsway

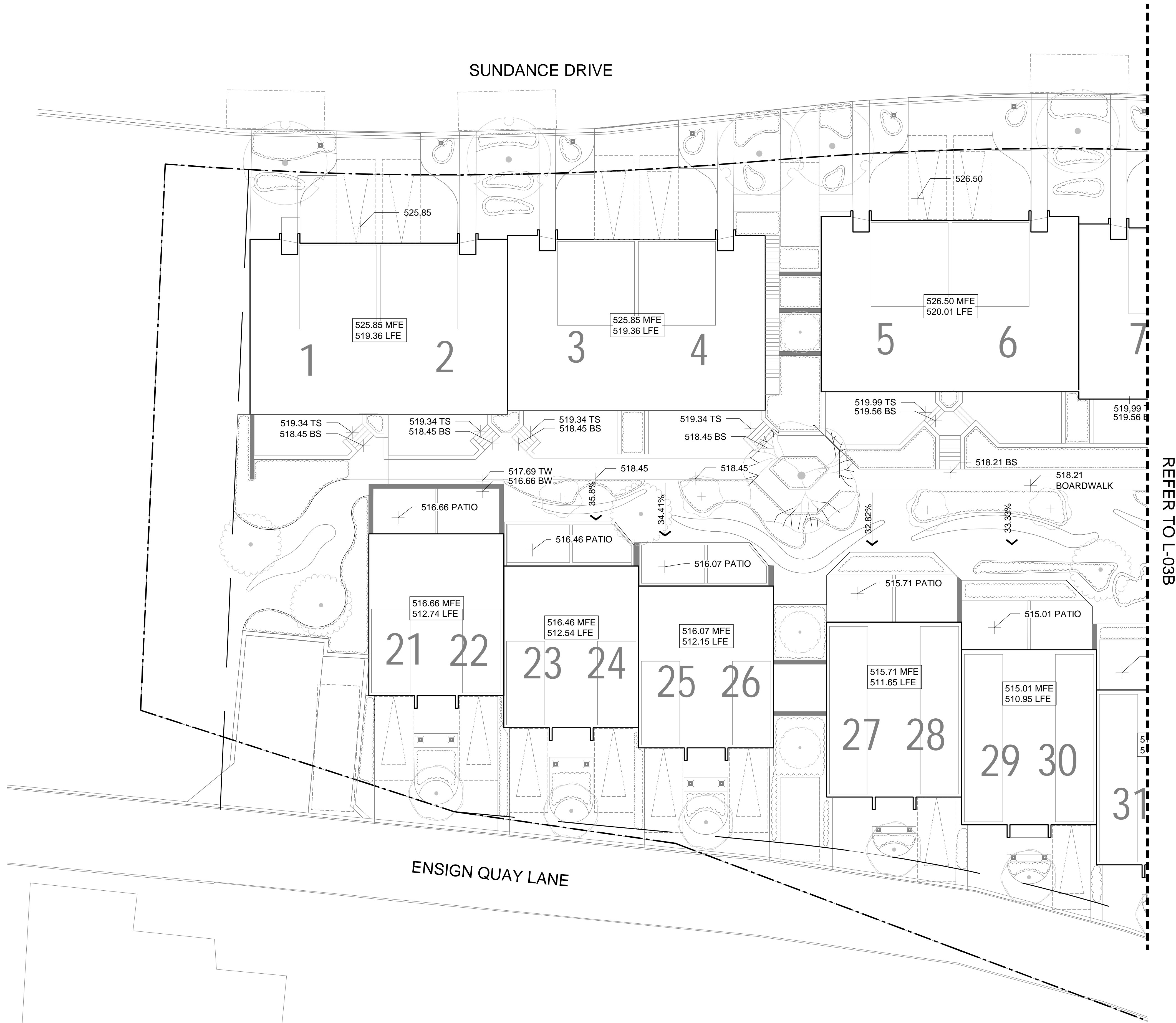
Fort Langley, BC | Vancouver, BC

V1M 2B8 | V5T 3J7

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8	KM	Issued for Development Permit	2023-04-18
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1	SH	Issued for Review	2022-05-18
No.	By:	Description	Date
REVISIONS TABLE FOR DRAWINGS			
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No.	By:	Description	Date
REVISIONS TABLE FOR SHEET			
Project: PROVIDENCE / SUNDANCE			
Location: 3401 Sundance Drive West Kelowna, B.C.			
Drawn:		Stamp:	
ET			
Checked: SH			
Approved: APPROVED			
Scale: 1:200		CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY/DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REZONING/DPP/PPA/HABP DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.	

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REFER TO L-03B

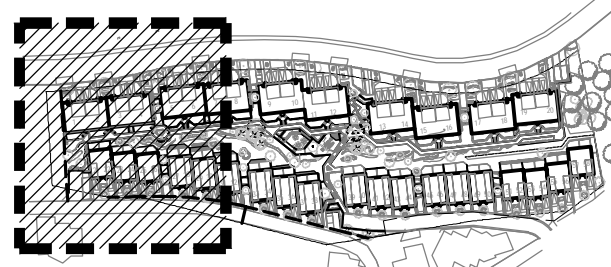


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Key Map (NTS)



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No.	By:	Description	Date
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Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn:	Stamp:
ET	

Checked:	
SH	

Approved:	Original Sheet Size:
APPROVED	24"x36"

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Drawing Title:

FENCE AND GRADING PLAN WEST

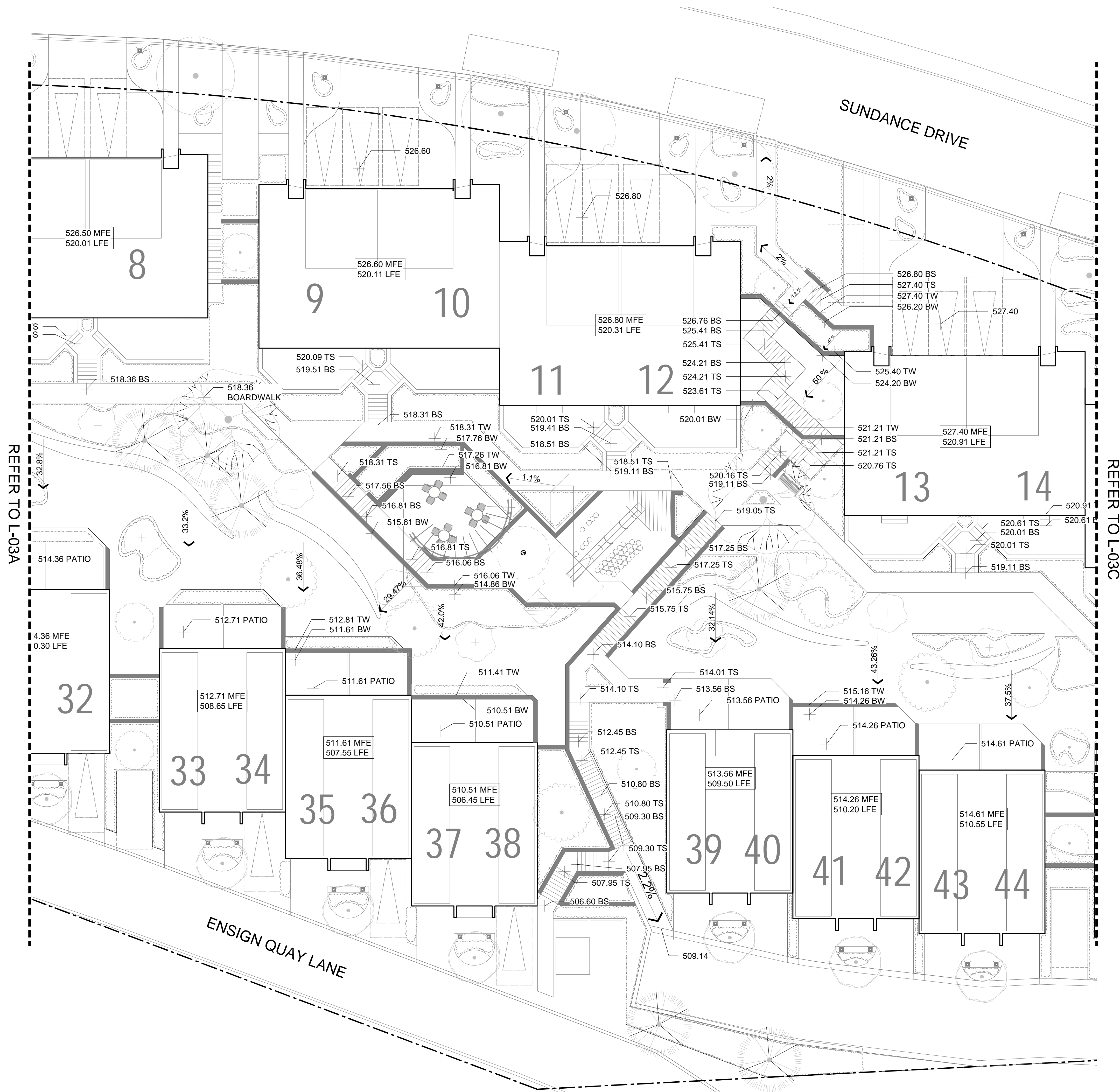
VDZ Project #:

DP2022-16

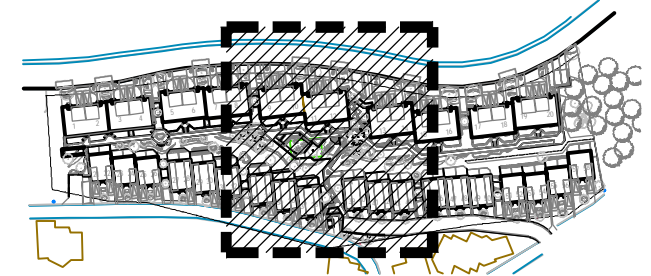
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Drawing Title:
FENCE AND GRADING PLAN CENTER

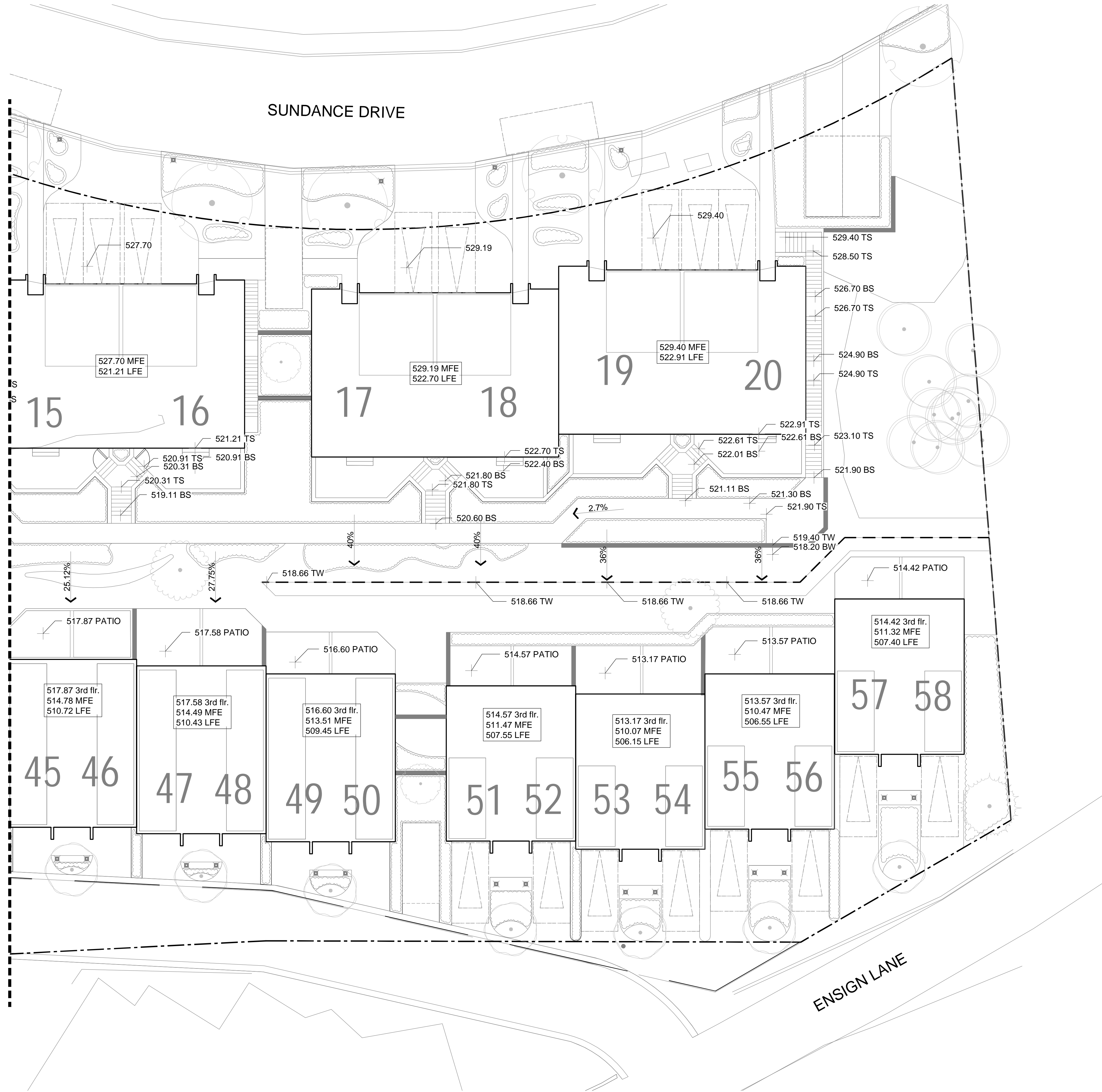


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DP2022-16

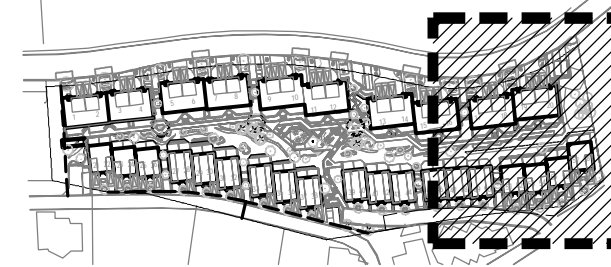
Drawing #:
L-03B

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REFER TO L-03B



Key Map (NTS)



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Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

Drawn: ET	Stamp:
Checked: SH	
Approved: APPROVED	Original Sheet Size: 24"x36"

Scale:
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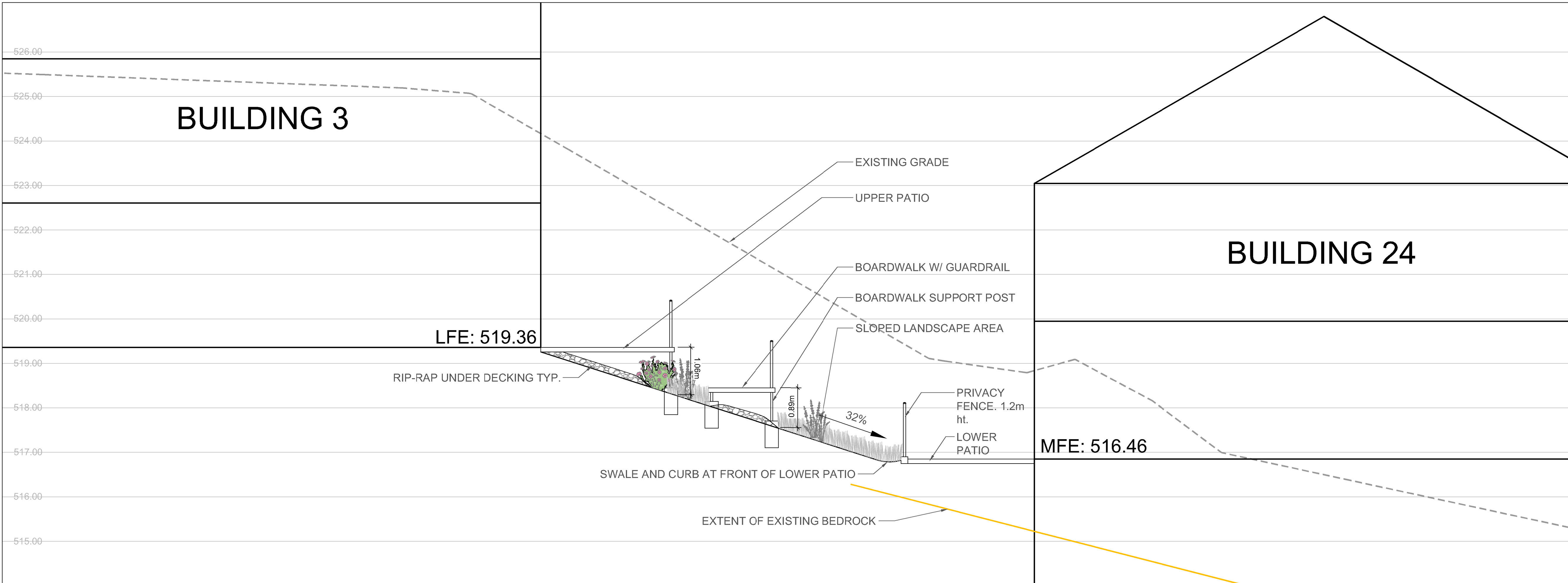
CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REVISIONS MUST BE LABELED AND DRAWINGS MUST NOT BE REPRODUCED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

Drawing Title:
FENCE AND GRADING PLAN EAST

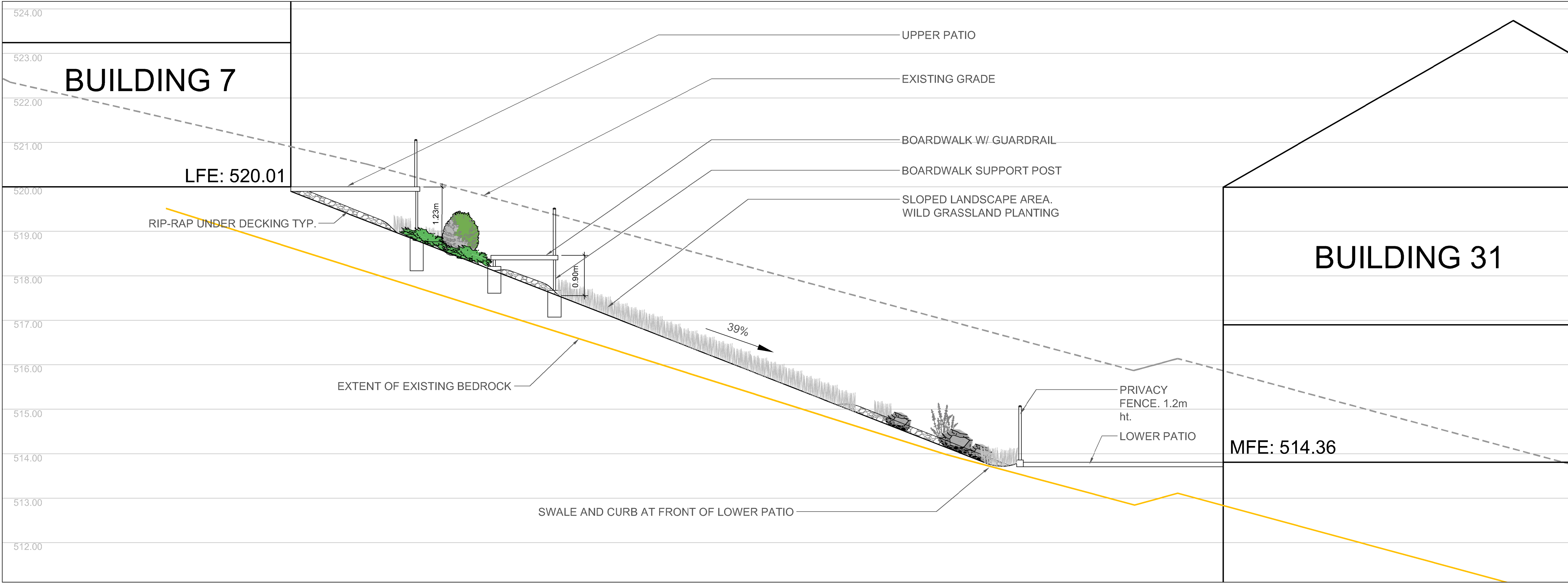


VDZ Project #:
DP2022-16

Drawing #:
L-03C



A SECTION THROUGH UNIT 3 AND 24
Scale 1:50



B SECTION THROUGH UNIT 7 AND 31
Scale 1:50

LEGEND	
	EXISTING GRADE
	EXISTING BEDROCK

No.	By:	Description	Date
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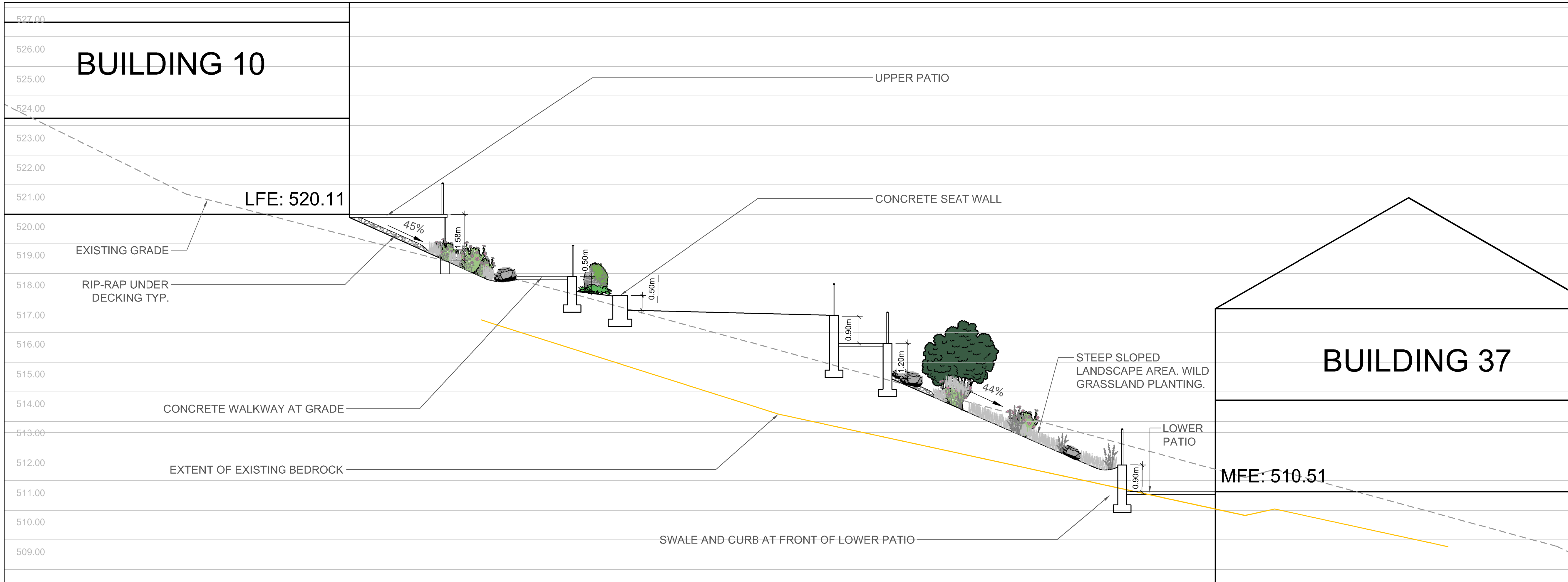
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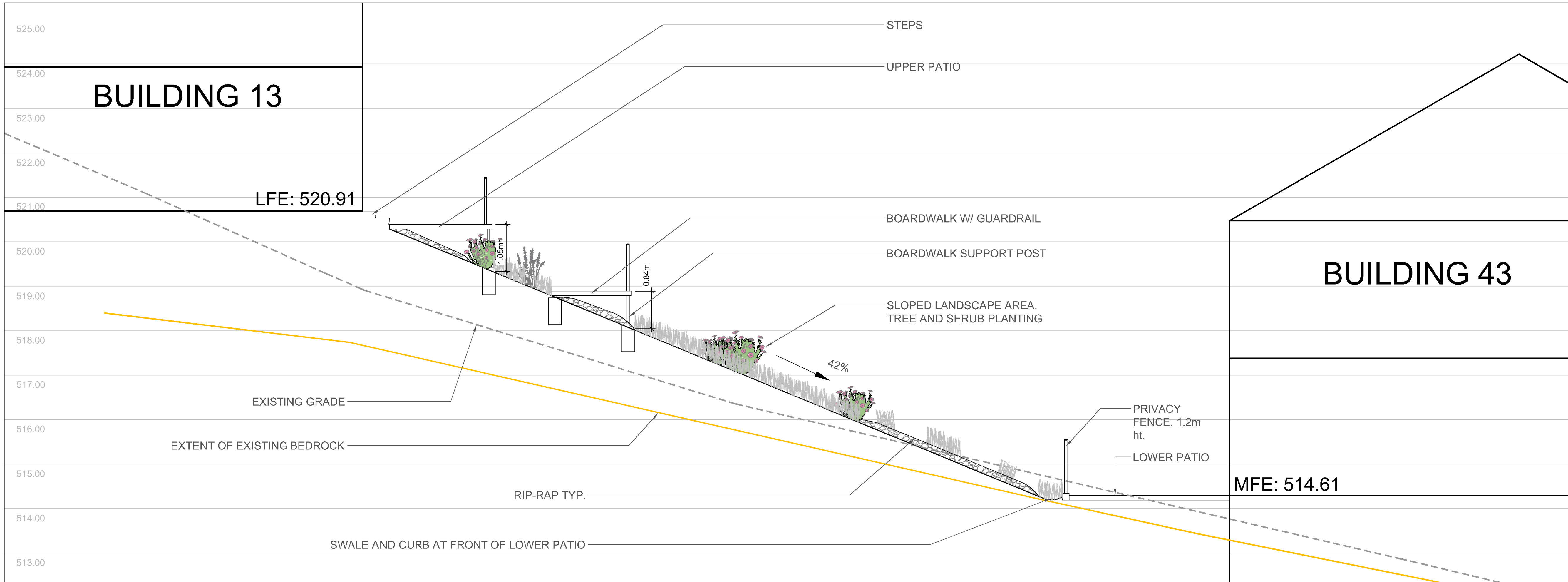
Location:
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C SECTION THROUGH UNIT 10 AND 37
Scale 1:75



D SECTION THROUGH UNIT 13 AND 43
Scale 1:50

LEGEND	
	EXISTING GRADE
	EXISTING BEDROCK

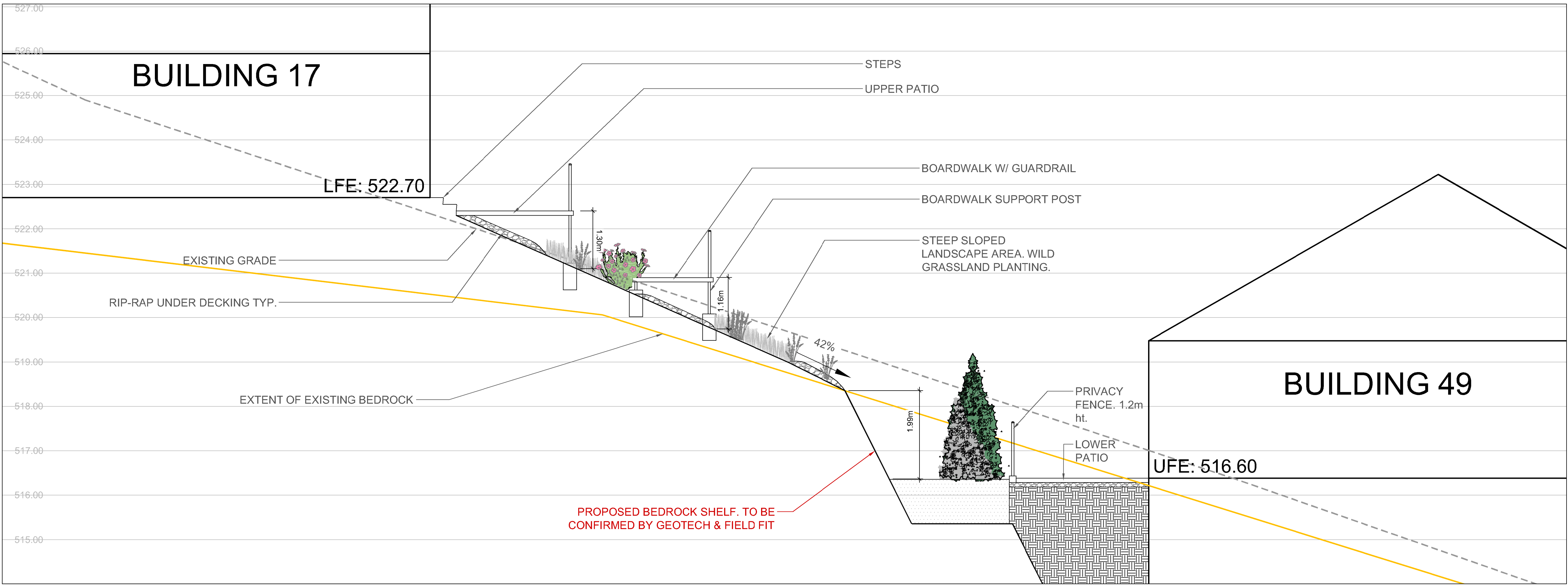
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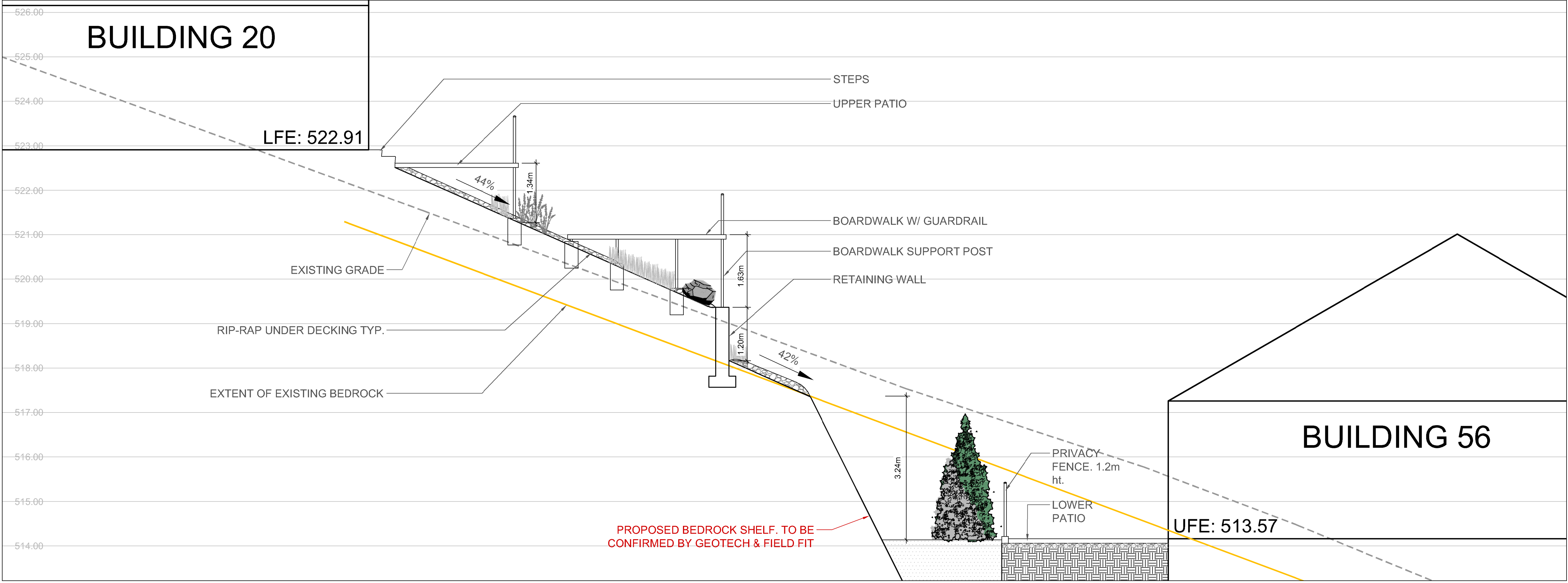
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REVISIONS TABLE FOR SHEET			

Project: PROVIDENCE / SUNDANCE	
Location: 3401 Sundance Drive West Kelowna, B.C.	

Drawn: KM	Stamp:
Checked: SH	
Approved: APPROVED	Original Sheet Size: 24"x36"
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E SECTION THROUGH UNIT 17 AND 49
Scale 1:50



F SECTION THROUGH UNIT 20 AND 56
Scale 1:50

LEGEND

---	EXISTING GRADE
---	EXISTING BEDROCK

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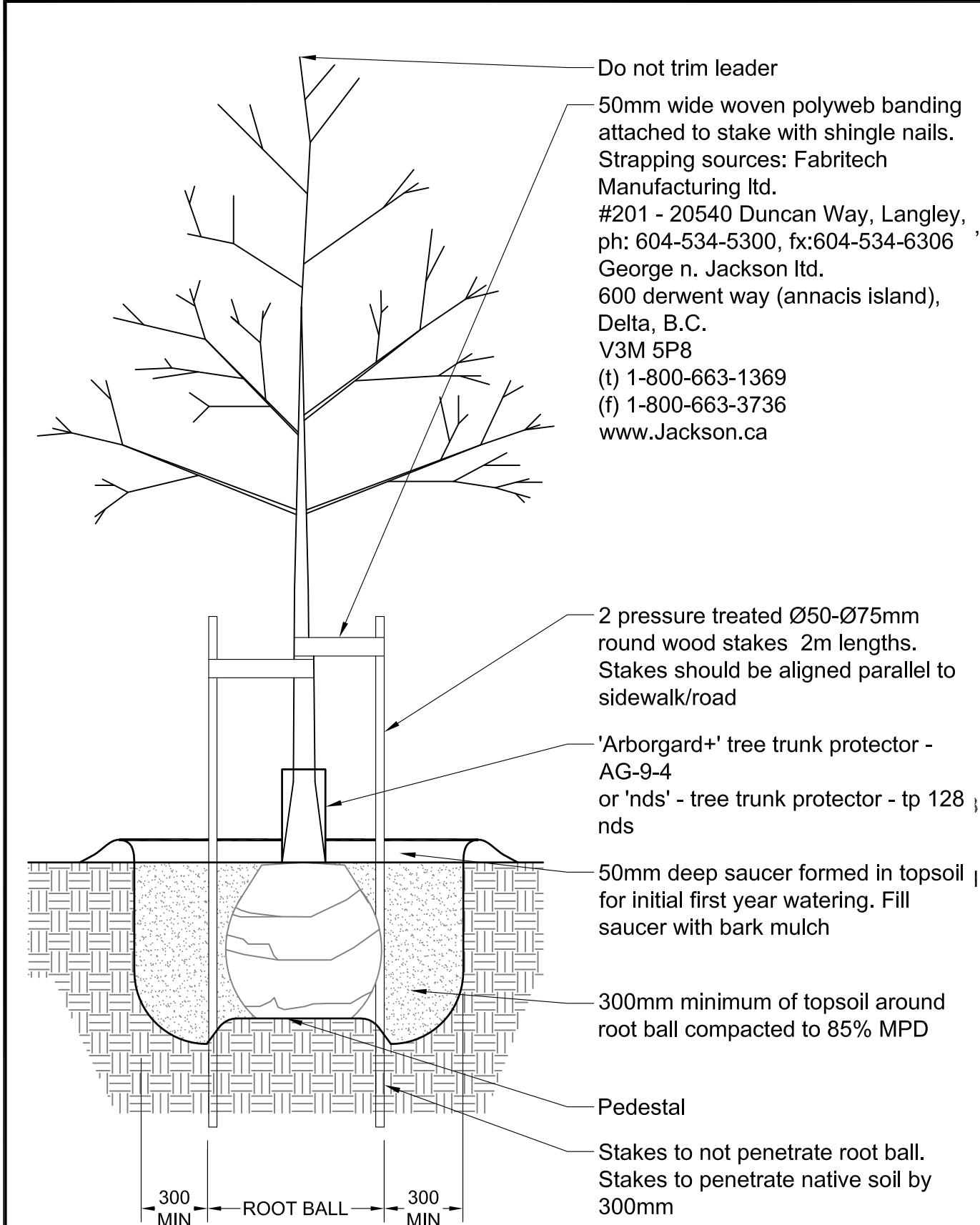
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Project:
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Do not trim leader

50mm wide woven polyweb banding attached to stake with shingle nails.
Strapping sources: Fabritech Manufacturing Ltd.
#201 - 20540 Duncan Way, Langley, ph: 604-534-5300, fx:604-534-6306
George n. Jackson Ltd.
600 derwent way (annacis island), Delta, B.C.
V3M 5P8
(t) 1-800-663-1369
(f) 1-800-663-3736
www.Jackson.ca

2 pressure treated Ø50-Ø75mm round wood stakes 2m lengths. Stakes should be aligned parallel to sidewalk/road

'Arborgard+' tree trunk protector - AG-9-4 or 'nds' - tree trunk protector - tp 128 nds

50mm deep saucer formed in topsoil for initial first year watering. Fill saucer with bark mulch

300mm minimum of topsoil around root ball compacted to 85% MPD

Pedestal

Stakes to not penetrate root ball. Stakes to penetrate native soil by 300mm

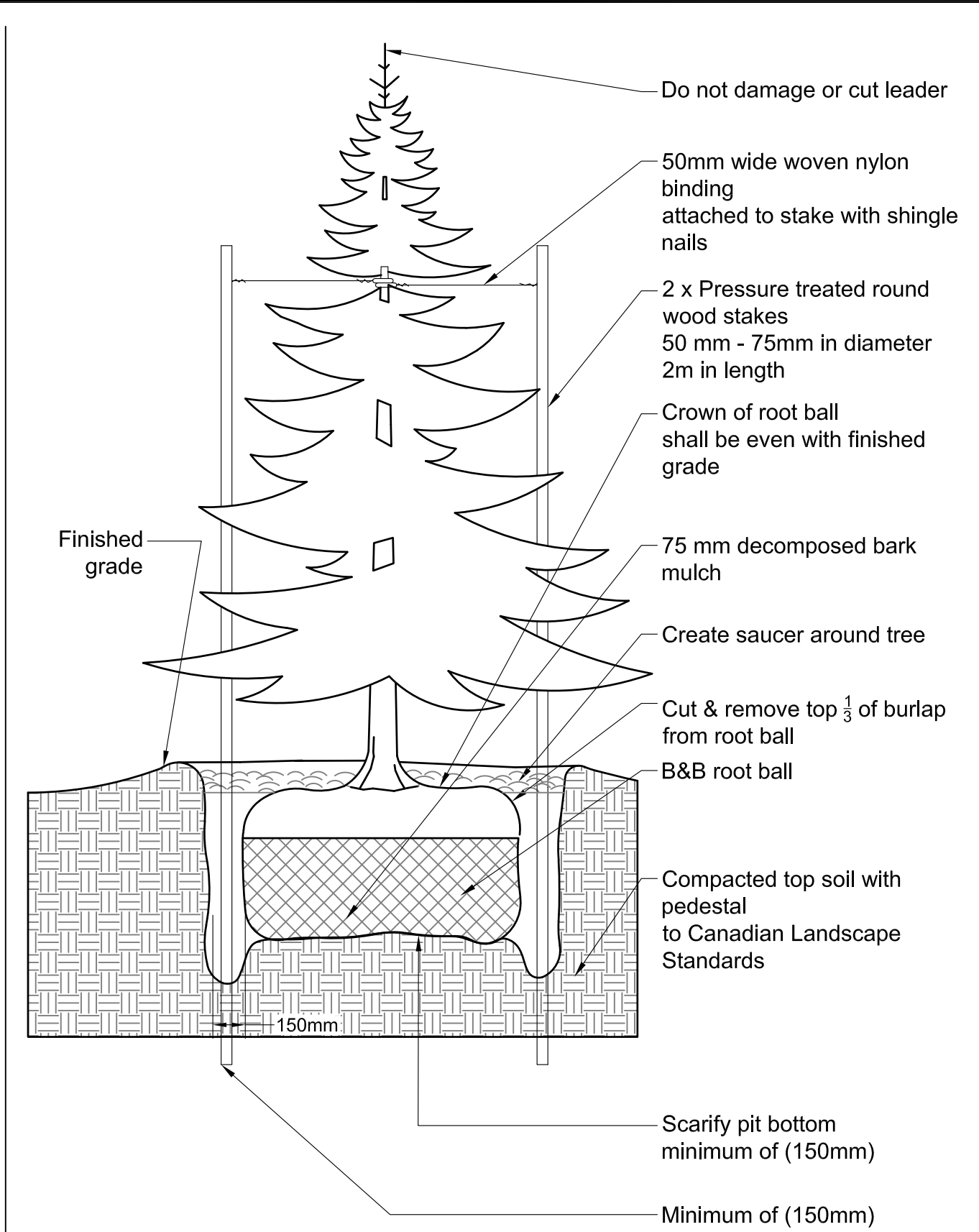
300 MIN ROOT BALL 300 MIN

Notes:

1. Sacking/burlap to be loosened and dropped to the bottom of the planting hole. all string, twine, etc.to be removed.
2. All wire baskets shall have the top 1/3 of the wire removed prior to planting.
3. All trees shall be single stem
4. 450mm depth root guard to be installed when tree planted within 2m of hardscape

1 DECIDUOUS TREE PLANTING

Scale 1:25



Do not damage or cut leader

50mm wide woven nylon binding attached to stake with shingle nails

2 x Pressure treated round wood stakes 50 mm - 75mm in diameter 2m in length

Crown of root ball shall be even with finished grade

75 mm decomposed bark mulch

Create saucer around tree

Cut & remove top 1/3 of burlap from root ball

B&B root ball

Compacted top soil with pedestal to Canadian Landscape Standards

Scarify pit bottom minimum of (150mm)

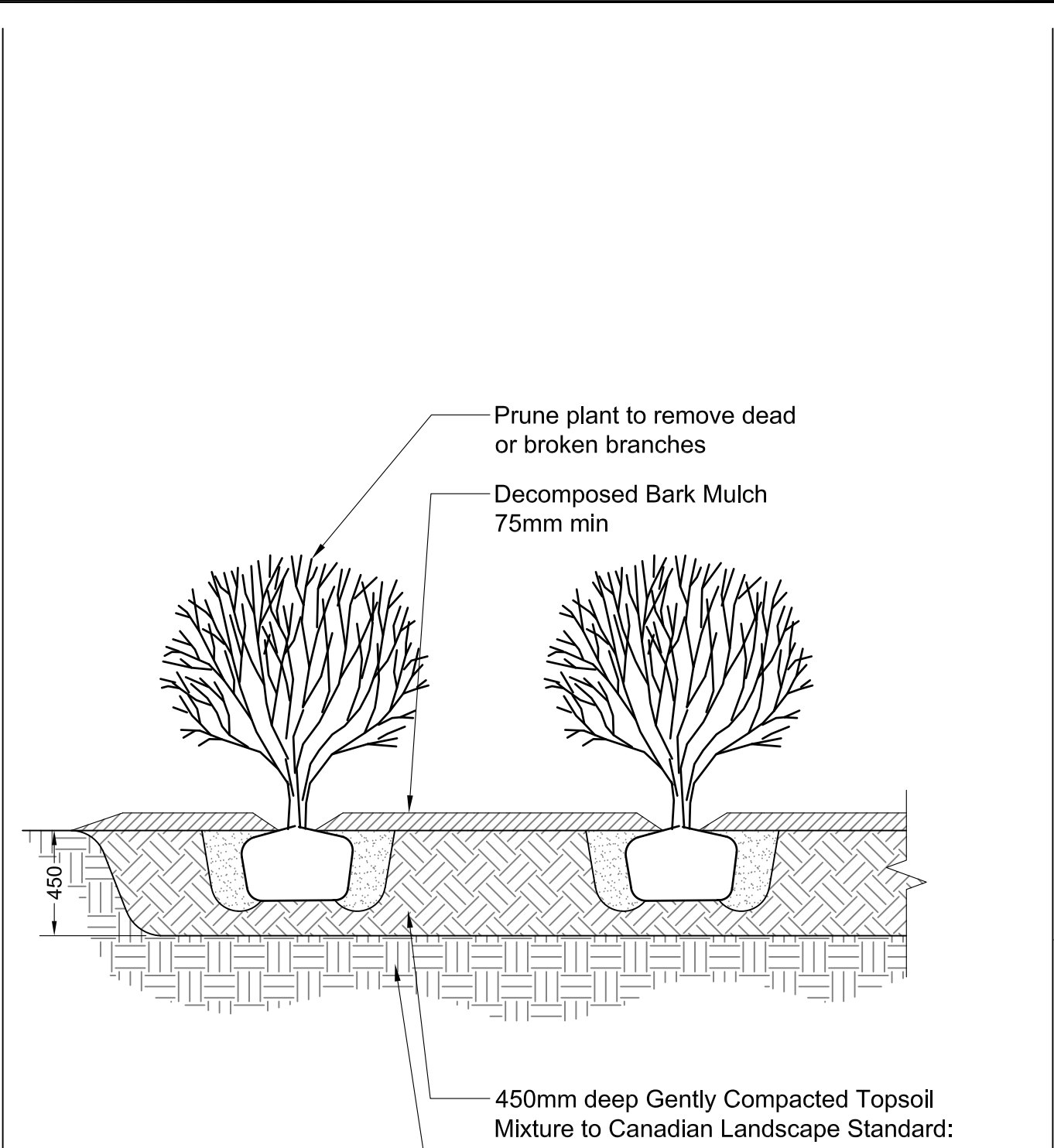
Minimum of (150mm) on all sides of root ball

Notes:

1. Do not damage main roots or destroy root ball when installing tree stake.
2. Water thoroughly after installation
3. Remove tree rings and stakes two years after installations
4. Provide drainage for planting pit in impermeable soil
5. 450mm depth root guard to be installed when tree planted within 2m of hardscape

2 CONIFEROUS TREE PLANTING

Scale 1:25



Prune plant to remove dead or broken branches

Decomposed Bark Mulch 75mm min

450mm deep Gently Compacted Topsoil Mixture to Canadian Landscape Standard: "Well Groomed" soil

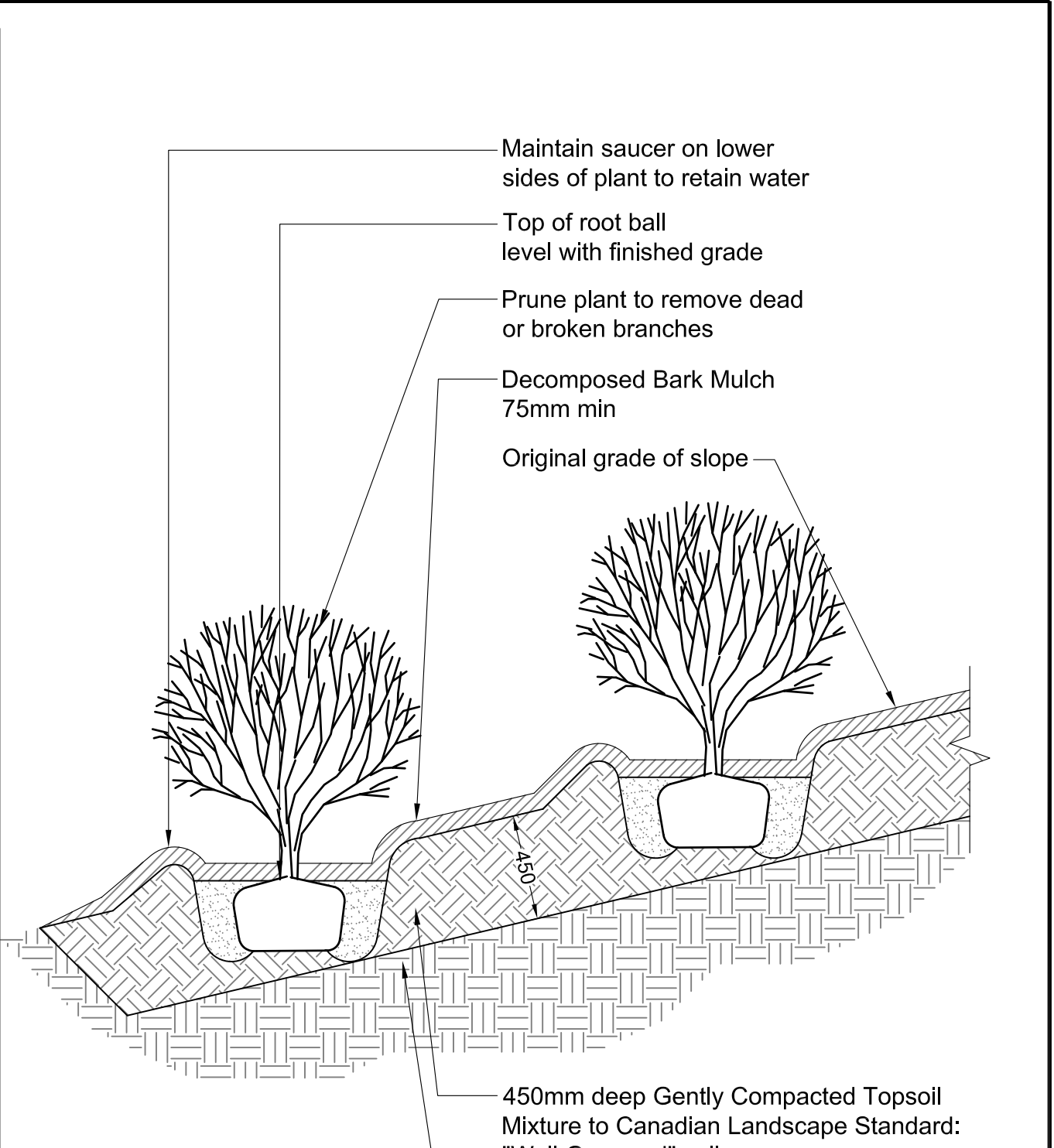
Compacted sub base

Notes:

1. Remove all string, twine, pots, tags from plant.
2. All soil, mulch and plant material to meet latest Canadian Landscape Standard.
3. Topsoil saucers diameter to be 1.5X diameter of plant container.
4. O.C. spacing per planting plan.

3 SHRUB PLANTING

Scale 1:25



Maintain saucer on lower sides of plant to retain water

Top of root ball level with finished grade

Prune plant to remove dead or broken branches

Decomposed Bark Mulch 75mm min

Original grade of slope

450mm deep Gently Compacted Topsoil Mixture to Canadian Landscape Standard: "Well Groomed" soil

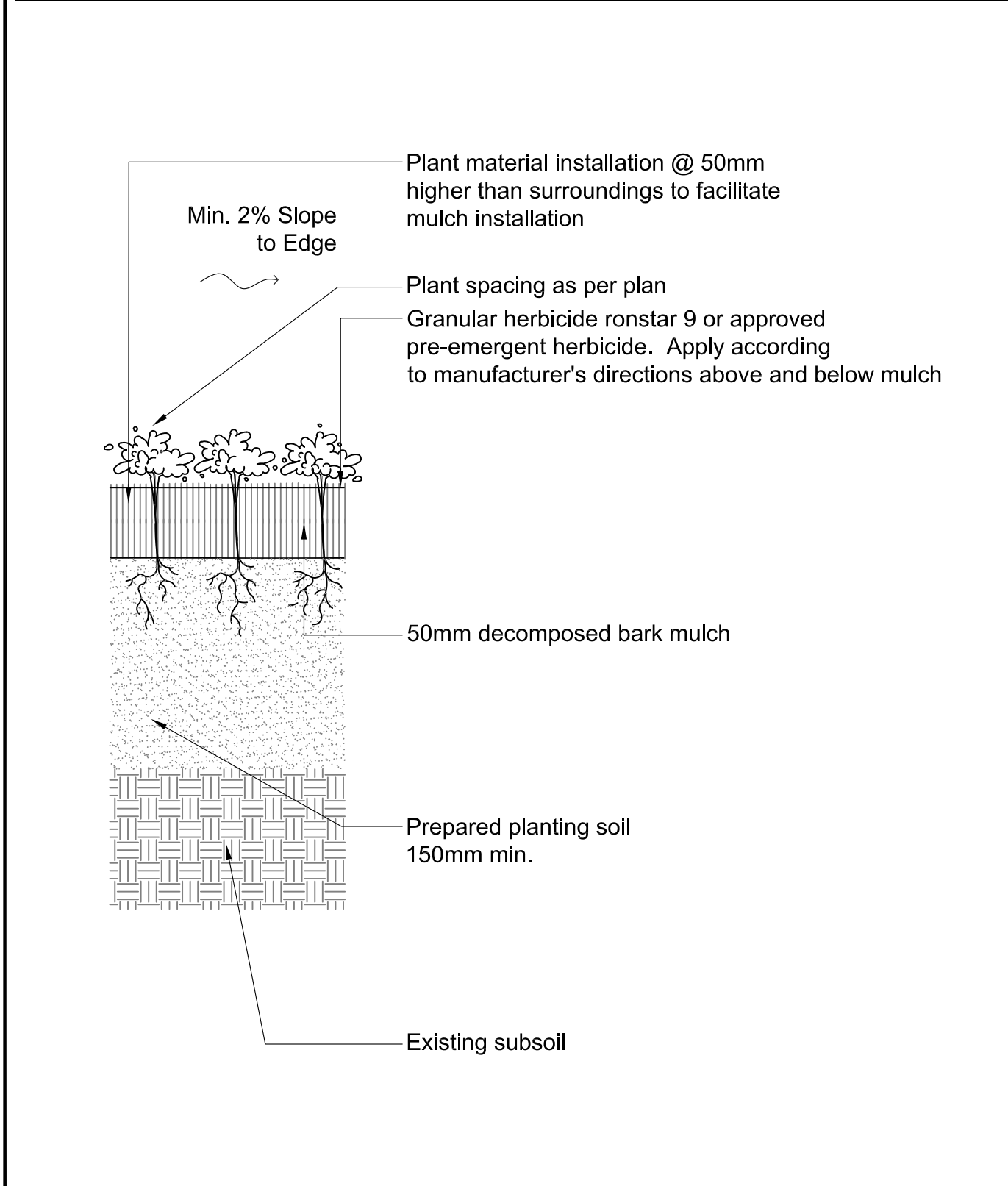
Compacted sub base

Notes:

1. Remove all string, twine, pots, tags from plant.
2. All soil, mulch and plant material to meet latest Canadian Landscape Standard.
3. Topsoil saucers diameter to be 1.5X diameter of plant container.
4. O.C. spacing per planting plan.

4 SHRUB PLANTING ON SLOPE

Scale 1:25



Plant material installation @ 50mm higher than surroundings to facilitate mulch installation

Min. 2% Slope to Edge

Plant spacing as per plan

Granular herbicide ronstar 9 or approved pre-emergent herbicide. Apply according to manufacturer's directions above and below mulch

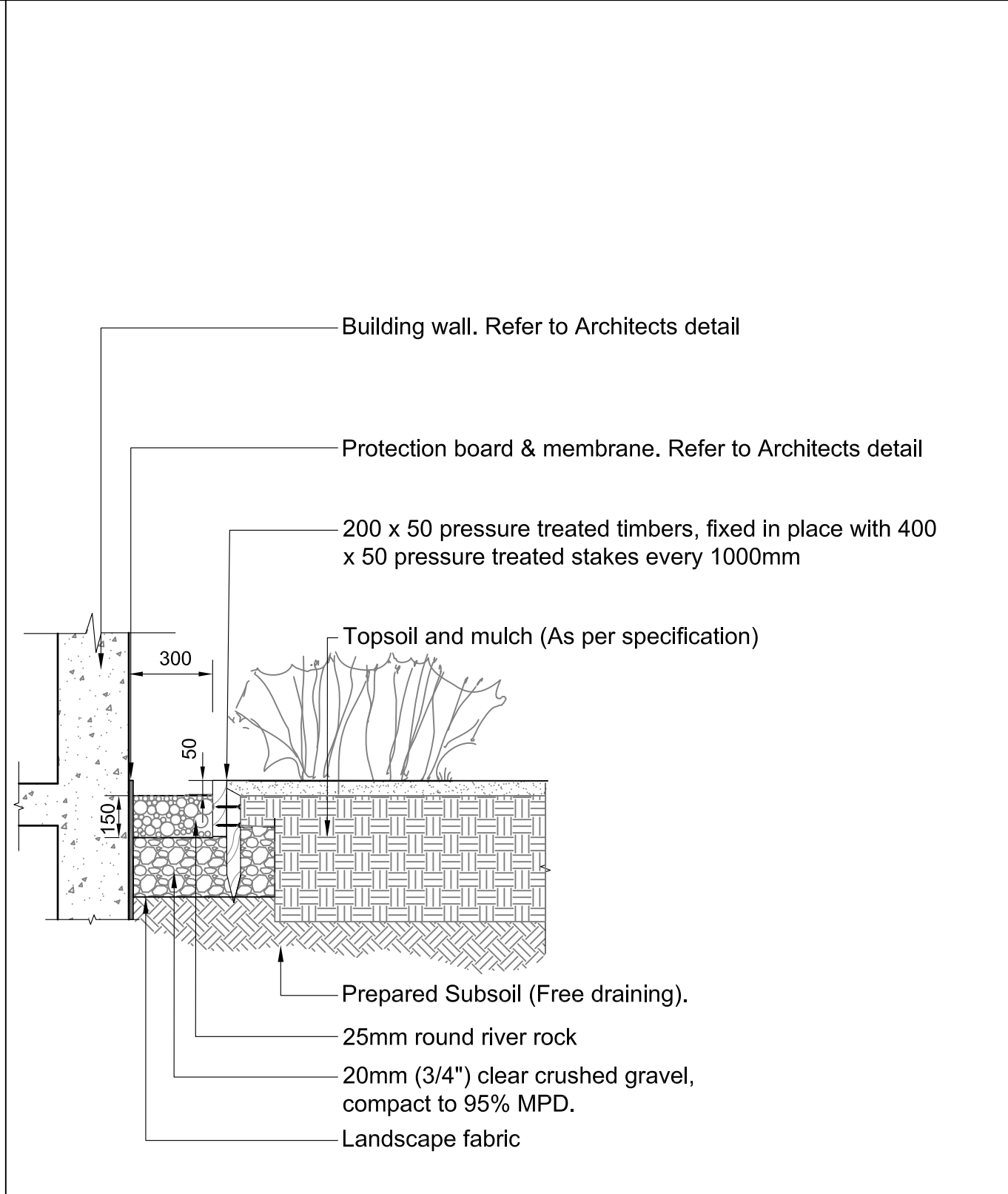
50mm decomposed bark mulch

Prepared planting soil 150mm min.

Existing subsoil

5 GROUND COVER PLANTING

Scale 1:10



Building wall. Refer to Architects detail

Protection board & membrane. Refer to Architects detail

200 x 50 pressure treated timbers, fixed in place with 400 x 50 pressure treated stakes every 1000mm

Topsoil and mulch (As per specification)

Prepared Subsoil (Free draining).

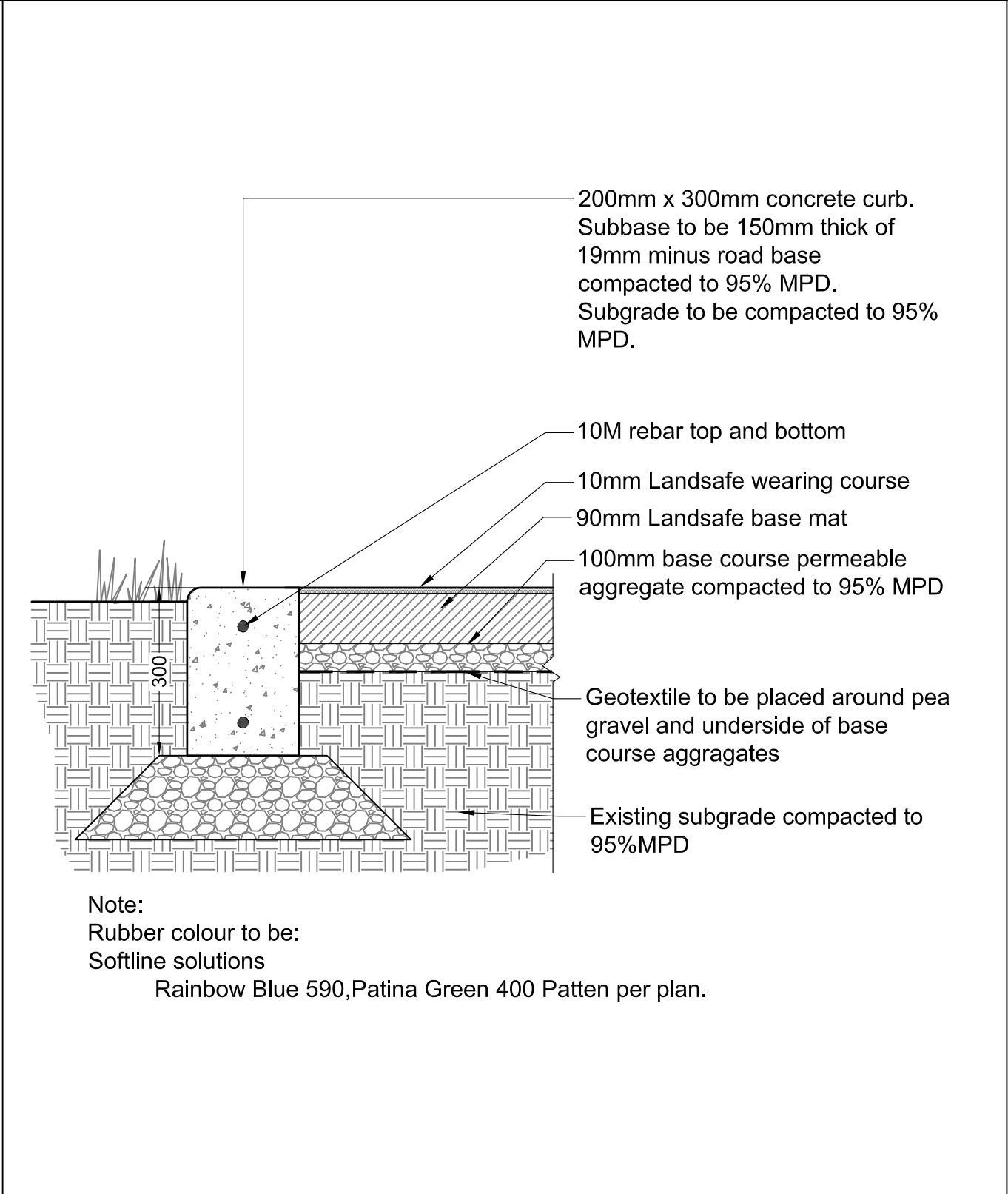
25mm round river rock

20mm (3/4") clear crushed gravel, compact to 95% MPD.

Landscape fabric

6 DRIP STRIP

Scale 1:20



200mm x 300mm concrete curb. Subbase to be 150mm thick of 19mm minus road base compacted to 95% MPD. Subgrade to be compacted to 95% MPD.

10M rebar top and bottom

10mm Landsafe wearing course

90mm Landsafe base mat

100mm base course permeable aggregate compacted to 95% MPD

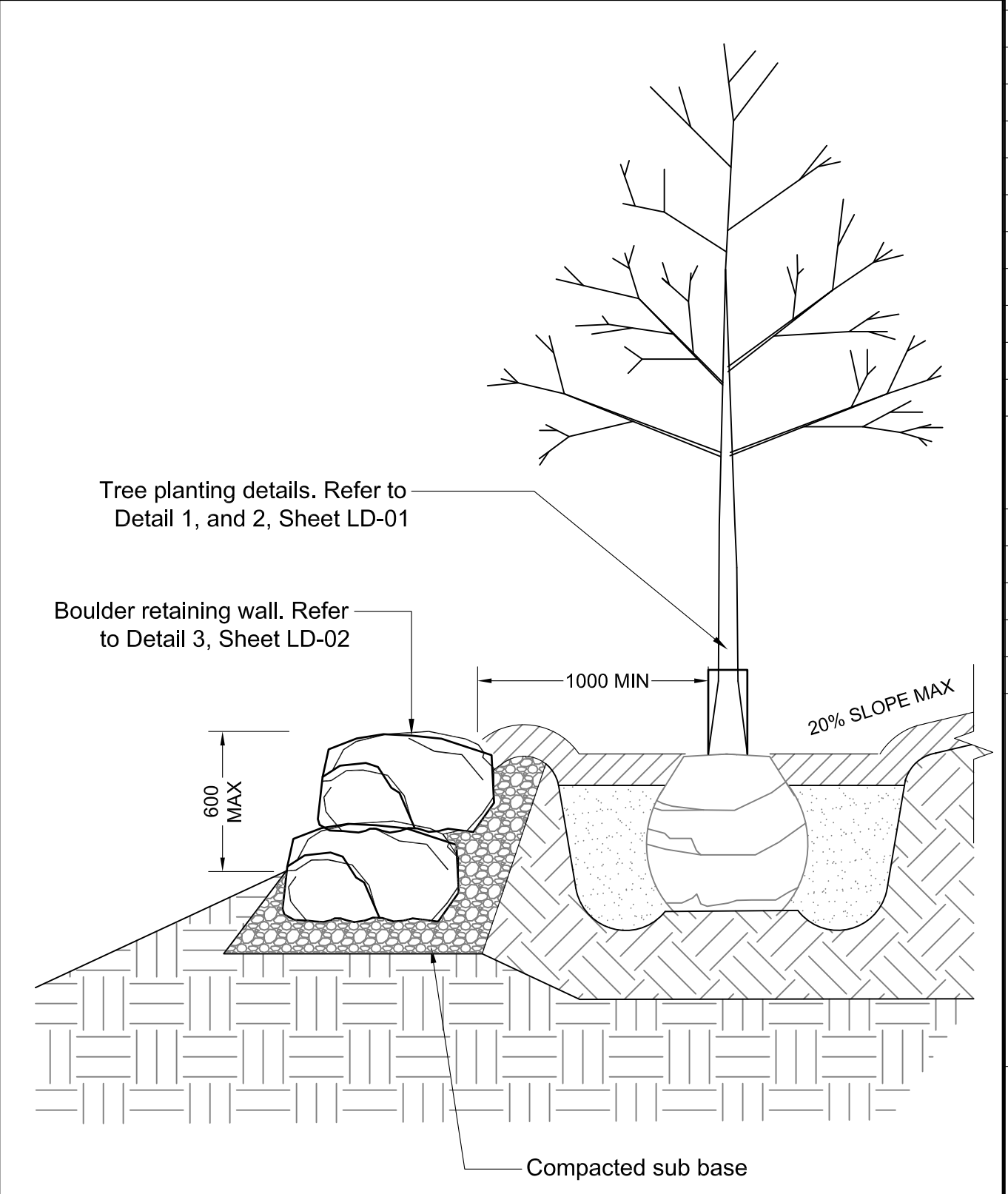
Geotextile to be placed around pea gravel and underside of base course aggregates

Existing subgrade compacted to 95%MPD

Note:
Rubber colour to be:
Softline solutions
Rainbow Blue 590,Patina Green 400 Patten per plan.

7 RUBBER SAFETY SURFACE

Scale 1:10



Tree planting details. Refer to Detail 1, and 2, Sheet LD-01

Boulder retaining wall. Refer to Detail 3, Sheet LD-02

1000 MIN

20% SLOPE MAX

600 MAX


Compacted sub base

Notes:

1. Refer to plans for extents of shrub planting build-up
2. To be used in interior west-east corridor of site where soil build-up and slope leveling is required

8 PLANTING BEDS ON SLOPE

Scale 1:25

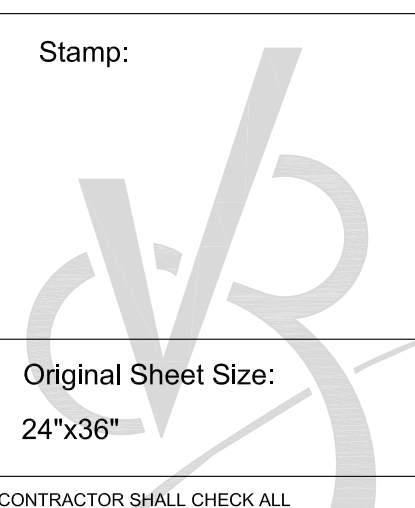


LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

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100-9181 Church St Fort Langley, BC V1M 2B8 102-355 Kingsway Vancouver, BC V1M 3J7

www.vdz.ca 604-882-0024

8	KM	Issued for Development Permit	2023-04-18
7	KM	Issued for Development Permit	2023-02-24
6	KM	Re-Issued for Coordination	2023-02-17
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REVISIONS TABLE FOR SHEET			
Project: PROVIDENCE / SUNDANCE			
Location: 3401 Sundance Drive West Kelowna, B.C.			
Drawn:	Stamp:		
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APPROVED	24"x36"		
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Drawing Title:

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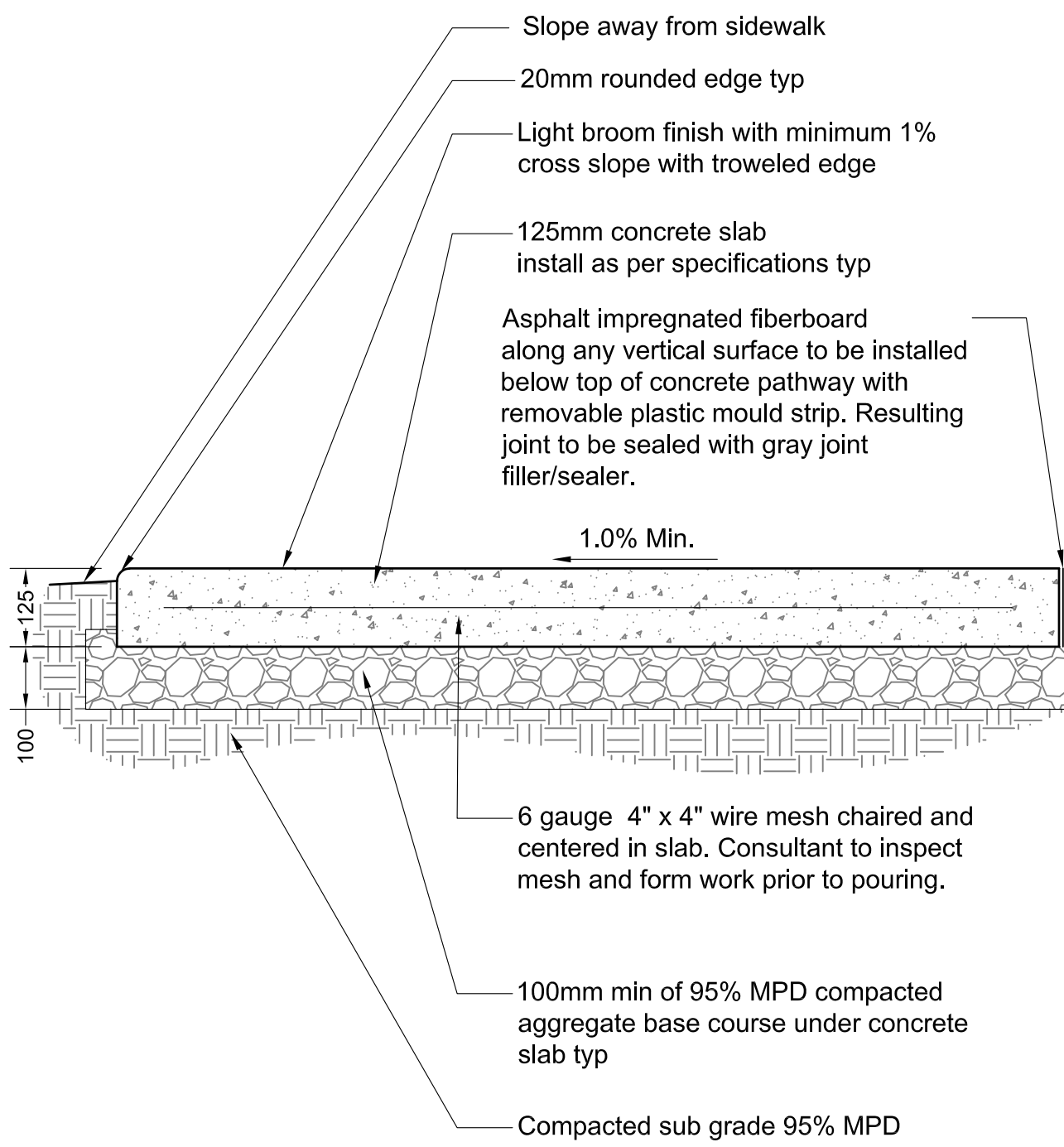
Drawing #:

LD-01

VDZ Project #:

DP2022-16

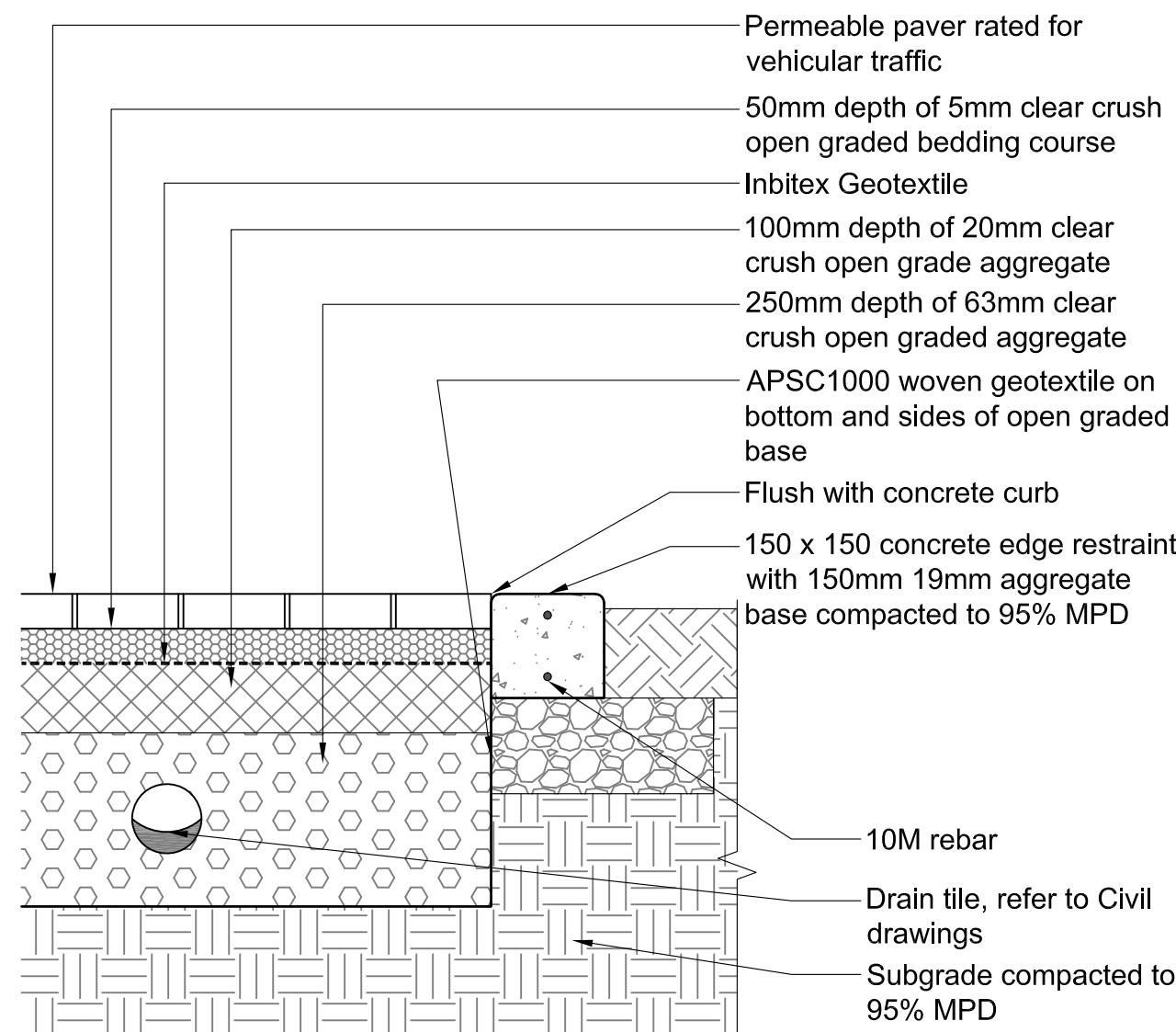
Z:\PROJECTS\DEVELOPMENT PERMIT\ACTIVEDP2022-16 GATEHOUSE SUNDANCE DEVELOPMENT\5. DWG\SHEETS\LD-01 DETAILS.DWG



- Notes:
- Contractor to provide expansion joints where concrete meets all vertical structures.
 - Horizontal scorelines at 1500mm o.c. both ways unless shown otherwise. Refer to plans.
 - Center scoreline on 1500mm smooth finish or to match existing concrete pathway.
 - All components as specified or approved alternate.
 - Concrete to slope to drain.
 - Expansion joint every 3000mm o.c. both ways.
 - Contractor to contact Landscape Architect for scoreline approval prior to cutting.

1 CONCRETE PAVING

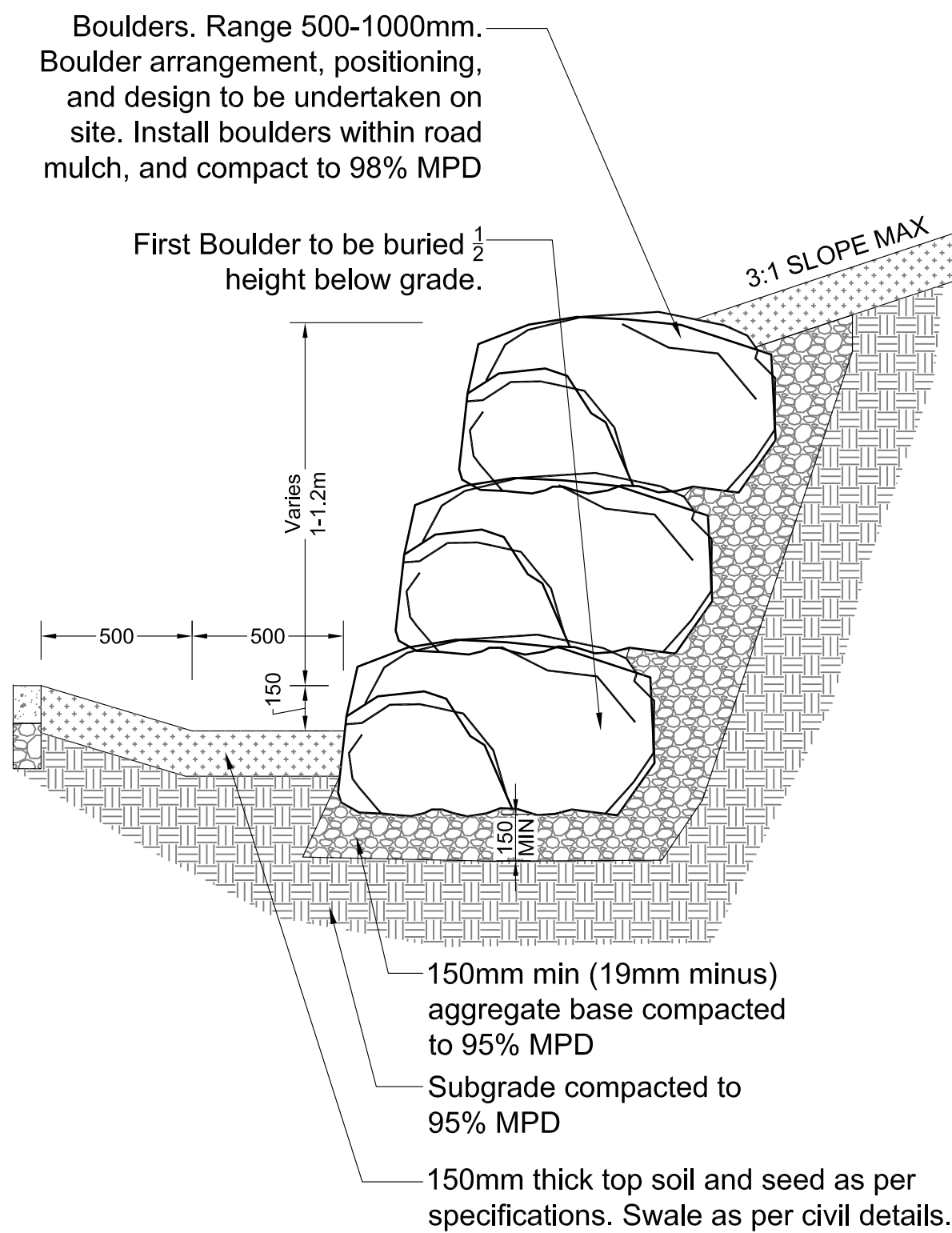
Scale 1:10



- Notes:
- Install all components as per manufacturer's specifications.
 - Permeable Mission Paver - by Basalite
 - Colour: Charcoal
 - Herringbone pattern.
 - Concrete curb control joint every 3m.

2 VEHICULAR PAVING

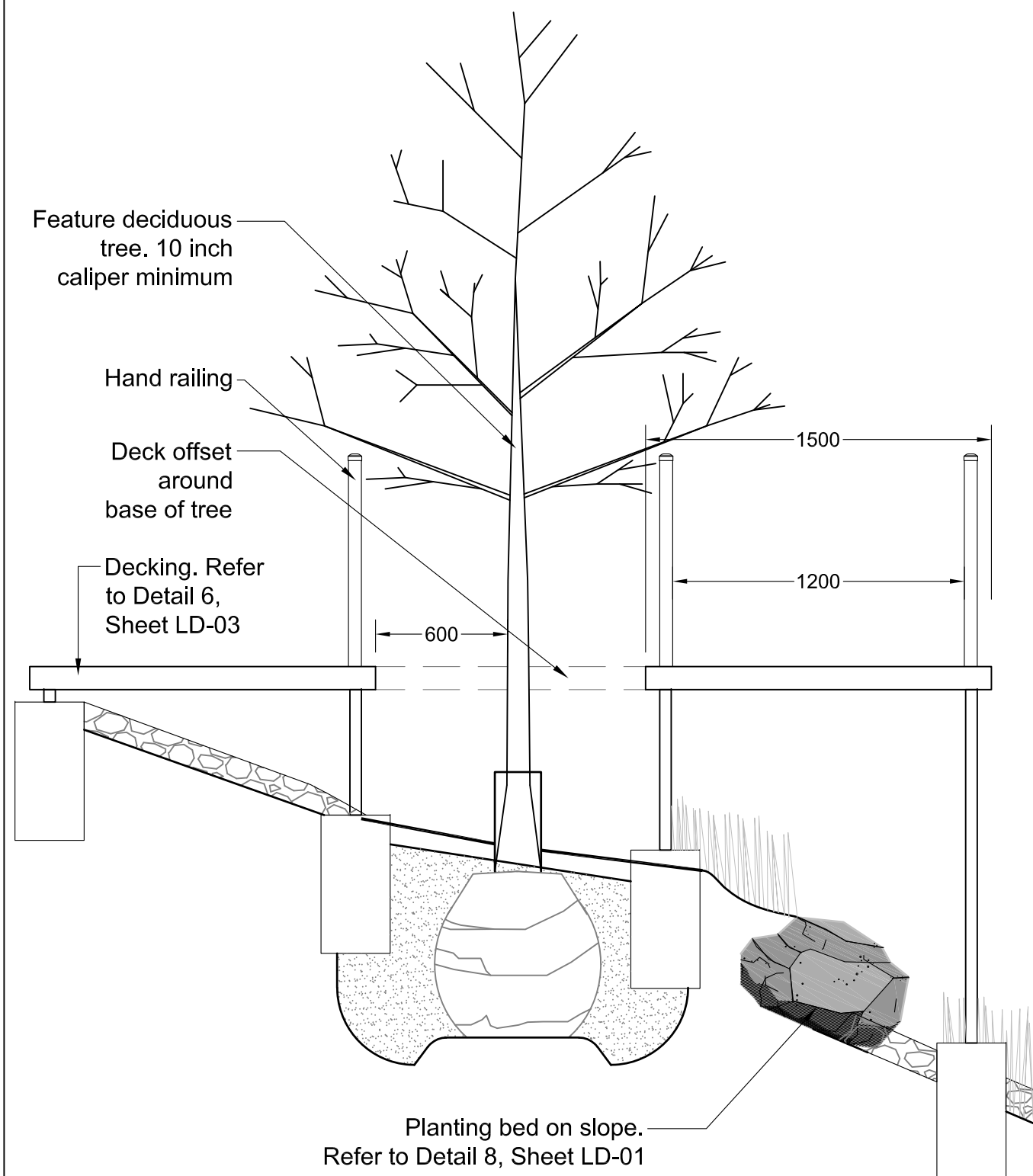
Scale 1:10



- NOTE:
- All walls over 1.2m in height will need to be Structurally Engineered.
 - All planters to be connected to Slab drainage system.

3 BOULDER RETAINING WALL

Scale 1:20



4 BOARDWALK TREE WELL

Scale 1:25



- Supplier: TBD
Product: Hill climbing hand holds
Age Group: 4+
or approved equal
- Supplier: KOMPAN
Product: EMBANKMENT SLIDE- PCM110121
Dimensions: 463 x 161 x 110 cm
Age Group: 4+
Play Capacity: 3
or approved equal

5 ADVENTURE CLIMBING HILL

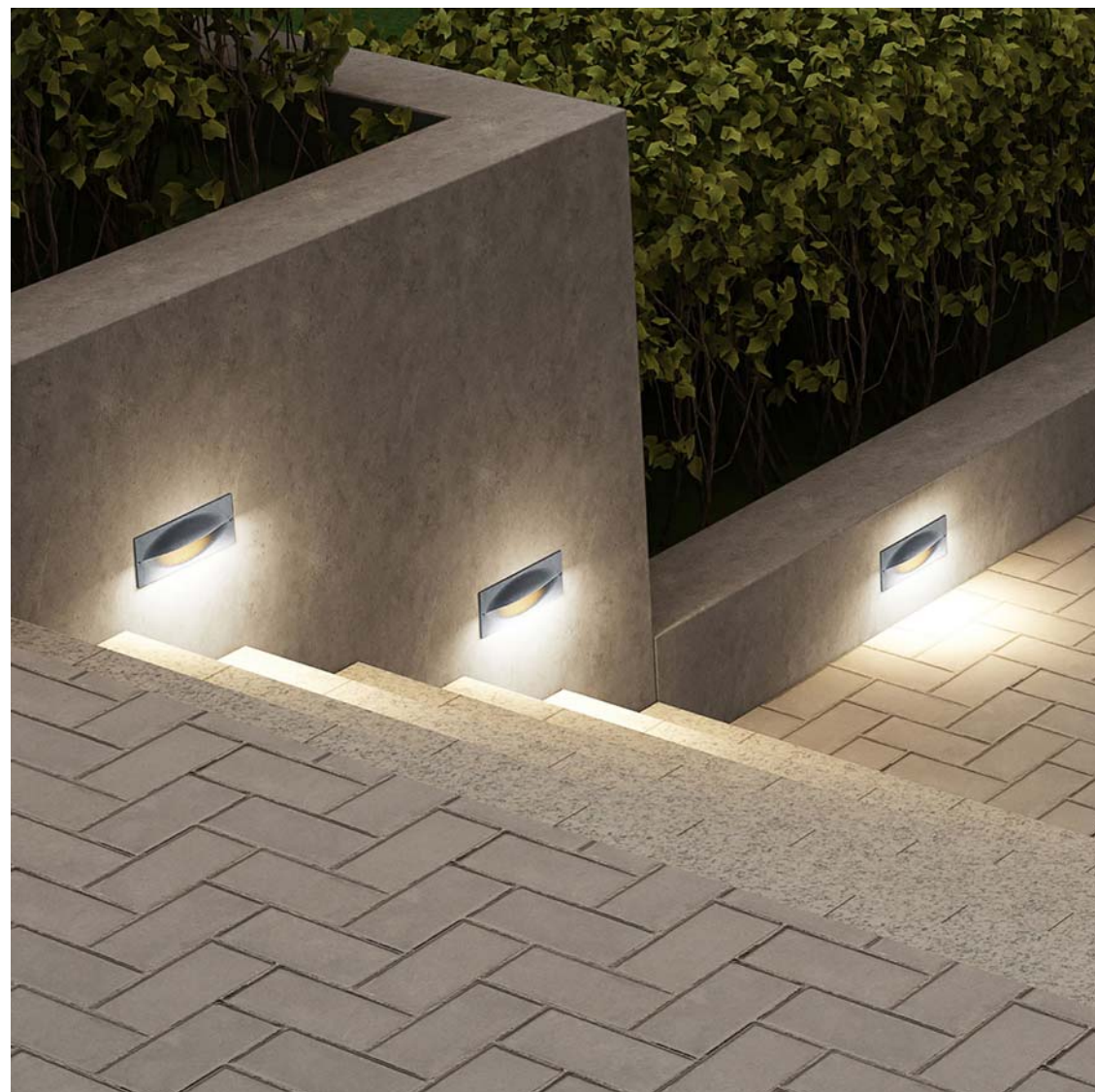
NTS



- Lighting bollard
- Finish: brushed stainless steel or oxidized steel
Mount: surface mounted to deck
- Product and supplier: TBD

6 LIGHTING BOLLARD

NTS



- Supplier: ERCO Lighting
Product: Lightmark - Recessed Wall/Step Light

Or Approved Alternative

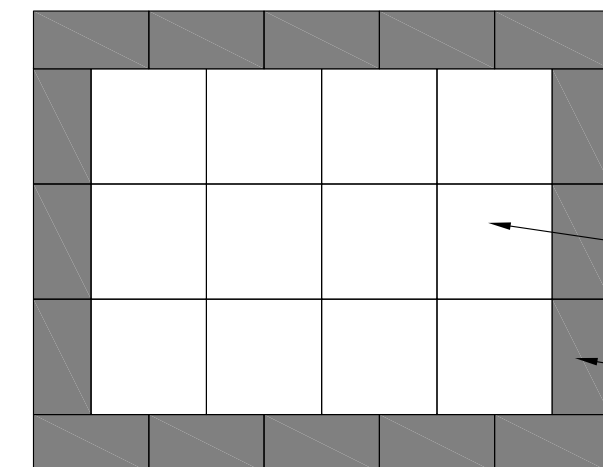
- Finish: Silver
Mount: Surface Mount

Contact: 1-877-372-6584

- Notes:
- Lighting through common amenity areas

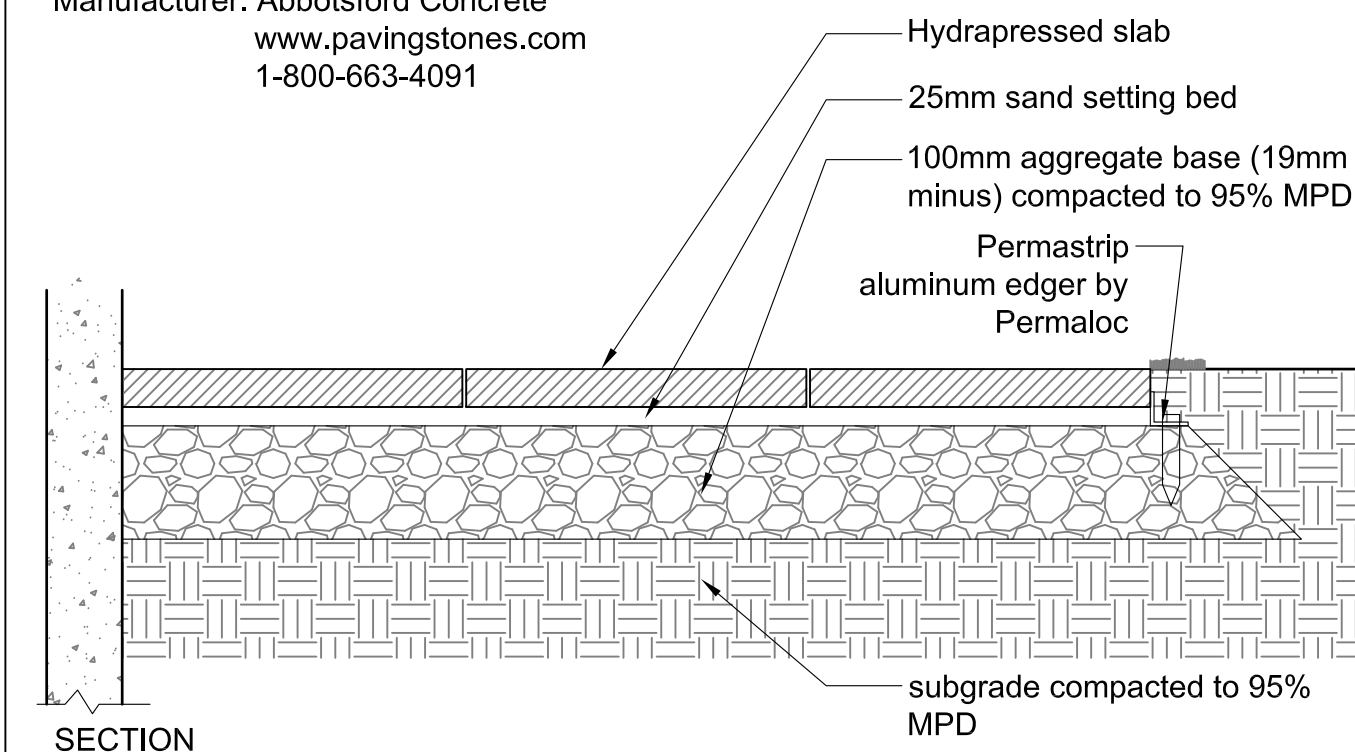
7 WALL / STEP LIGHT

NTS



PLAN
Scale: 1:40

Unit Paver Details:
Type: Cortez HydraPressed Slabs
Size: L610 X W610 X T50mm min.
Colours: Natural, Charcoal
Manufacturer: Abbotsford Concrete
www.pavingstones.com
1-800-663-4091



SECTION

8 HYDRAPRESSED SLABS

1:10

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REVISIONS TABLE FOR SHEET

Project:
PROVIDENCE / SUNDANCE

Location:
3401 Sundance Drive
West Kelowna, B.C.

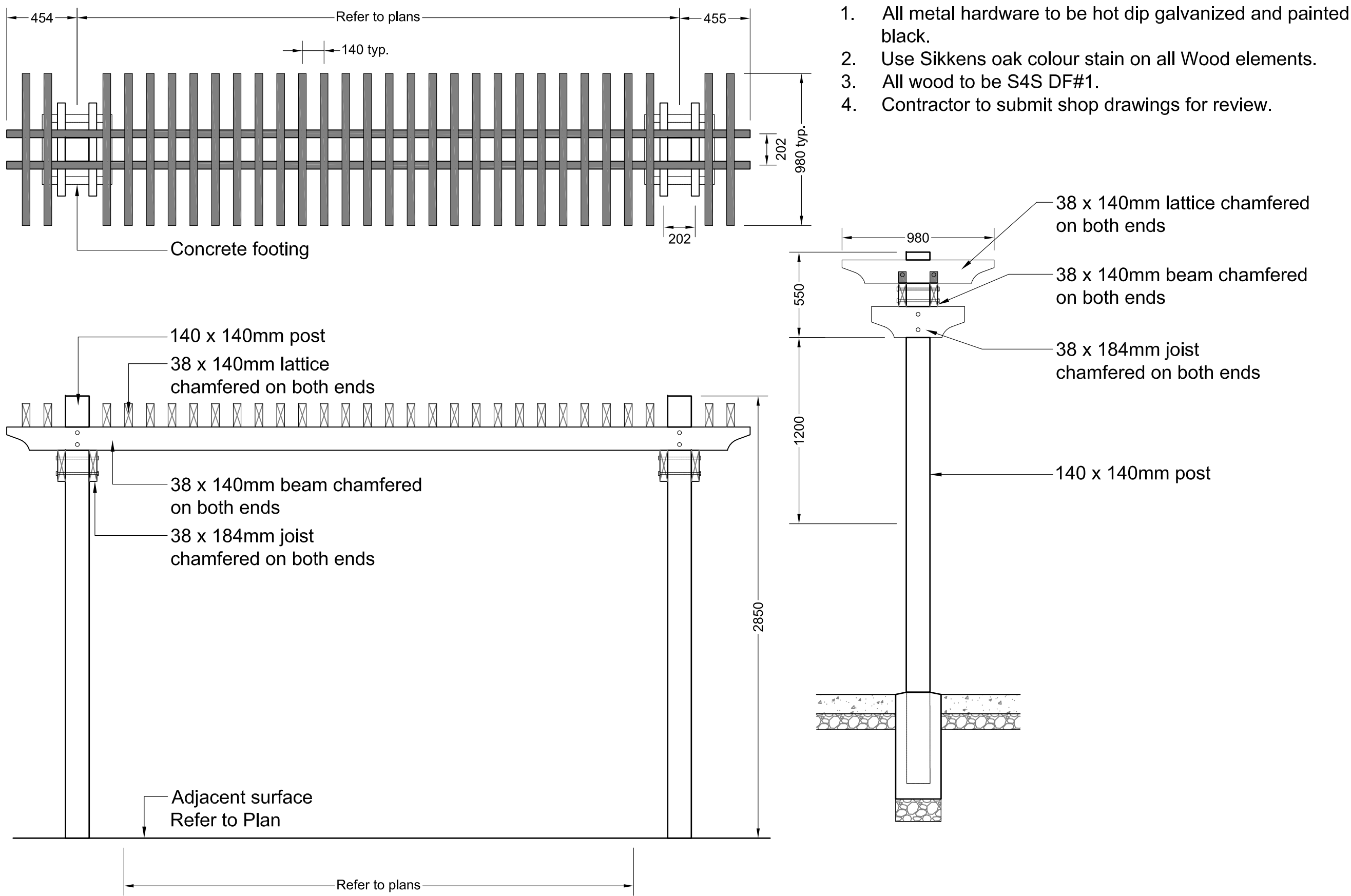
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Drawing Title:
DETAILS

VDZ Project #:
DP2022-16

Drawing #:
LD-02

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1 LINEAR TRELLIS
Scale 1:25

- NOTES:
1. Install to manufacturers specifications.
 2. Or approved substitute.



Picnic Table
Model Number: MLPT400-S-W
Size: 749mm x 1581mm x 445mm
Options: Surface Mount
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approved equal

3 PICNIC TABLE
N.T.S



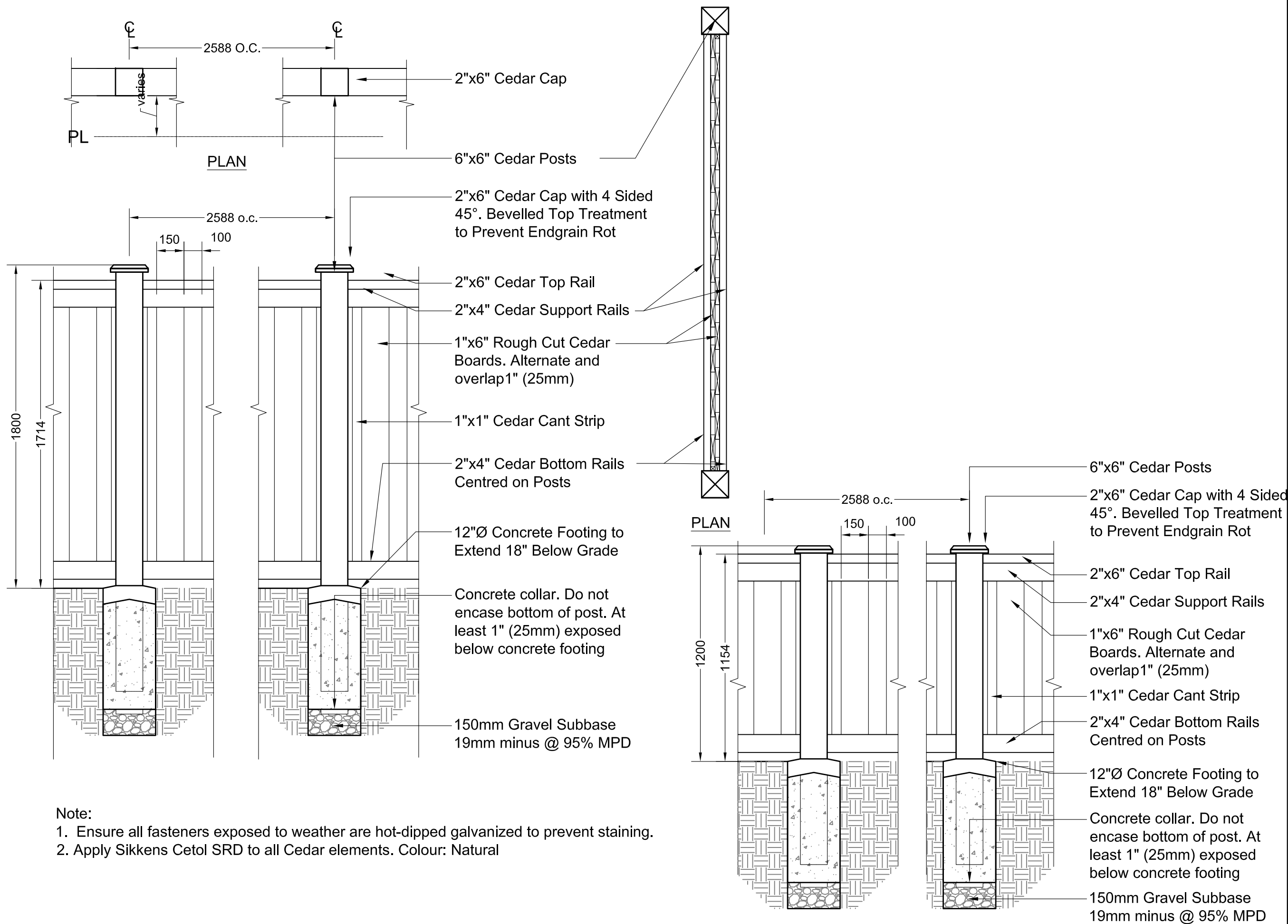
Bar Counter

Material: Polished concrete top

Chairs

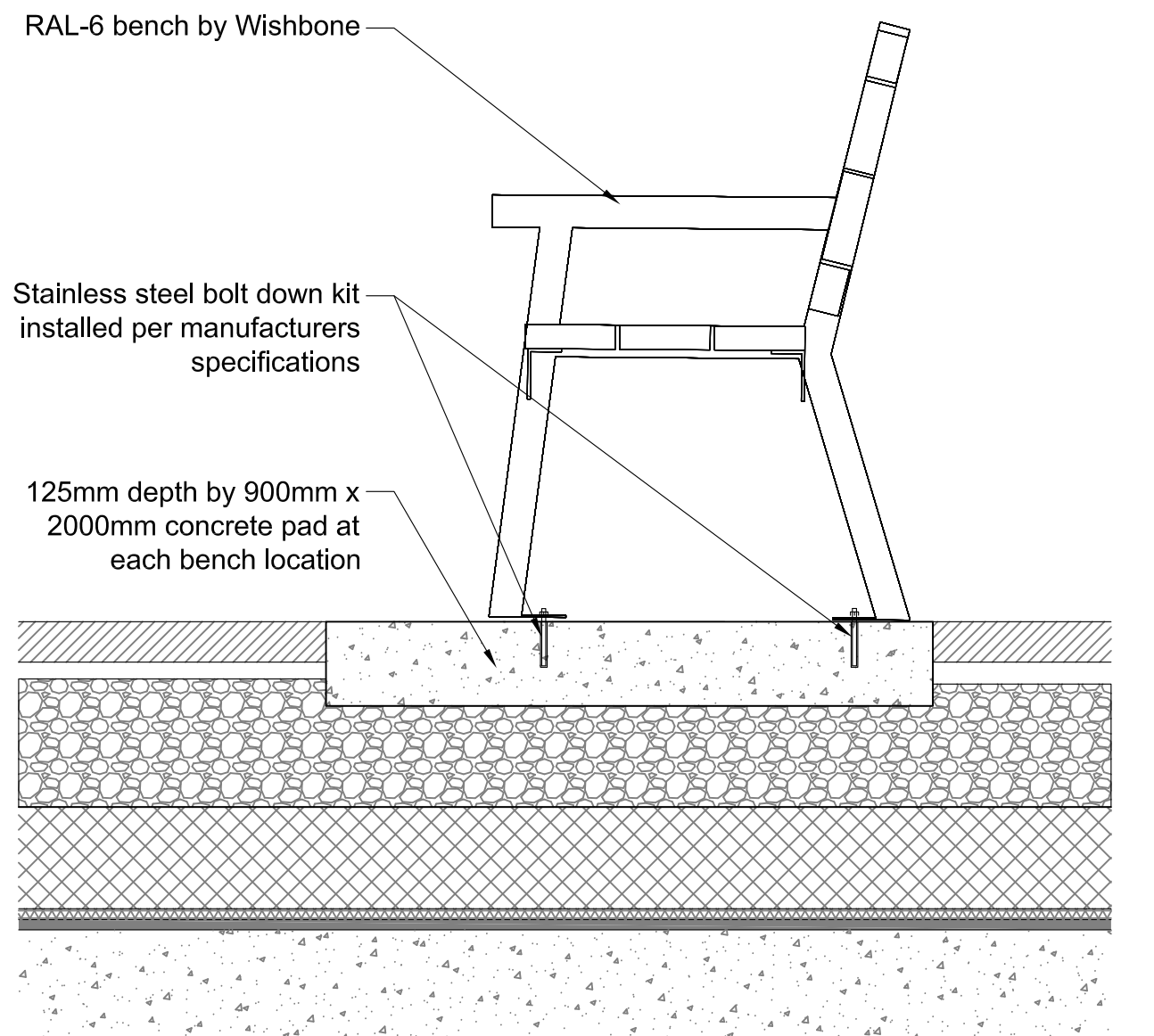
Details: Bar stool height
Material: Metal

4 BAR TABLE
N.T.S



2 CEDAR FENCE
Scale 1:20

- NOTES:
1. Install bench to manufacturers specifications.
 2. Concrete pad to be included at all bench locations.
 3. Size of concrete pad as noted on details unless shown otherwise on plans.
 4. Or approved substitute.
 5. Refer to concrete paving detail for additional information on concrete pad.



Bench
Model Number: MLB870-W
Size: 1765mm
Options: Center Arm
Wood Colour: IPE Wood
Metal Colour: Gloss Silver 14

Manufacturer: Maglin
maglin.com
Note: or approved equal



5 BENCH
N.T.S

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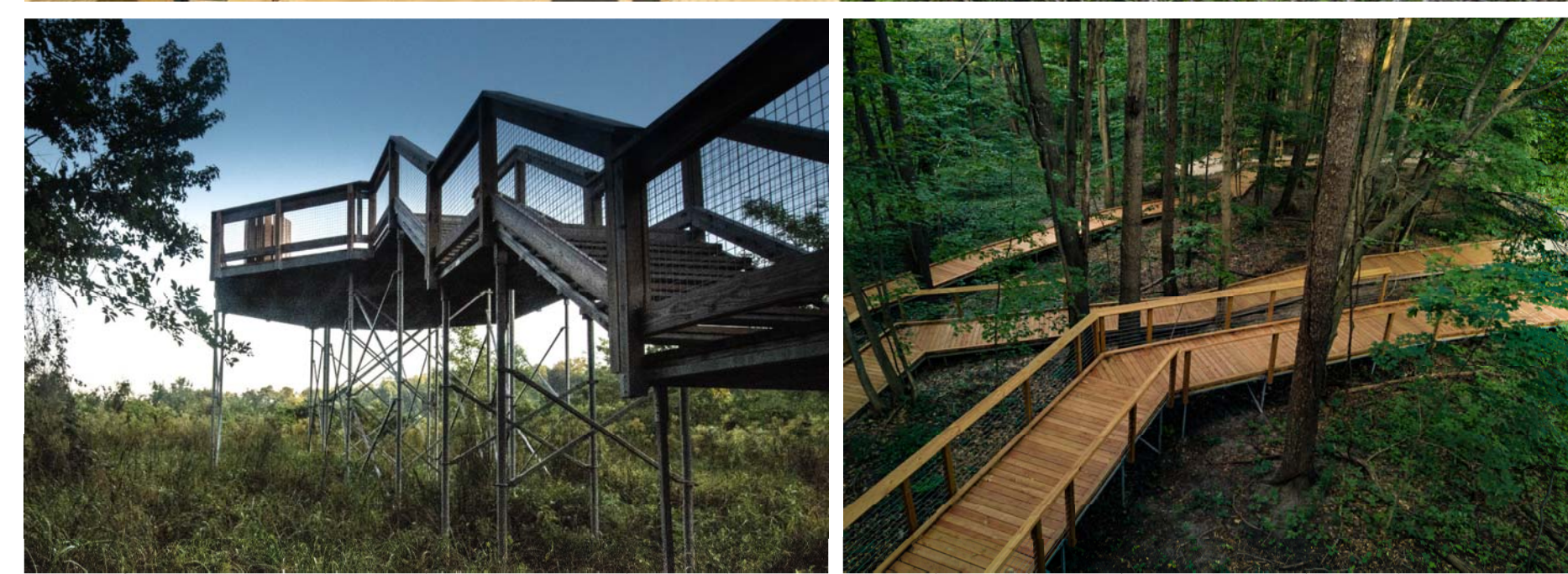
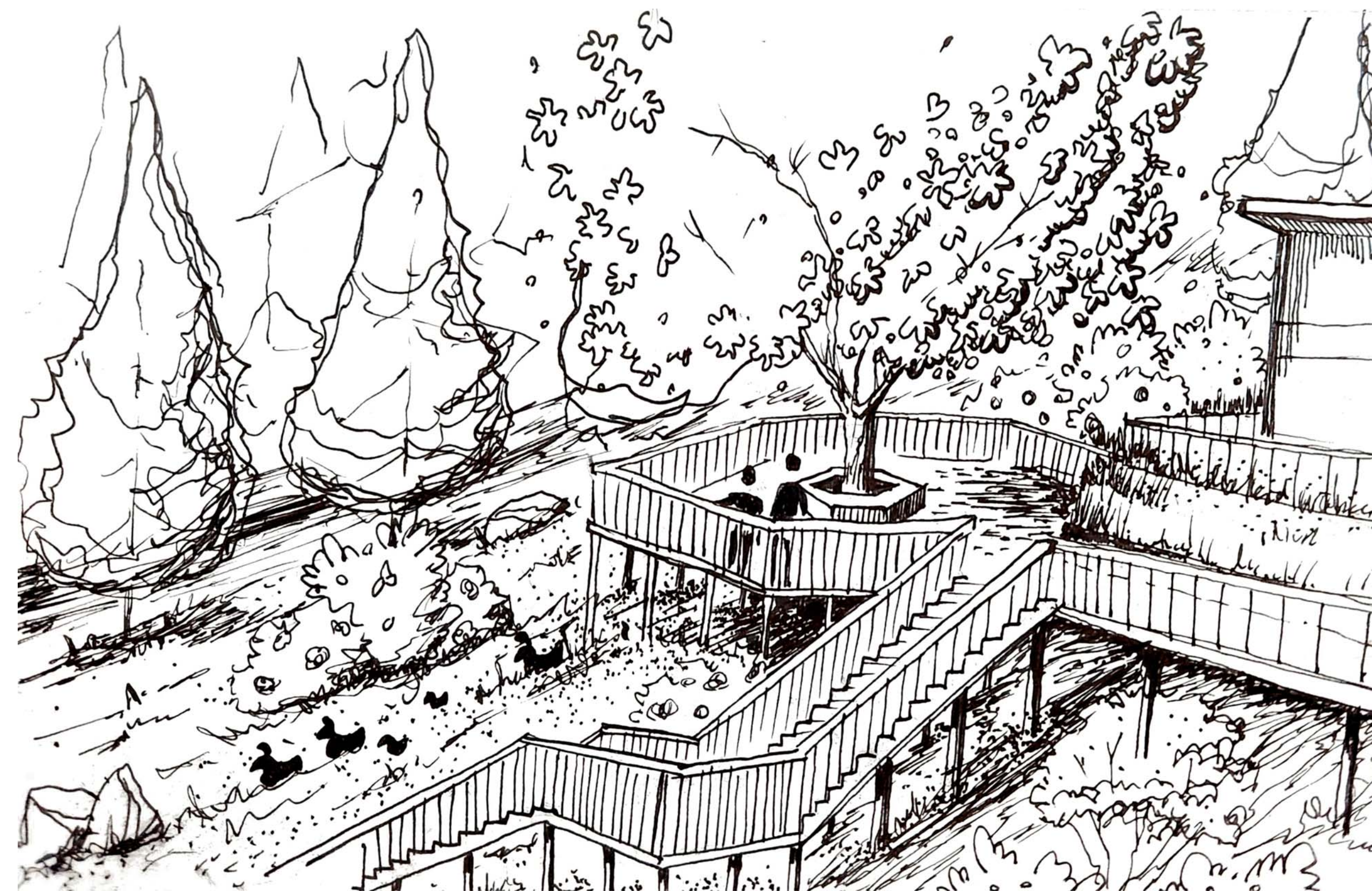
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Notes:

1. Contractor to provide shop drawings for Landscape Architect for review and approval.
2. Refer to grading plan for boardwalk/deck heights.

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Drawing #:
LD-04

GATEHOUSE

The City of West Kelowna
2760 Cameron Road
West Kelowna, BC, V1Z 2T6

Attention: Jayden Riley, Planner III

RE: Providence Townhomes – Construction Management Plan

Dear Mr. Riley

The following Construction Management Plan is being provided to City of West Kelowna Staff and Council to help address any concerns related to the project. The following Plan will outline the construction phasing and steps to mitigate impacts to adjacent properties.

Introduction

The Providence Townhouse project consists of 58 units on the hillside between Sundance Drive to the northwest and Ensign Quay Lane to the southeast, divided as follows: 20 Side by Side units fronting Sundance Drive and 38 Tandem units with access off of Ensign Quay Lane & Ensign Lane as illustrated below.



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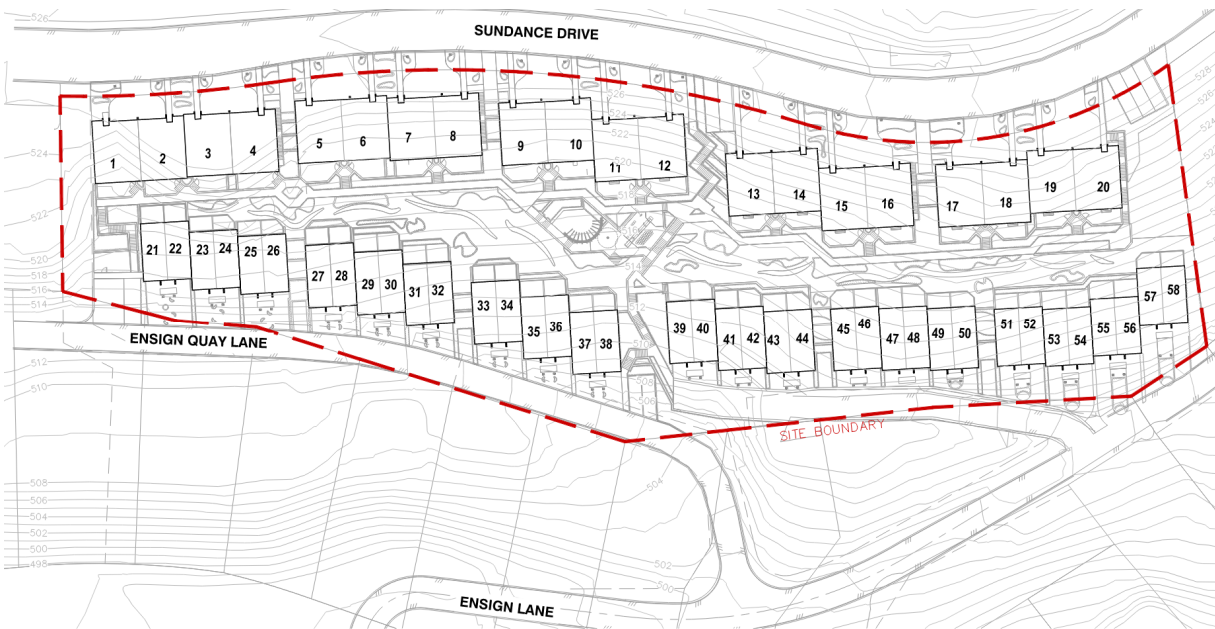
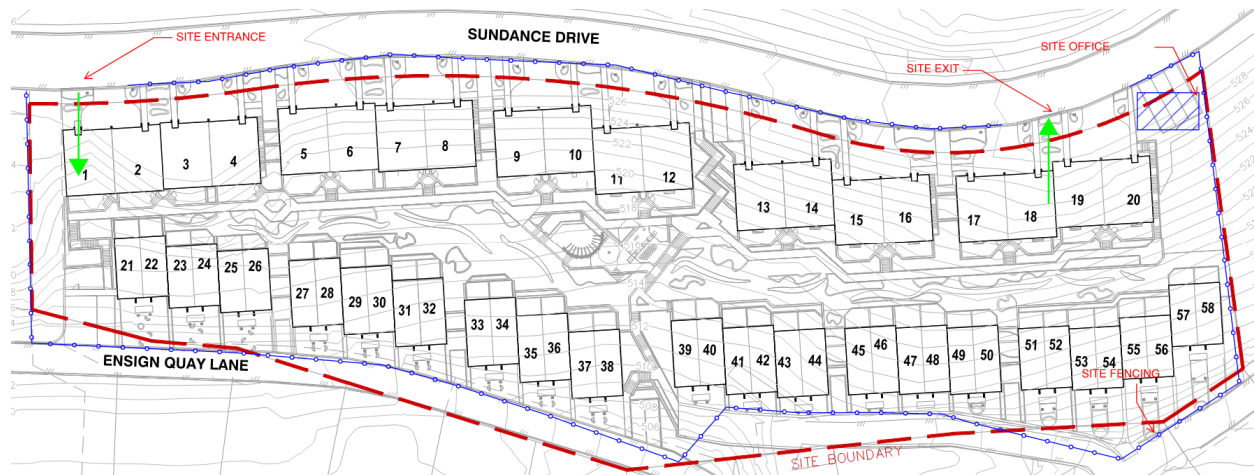


Figure 1: Site Location

Site Access

Gatehouse Developments is aware of the importance of mitigating the impacts of this development to the neighbouring properties. To ensure this is achieved, the primary site access will be from Sundance Drive to the Northwest of the site as per the figure below:



GATEHOUSE

Figure 2: Site Access

The lanes to the south of the site will only be used in case of emergency and as needed to safely complete construction activities otherwise not feasible from Sundance Drive. Anticipated work in the lanes will include underground pipe work, concrete placing, landscaping and minimal deliveries of construction materials. When work is required within the lanes, advanced notice will be given to adjacent properties and local access will be maintained via traffic control personnel.

Construction Sequencing

Phase 1: Gatehouse will subcontract a professional site servicing contractor experienced in hillside developments to undertake this phase. Work will include clearing trees and grubbing as well as removing organics from the site. It is anticipated during this phase that construction traffic will be increased as organic material will need to be hauled off site via dump truck to an approved dump site. At completion of this phase an interior construction road will be installed for the completion of the next phases of construction.

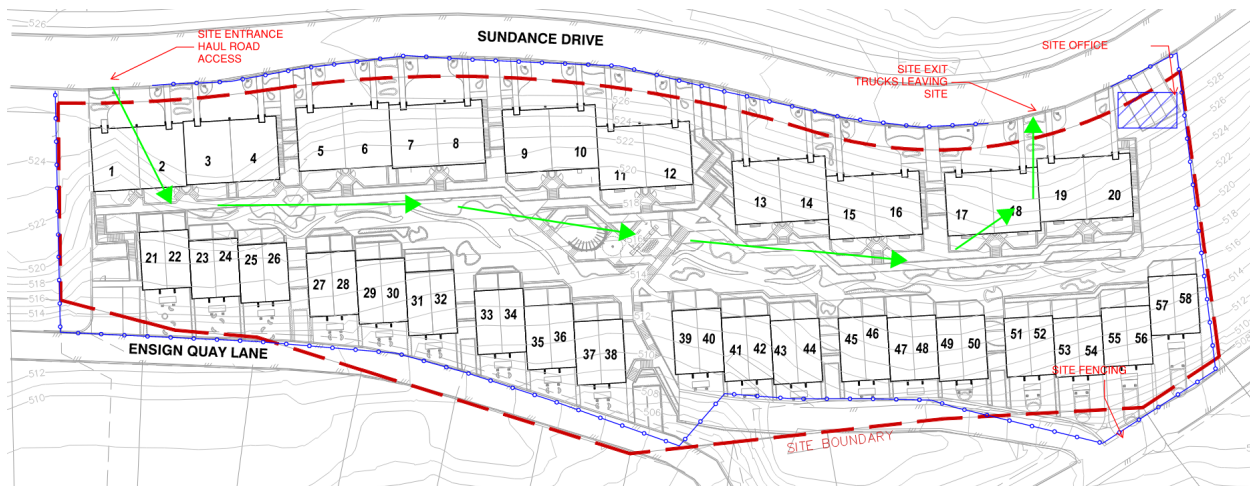


Figure 3: Construction Road & Site Access

Phase 2: The site servicing contractor will commence with excavation of the foundations for both the side by side and tandem units. Organic material within the building footprints will be hauled off site. Construction of the townhouse units is anticipated to be broken into 5 Phases as shown below:



GATEHOUSE

Figure

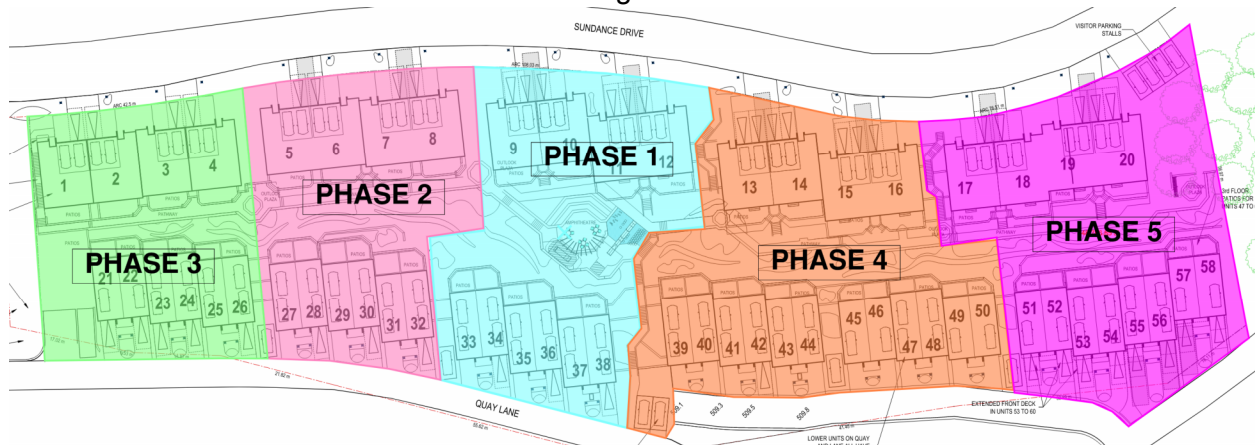


Figure 4: Construction Phasing

Phase 3: Construction of all residential units will be managed by Gatehouse Construction with work subcontracted out to local professional trades. Work will include concrete forming, framing, exterior finishes, interior finishes and landscaping.

Lane Work

Gatehouse Developments understands the need to minimize the impacts of this development on the neighbouring properties. While the majority of the work will be confined to the limits of the project site, some work is required with the lanes. Work will include utility tie-ins to existing City infrastructure on Ensign Quay Lane and the Ensign Lane. As previously noted a more comprehensive plan will be provided well in advance to the affected properties. A high-level conceptual plan is provided below for your information:



Figure 5: Lane Works

Impact Mitigation Strategies

Gatehouse Development is committed to ensuring that all of our developments minimize impacts to the surrounding areas. Below is a high-level table for outlining impact mitigation strategies. These will be further refined as we engage local professional contractors to complete construction of the Providence Townhouse Development

GATEHOUSE

Impact	Mitigation Strategy
Construction Noise	<ul style="list-style-type: none"> • Work only conducted during CoWK permitted hours
Storm Water Management	<ul style="list-style-type: none"> • Silt fence installed at perimeter of site • Gravel pads at entry/exit to site • Silt sacs on all existing manholes • Treatment system as per submitted ESC Plans
Dust	<ul style="list-style-type: none"> • Street sweeping as required • Water down services onsite to mitigate dust
Traffic Control	<ul style="list-style-type: none"> • Develop traffic management plans for specific work in lanes • Traffic management personnel (flaggers) to ensure local access to neighbouring properties
Fire	<ul style="list-style-type: none"> • Daily forest fire risk monitoring • Suppression equipment as required by risk level
Trade Parking	<ul style="list-style-type: none"> • No trade parking/staging in lanes
Wild Life	<ul style="list-style-type: none"> • Assessment and monitoring as required

We trust that this provides an overview of the anticipated construction phases and outlines the strategies to mitigate the impacts on the neighbouring properties. As construction is a fluid undertaking, this framework will be used to assess the site conditions throughout the project and changes may be necessary. Should any aspect of the work plan change in a way that would impact residents, our construction team will work with all stakeholders to ensure this development is completed to the satisfaction of all.

If City Staff or Council have any additional questions, please feel to contact us at your convenience.

Sincerely,

Kyle Weiher

GATEHOUSE

Senior Project Manager
Gatehouse Construction Inc.



From: [REDACTED]
To: [City of West Kelowna Submissions](#)
Subject: Attention City Clerk - DP 22-26
Date: May 10, 2023 8:29:53 AM

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From Sharon and Robert Jones

LOT 4 PLAN KAP78710
DISTRICT LOT 2044, OSOYOOS DIV. OF YALE LAND
DISTRICT PID: 026-380-749

After reviewing the delivered Notice for Development Permit with Variance (DP 22-26) with the members of our community, we believe the decision to move forward with the proposed development variances for the construction of 58 townhouses would be unsafe and problematic for a number of reasons.

Our primary concern is with safety. In the past several years, we have had very dry summers and with plans for these townhomes to be built so close together, along with the reduced boundary setbacks, any severe situation involving a fire would bring large-scale destruction to the homes and structures located nearby. In the event of an emergency, fire trucks could not properly tend to the buildings, as the lane has a sharp, blind corner with a narrow width under 17 feet and a sharp turning radius that a vehicle over 32 feet would not be able to navigate. Even if a small fire truck, ambulance or other emergency vehicle was able to manage their way up the lane, there is no turn around location, thus causing extended time concerns and even unnecessary deadly circumstances.

Another major concern is the likely degradation of the physical landscape on the hill side which we worry will cause some level of instability and even possibly water runoff concerns, erosion and potential flooding with landslides. In regard to maintenance and upkeep around the new properties during winter months, there should be a designated area on the property for snow removal and snow storage.

We believe the situation of the resulting reduced parking, loading and visitor parking for the new properties should be considered a non-starter, as there is already insufficient space for parked vehicles on Ensign Quay Lane and Ensign Lane, with a width of under 17 feet. With the reduced driveway sizes, it is simply not feasible to suggest an alternative for additional parking.

The city's waste disposal trucks currently do not drive up Ensign Quay Lane, so there are serious concerns about what will happen with emptying garbage, recycle and compostable bins for all of the newly proposed townhomes.

Ensign Quay Lane has a width of less than 17 feet wide with no turnaround and with the 7 current homes, the proposal of an additional 20 homes, the expectation is a minimum of 60 additional vehicles using this laneway with no parking, no turnaround and only one shared egress and ingress entryway..

To possibly remedy many mentioned safety issues above access to the new development should be from Sundance Drive and townhomes should be reduced by a third in size. We recommend a council member physically visit our community on Ensign Quay Lane to see how apparent these concerns will become.

The mixture of juvenile and adult trees will be a loss to the neighbourhood, not only in regard to screening, shade and greenery but also with the loss of the roots would most likely destabilize the hillside. As a whole, this green area is a visually enjoyable aspect for the entire community and will have a negative impact with insensitive and obstruction overbuilding on greenery and openness.

In conclusion, it is not safe to add that number of dwellings to a blind drive, narrow single entrance/exit and non-turn around lane.

Thank you for your assistance with this matter,
Owners Lot 4

From: noreply@esolutionsgroup.ca
To: [Delegation Requests](#)
Subject: New Response Completed for Request to Appear As a Delegation
Date: May 8, 2023 3:37:35 PM

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Hello, Please note the following response to Request to Appear As a Delegation has been submitted at Monday May 8th 2023 3:36 PM with reference number 2023-05-08-004.

- **Request to appear as a delegation on:**
5/16/2023
- **Name of person making the presentation:**
Ryan Jones
- **Name of the group or organization that the person is representing:**
Gatehouse Developments
- **Daytime phone number:**
7789680230
- **Email address:**
rjones@stewartland.ca
- **Presentation title:**
The Providence Development
- **What is your request to Council?**
Support of the Development Permit and Variances.
- **Presentation points:**
TBD

[This is an automated email notification -- please do not respond]



INFORMATION ONLY COUNCIL REPORT

To: Mayor and Council

Date: May 16, 2023

From: Paul Gipps, CAO

File No: P 21-01

Subject: **P 21-01; Official Community Plan Update; Stakeholder and Public Feedback**

Report Prepared by: Carla Eaton, Senior Planner (Long Range)

EXECUTIVE SUMMARY

This report has been prepared to provide Council with an update on the Draft Official Community Plan (OCP) 2040, and to present a summary of the Phase 4 public engagement process and formal external referral feedback. The workshop format is intended to provide an opportunity for Council to clarify any questions and to provide direction regarding the Draft OCP prior to Council consideration of first reading.

STRATEGIC AREA(S) OF FOCUS

Invest in Infrastructure – We will invest in building, improving and maintaining infrastructure to meet the needs of, and to provide a high quality of life for, current and future generations.

Pursue Economic Growth and Prosperity – We will work with stakeholders throughout the region to advocate for and support efforts aimed at helping West Kelowna businesses prosper. With a focus on the future, we will advance opportunities to expand our economy, increase employment, and develop the community in ways that contribute towards prosperity for all.

Strengthen Our Community – We will provide opportunities for the residents of West Kelowna to build connections, celebrate successes, embrace the community's strengths and diversity, address shared needs, and contribute to shaping the community's future.

Foster Safety and Well-Being – We will pursue through direct action, advocacy, and collaboration with local and regional service providers, investments in community health, needs-based housing, emergency preparedness, policing, and other services that foster safety and well-being in West Kelowna.

BACKGROUND

OCP Consultation as per the *Local Government Act*

The Official Community Plan (OCP) Update included public consultation and community engagement throughout the first three phases, prior to this fourth and final phase to draft and prepare for adoption. As per *Local Government Act (LGA)*, Section 475, the local government must provide one or more opportunities it considers appropriate for consultation with persons, organizations and authorities it considers will be affected, which is in addition to the public hearing requirements. Council had opportunity to review and endorse the proposed and evolving engagement strategy for each phase via reports dated May 21, 2019, June 8, 2021, and September 28, 2021, and by workshop held January 27, 2022.

Phase One saw the development of West Kelowna's Community Vision in February of 2021, which was completed through a comprehensive public engagement process and resulted in the creation of Five Foundations and 23 Key Directions to guide future decisions and actions for West Kelowna over the next 20 years. The engagement process included a comprehensive outreach program of social media, print campaigns, digital and video media, direct contact via email and postcards, stakeholder meetings, online meetings and workshops, and questionnaires as part of an award winning process.

Phase Two of the OCP Update was completed in October of 2021 and focused on gathering technical growth forecast information and input on growth ideas for West Kelowna to develop a preferred growth concept as the basis of the OCP's land use plan and key policies. The public engagement process included both online and in person pop-up stations that asked participants to engage in exercises that gauged their opinion on how West Kelowna could grow, as well as in-person engagement sessions, stakeholders meeting and mapping exercises that were added once permitted by health rules.

Phase Three included public consultation and community engagement on the Growth Concept, which took place between December 2021 and April 2022. This phase of consultation included a focus on gathering input into the concept of Vibrant Centres and Complete Neighbourhoods and alignment with key stakeholders in the identified growth areas. The public engagement process included in-person pop-up stations, a public open house, key stakeholder meetings, referrals to community partners and an online survey that asked participants to engage in exercises or discussions to gauge their opinion on the proposed Growth Concept.

Phase Four included a public information session and the formal referral process to introduce the first full draft of the OCP to the public and stakeholders for their feedback, which is elaborated on below. Following this final phase and the formal referral process, the consultation requirements for the OCP as per the LGA requirements are deemed to have been met.

Phase 4 – Public Engagement and External Agency Referrals

The draft OCP January 2023 version was presented to the public at the February 1, 2023 Public Information Session and was formally referred out to key stakeholders and

agencies on February 7, 2023. The public engagement period was open from January 24th to February 7th, and the referral deadline was March 7th.

A *What We Heard* Report was prepared to summarize¹ the comments from the Public Information Session, including the in-person, questionnaire form, and email feedback received during the engagement period (*Attachment 1*). The formal referral feedback from key stakeholders and agencies is summarized in this report and the detailed responses are provided separately (*Attachment 2*). The feedback from the public and referral process primarily indicated strong support for the key policies and proposed land use changes in the draft OCP. However, the feedback also included conditional support as well as non-support for some policy areas and land use designations. The following section outlines additional detail regarding analysis of this feedback and how the areas of conditional or non-support are recommended to be addressed.

Feedback Review Process

As part of the feedback review process, staff completed a detailed analysis and then categorized the feedback and referral responses within one of the three following categories:

- 1) **Clarification Items** – where staff are able to provide a response to answer the questions raised to clarify policy and improve understanding of the intent, and/or the response to the issue requires implementation of the OCP and/or other plans identified within the OCP;
- 2) **Minor Policy Adjustments** - where minor amendments can be made to improve or clarify policy and do not change policy intent; and
- 3) **Major Policy Issues** – where suggested policy revisions have the potential to affect major policy areas and be contrary to the Community Vision or previous Council direction on the issue.

For example, where conditional support was expressed, the issue or area of concern is being revised through minor amendments when supported by the Community Vision and previous Council policy direction. Alternately, the critical implementation of policy, planning or action items in the Draft OCP were highlighted as necessary to help address the issue or concern.

In some cases, it is difficult to address a specific area of concern without wholesale changes to the principles or vision of the plan, or there were a limited number of responses on a specific issue. For these types of feedback, while no changes may be recommended the Draft OCP is still subject to the public hearing process and further Council direction.

¹ As part of the analysis of the public feedback, each individual response was reviewed and then summarized through both quantitative and qualitative methods. The report includes the summaries, as well as the full responses as an attachment to the report.

Summary of Key Highlights of the Public and Referral Feedback

Public Feedback:

The following summary is an excerpt from the What We Heard Report (*Attachment 1*):

- 1) Support focused development within Centres that provide convenient access to services within a walkable and visually pleasing environment.
- 2) Believe the provision of quality housing across a broad spectrum of type, tenure, size, location and cost are essential to the City's future.
- 3) Strongly support the protection, preservation and creation of greenspaces and waterfront areas, not only for recreational use but for the protection of undeveloped natural areas for their ecosystem value.
- 4) Desire enhanced connectivity between new Centres and key destinations within the City, as well as through the community, with a greater focus on options for walking, cycling and transit as a priority and not just on vehicular movement.
- 5) Have a variety of concerns and different perspectives on the proposed higher density and taller buildings, but generally agree that the transition areas must consider potential impacts to mitigate adverse impacts from taller buildings and that adequate infrastructure must be in place to support the density.
- 6) Want public gathering spaces woven into our new Centres that support all-season cultural, art, food vendors and festivals that tie into the commercial/retail and mixed use areas with inspiring design features that really welcome the public into these areas.
- 7) Support the preservation and enhancement of the industrial and business park area as a key economic driver within the City.
- 8) Have reservations about the impact of the Gellatly NC on the adjacent residential development and the waterfront recreational area.

While the extensive public feedback resulted in minor recommended updates to the OCP (outlined below), it is important to note that the implementation of the identified objectives, policies, action items, and Development Permit Guidelines is critical to address much of the remaining public feedback. The successful implementation of the OCP and other related master plans and strategies will both alleviate or reduce individualized concerns, as well as ensure the continued support by the larger majority who supported the draft plan. Additionally, the public hearing will allow for formal public comment on the revised Draft OCP.

Updates related to Public Feedback:

- 1) Revised Action Item 3.6.7.4 to ensure consideration and study of the City's recreational facilities based on considerable public feedback noting the importance of providing additional community amenities for all ages, where the public noted facilities and services for community centres, art and cultural activities, and sporting activities including the pool, arena, etc.
- 2) Mapping amendments completed within Urban and Neighbourhood Centres to highlight existing and potential park and school sites. This addresses impacts related to revised land use mapping of these centres.

Referral Feedback and Related Updates:

The following is a summary of the referral responses from external agencies and stakeholders and proposed updates to address their comments where necessary (please see *Attachment 2* to read the full responses in detail):

1) Agricultural Land Commission (ALC)

- Generally, the ALC noted they appreciate and support the proposed agricultural policies which prioritize agriculturally designated land for farm uses but made some recommendations regarding policy language in a few areas, as well as some map adjustments to reflect on the status of ALR designated lands. They acknowledged the proposed exclusion language that considers City infrastructure and existing City-owned land historically managed as parks but would like to see the updated Transportation Schedules 5-8 once completed through the Transportation Master Plan (TMP) process.
- The required amendments to address the ALC concerns do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments.

2) BC Transit

- BC Transit noted they are generally supportive of efforts to intensify land uses, especially around identified transit hubs and corridors, as well as supporting increased standards for pedestrian mobility around areas that have existing transit services, such as the Industrial and Business Park as an example. They note concern with oversupply of parking as a negative impact on promoting transit use. They also made some recommendations regarding policy language to support the development of transit infrastructure through the City's development processes, to clearly define transit terminology, and to link back to details perhaps outlined in the TMP.
- The required amendments to address the BC Transit concerns do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments or have been provided to the Transportation Master Plan as applicable.

3) City of Kelowna

- City of Kelowna commended the draft OCP as a progressive and forward-thinking vision for the future through compact and focused urban development, and retention and expansion of employment lands. They noted alignment in many of the two cities common issues and challenges and see benefit in our continued collaboration and sharing of ideas and processes as one of Canada's fastest growing areas. They also noted opportunities for West Kelowna to consider incorporating a greater lens of equity and inclusion into the OCP, as well as exploring opportunities for housing variety within infill areas and elsewhere. The use of office intrusion into the region's industrial areas was also noted as a caution.
- The required amendments to address the City of Kelowna's comments do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments or have been provided to the related planning projects, such as the ongoing Housing Strategy as applicable.

4) Fortis – No objections or concerns were noted.

5) Interior Health

- Interior Health provided comments from three separate perspectives on healthy community development, food systems, and larger water systems. They commended references to the Healthy Built Environment Toolkit, policies on equity and health, focused growth within walkable centres, and the allowable use of parks within all areas of the City and the goal for 20% of land within the City to be in the form of protected natural area and/or publicly accessible parks, trails and greenspace. They also provided some specific policy recommendations in a number of areas to enhance the OCP perspective on equity, climate resiliency, food security and water usage, as well as a definition for clarity. They also encourage the City to continue to review the impact of short term rentals on housing supply, which is related to the ongoing Housing Strategy.
- The required amendments to address comments from Interior Health do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments. Comments regarding housing have been provided to the ongoing Housing Strategy as applicable.

6) Ministry of Agriculture and Food

- The Ministry acknowledged the strong support for the protection and enhancement of agricultural lands and farming as an economic driver throughout the OCP, as well as the strong conditions for exclusions. They also provided a number of recommendations regarding suggested wording to clarify policy in several land use designations where agricultural uses are permitted, corrections to plans or organizations referred in the OCP related to agricultural programs, wording to clarify the City or other's role in conflict resolution with agricultural uses, and to clarify where the ALR regulations may also affect land uses permitted. They also commented on the preferred type of residential buffers adjacent to agricultural uses if provided with adequate setbacks and vegetative buffers.
- The required amendments to address the Ministry's comments do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments. The comment on residential buffers will be taken into consideration with the future Infill Strategy's review of transition areas adjacent to the identified Centres, which includes some interface areas with agricultural uses.

7) Ministry of Housing / Ministry of Municipal Affairs

- The Ministry of Housing and Ministry of Municipal Affairs provided a joint response noting that they had no concerns with draft OCP, subject to other ministries' interests. They acknowledged policies highlighting the City's relationship with Westbank First Nation, our housing policies based on our recent Housing Needs Assessment 2022, and references to asset management and development cost charges to support our community as it grows. They also made recommendations regarding a regional context statement.
- The required amendment to address the regional context statement are consistent with comments also made by the RDCO and do not conflict with

the Community Vision or previous Council direction and is considered a minor policy adjustment that will also address LGA requirements.

8) Ministry of Transportation and Infrastructure (MoTI)

- MoTI noted the importance of the OCP aligning with initiatives for the Highway 97 corridor through the Okanagan identified by the Central Okanagan Integrated Transportation Study (CO-ITS), as well as noting that planning for focused urban and neighbourhood centres as part of complete, compact and connected communities would also support initiatives for Transit Oriented Development (TOD). They also requested that development applications for development or those with impact on Highway 97 continue to be referred for Ministry comment, and that they will need to review the more detailed Transportation Schedules 5 – 8 as they become available.
- No specific amendments were requested by MOTI, and staff will continue to work with the Ministry in conjunction with the development of the TMP and the related OCP Transportation Schedules 5 – 8. Any applicable updates will be provided to Council with future consideration of the OCP and/or TMP.

9) Regional District of Central Okanagan (RDCO)

- RDCO commented that the Draft OCP included well thought out and progressive objectives and policies to address future growth, climate change, natural areas and parks, and sustainable transportation consistent with the Regional Growth Strategy. They made recommendations to consider additional opportunities to collaborate or utilize the results of potential future RDCO work regarding climate change planning, sensitive ecosystems inventory, and a Regional Employment Lands Inventory Project. They also noted that a regional context statement is required. Two proposed land use designations were also requested for amendment to park lands managed within the Glen Canyon Regional Park and Kalamo Regional Park.
- The required amendments to address the RDCO's comments do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments. The regional context statement will also address LGA requirements. The details of the proposed mapping amendments will be addressed with future consideration of first reading.

10) School District No. 23 (SD23)

- SD23 recommended policy amendments to address historical school sites and proposed and potential expansion of school facilities to accommodate the anticipated population growth estimated within the City where these sites are affected by Agricultural Land Reserve restrictions. They also provided updated school site requirements, and noted some concern with how potential and existing school sites were not indicated where located within proposed Centres.
- The required amendments to address SD23's concerns do not conflict with the Community vision or previous Council direction and are considered minor policy adjustments. The details of the proposed mapping amendment will be addressed with future consideration of first reading.

11) Urban Development Institute (UDI)

- UDI provided a very detailed referral response which included clarification questions about aspects of the document, positive comments in support of particular policy areas, recommendations for consideration of policy and mapping amendments, as well as information provided as background to their comments.

Promoting a higher degree of mixed land uses within growth centres was welcomed as a goal to reduce the need for vehicles and to ensure enough density to support retail and offices spaces within the centres. However, numerous concerns were also received within four consistent categories related to proposed floor area ratio (FAR) and density, typical building heights, building siting and separation, and housing and housing types anticipated by the OCP. In general, recommendations included the consideration of:

- Greater density and heights within centres, but also throughout the community, ranging from towers in Urban Centres to 6 storeys in all residential areas;
- Reduced setback, stepbacks, and tower separations in general;
- Support for reduced control over the type of housing permitted within defined areas, and more incentives and reduced controls to support the provision of housing.

Other concerns were also noted with regard to:

- Restriction of mixed use from industrial area;
- Size and location of the Growth Boundary and Neighbourhood Centres;
- Ensuring adequate density will in general support required retail/commercial components; and
- Treatment of boulevards related to landscaping and street tree installation and ongoing maintenance.

Staff met with UDI on May 5, 2023 to provide opportunity to clarify any of the outstanding questions regarding the feedback, to provide comment on required amendments to address UDI's comments where consistent with the Community Vision or previous Council direction, and to note where recommended major policy adjustments would require the direction of Council before OCP changes were contemplated at the staff level.

- Many of the required amendments to address UDI concerns do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments that staff will incorporate in the Draft OCP for consideration of first reading.
- Other issues identified in the UDI comments are considered major policy adjustments which have not been addressed as they require further Council direction and include (see discussion section below):

- Proposed higher FAR/Density/Heights within Centres, and within the Medium Density Residential land use designation;
- Allowing tower heights in excess of 20 storeys as a base density within Centres; and
- Expanding the boundary of the Growth Boundary and/or the Neighbourhood Centres.

12) Westbank First Nation (WFN)

- WFN provided a number of recommendations to address the preferred references to WFN on maps, to correct some errors regarding references to WFN, to revise policy to enhance cultural and heritage linkages, to support collaboration between the City and WFN regarding land use planning, connected trails and greenways, as well as infrastructure and transportation network planning. They also asked for clarification on a few policy areas and suggested that the City consider an expansion of the Westbank Urban Centre Mixed Use Corridor to include additional lands lying between the corridor and Westbank First Nation lands as they felt the anticipated heights would be more comparable.
- Many of the required amendments to address WFN concerns do not conflict with the Community Vision or previous Council direction and are considered minor policy adjustments that staff will incorporate in the Draft OCP for consideration of first reading.
- The suggested mapping amendment to enlarge the Westbank Urban Centre Mixed Use Corridor would be considered a major policy adjustment that is not consistent with previous Council direction and would require the direction of Council before OCP changes were contemplated at the staff level. Additional heights within this area is intended to be addressed through the discussion below regarding extraordinary community benefit.

The items noted above are provided as a generalized summary of the feedback and related anticipated changes to the Draft OCP. However, a detailed list of the major content and mapping changes, as well as a red-lined text only document, will be provided at first reading to highlight updates to the draft OCP to address Council, agency or stakeholder issues identified through this feedback process, as well as highlight where minor document corrections and revisions have been addressed through the internal review process. The following discussion section will outline further detail regarding any feedback that would require Council direction to consider any major policy adjustments.

DISCUSSION

Densities (FAR) and Heights:

Additional density and heights have been recommended by the UDI within Centres to create opportunity to support the proposed mixed-use elements of these areas and to reflect the current economics of land and construction costs to attract investment at the proposed higher densities within the Centres. Additional density and heights have also been requested more generally within the Medium Density Residential (MDR) land use designation to allow for 6 storey apartments throughout the community.

In consideration of these comments the proposed FAR and building heights were reviewed for the Centres which included cursory analysis of comparative jurisdictions. The FAR for the MDR land use designation was also reviewed, but it was noted that this LUD is located outside any of the focused growth areas so consideration for additional height beyond the generally identified maximum building height is not supported by the Community Vision (outside of Neighbourhood Centres).

Based on this review, the density for all areas are recommended to be increased to generally reflect UDI comments and incorporated in the Draft OCP for consideration of first reading (Table 1). Should this be amenable to Council, the following policy amendment would also be incorporated which ensures the future Zoning Bylaw amendments would enact the land use designations:

- *Typical maximum densities outlined within the OCP may only be achievable through zoning regulations that require density bonusing provisions in consideration of objectives that meet community objectives of the City. These objectives are further defined in the Zoning Bylaw and other policies, which may generally support the provision of items such as rental housing, inclusion of underground parking, etc.*

Table 1: Potential FAR Increase to Reflect Referral Feedback

Land Use Designation	DRAFT OCP	UDI Request	Proposed FAR Increase
WUC (Area A - Mixed Use Corridor)	3.5 (12 storey)	4.1 - 4.65 (12 storey) 4.7 - 5.5 (14 storey) & 19 - 25 storeys where high rise is desired	4.1 (12 storey) *with Density Bonusing
WUC (Area B - Comm Core)	2.5 (10 storey)	4.1 - 4.65 (12 storey) 4.7 - 5.5 (14 storey) & 19 - 25 storeys where high rise is desired	3.5 (10 storey) *with Density Bonusing
WUC (Area C - Res Shoulders)	2.0 (6 storey)	2.35 - 2.75 (6 storey) 2.95 (8 storey) 4.1 - 4.65 (12 storey) 4.7 - 5.5 (14 storey) & up to 19 - 25 storeys where high rise is desired; should be minimum 2.5	2.5 (6 storey) *with Density Bonusing
BUC	2.0 (6 storey)	2.35 - 2.75 (6 storey) 2.95 (8 storey) 4.1 - 4.65 (12 storey) 4.7 - 5.5 (14 storey) & 19 - 25 storeys where high rise is desired	2.5 (6 storey) *with Density Bonusing
NC's	1.5 (6 storey)	2.35 (6 storey)	2.35 (6 storey) *with Density Bonusing

MDR Apartments	1.0 (4 storey)	1.85 (4 storey) 2.35-2.75 (6 storey) & consider more MDR apartments at more locations	1.85 (4 storey) *with Density Bonusing
MDR Townhouses	1.0 (3 storey)	1.5 (3 storey)	1.25 (3 storey) *with Density Bonus for underground parking

DECISION POINT #1: Does Council support the increased densities through density bonusing as outlined in the table above?

Building Heights in Urban and Neighbourhood Centres (20+ storeys)

Development stakeholders recommend allowing tower heights in excess of 20 storeys within Centres. Their recommendations are based on a number of related factors such as:

- the costs associated with concrete construction to support high rises over 10 storeys;
- the residential density necessary to support the commercial/retail businesses presented as part of the vision for the Centres; and
- the demand for housing from an availability and affordability perspective.

As the Growth Strategy and Community Vision do not specifically consider 20+ storeys for any of the Centres, and to provide clarity to the public and development community, it is recommended that the OCP document provide general direction on anticipated building heights, especially if these heights will be defined by an extraordinary community benefit rather than an outright permitted height. The final building heights will be established in the Zoning Bylaw through the new zones currently being drafted to accommodate changes to the OCP, including the consideration of Decision Point # 2 and Decision Point #3.

It is anticipated that extraordinary community benefit will be **defined, refined and prioritized by Council** as necessary and applied on a site-specific basis. This may include a range of items which address broader community objectives and challenges related to housing attainability, social well being or community need including items such as supportive / seniors housing, community amenities, parkland/public space improvements, parking, etc. These items are to be defined in more detail through subsequent policy.

DECISION POINT #2: Does Council continue to support increased building heights in Centres greater than the proposed 6/10/12 storeys only where supported by an extraordinary Community Benefit?

DECISION POINT #3: What is the maximum (general) height* that Council supports for increased building heights in Urban Centres where supported by an extraordinary Community Benefit?

*Note: Staff are working with industry professionals to build a better understanding of the implications of height above 12 storeys as it relates to feasibility and costs, which will be presented in person as part of this report.

Revisions to Growth Boundary - Raymer

Referral feedback included requests for consideration of expanding the Growth Boundary in a number of areas, as well as specific requests to restore the Growth Boundary associated with the previous Raymer Comprehensive Development Planning Area. Expanding the Growth Boundary in any location challenges the focused growth areas identified within the Growth Concept. Based on Council direction from the November 22, 2022 meeting, the Draft OCP reflects short term growth opportunities (within 5 years) at the southernmost extent of the Raymer area (Figure 1), and longer term growth opportunities in the Bear Creek/Raymer North area to accommodate future growth (beyond 5 year timeframe) within a comprehensive planning framework.

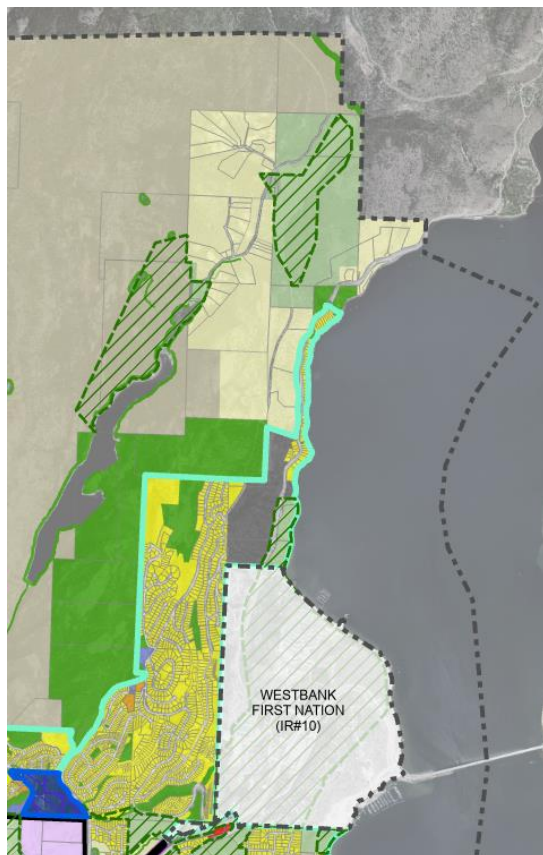


Figure 1: OCP 2040 Proposed - Growth Boundary (teal)



Figure 2: Current OCP - Growth Boundary (brown) including Raymer Area (red)

However, based on feedback received, including the delegation presented to Council on May 9, 2023, additional consideration of revising the growth boundary to reflect the original Raymer Area has been requested (Figure 2). It is recognized that the addition of

the Raymer area provides additional opportunity to meet the City's housing objectives and is intended to be a complete community concept, which is aligned with broader housing policy and objectives within the City and OCP.

However, alterations to the growth boundary should also consider the following:

- Expansion/reversion of the growth boundary is largely inconsistent with the focused growth areas identified within the Growth Concept;
- Expansion/reversion may detract from the viability of recently completed growth areas including Smith Creek and Goats Peak, which are proximal to existing municipal servicing/transportation networks;
- Growth projections are currently not inclusive of the Raymer area;
- Related long-term infrastructure costs associated with maintenance, operation and replacement of an expanded greenfield infrastructure network (water, sanitary sewer, storm water, and roads);
- Agency support which focuses on the objective of compact, intensified urban development;
- Opportunities to review/revise the growth boundary as part of future OCP updates (5 year increments)
- Impacts to departmental staff resources to facilitate significant technical review, and implementation of the plan;
- Related impacts to Development application processing timelines; and,
- Impacts to the timing/delivery of Long Range Planning projects anticipated in 2023/24 such as Parking Strategy, Westbank Centre Revitalization Plan update, Transit Planning, Short Term Accommodation Update, Zoning Bylaw development, Infill Strategy, Housing Strategy implementation, Community Climate Action Plan, etc.

DECISION POINT #4: Does Council support expansion/reversion of the Raymer CDP area to its previous extent as outlined in Figures 1 and 2?

Expansion of Neighbourhood Centres (NC)

Based on referral feedback that included requests for expansion of Neighbourhood Centres, a review of the NC's was also completed to assess the potential impact associated with any increase in their sizes. This review included consideration of historical commercial and retail space studies completed for the City, as well as the more recent Collier's reports completed as part of the original development of the Growth Concept and growth models considered by Council as part of that process. Based on this, no changes to the Neighbourhood Centres have been proposed.

However, additional requests to expand the Goat's Peak NC was brought forward in the UDI's response. A previous request was submitted through the Brainstorm Portal survey completed in the summer of 2022. Staff recommendation to not proceed was based on concerns with the inconsistency with the Comprehensive Development Plan, the Growth Concept, the potential to detract from the development of other mixed use areas given the size of the proposed expansion, and the lack of immediate need to expand the NC when no development has occurred. The current request is for a similar expansion of the Goat's Peak NC (Figure 3).

Comparative analysis to other NC's when institutional uses are excluded suggest that the NC is currently comparable to other proposed NC's (Table 2) and that the request is significant.

Table 2: Comparative Neighbourhood Centres

Neighbourhood Centre	Area (m ²)
Goats Peak - Existing (with institutional)	109,000
Goats Peak – Existing (without institutional)	60,000
Lakeview Village Centre	66,000
Rose Valley (with institutional)	172,000
Rose Valley (without institutional)	62,000
Gellatly Village	77,000
Proposed/Revised	
Goats Peak - Proposed (with institutional)	300,000
Revised Goats Peak (with institutional)	166,000
Revised Goats Peak (without institutional)	117,000

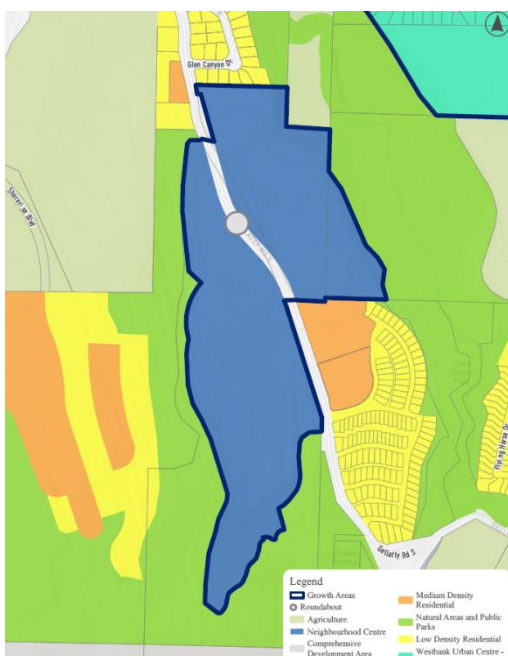


Figure 3: Requested NC Expansion (blue)

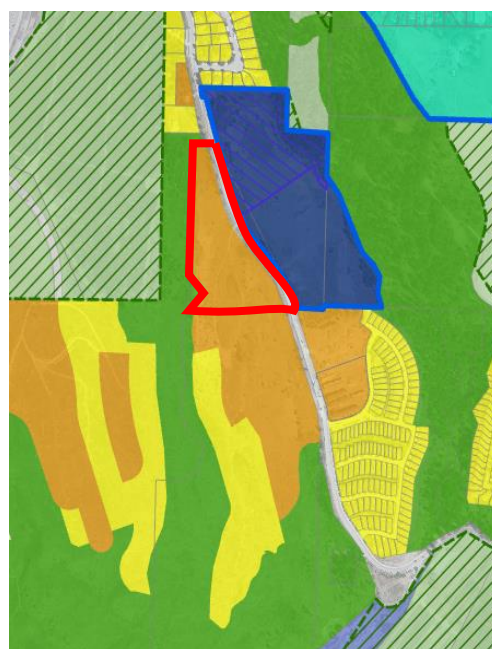


Figure 4: Existing NC (blue) & Proposed Revised NC Expansion (red)

As the proposed expansion is a substantial deviation from the standard NC's throughout the City (from ~109,000 m² to ~ 300,000 m²), this request was not previously incorporated into the OCP. However, should Council wish to consider expansion of the NC, a smaller expansion area of the NC is recommended (see Figures 3 & 4 above) and based on the following:

- Full expansion (as requested) is not consistent with the Growth Strategy to accommodate 6 storey development in focused NC's only;
- Increasing short term development opportunities to the site may contribute to housing supply and targets;

- A modest expansion of the area would reduce impacts on other commercial/retail objectives throughout the remainder of the City;
- Additional residential densities associated with the NC may support the required commercial/retail development within the NC;
- Access to residents to a mixed use area on either side of Gellatly Road support walkability objectives; and
- A smaller expansion will have a reduced impact on growth projections.

Should Council wish to direct revisions to the Goat's Peak NC size, it is recommended that the revisions are directed in accordance with Figure 4.

DECISION POINT #6: Does Council support expansion of the Goats Peak Neighbourhood Centre in accordance with Figure 4?

NEXT STEPS

- Complete outstanding edits as a result of the external referral and public engagement process
- Complete necessary edits related to Council direction as outlined in this report
- Council to consider first reading of the OCP (anticipated for June 2023)
- Council to consider the Financial Plan and Waste Management Plan (anticipated June 2023)
- Refer the OCP to the Agricultural Land Commission
- Council to hold Public Hearing (anticipated June 2023)
- Amend OCP as necessary to reflect Council and Public feedback (summer 2023)
- Council to consider second, third, and final readings of the OCP, as applicable (summer 2023)

COUNCIL REPORT / RESOLUTION HISTORY

Date	Report Topic / Resolution	Resolution No.
November 22, 2022	Council received the Official Community Plan Update Report for information purposes.	N/A
May 24, 2022	Council received the Ideas in Place (Phase 3) What We Heard Report for information purposes.	N/A
December 14, 2021	THAT Council direct staff to proceed with development of a draft land use plan and initiate policy development based on the preferred Growth Concept – Vibrant Centres and Complete Neighbourhoods.	C382/21
October 26, 2021	Council received the Phase 2 What We Heard Report for information purposes.	N/A
September 28, 2021	Council received the Phase 2 Status Update for information purposes.	N/A
June 8, 2021	Council received the Engagement Plan Overview for information purposes.	N/A
April 6, 2021	Council received the 2021 Long Range Planning Work Plan for information purposes.	N/A
February 23, 2021	THAT Council endorse the West Kelowna, OurWK Community Vision Final Draft.	C093/21

REVIEWED BY

Brent Magnan, Director of Development Approvals

Corinne Boback, Legislative Services Manager / Corporate Officer

APPROVED FOR THE AGENDA BY

Paul Gipps, CAO

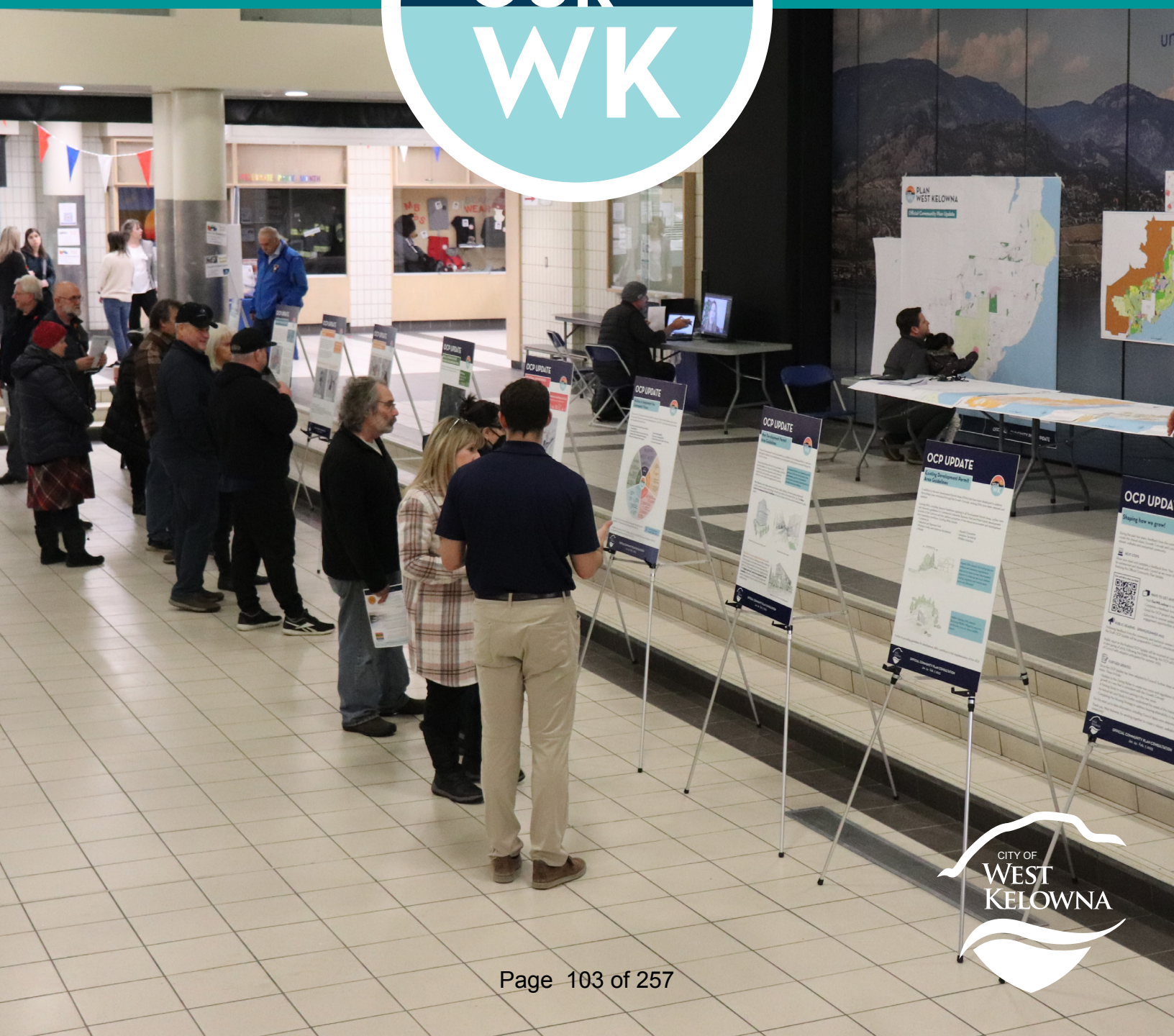
Powerpoint: Yes ☒ No ☐

Attachments:

1. Public Information Session Summary Report: What We Heard – Spring 2023
2. Agency and Stakeholder Referral Responses

WEST KELOWNA OFFICIAL COMMUNITY PLAN 2040

PUBLIC INFORMATION SESSION SUMMARY REPORT:
WHAT WE HEARD - SPRING 2023



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ENGAGEMENT HIGHLIGHTS

WHAT WE ASKED

What do you think about growth boundaries and focused growth in the Growth Concept areas?

What do you think about the additional height and density within the Urban and Neighbourhood Centre with a mix of uses to create vibrant centres?

What do you think about the policies to support/encourage the development of the business park/industrial land to increase economic sustainability

What do you think about the preservation and enhancement of green space and waterbodies through the use of buffers and other green space policies?

Do you think the Draft OCP Plan Update addresses these four focus areas and is anything missed that may contribute to the success of the Growth Concept?

HOW WE ASKED



IN-PERSON ENGAGEMENT

- Well attended by 87 residents with meaningful engagement.



ONLINE FEEDBACK ACTIVITY

- A concise interactive online activity for two weeks with a feedback form and Q and A portal.



SOCIAL MEDIA POSTS, EMAILS, PRINT ADS, WEB NOTICES

- Targeted and open ended engagement over four weeks with stakeholders and the general public with extensive outreach.

Who is "We"?

The City of West Kelowna staff completed the Phase 4 Public Information Session engagement activities both online and in-person.



SNAPSHOT OF WHAT WE LEARNED



At a glance, people told us that they:

- Support focused development within Centres that provide convenient access to services within a walkable and visually pleasing environment.
- Believe the provision of quality housing across a broad spectrum of type, tenure, size, location and cost are essential to the City's future.
- Strongly support the protection, preservation and creation of greenspaces and waterfront areas, not only for recreational use but for the protection of undeveloped natural areas for their ecosystem value.
- Desire enhanced connectivity between new Centres and key destinations within the City, as well as through the community, with a greater focus on options for walking, cycling and transit as a priority and not just on vehicular movement.
- Have a variety of concerns and different perspectives on the proposed higher density and taller buildings, but generally agree that the transition areas must consider potential impacts to mitigate adverse impacts from taller buildings and that adequate infrastructure must be in place to support the density.
- Want public gathering spaces woven into our new Centres that support all-season cultural, art, food vendors and festivals that tie into the commercial/retail and mixed use areas with inspiring design features that really welcome the public into these areas.
- Support the preservation and enhancement of the industrial and business park area as a key economic driver within the City.
- Have reservations about the impact of the Gellatly NC on the adjacent residential development and the waterfront recreational area.

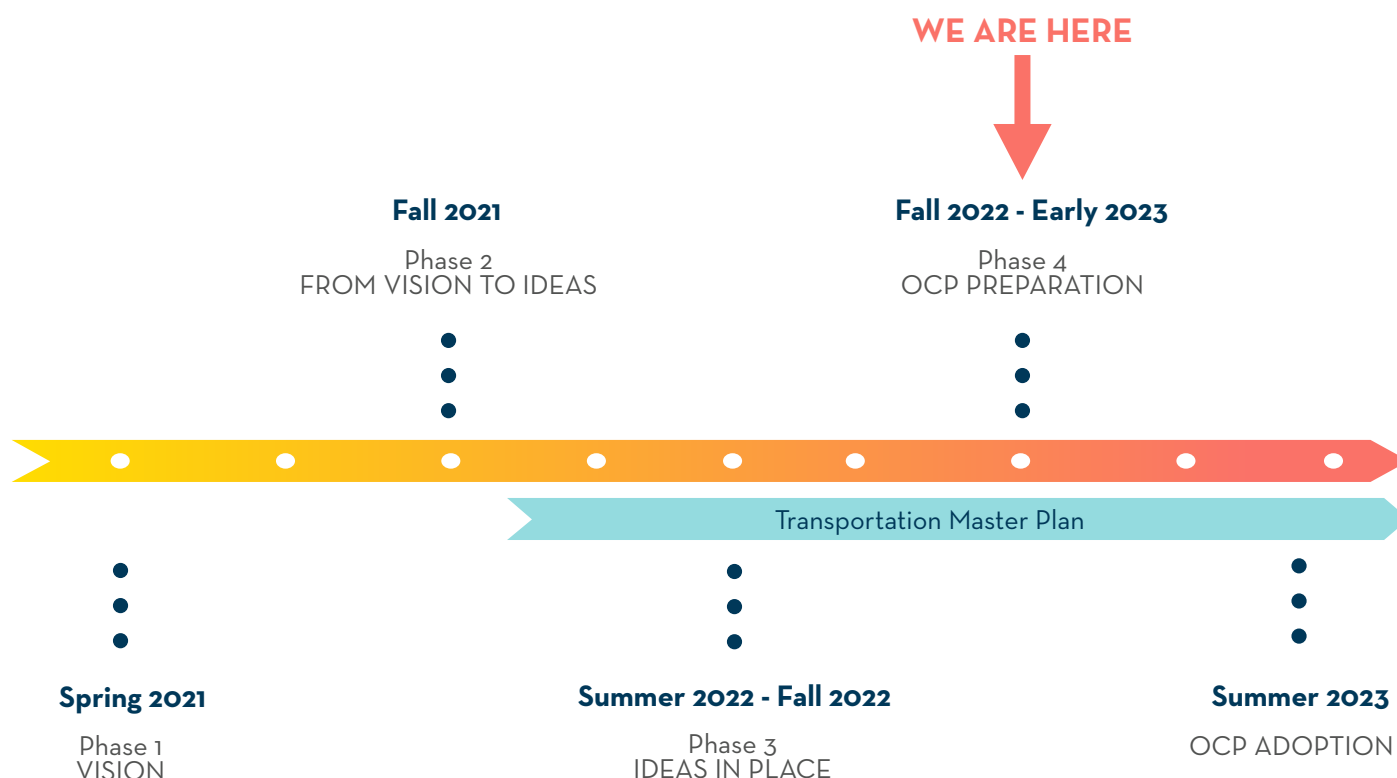
1 HOW WE CONNECTED



PURPOSE OF ENGAGEMENT

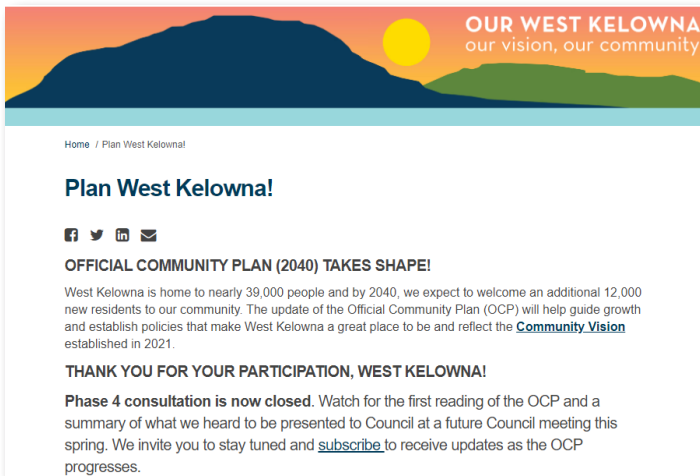
During the previous phases of consultation, participants told us they want a West Kelowna with vibrant city centres and complete, connected communities where they can walk, bike and take transit easily. They also want to preserve the beaches, parks and urban agriculture that makes West Kelowna unique.

The following Phase 4 engagement summary outlines the public feedback on the Draft OCP update and clarifies next steps in the OCP preparation. This feedback will aid in the development of a final plan to ensure the draft land use plan and policies create the vision for a vibrant, walkable and connected West Kelowna.



GOALS AND OBJECTIVES

- ▶ Gathering public feedback on the draft OCP to ensure the Growth Concept, Land Use Plan and Policies meet the Community Vision to guide development for the next 20 years.
- ▶ Generating interest and excitement in the community regarding the Draft OCP, and the relationship it has with the Draft Transportation Master Plan.
- ▶ Using this input to help guide revisions of the OCP document prior to Council's formal consideration.



Online engagement platform - OurWK.ca/ocp



In-person Information Session

CITY OF WEST KELOWNA

In your community

Help shape the future of West Kelowna! Share your feedback about growth and transportation

PLAN WEST KELOWNA **CONNECT WEST KELOWNA**

FOR IMMEDIATE RELEASE
Tuesday, Jan. 24, 2023

The City of West Kelowna is continuing to move ahead with two major planning initiatives that will guide how and where the city will grow and how people will get around over the next 20 years. Phase 2 of the Transportation Master Plan 2040 (TMP) Update and Phase 4 of the Official Community Plan (OCP) Update are now open for public input.

"We are a fast-growing community and it's important to hear from our residents as we update these guiding long-term plans," said Mayor Gord Milsom. "Input from the community will help shape our city now and into the future, building on the Community Vision to achieve a vibrant, walkable and connected West Kelowna."

The OCP's new Growth Concept focuses most future growth in two urban centres, five neighbourhood centres and accommodates continued development beyond these centres with a focus on infill housing. It also enhances key economic opportunities in the City's industrial and business park areas.

The TMP is being developed in conjunction with the OCP. This phase of the TMP recommends a future network for roads, transit and active transportation, based on how and where the City plans future growth.

Share your feedback about future land use and transportation by visiting [OurWK.ca](#) and drop by the public information session:

- Date:** Wednesday, Feb. 1
- Location:** Mount Boucherie Secondary School
 - 2751 Cameron Road, West Kelowna
 - Multi-purpose Room
- Drop-in event from 4:30 p.m. – 8 p.m.

This phase of consultation is open from Jan. 24 to Feb. 7. Next steps will include the Draft OCP Bylaw presented to Council in the spring for consideration prior to adoption and the Draft TMP is scheduled to be shared for public feedback in the summer.

Subscribe for updates about the OCP and TMP and other engagement opportunities at [westkelownacity.ca/subscribe](#).

Submit a service request here: <https://forms.westkelownacity.ca/Service-Request>

City of West Kelowna | 778-797-1000 | [www.westkelownacity.ca](#)

Facebook Twitter Instagram

PLAN WEST KELOWNA **CONNECT WEST KELOWNA** **WEST KELOWNA**

Official Community Plan and Transportation Master Plan Feedback Form

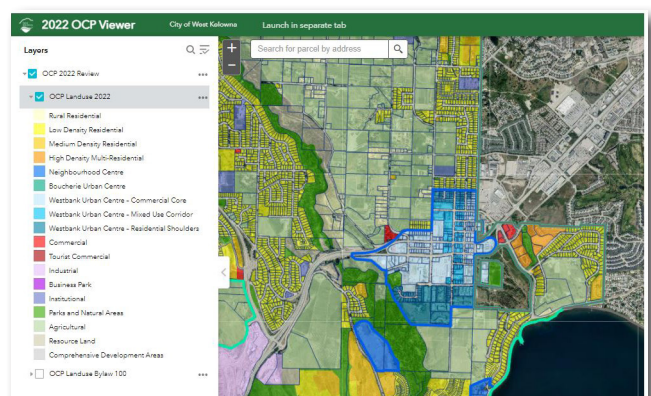
Official Community Plan Questions

The City's [Growth Concept Plan](#) supports the OCP and identifies where and how West Kelowna will grow over the next 20 years. It is anticipated that we will need an additional 5,300 housing units to accommodate approximately 12,000 additional residents by 2040.

The Draft OCP identifies key directions to achieve the City's Growth Concept, and the OurWK Community Vision established in 2021, to accommodate our fast-growing community.

Benefits of concentrating growth:

Online Feedback Form



Interactive Map

e-News update to community members

1.1 HOW WE COMMUNICATED

How we communicated



2 e-News update sent to
3,602 people (one launch update
and one open house announcement)



2 public service announcements



3 email invitations to OCP
stakeholder groups and 22 community
organizations



2 print newspaper ads



2 online calendar events postings



1 ads on social media with a total
reach of **4,349**



1 new OCP fact sheet with
approximately 87 handed out



Website updates and materials
provided at **OurWK.ca/ocp** with
1,200 visitors from Jan 24 to Feb 7

Social media interactions



17,379 total social media
impressions



8,497 people reached through
ads about the open house with
265 clicks, likes, shares, reactions
and comments



3,912 Facebook impressions with
321 clicks, likes, shares, reactions and
comments



1,619 Twitter impressions



2,048 Instagram impressions
with **55** clicks, likes, shares, reactions
and comments



1303 OCP document views from
materials provided at **OurWK.ca/ocp**

1.2 HOW WE ENGAGED

Public Information Session - Feb 1, 2023

► 1 Open House

- 3.5 hour public information session where staff met with members of the public, including property owners, residents, and developers, to review the draft OCP policies and land use plan
- Wall display of draft Schedule 1 Land Use Plan and Schedule Maps 2 to 8
- 9 display boards focused on OCP highlights (Growth Concept, Land Use, Policies, and Development Permit Areas)
- Fact sheets available on the previous phases, as well as draft OCP
- Staff available in-person and via computer link to answer questions

► 1 Interactive Map

- Residents asked to identify their neighbourhood.

► 1 Online Computer Station

- Interactive draft Schedule 1 Land Use Map
- Access to the OCP project page on ourwk.ca with full draft OCP document, including proposed schedules

► 1 Paper or Online Feedback Form

- Available to the public between January 24 - February 7
- **163 contributions to the Feedback Form**
 - This feedback form asked participants to review the Growth Concept and draft land use plan, and provided an opportunity to make open ended comments





2 WHAT WE HEARD

"I completely agree with the Urban growth centres in West Kelowna and Mt. Boucherie...would love to see some more shopping and food venues near the Mt. Boucherie Transit exchange, especially a coffee shop that is open in the evenings." - Participant

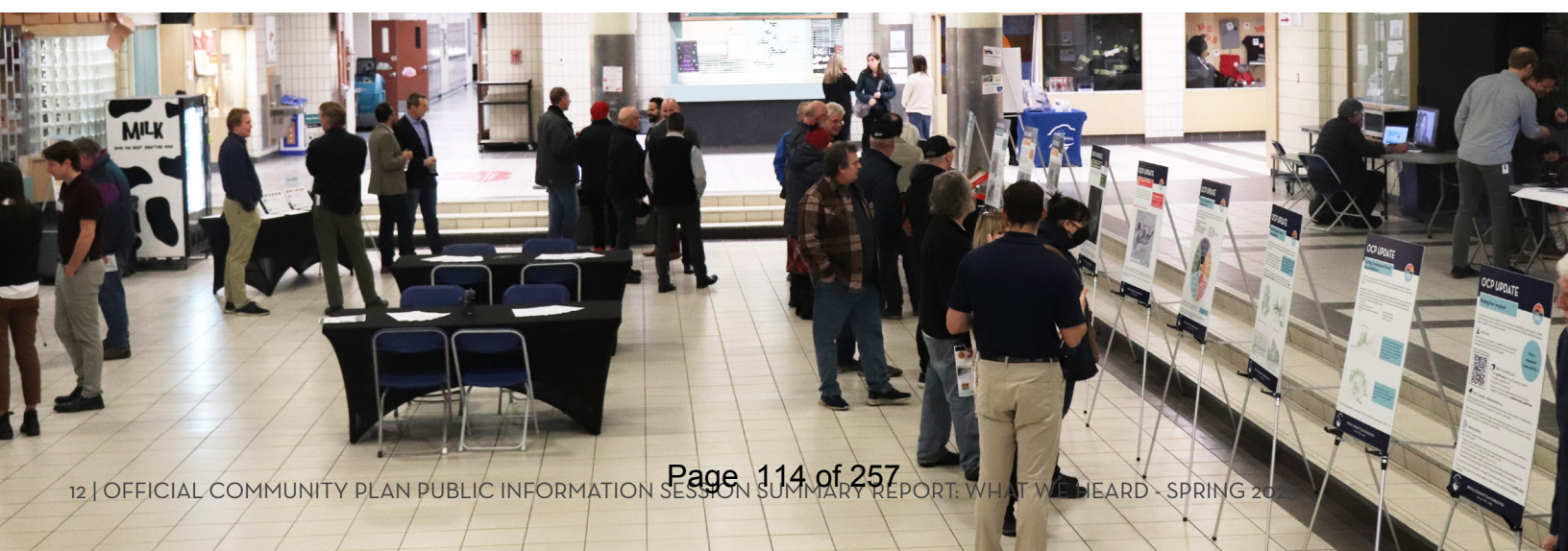
2.1 INFORMATION SESSION FEEDBACK



GENERAL PUBLIC FEEDBACK

During the public information session, residents were encouraged to review display boards, maps and fact sheets related to the Official Community Plan, and to interact with staff to answer questions regarding the presented material. Through this open feedback process, the following common themes and general questions were communicated to staff:

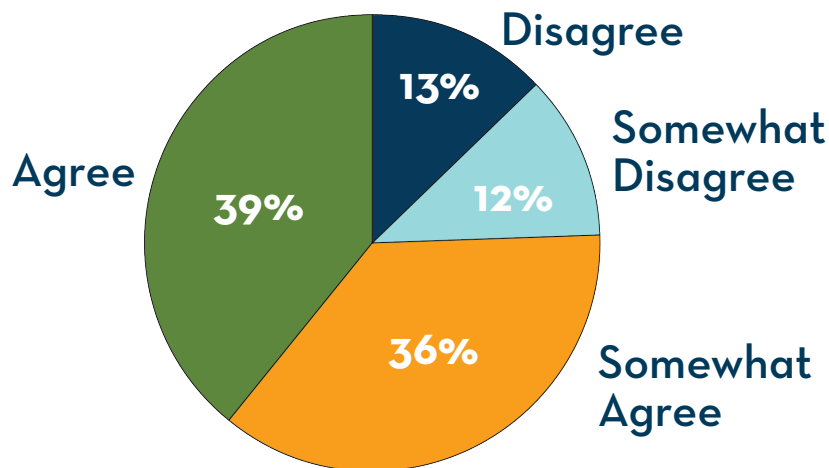
- ▶ Gellatly Bay densification/ neighbourhood centre – particularly regarding traffic, safety, access, shadow casting over the beach, and parking concerns
- ▶ Requests for more pedestrian, cycling infrastructure and connection to the waterfront
- ▶ Questions about the rural reserve/ growth boundary, including what is planned for the areas outside the boundary
- ▶ Concerns about high density development in the Westbank Urban Core (WUC) immediately adjacent to existing single detached
- ▶ Questions regarding frequency of an OCP update and clarification of land use vs. zoning
- ▶ Support for a City-owned public space within WUC as an intentional space designed to bring people in
- ▶ Support cultural amenities, art galleries and installations and public open spaces near small scale commercial/retail areas in the WUC to support walkability and community connection
- ▶ Questions about why certain land use designations are selected, (i.e. Institutional vs. Natural Assets and Public Parks)
- ▶ What opportunities will there be to designate new industrial areas with the proposed Industrial Land Use Study
- ▶ General support related to the Growth Concept and Centres as a way of creating core urbanized areas with various services available throughout the city
- ▶ Feedback related to being discouraged in participating in public consultation activities as some residents feel “decisions have already been made” with some projects



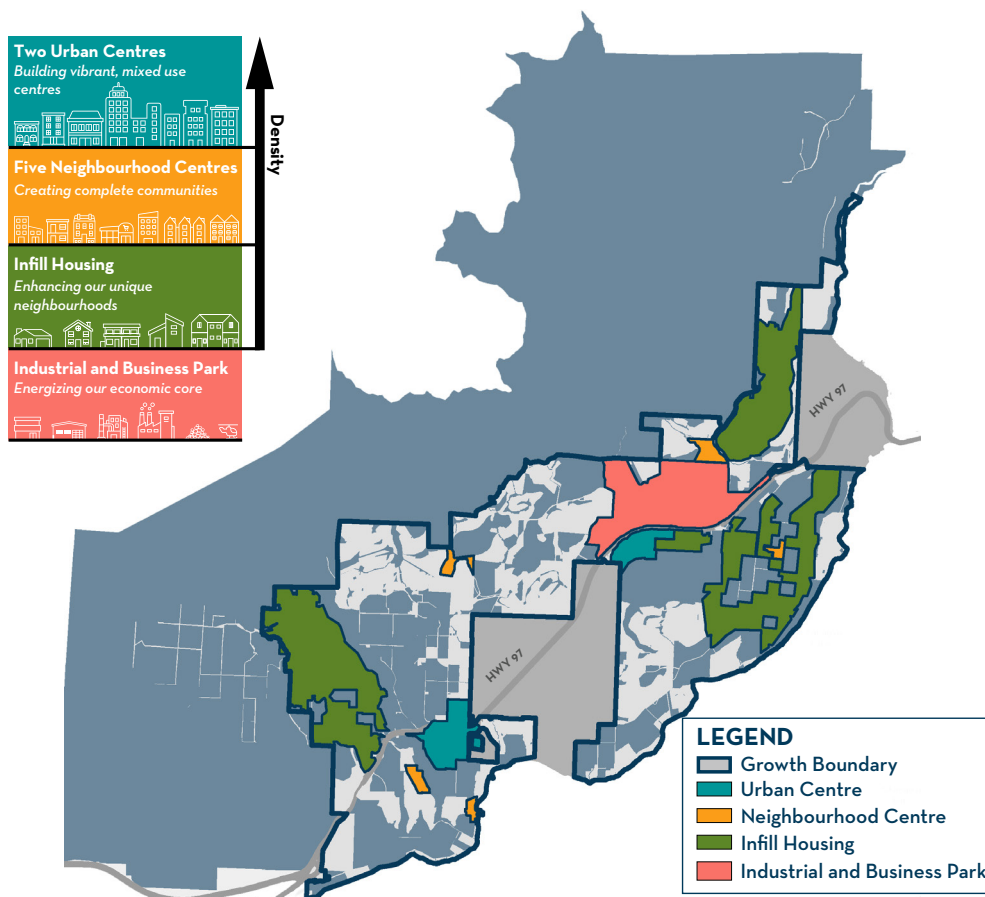
2.2 ONLINE FEEDBACK FORM RESULTS

Question 1: Do you agree or disagree that establishing formal growth boundaries to ensure growth remains focused within the Identified Growth Concept areas reflects a shift to a vibrant, walkable and connected community?

Participants were asked to scale their response between (1) disagree to (4) agree.

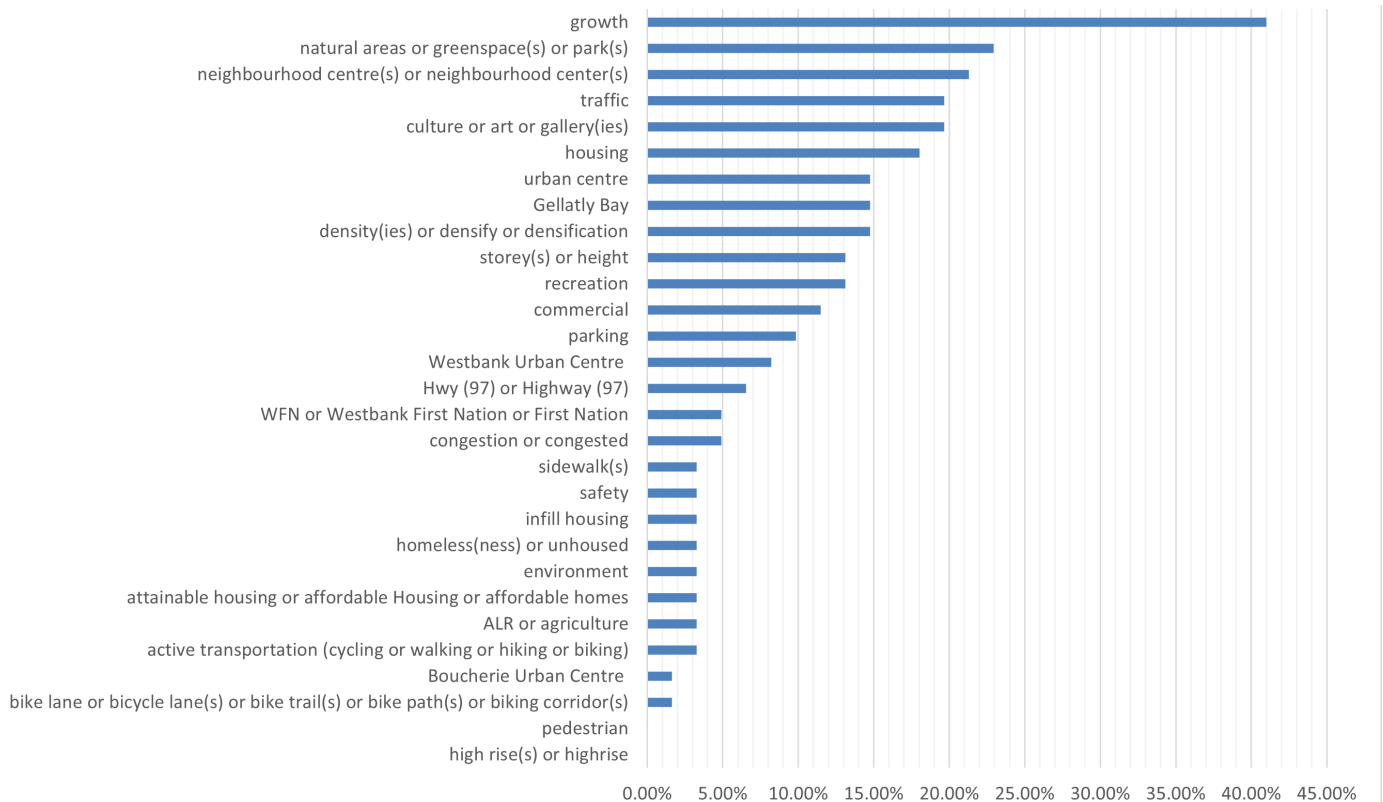


138 Respondents



Question 2: Tell us more about the growth boundaries and focused growth in the Growth Concept areas?

Common topics and how frequently were they noted in the **61 written responses**:

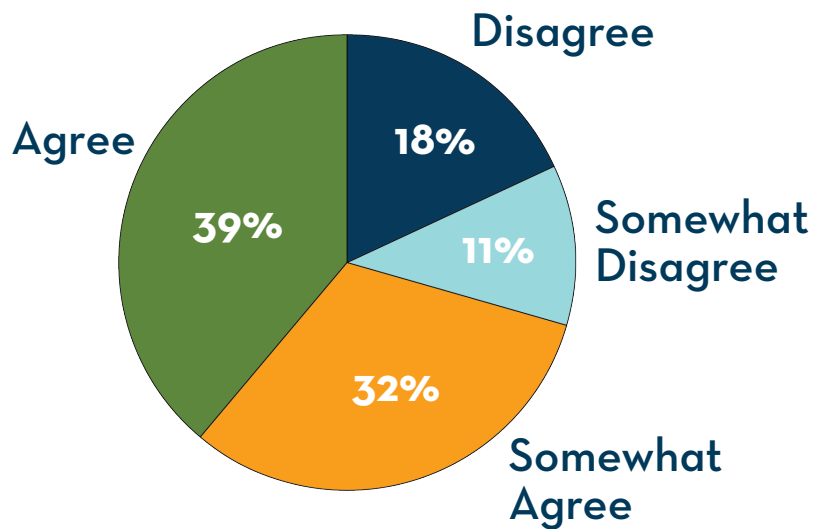


Common themes being expressed:

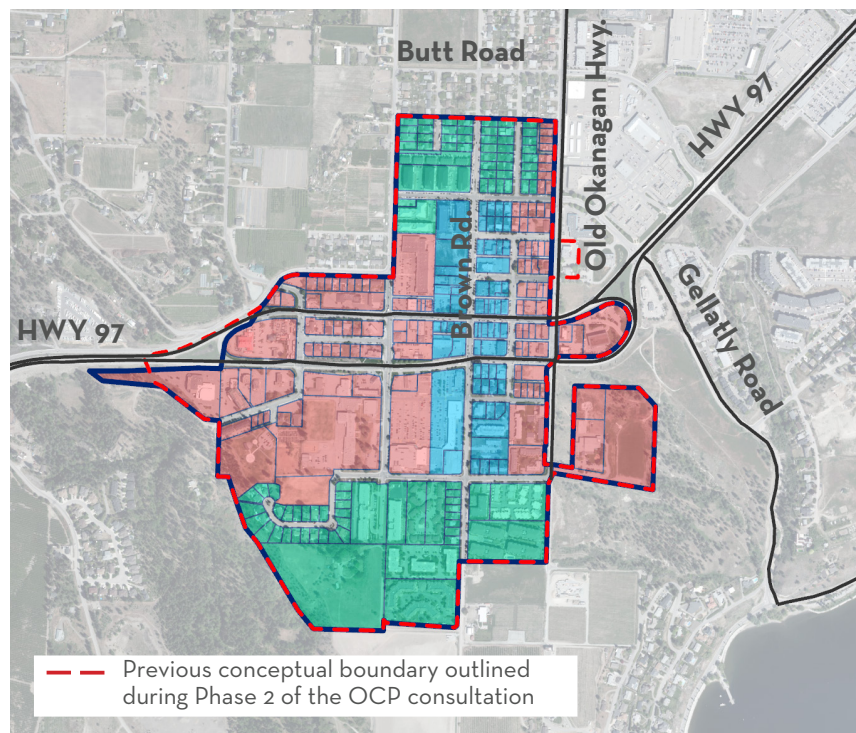
- General support for density planned for Urban and Neighbourhood Centres to concentrate growth and decrease urban sprawl.
- Specific concerns noted for the Gellatly Neighbourhood Centre related largely to potential impacts to the environmental and recreational area, traffic flow and pedestrian movement.
- Important that the City preserve natural and riparian areas, waterfront and other recreation areas for both people and wildlife for the future.
- Services, amenities and accessibility are key elements to the Urban and Neighbourhood Centres, including businesses, art/culture, recreation/sporting opportunities within a walkable area.
- Concern with the type, variety, and cost of housing available to support all residents needs.
- General questions about appropriate heights and where these might be appropriate, 4 storeys vs 6 storeys, vs 12 storeys.
- Concern with traffic congestion noted in certain areas with overlapped concerns with parking, safe pedestrian/cycling, and whether the road system will support the proposed densities.

Question 3: Do you agree or disagree that the additional heights and densities proposed up to 12 storeys in Westbank Urban Centre with a mix of residential, commercial and mixed use buildings will create a vibrant urban centre with increased economic opportunities and increased access to amenities?

Participants were asked to scale their response between (1) disagree to (4) agree.

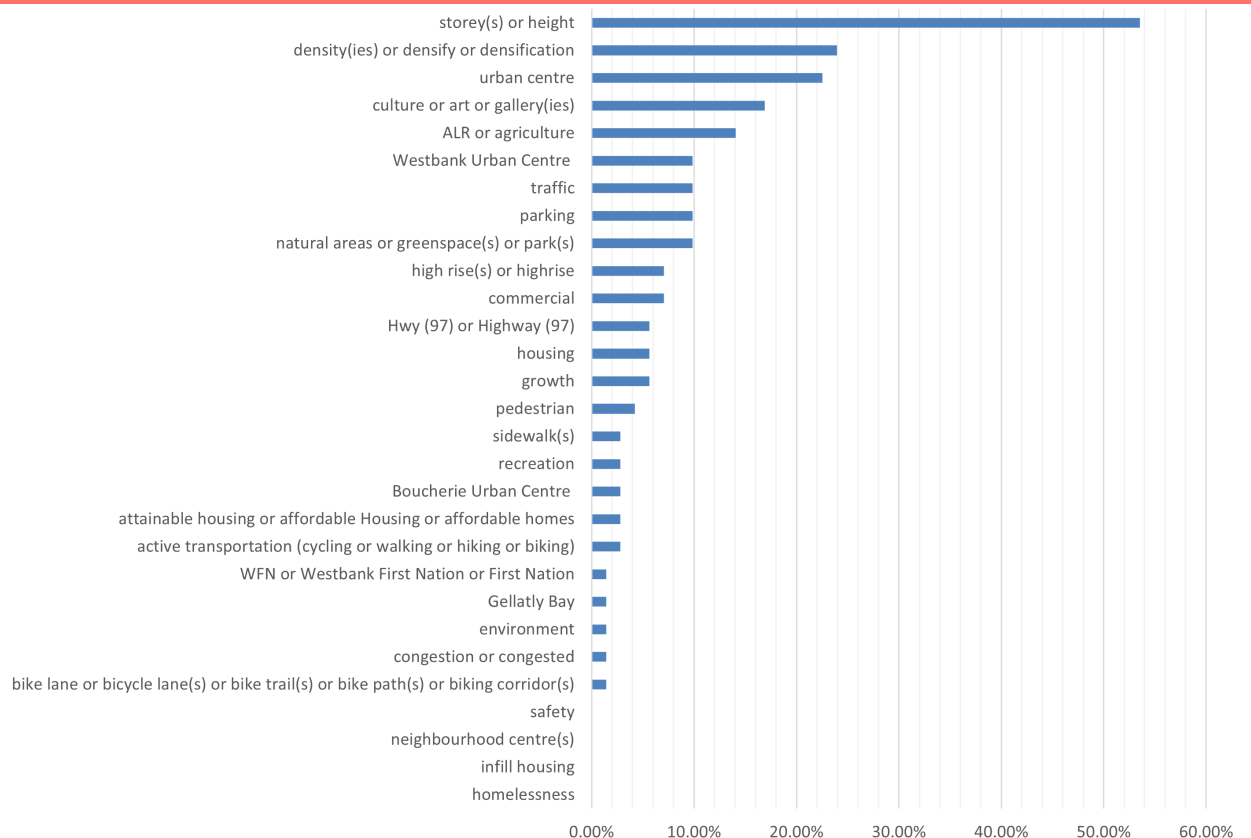


142 Respondents



Question 4: Tell us more about the proposed Westbank Urban Centre?

Common topics and how frequently were they noted in the **71 written responses**:

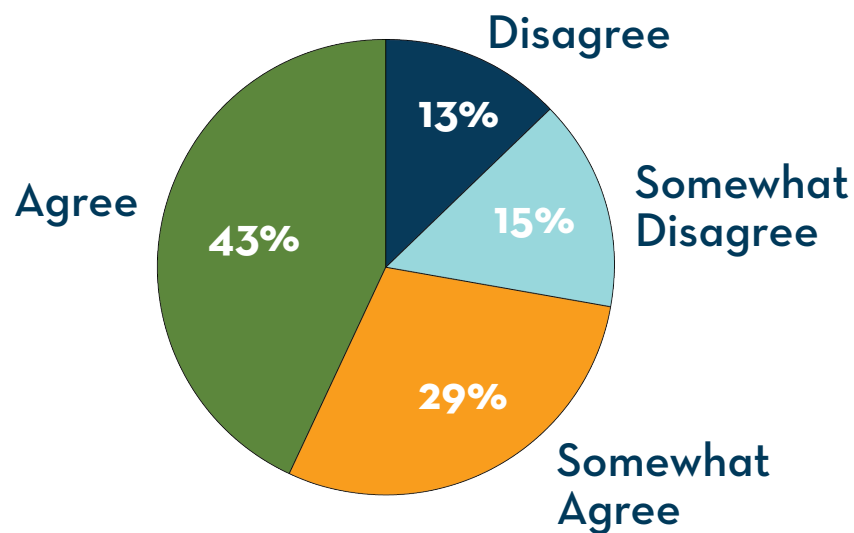


Common themes being expressed:

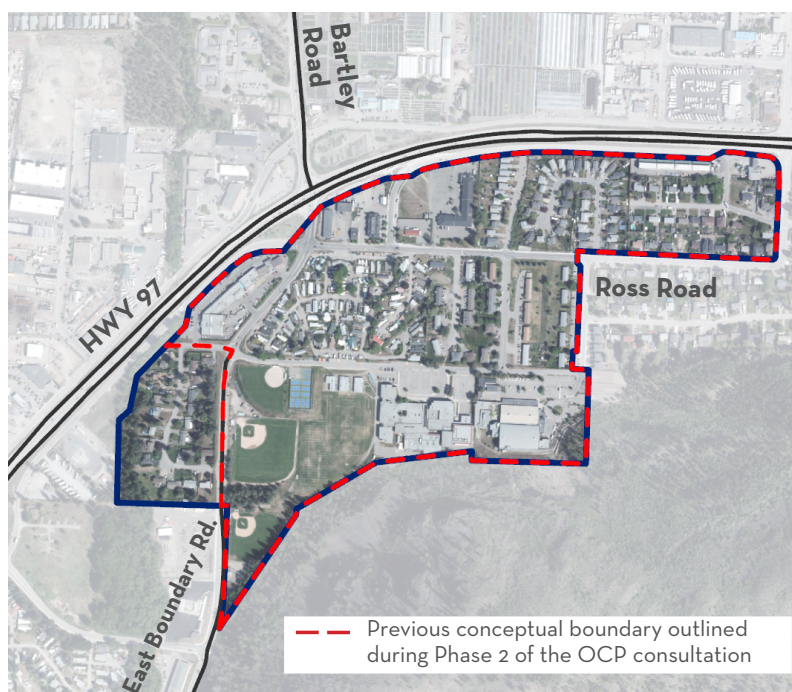
- Density/height may be acceptable where it comes with good design, enhances walkability, and provides a level of services and amenities to support art/culture, recreation, commercial, and public spaces.
- General support for the location and need to create a town centre with commercial at ground level with higher residential heights above to support the future growth of the City.
- Concern that the residential heights do not address affordability or housing variety issues.
- Questions as to the appropriate heights for WUC - comments ranging from support for 12 as proposed, to those wanting less than 12, and others wanting flexibility for greater heights where appropriate based on specific circumstance.
- Concern regarding the appropriate transition to existing lower density residential areas, the view impact of taller buildings, and the visual aesthetic of taller buildings on the character of our community.
- If additional density/height is considered, concern that adequate infrastructure is provided to support (roads, servicing, sidewalks, trails, transit, etc.), and specifically that parking is reviewed.
- Support for reducing the impact of Hwy 97 - consider options to remove separation in WUC.

Question 5: Do you agree or disagree that the additional heights and densities proposed up to 6 storeys in Boucherie Urban Centre with a mix of residential, commercial and mixed use buildings will create a vibrant urban centre with increased economic opportunities and increased access to amenities?

Participants were asked to scale their response between (1) disagree to (4) agree.

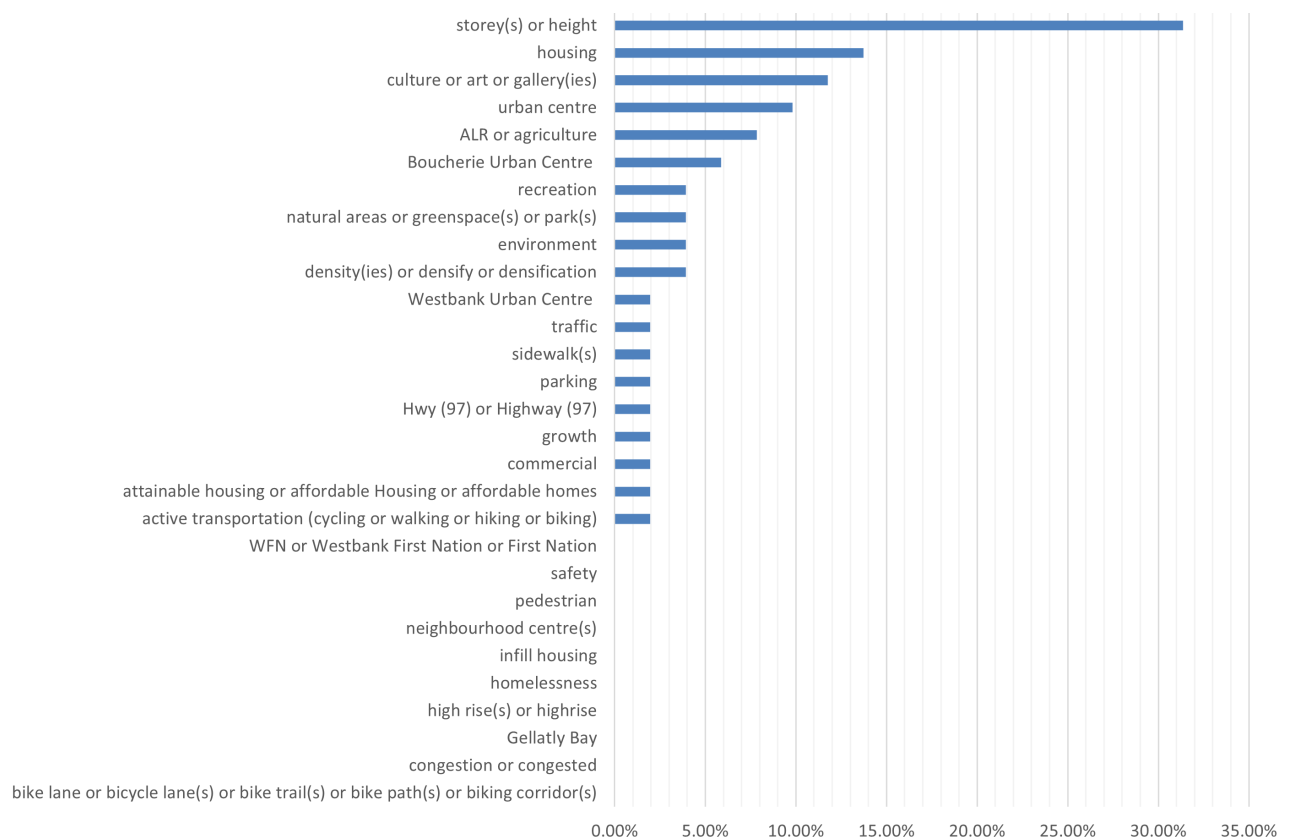


140 Respondents



Question 6: Tell us more about the proposed Boucherie Urban Centre?

Common topics and how frequently were they noted in the **51 written responses**:

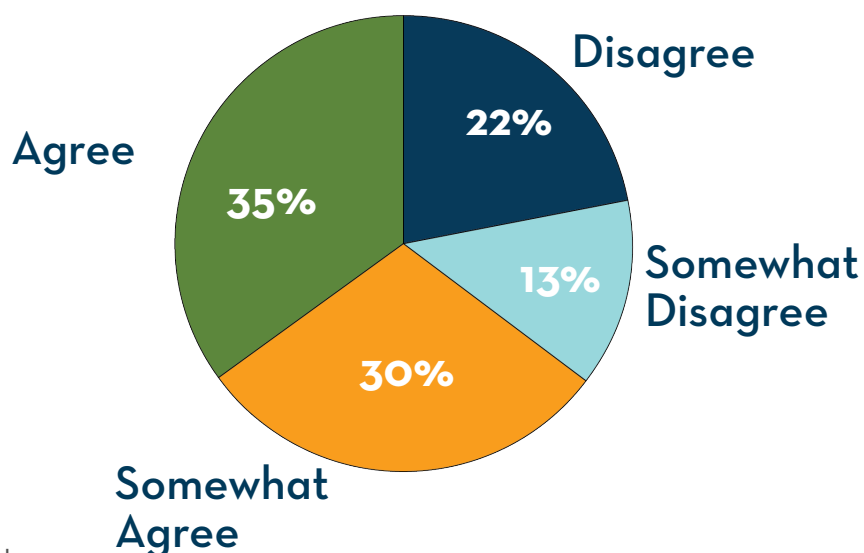


Common themes being expressed:

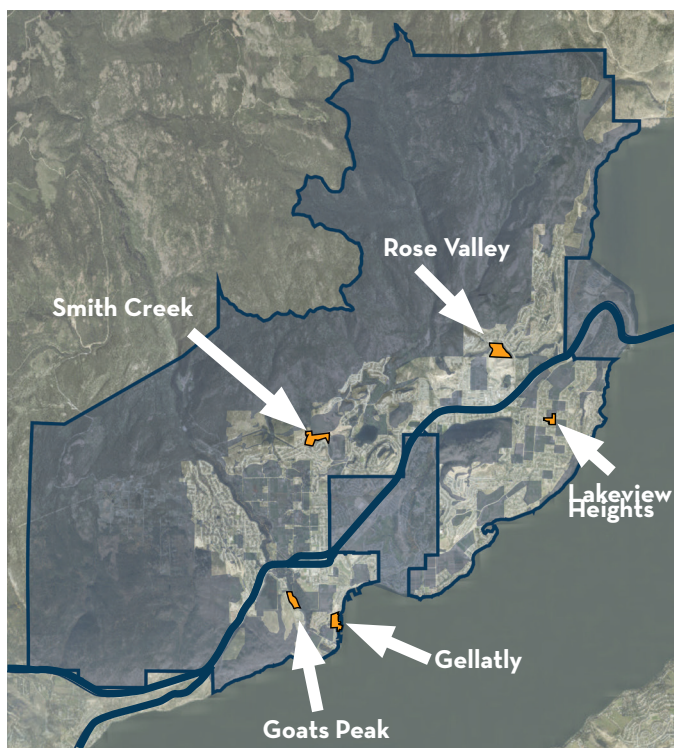
- General support for BUC as a good location for growth between WUC and bridge, with reasonable scale and providing a good mix of commercial and residential with access to public transportation.
- Consider preserving the area as a walkable recreation hub for youth and seniors with sidewalks/connected trails to Mt. Boucherie, access to art/cultural centres, and indoor/outdoor recreation services/sports.
- Concerns regarding housing availability, attainability, variety, type and quantity, and the potential redevelopment pressure or impact on existing lower cost rental and mobile home parks within the area.
- Some question regarding the appropriate height for this area - ranging from general support for the proposed 6 storeys, to some that prefer less than 6 storeys, and to some that support even taller 12+ storeys given the location has less impact on lake views, especially compared to WUC.
- Proposed density/height needs to be supported by aesthetically pleasing mixed use (shopping/services/amenities) and adequate infrastructure, such as transit, sidewalks, parking, road network, to avoid congestion and promote walkability in order to attract residents and visitors.
- Concern with the competition with WUC creating vacant commercial spaces, how to improve the aesthetic of the area, and the appropriate transition to existing lower density residential areas.

Question 7: Do you agree or disagree that the additional heights and densities proposed up to 6 storeys in Neighbourhood Centres with a mix of residential, commercial and mixed use buildings will create a vibrant neighbourhood centre with increased economic opportunities and increased access to amenities?

Participants were asked to scale their response between (1) disagree to (4) agree.

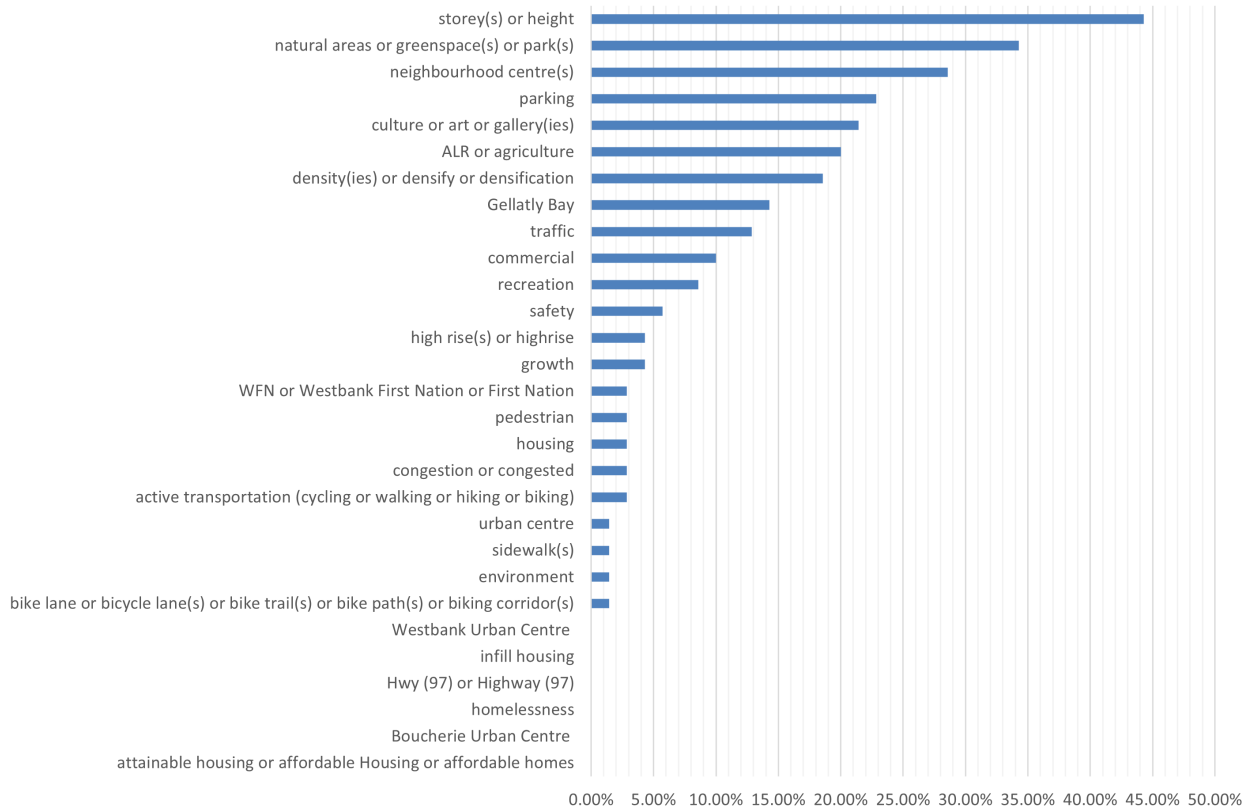


141 Respondents



Question 8: Tell us more about the proposed Neighbourhood Centres?

Common topics and how frequently were they noted in the **70 written responses**:

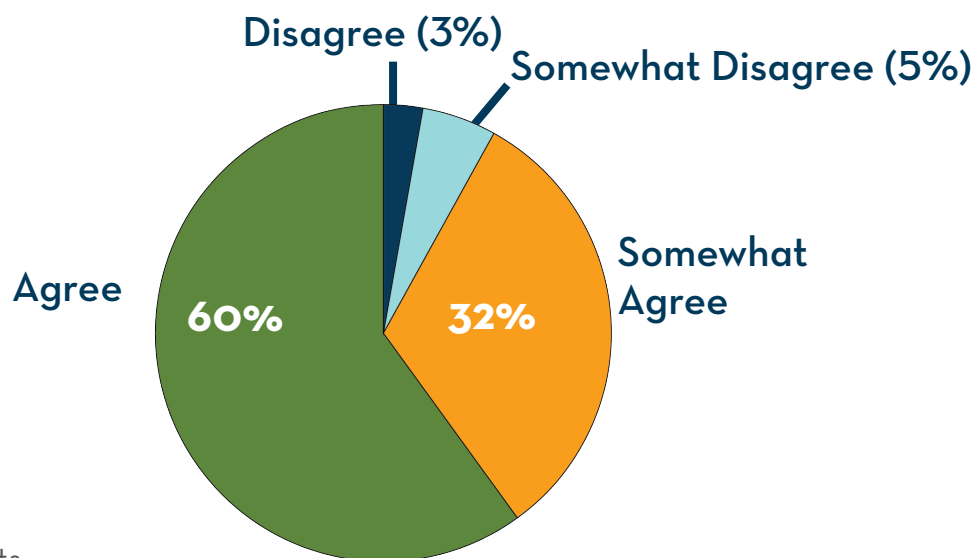


Common themes being expressed:

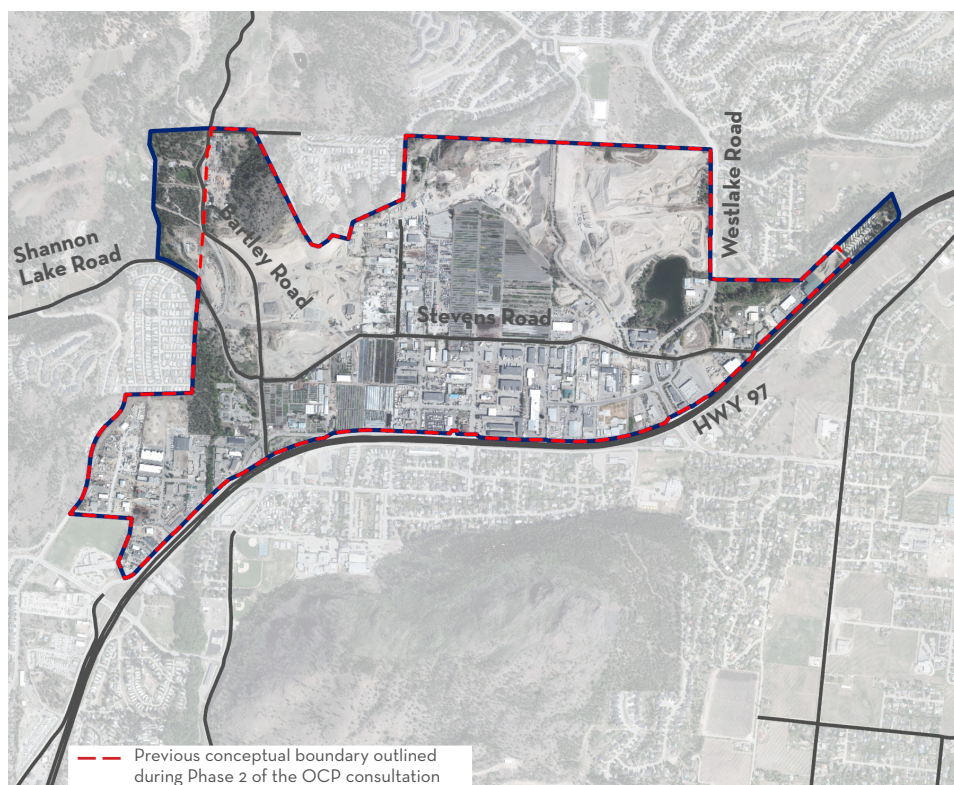
- General support for Neighbourhood Centres (NC) noting that mixed use is essential to the success of creating a walkable neighbourhood, with the composition of uses tailored to the appropriate scale and focused on the local nature of service commercial/retail options for people to visit, shop, and dine out.
- Some question regarding the appropriate height for the NC's - ranging from general support for the proposed 6 storeys, to some that prefer less than 6 storeys, and to some that support even taller 6+ storeys to accommodate the City's growth, but noting that negative or partial support comments were most often related to Gellatly NC.
- Specific feedback noted for Gellatly NC from adjacent residents - concern with density/height related to view impacts, traffic congestion and parking issues, potential impacts to beach/recreation/wildlife area along with pedestrian/cyclist access to what is perceived as an already busy area for existing residents/visitors, where height, parking and traffic impacts were the most common elements.
- NC's need to provide amenities, services and pleasant outdoor spaces for people to gather, to reflect the look and feel of each neighbourhood, and to ensure that adequate infrastructure supports the walkability through appropriate road network, transit services, sidewalks, bike lanes, servicing, fire protection, and trails and connections to recreational opportunities.

Question 9: Do you agree or disagree that OCP policies support and encourage the development of the business park and industrial land in the economic core to help increase economic sustainability?

Participants were asked to scale their response between (1) disagree to (4) agree.

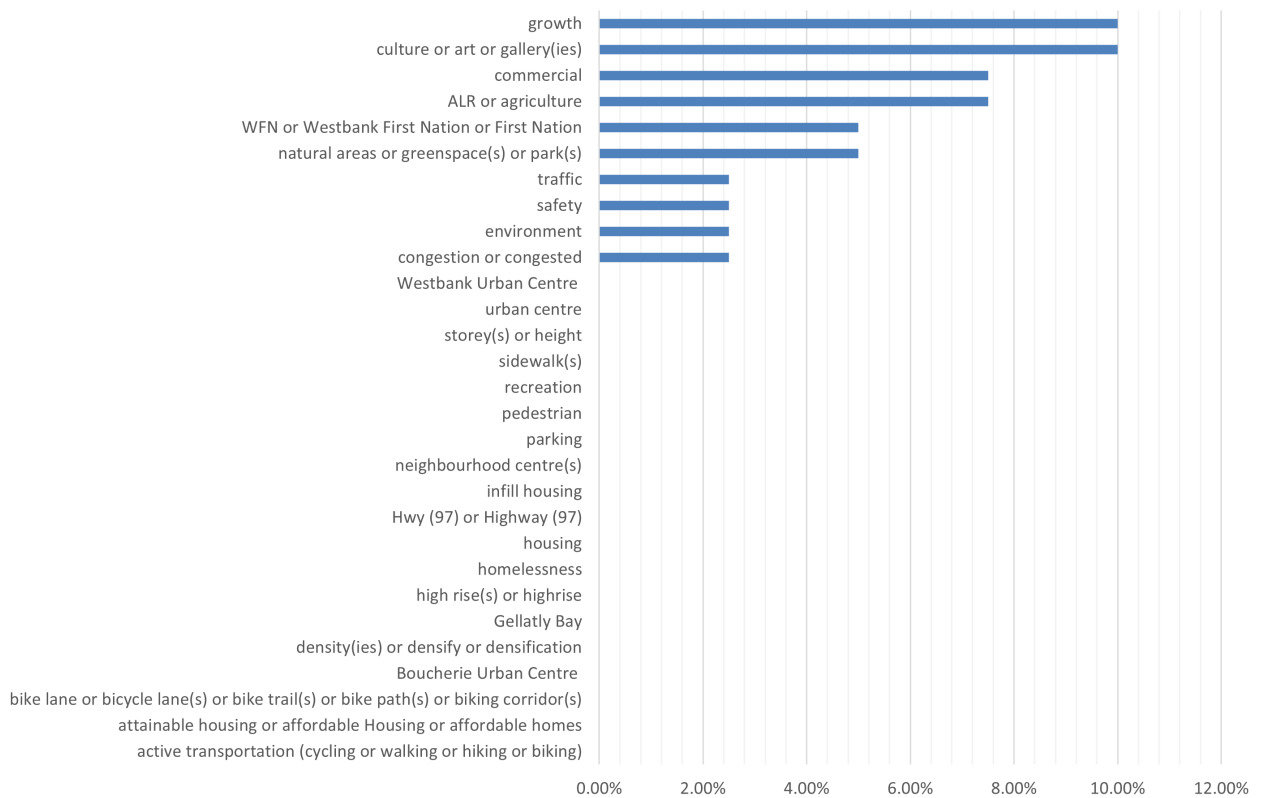


135 Respondents



Question 10: Tell us more about supporting and encouraging the business park and industrial lands as an economic core?

Common topics and how frequently were they noted in the **40 written responses**:

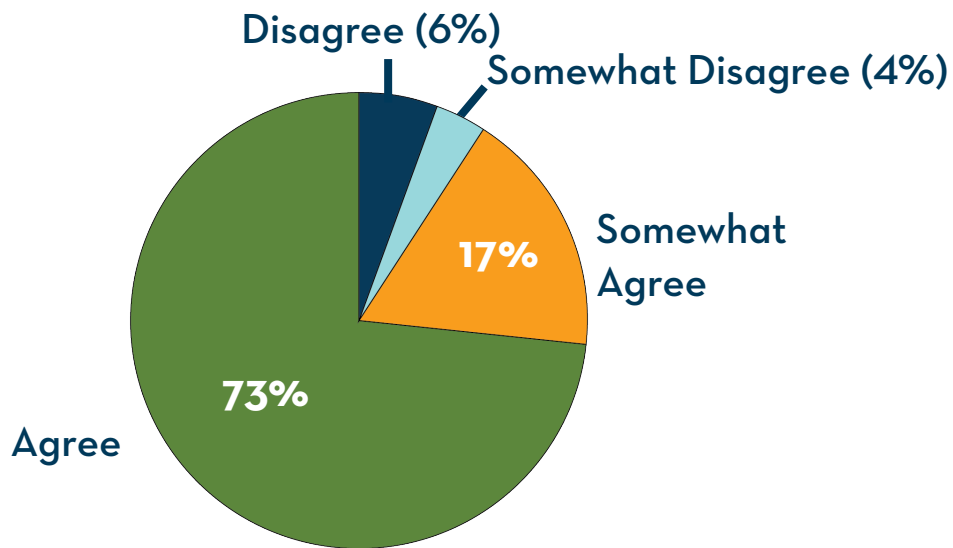


Common themes being expressed:

- Very strong support for location and focused business park/industrial land uses to support the economic development of the City and to generate more local jobs, but desire to improve the overall aesthetic.
- Support proposed Industrial Land Use Study to examine additional area, to create appropriate transition plans for existing gravel pits and to enhance the existing area along with the road network (local roads and highway congestion) and other necessary infrastructure.
- Support for the City as a leader to support development/promotion of emerging manufacturing and technology that focuses on more sustainable practices with less environmental impacts.
- Support to maintain the integrity of industrial lands for large scale employers/business rather than allowing for retail encroachment, with some alternate comments that the area should be allowing for more retail use/cultural and recreational areas.
- Concern with the appropriate transition from more intensive land uses to the surrounding community, especially where adjacent to residential areas, and considering noise, light, air, chemical, etc. impacts.
- Concern with impacts to and from the existing temporary emergency shelter on the development of the area.

Question 11: Do you agree or disagree with protecting and preserving major creeks, streams and lakes, and enhancing green space opportunities in the community through land use buffers and other green space policies?

Participants were asked to scale their response between (1) disagree to (4) agree.



138 Respondents



Identify new land use buffer that will protect and preserve major creeks, streams and lakes



Collaborating to steward these lands and waters



Protecting and connecting green and blue spaces for future generations



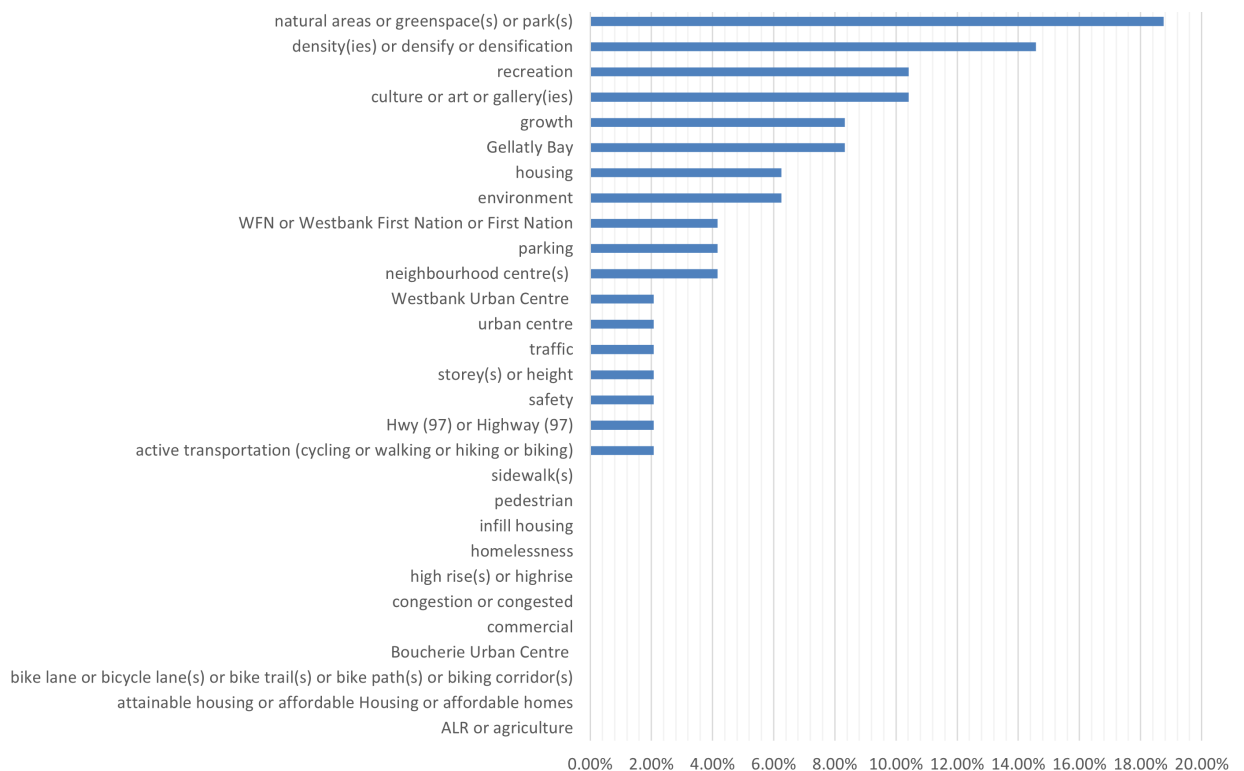
Expanding our connected, accessible and vibrant waterfront



Majoros Pond Park

Question 12: Tell us more about protecting and preserving waterbodies and enhancing greenspace?

Common topics and how frequently were they noted in the **48 written responses**:



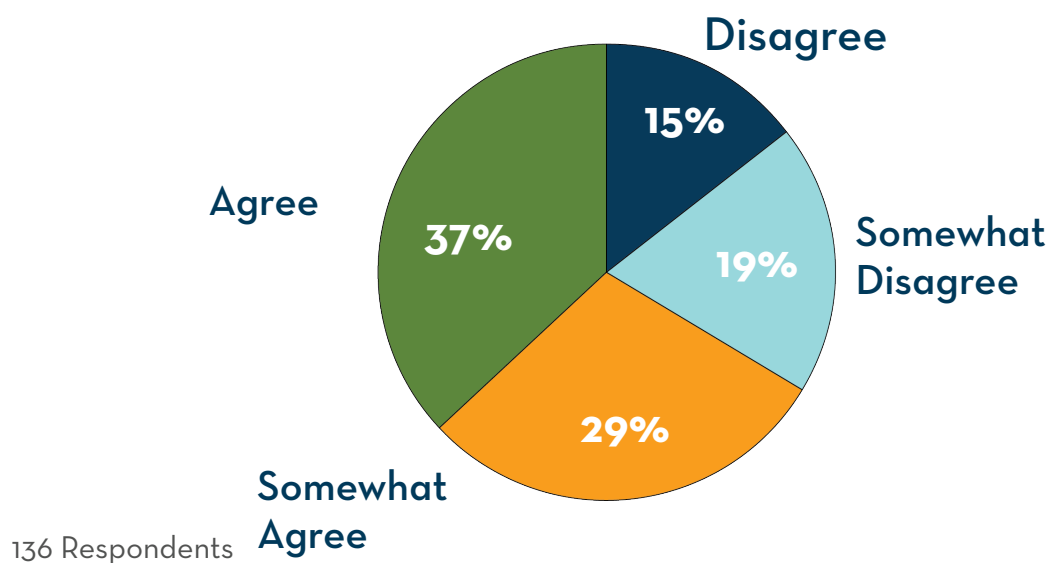
Common themes being expressed:

- Overwhelming support to protect and preserve waterbodies and greenspaces as essential to not only sustain our natural ecosystem, but to act as a valuable resource that contributes to a way of life and the community aesthetic and beauty allowing for places to play, exercise, gather, and rest.
- Interest in not just protecting existing greenspace and waterbodies, but to be a leader by adding to our greenspace or protected areas to provide for additional wildlife areas, resident recreational areas, and natural areas for their intrinsic environmental values and contribution to community well-being.
- Increase access to waterbodies and greenspaces, including connection to and between existing areas with enhanced walkways, sidewalks, and nature trails, as well as considering distribution across the whole community to increase equity and reduce overcrowding on beaches and greenspaces.
- Proposed density within the overall plan should be supported by greenspace, as well as consider the appropriate road network, bike paths, sidewalks, and trails along key recreation corridors to support use by residents, visitors, and the tourism industry where growth is proposed.
- Support policies to require the use of drought tolerant plant species and consideration of water saving measures within our greenspaces, as well as protection of existing tree canopy wherever possible.

Question 13: Do you agree or disagree that the Draft OCP Plan Update addresses these four focus areas:

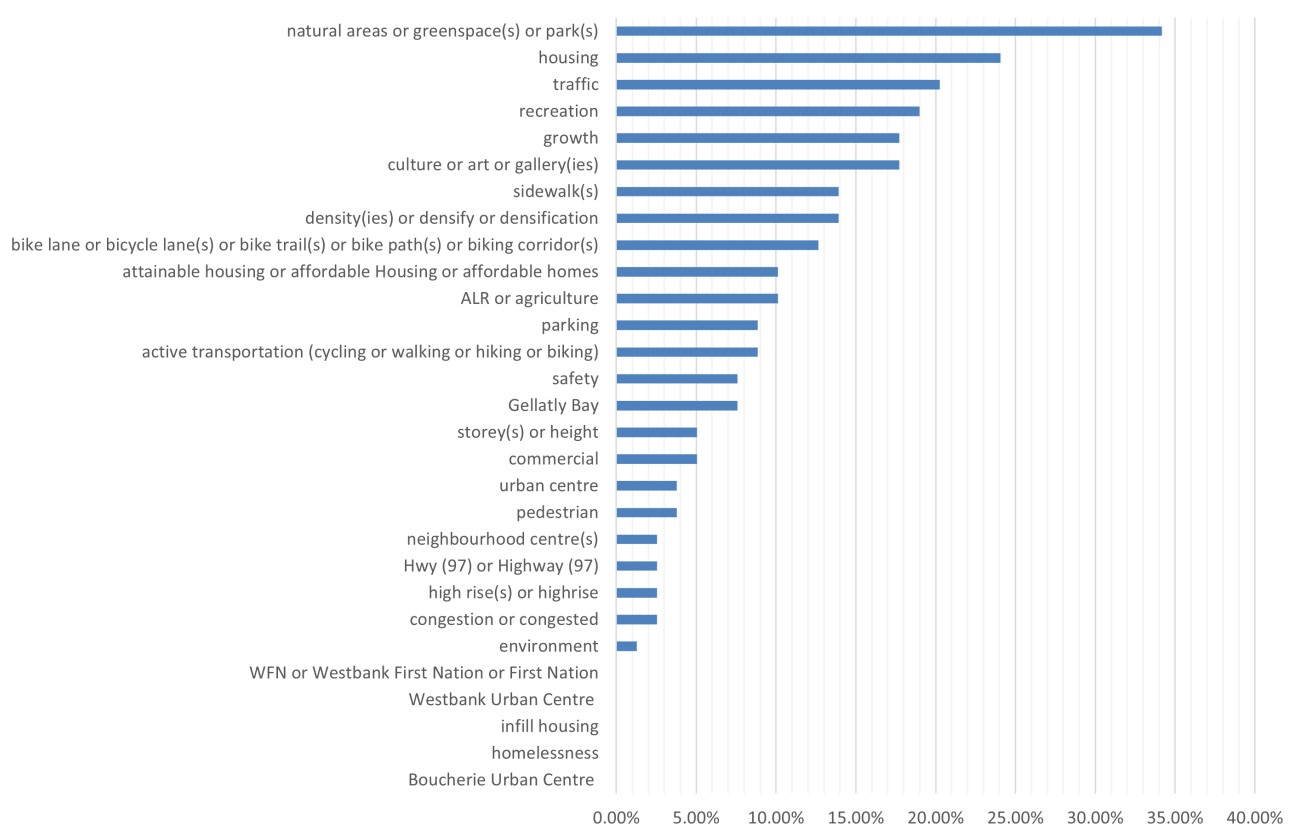
- 1) Establishes growth boundary as per Growth Concept?
- 2) Creates vibrant walkable Urban and Neighbourhood Centres?
- 3) Supports and encourages Business Park and Industrial land as economic core?
- 4) Protects and preserves waterbodies and enhances greenspace?

Participants were asked to scale their response between (1) disagree to (4) agree.



Question 14: Tell us if you feel there is anything missed that may contribute to the success of the Growth Concept?

Common topics and how frequently were they noted in the **79 written responses**:



Common themes being expressed:

- Strong support for parks - comments asking for larger park areas for ecosystem diversity, leaving more areas in their natural state, improving existing parks, adding and preserving waterfront park area, more access for recreation, and adding more small boat access (kayaks, canoes, etc.).
- Consider transportation options such as bike lanes, sidewalks, and transit as a priority, as well as hiking, biking and walking trails to support access to schools, connect neighbourhoods and along corridors that support tourism and economic development.
- General support for WUC, but concern noted about highway separation of the area, as well as overall concern with highway congestion, potential bypass options, and second bridge crossing to Kelowna and the potential impact on the proposed Centres throughout the community.
- Specific areas noted for potential road safety improvements, from vehicle to bicycle to pedestrian focus.
- Consider additional community amenities for all ages (community centres, arts, culture, pool, arena, etc.).
- Concern with the provision of housing availability and affordability, noting variety options as important.
- Concern with the proposed density and heights at Gellatly NC, especially waterfront impacts.



2.3 UNSTRUCTURED FEEDBACK



EMAIL AND WEBSITE INQUIRIES/RESPONSES

The public had additional opportunities to ask questions about the draft OCP through general email and the City website.

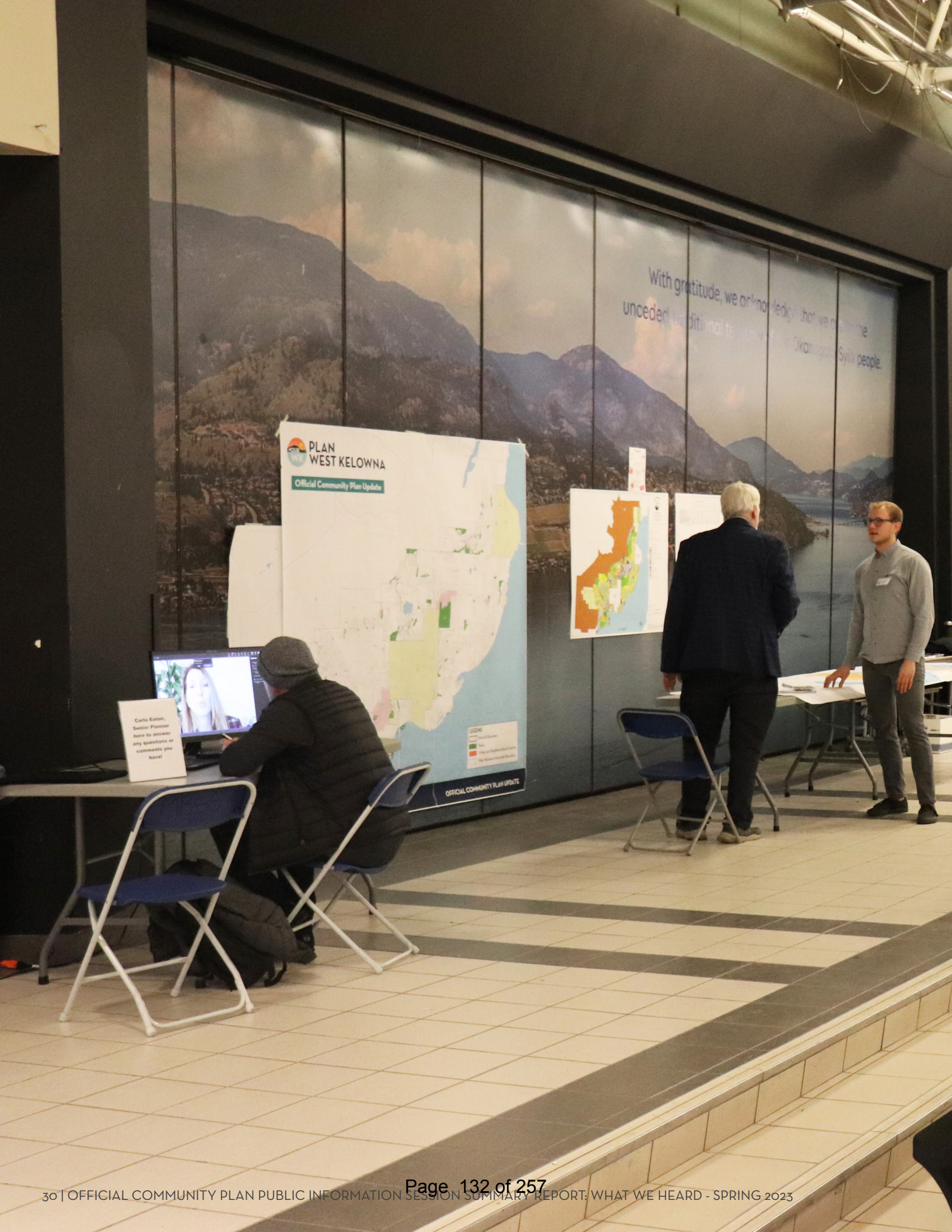
The following common themes and general questions were communicated to staff:

- ▶ Create an intentional vision for WUC that goes beyond basic form and character to tie in key cultural elements with commercial services to create public gathering spaces for all-season use and connected by walking, biking, transit, or shuttles away from parked cars.
- ▶ Support indoor and outdoor cultural, art, food, and farmers market venues with street performers and festivals to create beautiful and functional activated spaces, including consideration for design competitions that include art and garden spaces to attract tourism to West Kelowna.
- ▶ Request to remove the Gellatly area from the NC land use designation, or to revert to the Gellatly Village concept within the current OCP, based on concerns regarding the proposed density and heights at Gellatly Bay NC, with potential:
 - Impacts to Willow Beach and access to this recreational corridor due to overcrowding,
 - Traffic impacts along Gellatly Road and associated lack of vehicle and boat trailer parking areas to support the recreational area and proposed future mixed use developments,
 - Impact to the character of the neighbourhood if short term rentals are permitted,
 - Abuse of the extraordinary community benefit policy by the proposed developments to allow higher density and height with the provision of parking areas that will not help alleviate the current public vehicle and boat trailer parking issues within the area, and
 - Blasting/pile driving impacts associated with proposed higher density developments adjacent to existing homes from deeper foundation/piles to support the taller buildings.
- ▶ Specific Mapping requests:
 - Some concern expressed over the way that park and institutional land use designations are not identified within Urban and Neighbourhood Centres, despite those uses being permitted within both designations.
 - Request for the City to consider providing mapping details for the Westbank First Nation lands to enhance connectivity between our planning processes.

► Specific Land Use Designation or Growth Boundary requests:

- Concern with City-owned properties at Harmon Road and concern that either institutional or medium density residential designations may have an impact on adjacent properties if either a low-income rental facility, or a medium density residential development were permitted. Some residents expressing interest in also being permitted to develop as a medium density residential area if the City continues with its current medium density residential land use designation for the City-owned Harmon Road lots.
- Specific concern noted with a property located along the transition area between a higher density mixed use land use designations and the Agricultural land use designation, where the agricultural property is requesting some consideration for flexibility in the future land use based on their current rural residential zoning.
- Specific requests to amend a proposed Rural Residential land use designation to a Low Density Residential designation.
- Specific requests to amend the Growth Boundary to include a number of adjacent properties lying adjacent to the boundary, and for the City to consider servicing areas lying outside the proposed Growth Boundary.





3 NEXT STEPS

"A thriving community is one that has green space and ways for community members to connect in meaningful ways. We need to encourage and create maker spaces and places for people to come together in a spirit of connection and forming a safe, respectful and equitable community." - Participant

OCP UPDATE

Policies to Implement the Community Vision

Policies in the OCP seek to achieve the OurWK Community Vision for the year 2040. These policies are also linked graphically in the Community Vision and Foundations illustrated below.

The policy sections include topics such as:

- Transportation
- Community Infrastructure and Servicing
- Climate and Environmental Resiliency
- Natural Environment and Hazards
- Social Sustainability
- Economic Sustainability
- Collaborative Governance
- Financial Foundations



Caring and supporting one another and celebrating our diversity



Creating a healthy community that supports healthy individuals



Engaging young minds that will become future leaders

ADDITIONAL FEEDBACK

In addition to the feedback highlighted in this WWHR, an accompanying Council Report will be provided with the formal consideration of the draft OCP (1st reading anticipated Spring 2023). As part of the Council Report package, additional detailed summaries will be provided from the formal referral process, as well as feedback from Council's advisory committees. The report will include an analysis of the feedback received to date, as well as a summary of any resulting recommended revisions included within the draft OCP being presented for first reading.

WHAT'S NEXT?



LAND USE PLAN REVISIONS AND OCP DRAFTING

Community input received as part of the Phase 4 public engagement in combination with the formal referral responses from various agencies and formal stakeholders will be used along side Council direction and best practices to inform the update of OCP policies and land use plan for Council's consideration.

Following potential first reading, a formal Public Hearing will be scheduled to received feedback on the draft OCP as per the *Local Government Act*.

A scenic landscape photograph showing a valley with a town in the foreground, surrounded by mountains and forests. The text is overlaid on the right side of the image.

The Growth Concept determines where and how we grow and forms the foundation for the City's Land Use Plan, which will help the city manage development and bring the community's vision to life.



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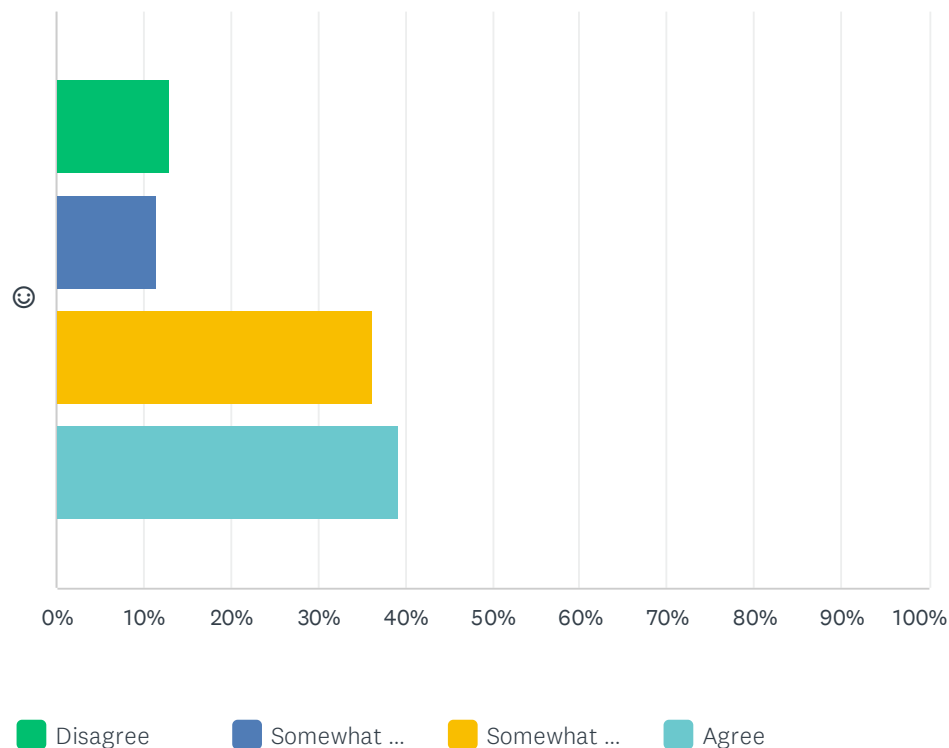
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Appendix A: Feedback Form Individual Response Table

Q1 Establish formal growth boundaries to ensure growth remains focused within the identified Growth Concept areas. View the Growth Concept display board. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 138 Skipped: 25



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	13.04% 18	11.59% 16	36.23% 50	39.13% 54	138	3.01

Q2 Tell us more:

Answered: 61 Skipped: 102

#	RESPONSES	DATE
1	Re: Concentrating growth around Brown Rd as one of the Urban Centre's. That area is already overly congested during rush hours and tourist season. Adding more foot traffic controlling the lights and vehicles will make it a nightmare for those traveling through. Can the Urban Centre be on one side of the hwy, not spanning across both lanes of hwy?	2/7/2023 11:19 PM
2	I agree with densifying and "growing up, not out" and creating a town center which attracts unique businesses and people. Concerned with land use in downtown areas currently designated residential but planned for commercial and/or multi-story- how this impacts residential homeowners with respect to resale, increase in crime related to densification. Also concerned with rate of growth- can we keep up with the pressures of more people while maintaining (or improving) a safe environment for people/wildlife and not exerting too much pressure on natural ecosystem values which ultimately benefit quality of life and cost of living (water quality, high water capacity along creeks and shorelines, urban heat island, light/noise/chemical pollution, appropriate waste management, etc).	2/7/2023 9:54 PM
3	The growth of our community should not be restricted to be only within a designated area. This is not a healthy manner in which to encourage and nurture a young City's growth. The City needs to respect all stake holders and their specific interests within the community in order to encourage unity and prosperity. Although a City may wish to focus on a designated areas of growth it is still obligated to provide direction and guidance for all areas of the City. The City should at the very least ammend the OCP to reflect all current uses thru out the tire City boundary.	2/7/2023 9:42 PM
4	I love the neighborhood centres . I think those are essential and key for Westbank culture! They are so convenient for a large population without developing into a huge city , smaller hubs create more community !! I love it : Love the industrial park . I don't love the city centres , I think the neighborhood centres create more culture . As well as creating higher more housing does not make it any more affordable or available . Cities are the most espensive places to live in Canada , even with the most dense buildings . Most people want space and privacy, that should be attainable for everyone - rich or not .	2/7/2023 9:01 PM
5	Please see summary in Item 4 below.	2/7/2023 4:10 PM
6	The growth concept as it pertains to the beach area on Gellatly Bay should not have dense housing as it taxes the integrity of the natural area. It adds to much traffic	2/7/2023 2:22 PM
7	- Concentrating growth in some areas works but in others it is detrimental. eg, Gellatly Bay cannot handle growth and concentration there. It will seriously hinder it's primary function as our recreational centre - If you want Westbank Centre to succeed, you cannot put it in competition with nearby neighbourhood centres offering the same amenities eg. Gellatly Bay	2/7/2023 2:08 PM
8	Yes - Westbank centre - the Growth Boundary - offers many avenues for opportunity	2/7/2023 2:02 PM
9	Growth is not what this community needs	2/7/2023 2:00 PM
10	I agree for providing a range of housing types but not high density in areas that are already at their limit. ie. Gellatly Bay. High Density in these one way in and out is not reasonable for the enjoyment of now and future users.	2/7/2023 1:50 PM

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11	The proposal for the Gellatly Neighborhood Center is too broad. There should be no buildings higher than 4 storeys along Gellatly Rd. The proposed development across from Willow Beach is absurd! It is too high and too dense and will have a serious negative effect on the popular beach. This is a wonderful recreation area for all the people of West Kelowna. During the summer months the beach is packed with people. Gellatly Road is lined with vehicles all the way to Whitworth Road. Developments such as the one suggested across from Willow Beach at the present boat storage facility would hurt all the West Kelowna residents that currently use this recreational gem.	2/7/2023 1:39 PM
12	infill housing as long as well planned and studied in case by case.	2/7/2023 8:40 AM
13	Would like to see more for commercial & business to pay more tax, neighbourhoods in Gellatly can not expand without more parking around water, walking biking safe lanes, boat trailer parking, we don't need a huge development taking up the space...it's another greedy developer like cassaloma that won't work...more senior and affordable housing...	2/6/2023 7:11 PM
14	I agree with concentrating growth in appropriate areas, but am concerned that the neighborhood centres be thoughtfully scaled, so as to respect the neighbouring communities. In particular the Gellatly Bay Neighborhood Centre needs to be significantly scaled down in order to retain this area as the primary recreational corridor for the people of West Kelowna. More parking will not support this goal. Safe bike paths and sidewalks will be more consistent with accommodating recreational use.	2/6/2023 3:50 PM
15	Since the smiley faces are not working on my survey, I will answer here :) Somewhat agree - It is difficult to see where the Green "Blobs" fall as there are few roads/admin boundaries to see where it is on the map. Also where does the OCP identify how the city will work with local FN's in coordinating planning on both respective lands? This is a gap that would be helpful. There is a very large development along the lake (Old Ferry Wharf road for example) ...there may be ways to coordinate and align the needs of the overall area, for example, providing small boat/canoe launch access to the lake, etc.	2/6/2023 2:46 PM
16	Concerned to see densification of up to 6 stories in the urban neighbourhood centers because the envelope of a structure this size is substantially bigger than that of a 4 story building which better harmonizes with existing single family dwellings. Densification up to 12 stories is appropriate in the urban centers only.	2/5/2023 11:30 PM
17	Willow Beach area should NOT be part of the neighborhood center plan. It is a widely used recreational corridor, and to add more population which means more vehicles and pollution. The creek and park are a huge part of this area, and allowing a development steps from it will most certainly damage this natural habitat. Please consider the value of the Gellatly Bay area as it is now, and do not overrun it which has nothing to do with improving it, and will only harm it. West Kelowna is very fortunate to have such an overwhelming beautiful area, which we should ALL be wanting to preserve. It is unique, special and once developed there is no turning back from such a mistake. The increased traffic along a 2-lane road with playgrounds and parks will put public safety at risk.	2/5/2023 10:20 PM
18	When considering growth the planners must take into consideration the network of roads leading into the areas. Most of our arteries are so busy. Proper arteries parking and public transportation must be carefully considered. I'm finding traffic congestion a real problem in the Gellatly road area turning off onto Witt road and Boucherie road.	2/5/2023 2:12 PM
19	We are grateful to be seeing a plan for more development in community centers in West Kelowna. A restructuring of the current one way traffic zone, would create a much more appealing community.	2/5/2023 8:08 AM
20	I believe the growth should be centralized to the urban centres. Sprawling out with higher density will put more pressure on fire and other services.	2/4/2023 6:48 PM
21	Whilst I mostly agree with the neighborhood concept, I cannot possibly see how Gellatly Bay could be designated as such. Gellatly Bay is not a neighborhood center, it is a recreational playground for the most of the Westside. I suspect the motive for designating it as such is to accommodate the developer of the present boat storage site's desire for additional height.	2/4/2023 4:31 PM

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Space on that site is limited - any additional parking he includes will immediately be snapped up by the residents of that development therefore adding no extra availability to the general public. The present summer gridlock down there will only become more dense gridlock! Also these apartments will be mostly be purchased by out of town residents who will utilize them as summer vacation homes and leave them empty in winter. As a result, during the slow months this development will add little to the local economy but in the summer, who will be first on the beach by nine a.m.? Willow beach is tiny - by the time momma has dressed and fed her kids in Westbank, Glenrosa or Smiths creek and made her way to the bay there will be no room at the Inn (beach)! I suspect the final result will be a lot of fulltime local residents very unhappy with the council that permits this to happen.

22	Growth just for the sake of growth and especially to satisfy developers and investors is not what the local population needs. We need to be able to access areas without finding it is already full of cars and having our crowded roads full of even more cars. We do not want to be a miniature Vancouver.	2/4/2023 3:54 PM
23	We are placing concentrated development in areas where our traffic systems are not designed to handle them. Concentrate those types of development where they have immediate access and egress to major arterial traffic	2/4/2023 3:19 PM
24	I don't want to see increased density in the Boucherie rd Willow beach area	2/4/2023 2:12 PM
25	While on the surface the concept seems sound, planners have not considered existing neighborhoods. The approach is too simplistic and cookie cutter. You shouldn't use the same formula for each neighborhood to ram density into areas that should remain more rural in feel. You shouldn't try to increase density in areas that can't handle the increased traffic due to existing development, ALR and geography (steep hills) just because your planners want to live there. It was clear that your planners and traffic engineers needed more time together and that your planners had a pre-existing agenda and were unwilling to hear feedback. Further the use of infilling was promoted but then no estimates were provided to confirm the amount of density that would provide. The concepts fail to consider the impact to the highway. I was told by your planner that it was moti's problem. Finally, there was no consideration of green space and other recreational amenities in the approach. West Kelowna already has some of the lowest green space per capita in BC.	2/4/2023 8:03 AM
26	I believe growth boundaries are very important. Some kinds of development cause damage that would take a very long time to heal, were that even allowed. Communities cannot be allowed to sprawl, and it is a self-destructive practice for humans to take all of the productive land for themselves. Please ensure this boundary is well enforced.	2/3/2023 8:05 PM
27	Growth concept - West Kelowna is not that big of an area, you can get to the main stores within a 10 min drive from almost anywhere in West Kelowna. Having some amenities in each neighbourhood is nice (local pub or convince store) but I would sooner see more activities (parks, pickle ball, hiking trails) than more shops etc. Two of the neighborhood centres are very close together (looks like Gellaltly rd area). What does infill housing mean? Not every neighbour hood needs to have a variety of housing. We bought and sold throughout the years to end up in an area that was houses of similar size and value. I have also seen smaller lots but still huge houses on them - majority with suites - how about smaller lots with smaller houses like our grandparents used to own. This would bring the house costs down for those not wanting a condo but also not wanting a huge house. In areas that have allowed the big houses with suites (ie Tallus Ridge, Smith Creek), parking is horrible, vehicles line all the roads.	2/3/2023 5:09 PM
28	Conceptually I like the idea of Urban and Neighborhood Centres as long as they take into account traffic,safety,parking and view considerations and take wildlife habitats into account. Also building heights should be no more than 6 stories so as not to disrupt lake and mountain views and our current town and country style of living. Also development should not out strip the infrastructure's ability to handle it such as Gellatly Road.I think the Gellatly Bay Goats Peak Neighborhood Association has admirably expressed many legitimate concerns.	2/3/2023 4:38 PM
29	Question Gellatly Bay as a neighbourhood centre as it is more of a recreational area	2/3/2023 3:39 PM

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30	I don't believe the city can plan for where growth will occur over the next 20 years. This is excluding a lot of land and potential for growth by cutting out certain parts of the city that contain a large amount of land. The city may have it's central focus on certain areas, but you cannot account for or predict the personal plans of the residents.	2/3/2023 3:38 PM
31	Important to avoid urban sprawl.	2/3/2023 2:55 PM
32	I live in the area east of Gellatly Rd, south of Carrington Rd, and north of Boucherie Rd. I question why our neighborhood was not included as a potential growth area. It has all the characteristics identified as an area that could achieve further density. High walkability, close to all amenities including shops, schools, parks, transportation, etc. With the 4 storey apartment blocks off of Carrington Rd and Majoros Rd and the townhomes slated for Wetton Rd, our neighborhood already has been forever changed in character from the formerly exclusively single family neighborhood it was. Would it not make sense to encourage duplex, row townhouse type of "in-fill" into our neighborhood now? I believe this is a missed opportunity if this area is overlooked at this critical time.	2/3/2023 2:51 PM
33	want more growth. want to see community hubs similar to Lakeview village, in Smith Creek, Rose Valley	2/3/2023 2:06 PM
34	A significant part of residential and most of the commercial developments and stores are on Westbank First Nations land. West Kelowna City must have more integrated planning with the Westbank First Nations Government Office so the 2 plans are compatible and complementary.	2/3/2023 8:59 AM
35	I am in favour of concentrating commercial development in the Downtown Centre. I am not in favour of commercial development at Gellatly Bay due to its waterfront location and narrow corridors, as well as its close proximity to the city centre. The city should be concerned about the commercial viability of small business due to competition between 2 designated centres within a 2 kilometre radius.	2/2/2023 11:34 AM
36	slower growth please and better accommodations for the residents that have paid there share of the town to date. why are the first nation accommodations better than those you have created. Many have disagreed with your sky rises and maybe some enjoy but they could be the workers and not the retired people down sizing, trying to be independent in their own homes not into old folks homes. why not think of the people first and not only taxes and money. quality of life important please. slower growth possible.	2/2/2023 10:44 AM
37	Housing is essential for middle income residents and the homeless population. The homeless numbers are growing and if Motel 6 is being closed it means they will migrate to the woods areas. They continue to be displaced to the small woods strip between Shannon lake and the Crystal Springs park. It is not safe for residents close by as they chop trees, build fires, and leave garbage which attracts bears and coyotes. Make a plan with all of the expected growth to build or buy housing for the homeless as it will only continue to grow.	2/2/2023 9:39 AM
38	Agree	2/2/2023 9:12 AM
39	This is a classic example of what happens when you hire a consultant from outside the area. You get a cookie cutter OCP. Questions: What will ever drive the development of an urban centre in the Boucherie area, when it currently doesn't reflect the appearance of an urban centre at all; At least half of the area in the Smith Creek/Shannon Lake neighbourhood centre is currently park land or land dedicated to a future school. What are you saying here? Six story commercial buildings should be built on land currently zoned for parks and schools?????	2/1/2023 4:14 PM
40	Urban sprawl is bad for the road network, the environment, neighbourhood connectivity/cohesiveness, etc. I hate it.	2/1/2023 2:42 PM
41	The CoWK has done a great job outlining growth concept areas and the Westbank Urban Centre specifically is a very exciting opportunity for the future image and status of West Kelowna.	2/1/2023 7:45 AM
42	Repave the highway in West Bank	1/31/2023 9:21 PM

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43	Neighborhoods is what brought me to west kelowna and what I loved about the city	1/31/2023 3:08 PM
44	WK needs much better access. You cannot keep piling vehicles onto your very limited and so-called arterial roads. Growth is fine but the transportation infrastructure cannot even support people there already. The TMP shows no forward thinking in reducing traffic on main roads.	1/30/2023 9:52 PM
45	Neighbourhood Centres - The intent is to create a NC in the Smith Creek Neighbourhood. If it is assumed that this NC will provide Shannon Lake with local services, what consideration has been made for access and related traffic. In-fill housing should be included in the Westbank Town Centre as well. Mixed commercial and regentrification should be included in the Westbank Town Centre.	1/28/2023 9:25 AM
46	Needs to be well planned and all aspects thought out to each developing area.	1/27/2023 2:48 PM
47	I think it's important to co-mingle small business within some of the infill and 5 neighborhood centres. Such as hairdressers, cafes, independent retail. This gets the community walking to access these services more and is an area that is very underserved in West Kelowna	1/27/2023 12:02 PM
48	Some of the growth areas do NOT have adequate roads for the amount of traffic!! For example, there is STILL just one viable exit from the Glenrosa neighborhood.	1/26/2023 11:26 AM
49	Looking at current challenges flexibility will be important.	1/26/2023 9:59 AM
50	looking at the concept display board, it is not really clear what areas are within the growth boundary	1/26/2023 9:43 AM
51	I am concerned that the even side of Delray Rd isn't included in the Westbank Urban Centre - Commercial Core portion of the OCP. As a resident of Delray Rd, it is UNBELIEVABLY busy. As a key transportation corridor for access to Glenrosa, it doesn't make sense that one side of the road would allow for commercial buildings, but across the street is Low Density Residential. At the end of the even side of the street are two 4-plexes. This is not Low Density Residential. I would like to see this group of 18 lots be included in the Westbank Urban Centre - Commercial Core.	1/26/2023 9:12 AM
52	Does the range of housing you are to provide include RM1 zones (Modular)? Not everyone wants to live in condos or row housing. Learn from WFN, and create more single level houses like near Elk Road. The high cost of living in the Okanagan should not force residents to live in small condos and without enjoying their own personal greenspace and/or garage/shed. People choose to live on streets rather than be confined to a concrete building. This is why gated communities and RV & Trailer parks are attractive.	1/26/2023 8:51 AM
53	I live on Old Okanagan Highway north of Butt road. There are no crosswalks, stop signs in front of my building XXXX and XXXX and crossing the street to get to the sidewalk side is precarious as people treat it like a highway (the name should be changed to old Okanagan road as it is a two lane road not a highway). This is a high density area now with children and elderly people. sidewalks on both sides would be an improvement too. thank you XXXX XXXXX	1/26/2023 8:28 AM
54	Figure out the traffic situation through west Kelowna FIRST!!! The congestion is horrible and needs to be dealt with before major centres are jam packed full of other things including condos. Overpasses to relieve the congestion during rush hour is a must! Also the Westbank centre needs to be cleaned up and the homeless population that is there needs to be attended too first. Theft is an all time high.	1/26/2023 6:54 AM
55	Albeit the City wishes to focus on designated areas within the OCP it is obligated to amend the OCP for lands that are outside of these areas that are already being used and approved for other uses such as Gravel extraction and country residential lands that are defined in the OCP as rural resources. As well as lands that are not in the ALR that are still defined as Agricultural. A community plan is intended to not only cleanup approved growth areas but it is suppose to	1/26/2023 12:36 AM

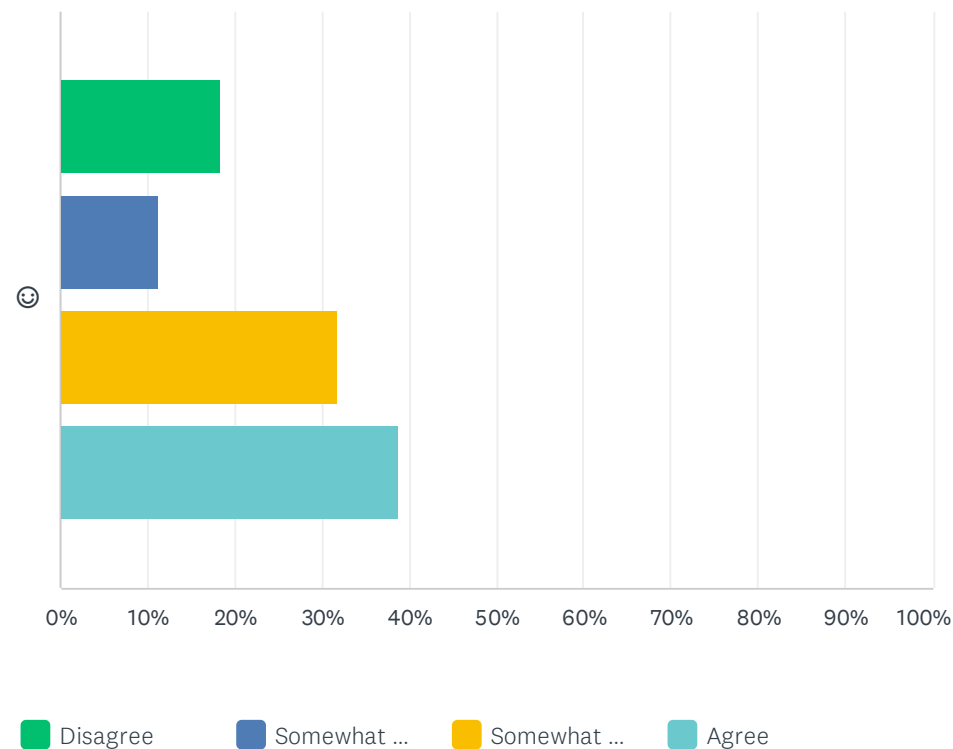
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encourage growth by acknowledging existing approved land uses and incorporate there use into a community plan. For example the City was able to acknowledge and change the OCP and zoning of there lands that are outside the proposed focused OCP areas. If the City continues to ignore existing uses by not recognizing there current land uses yet again in this second updated OCP then the OCP will and does not hold any merit within the business community. This OCP needs to acknowledge current uses that are outside of the designated focus centers in order for the City to start growing as a community working to a combined goal of creating more affordable housing rather than ignoring these growth areas and driving the cost of housing up.

56	Water is a finite resource that is currently poorly managed by all the various levels of government and stakeholder groups. Currently there are hundreds of water providers in the valley that are disjointed and mismanaged. West kelowna should be a leader by way of being a strong voice for a unified, proper management system of our valued resource. Water, as a finite resource, should be the driver and limiter of growth. Who is going to be managing and considering this aspect of our population growth???	1/25/2023 9:35 PM
57	The rezoning of Harmon rd....we were told by the Mayor in Nov the City has no plans for this site. It was not on the last version of the proposed OCP but suddenly it is. And it's going to BC Housing for low income homes. This is not an area people can manage without a vehicle, with no access to stores and doctors. Unless you count the premium priced convenience store, Nesters. Low income people won't be able to afford their prices. The people who live in the rental properties on this site can't even afford snow tires on their vehicle.	1/25/2023 11:51 AM
58	It's very challenging to get a grasp on actual areas. It would be helpful to have a map that one can zoom in to see up closer the boundary lines and plans for connected trails and walkways. I think it's very important for youth in each area to have walkable cultural centres where they can participate in meaningful after school activities (art centres for after school and weekend art classes and access to outdoor and some indoor recreational activities and sports. The recreation centre and pool we have is overcrowded and it's very difficult to enrol children in swim lessons as there are never enough spaces.	1/24/2023 7:55 PM
59	I completely agree with the Urban growth centers in West Kelowna and Mt. Boucherie...would love to see some more shopping and food venues near the Mt Boucherie Transit exchange, especially a coffee shop that is open in the evenings. Was sad that Lunch Queens closed and Bliss Bakery is not open past 2:30 PM	1/24/2023 7:07 PM
60	I think west kelowna needs more often bus routes and city bus coming into tallus ridge. I believe west kelowna would highly benefit from a mall or an outlet mall. West kelowna needs more shops here as it's too crowded in Kelowna to withhold service for that many people.	1/24/2023 5:03 PM
61	I agree, although for some reason all four options have lit up ! I feel it's important for West Kelowna to have a dedicated transport plan. Not just for cars, but as the population ages, for buses & for walking to all shopping points as well as walking trails too	1/24/2023 4:54 PM

Q3 1. Create vibrant urban and neighbourhood centres with a mix of residential, commercial and mixed use buildings at additional heights and densities that create economic opportunities and increase access to amenities for the following: 1.1 Westbank Urban Centre additional heights and densities proposed up to 12 storeys. View the Urban Centres display board. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 142 Skipped: 21



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	18.31% 26	11.27% 16	31.69% 45	38.73% 55	142	2.91

Q4 Tell us more:

Answered: 71 Skipped: 92

#	RESPONSES	DATE
1	Again, agree with growing "up", but also concerned with impacts to residential homes in areas where the multi-stories are currently proposed to occur directly on top of. Consider lower height/story limits	2/7/2023 9:54 PM
2	All cross rds Brown , Elliot and Hoskinson need to be closed to traffic and made into parking. The back alley lane should be upgraded into a walking area and all business encouraged to face the walkway and back the HWY. Yes create a focal point or open air community space. Yes encourage 12 storey high rises with commercial businesses on lower floor	2/7/2023 9:42 PM
3	Like I said above , it's not about going higher that will solve the affordability crisis or availability of housing . I think for commercial spaces it's great to go higher , and 100% would love Westbank to have a more commercial shopping /cafe /restaurant vibe downtown like Peachland or Kelowna. But just not residential . Building high and high density housing does not make it affordable . Love the idea of more of an urban center though and the locations are great !	2/7/2023 9:01 PM
4	I disagree with the increase to 12 storeys unless absolutely critical. Building that high should lead to more affordable homes, not unaffordable apartments that do not help growth in the community.	2/7/2023 7:15 PM
5	12 storeys is too tall. Westbank Centre is on a hill, it is not down on the lake. The tall buildings will detract from the natural beauty of the area. Already we are seeing the taller buildings along Carrington hide the lake views. Visually, tall buildings on a hill do not look good. Both Kelowna and Penticton have tall buildings, but these are down by the lake.	2/7/2023 6:48 PM
6	Please see summary in Item 4 below.	2/7/2023 4:10 PM
7	Saddened that the plan cannot realize how congested this area will be and how more natural amenities will be almost destroyed	2/7/2023 2:26 PM
8	The housing density on Gellatly bay should be minimal	2/7/2023 2:22 PM
9	This area should be our focus of growth for both business and entertainment as well as residential. It cannot compete if it is up against the same type of facilities next to the lake. Give it a chance.	2/7/2023 2:08 PM
10	There must be respectful community feedback particularly for tall building impacting the view of existing residents. Westbank needs to grow but it also needs to NOT be so reliant on cars. If there can be more consideration for people getting around on transit / bikes / walking then that would be good. People should be able to easily walk down to the beach (Gellatly) from Westbank. It also needs to serve a purpose and feel more like a town. Not easy. Please don't just build more and more storage units. No-one wants to live in a house overlooking an industrial estate. Please keep pleasant outside spaces. Footpaths! Nobody wants to walk their dog or push their baby in the road.	2/7/2023 2:02 PM
11	12 stories Retired people need small bungalow one level homes. The First Nation actually thought of the retired people. Why don't you?	2/7/2023 2:00 PM
12	I have no problem with the urban centre but again 12 storeys seems extreme.	2/7/2023 1:50 PM

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13	Westbank Urban Center is an eyesore. This is where high density development should take place. With more people in the area more restaurants and service industries will be attracted to the development and this will allow for beautification. Pedestrian only developed areas will help reduce traffic.	2/7/2023 1:39 PM
14	As a homeowner and resident of the 3 story town homes at XXXX Brown Road, I do NOT agree with the proposed Residential Shoulder areas allowing up to 6 stories past Springer Lane/Gateway Lane. If the single family homes that are currently along the north ends of Brown Road, Springer Road and Elliot Road between Springer Lane/Gateway Lane and Berring Road are to be replaced by developments, they should not be allowed to be taller than the units that are currently built. I would also like to bring attention a possible error on the Westbank Urban Centre Map: Butt Road is incorrectly marked, the road that is marked as "Butt Road" is Berring Road and the OCP perimeter shows in the middle of the block. If this is not an error and the intention is to allow 6 story building to be built directly beside single family home, I very much DISAGREE with this proposed perimeter. And this further confirms that the perimeter should be Springer Lane/Gateway Lane.	2/7/2023 11:45 AM
15	Planning growth and higher density in city centre is needed for future	2/7/2023 8:40 AM
16	West Kelowna Council ran in the last election of maximum 6 storey heights What happened? We will have Gellatly Road looking like Carrington Road which is nothing but tall apartments and vehicles parking all along the street because there was only one parking spot allowed for each apartment unit. The Gellatly Road recreational area is already overloaded with visitors and residents with not enough parking along the street because boat trailers, campers are also allowed to park on the street (and overnight). This is the only recreational area we have in West Kelowna - keep it that way and do some upgrades for a bicycle lane and sidewalks. Don't take away the only easily accessed recreational area we have.	2/7/2023 5:15 AM
17	Not a fan of 12 storeys anywhere in West Kelowna but if there is going to be any density and apartments with heights of up to 10 storeys, I would say they only be in the urban centres.	2/6/2023 9:19 PM
18	There needs to be more done to address missing middle housing for families. Density is great but if it's only 1-2 bedrooms many people will continue to be displaced and they'll just be short term rentals	2/6/2023 8:21 PM
19	It is vital to create an Urban Centre and all that it will provide. Buildings up to 12 stories might be a little high.	2/6/2023 3:50 PM
20	Agree	2/6/2023 2:46 PM
21	I agree the density should be highest in the town centre. The transportation plan needs to have the highway bypass the town centre.	2/6/2023 9:55 AM
22	It looks like Harmax Farm is slated for residential up to 6 stories. Is this property not ALR Land? If so, it should be preserved as such, please.	2/5/2023 10:20 PM
23	I like these areas are near Hwy 97 and do not obstruct views of the lake	2/5/2023 2:12 PM
24	I think 12 storeys is too high	2/5/2023 1:15 PM
25	We have lived in West Kelowna for over 24 years and have seen many great changes, especially in the waterfront areas. It would be nice to see a more defined town center area for the community.	2/5/2023 8:08 AM
26	In my opinion there should be some variances allowed to go potentially go higher than 12 storeys. I would recommend doing a cost analysis for developers to determine with the land acquisition numbers (what home owners are expected to get ie 20-30% premium on the normal resale value) what numbers would make sense to a developer so they can make the numbers work to buy the land for what the sellers are willing to sell the land for and in turn put out a good viable product/ more rentals. If this is not done I fear developers will propose 4-6 storeys due to the cost difference of building with wood vs concrete. I also fear not giving the developers an ability to recoup some costs with additional density they may not be able	2/4/2023 6:48 PM

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to find the land to make viable developments as home owners may not be willing to sell for the price they need to buy it for. In turn lower development will happen in West Kelowna urban areas and less progress made.

27	Ten floors would be better - five is a little extreme.	2/4/2023 4:31 PM
28	Higher is not better it is usually ugly.	2/4/2023 3:54 PM
29	We do not want 10 to 12 story buildings at willow beach, the city does not have the infrastructure to handle all the traffic. The beach area needs to be for all citizens of West Kelowna, having highrises is out of place on the beach. We cannot make any more waterfront property.	2/4/2023 3:50 PM
30	While I agree with the concept of concentrating the majority of density into two areas where there is already density. It's unclear why there were only small changes made to the boundary of Westbank Center. This "downtown" should be as large as plausible	2/4/2023 8:03 AM
31	Westbank is the historic and sentimental heart of West Kelowna, so it is appropriate that it be designated the primary urban centre. Though some of its small town charm will be destroyed, focused density is better than unfocused laissez-faire development.	2/3/2023 8:05 PM
32	Do not need 12 stories. The density your predicting is unreasonable. They are building unaffordable homes and apartments that are sitting vacant already. Interest rates and mortgage rates are rising and your proposing more of the same. People from west kelowna and the okanagan are leaving BC for Alberta and more affordable homes and better paying jobs. We can't even keep the Doctors here.	2/3/2023 7:36 PM
33	It would be nice to have a pedestrian area close to traffic, with shops and amenities in the Westbank area in order to have a place to call "Downtown". With a parking lot where to park the car and then walk to this area. Main street could be a good place to do that but that would mean redesign the highway. Anyway If Kelowna and West Kelowna will keep growing at this pace it is likely that traffic through the 97 will increase consequentially. I think that something needs to be changed there.	2/3/2023 6:17 PM
34	Not an appealing area	2/3/2023 6:04 PM
35	I do not like the height of up to 12 storeys anywhere. People like West Kelowna for its small town feel. Our current roads cannot handle this many people. They can't even handle the amount we have now. The best way to implement higher buildings as they are in the middle and it gets lower as you go out. Since much of the "Urban Center" already has buildings on it, this would mean you could have a 12 storey, next to a 2 storey than a 6 on the other side. If it had been planned this way from the start it is a little different than changing a concept towards the end of development.	2/3/2023 5:09 PM
36	12 stories is too high. Too big cityish. Will negate the current town and country atmosphere of our community.	2/3/2023 4:38 PM
37	Like the idea of density in this area as not impacting views, creates a downtown that can grow and love the idea of taking over one side of the couplet. This needs to happen	2/3/2023 3:39 PM
38	Encourage an increase in mix use for this area.	2/3/2023 2:55 PM
39	We should grow here the most including adding essential services like a new arena, new Rec Facility	2/3/2023 2:06 PM
40	At the Feb. 1 Open House staff made it sound like the city is waiting for developers to make proposals. If the urban centres are going to be places people want to go to shop, dine, have a coffee etc. there needs to be more direct input from city staff to guide developers ideas and plans. Otherwise developers will focus on developments that maximize profit. Not on creating an environment that attracts residents and tourists. There should be plans for a West Kelowna hospital and the hospital site earmarked in the plan. The new Urgent Care Centre is inadequate for the current population of 39,000 residents and will be	2/3/2023 8:59 AM

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increasingly severely inadequate for the expected population of 51,000 by 2040. Urgent Care Centre hours are too short and wait times are already too long. We arrived before it opened at 4:30 pm, had to wait outside in line for 60 minutes after it opened to get inside and were then told we may not be seen before closing at 8:30 pm.

41	6 stories is an adequate height for buildings in the downtown core as well. High rises are incongruous with the mixed agriculture, residential and commercial character of West Kelowna. Somehow the very vibrant and densely populated European cities have restricted building heights to 6 stories in order to maintain the visual integrity of these urban centres.	2/2/2023 11:34 AM
42	I agree there should be centers in the communities to allow residents to access essentials in their areas. It will allow the traffic to flow outside hwy 97. Having said that, there has to be a plan to build overpasses to get residents to these centers. Infrastructure has to be addressed with the increased building and population. The current corridors are not working. Traffic is a problem on hwy 97.	2/2/2023 9:39 AM
43	Agree	2/2/2023 9:12 AM
44	12 stories feels too big. I'd prefer more 4-6 story mixed use buildings over a greater area. I hope this be supported by increased transit service.	2/1/2023 2:42 PM
45	I believe the CoWK has an opportunity to increase density and should not limit its height on buildings to 12 storey. In certain pockets of Westbank Urban Centre, there is an opportunity to bring healthy, livable, walkable density and these variables should be in priority to height limitations. This critical mass/density will be required to formally invigorate and revitalize this community. The CoWK has done a great job thus far with the current draft OCP, but I would propose some flexibility in the right spaces.	2/1/2023 7:45 AM
46	No high rises higher than 8 stories.	1/31/2023 7:38 PM
47	There is no faster way to ruin a 'community' than by adding high rise buildings.	1/30/2023 9:52 PM
48	The view coming into town from Penticton/Merritt is so beautiful. I would rather see taller buildings in the Boucherie Urban Center area than in the Westbank Urban center.	1/30/2023 12:08 PM
49	Westbank Urban Center 12 storey density is too high. Limit to 3.	1/29/2023 10:20 AM
50	While 12 storey residential, mixed commercial building would generate additional property/commercial tax dollars, this is a departure from the aesthetic and feel of Westbank. Currently there are 6 story residential building that fit nicely into the downtown and surrounding neighbourhood. I would not support heights/densities up to 12 storeys. I would support the current guidelines of up to 6 stories.	1/28/2023 9:25 AM
51	Our Fire Service cannot safely protect its firefighters and the public in any building height over 6 stories. This is not a wise move and the city is well aware of it.	1/27/2023 2:48 PM
52	Agree this makes sense. Most of the area mapped is already under constructions and not 12 storeys so would be interested to better understand what improvements can be made to increase when so much of this area is already built within the last 10 years. Biggest concern is that there is never enough parking for residents in these newer apartments. Most apartments have 2 cars and this is creating a lot of side-road parking and limits accessibility. Need to balance timing of increase density with ramp of of public transportation options	1/27/2023 12:02 PM
53	Westbank should never have been chosen to accommodate our new City Centre. It is not the centre of West Kelowna. If that is our only Urban Centre then we are forced to go there - mostly all on Native lands & not really contributing to West Kelowna's tax base,	1/27/2023 11:30 AM

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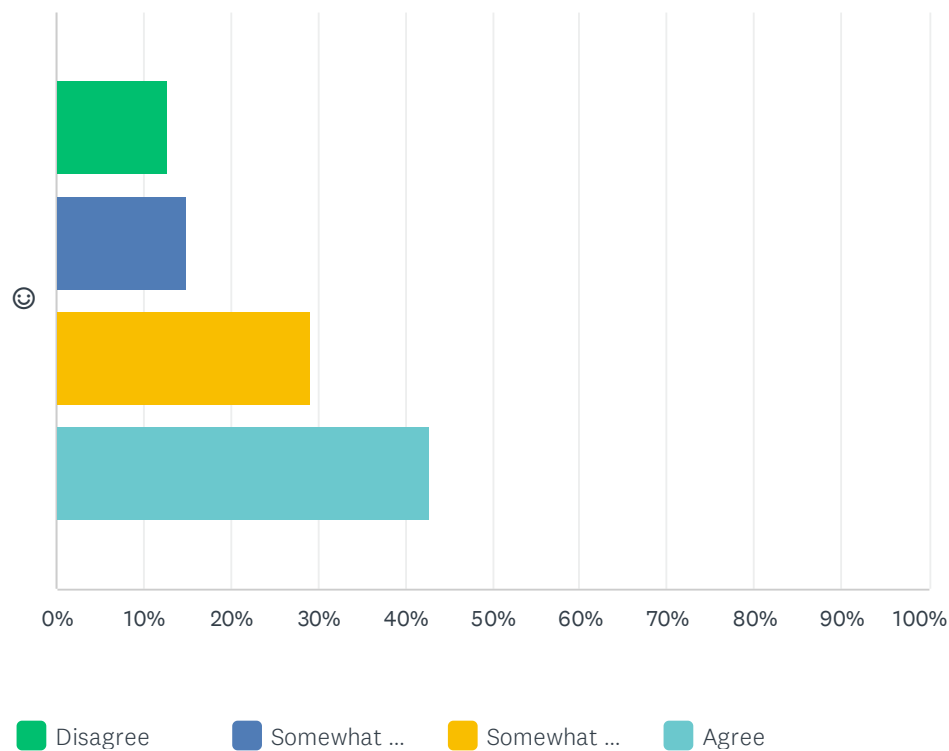
54	12 Storey built form can be expensive and will be counter-productive to what the COWK is trying to achieve. This should be based more on a project specific basis and FAR instead of a height cap.	1/26/2023 1:37 PM
55	Very concerned about infrastructure (roads, sidewalks and OFF STREET PARKING, for these areas.	1/26/2023 11:26 AM
56	We have a limited land base and a need for more homes. Densification is the key.	1/26/2023 10:01 AM
57	Why does the Boucherie urban centre have plans for lower buildings than the Westbank urban centre? The height of the buildings will potentially impact the views. Many residents buy their homes in this area for the views. This indicates that the Boucherie residents concerns ONLY have been taken into consideration.	1/26/2023 9:43 AM
58	As someone who has lived in a "mixed commercial and residential" area in the past, the entire reason I moved to West Kelowna in the first place was to avoid exactly this. These areas are noisy, have high traffic, and high crime rates. In addition, the nearby shops are typically more expensive as they offer a "convenience", and in every single case I have seen this development happen, the city does not plan the road or service infrastructure correctly, leading to insane traffic. (For example, the round about beside superstore and highway 97. It seemed like a good idea until the population moved in, and now it's a risk and liability that should be replaced)	1/26/2023 9:32 AM
59	6 stories	1/26/2023 9:19 AM
60	See question 1	1/26/2023 9:12 AM
61	Change is needed in the Zoning Bylaw 0265 Part 4, Section 4.4, Standard Parking Spaces. Every condo/mixed use building must need to provide 2 off-street parking spaces for every unit. Stop focusing on the developers dollars, and listen to city residents. Rent and values are so high, and condos charge for an extra parking stall. Residents park and clog up the street, block neighboring driveways and entrances, and create hazards for pedestrians and vehicle traffic. Until rapid transit is added to HWY 97 to Kelowna, and/or more transit options (Uber) available, a couple living in a condo will still need two vehicles to live their lives.	1/26/2023 8:51 AM
62	We already have so many large buildings thrown together in the last 10 years. Growth is important, but West Kelowna is losing its local feel and prices are going up with all this growth.	1/26/2023 8:05 AM
63	Increase parking stalls off street. 1 bdrm = 1 stall, 2 bdrm = 2 stalls	1/26/2023 7:28 AM
64	I picked agree not sure why all 4 circles are then checked.	1/26/2023 6:18 AM
65	Institute a requirement for system of grey water or recycled water for public green spaces and ensure that drought tolerant plants and little grass is used in these centers.	1/25/2023 9:35 PM
66	Believe we could go higher than 12, maybe up to 15 or 16 in the core. Would also like to see much more development in the Westbank town centre before density is encouraged in the Boucherie centre. Mixed use corridor should also be expanded further west / southwest to Elliot and include the Town Centre mall property.	1/25/2023 8:31 PM
67	It's very challenging to get a grasp on actual areas. It would be helpful to have a map that one can zoom in to see up closer the boundary lines and plans for connected trails and walkways. I went to google maps for a better look but many people likely won't bother to do that. I think it's very important for youth and seniors in each area to have walkable cultural centres where they can participate in meaningful after school activities (art centres for after school and weekend art classes and access to outdoor and some indoor recreational activities and sports). The recreation centre and pool we have is overcrowded and it's very difficult to enrol children in swim lessons as there are never enough spaces.	1/24/2023 7:55 PM
68	Agree totally and happy to Welcome Okanagan Grocery and Bakery to the area.	1/24/2023 7:07 PM

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69	West kelowna needs more housing and shops	1/24/2023 5:03 PM
70	I have the same views as above.	1/24/2023 4:54 PM
71	Same heights in all towns centres, you wouldn't approve a tall building on Anders rd because it block someone view, there is no one living above Anders rd, so let's keep things equal	1/24/2023 4:13 PM

Q5 1.2 Boucherie Urban Centre additional heights and densities up to 6 storeys. View the Urban Centres display board. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 140 Skipped: 23



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	12.86% 18	15.00% 21	29.29% 41	42.86% 60	140	3.02

Q6 Tell us more:

Answered: 51 Skipped: 112

#	RESPONSES	DATE
1	Could go higher? Not blocking anyone's lake view.	2/7/2023 11:19 PM
2	Some 6 storie commercial around Ross rd intersection would probably be okay Never been in favor of multifamily residence around a high school. That is an old taught planning technic that has been proven wrong several times In the City of Kelowna they removed all multifamily around the Rutland secondary school and placed single family surrounding it. The theory was that the surrounding stake holders had a vested interest in the school and the children with in it. Today RSS is one of the highest ranked schools in BC in both academic and atheletic.	2/7/2023 9:42 PM
3	I don't know if that's the most needed area ? It's close to the superstore complex . I know closer to rose valley is more in the middle - not close to either ends other than the neighborhood center but that nature fare is way over priced and may not be worth it for affordability purposes for everyone .	2/7/2023 9:01 PM
4	I do not agree with wood structures built at that height.	2/7/2023 7:15 PM
5	Please see summary in Item 4 below.	2/7/2023 4:10 PM
6	The housing density and building height on gellatly bay should be limited	2/7/2023 2:22 PM
7	This area needs services	2/7/2023 2:08 PM
8	Same as previous comment -	2/7/2023 2:02 PM
9	Again no problem here	2/7/2023 1:50 PM
10	The number of 6 storey buildings should be kept to a minimum.	2/7/2023 1:39 PM
11	Good location for more growth	2/7/2023 8:40 AM
12	See above	2/6/2023 8:21 PM
13	L	2/6/2023 7:11 PM
14	This is a reasonable scale and will provide a good mix of commercial and residential, with access to public transportation.	2/6/2023 3:50 PM
15	Agree	2/6/2023 2:46 PM
16	Density should be a maximum of four stories.	2/6/2023 9:55 AM
17	An Urban center is definitely needed between Westbank town center and the bridge	2/5/2023 2:12 PM
18	Additional housing in the Boucherie area, would help support West Kelowna wine trail and also provide more housing options for retiring seniors looking to downsize.	2/5/2023 8:08 AM

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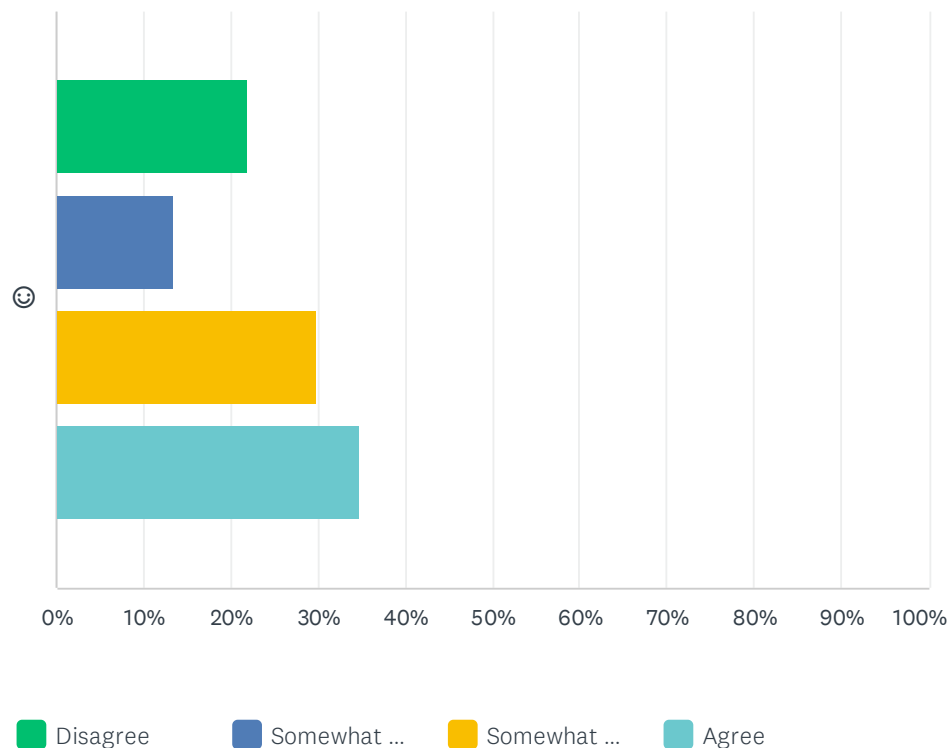
19	I think this is a great opportunity. Close to transit and other shopping. Would encourage some mixed use development here like spaces for daycares and residential up.	2/4/2023 6:48 PM
20	But six must be the maximum with no exceptions.	2/4/2023 4:31 PM
21	Considering the rural environment we feel the maximum height of structures should be less than 6 storeys	2/4/2023 4:04 PM
22	We do need some affordable housing.	2/4/2023 3:54 PM
23	I'm concerned with just focusing on increasing housing density in Boucherie. West Kelowna is already at a deficit with recreation complexes. There should be consideration of preserving this area for recreation.	2/4/2023 8:03 AM
24	I agree that the city is big enough and the highway corridor long enough that another centre of gravity is warranted. It might as well be at Boucherie. Six stories is a good height. Look how well it has been working in Paris, for example.	2/3/2023 8:05 PM
25	Very poor planning for this area. Mount Boucherie trails would be only accessed through buildings! Half of west Kelowna's commercial space is vacant already. You also are forcing a privately owned mobile home park and the many seniors and young family tenants (incapable of moving their homes) out on the street. Then I guess you can house them with the homeless at super8 or the center at the top of Bartley. Nice move.	2/3/2023 7:36 PM
26	This area is not appealing and has more of a industrial than residential vibe	2/3/2023 6:04 PM
27	As for the Boucherie Urban Centre the same questions apply - how do you implement change when it is already almost completely built. If I owned a house and the neighbours sold to a developer to build a 6 storey condo next to the house I saved up for and bought in a quiet neighbourhood I would not be pleased. In other words I don't believe in changing the zoning for an established neighbourhood.	2/3/2023 5:09 PM
28	See previous comments	2/3/2023 4:38 PM
29	At the Feb. 1 Open House staff made it sound like the city is waiting for developers to make proposals. If the urban centres are going to be places people want to go to shop, dine, have a coffee etc. there needs to be more direct input from city staff to guide developers ideas and plans. Otherwise developers will focus on developments that maximize profit. Not on creating an environment that attracts residents and tourists. There should be plans for a West Kelowna hospital and the hospital site earmarked in the plan. The new Urgent Care Centre is inadequate for the current population of 39,000 residents and will be increasingly severely inadequate for the expected population of 51,000 by 2040. Urgent Care Centre hours are too short and wait times are already too long. We arrived before it opened at 4:30 pm, had to wait outside in line for 60 minutes after it opened to get inside and were then told we may not be seen before closing at 8:30 pm.	2/3/2023 8:59 AM
30	Does this include the elimination of the use of the Super 8 motel for 'unhoused' and/or a place with rampant drug use?	2/2/2023 6:53 PM
31	Agree	2/2/2023 9:12 AM
32	There is no driver for this redevelopment in our foreseeable future. It is a meaningless designation.	2/1/2023 4:14 PM
33	I hope this be supported by increased transit service as well.	2/1/2023 2:42 PM
34	6 max is still high	1/30/2023 9:52 PM
35	The view coming into town from Penticton/Merritt is so beautiful. I would rather see taller buildings in the Boucherie Urban Center area than in the Westbank Urban center.	1/30/2023 12:08 PM
36	Limit to 3 storeys.	1/29/2023 10:20 AM

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37	The areas from across the street from city hall to the save on mall area should be regenerated to low cost housing. Its ideal for access for low income individuals to get to school get to shopping without the requirement for transportation,	1/29/2023 9:34 AM
38	I agree to limiting heights to 6 stories. This is in keeping with the neighbourhood and the vision for WK.	1/28/2023 9:25 AM
39	Care must be given to the waterfront view area on building heights.	1/27/2023 2:48 PM
40	see above comment	1/26/2023 9:43 AM
41	I would agree if the road infrastructure was in place. This will only make the traffic in the area, and highway 97 worse. Travelers will have to go through these areas to get to Kelowna, further increasing how long it takes to drive between Kelowna and the South Okanagan. Is a highway bypass planned as part of this development?	1/26/2023 9:32 AM
42	Change the Zoning Bylaw 0265 Part 4, Section 4.4, Standard Parking Spaces. Every condo/mixed use building must need to provide 2 off-street parking spaces for every unit.	1/26/2023 8:51 AM
43	Fix the Bylands road intersections at Daimler and Bartley first	1/26/2023 7:28 AM
44	I feel that 6 stories is too limited for the amount of growth that will happen in these areas. Limits should be set at no less than 10 stories.	1/25/2023 10:45 PM
45	Institute a requirement for system of grey water or recycled water for public green spaces and ensure that drought tolerant plants and little grass is used in these centers.	1/25/2023 9:35 PM
46	Don't want to see much density here until the downtown core has been more established.	1/25/2023 8:31 PM
47	It's very challenging to get a grasp on actual areas. It would be helpful to have a map that one can zoom in to see up closer the boundary lines and plans for connected trails and walkways. I went to google maps for a better look but many people likely won't bother to do that. I think it's very important for youth and seniors in each area to have walkable cultural centres where they can participate in meaningful after school activities (art centres for after school and weekend art classes and access to outdoor and some indoor recreational activities and sports. The recreation centre and pool we have is overcrowded and it's very difficult to enrol children in swim lessons as there are never enough spaces.	1/24/2023 7:55 PM
48	As per my first comment, I completely support this idea, as I live in the area. If anything more needs to be done to focus on this, such as completing the sidewalk along Ross Road to Bartley and the Hwy intersection to the transit exchange.	1/24/2023 7:07 PM
49	More affordable and housing is needed	1/24/2023 5:03 PM
50	As I live on Brown road, I don't travel much to Boucherie Urban Centre. Although the bus stops are obviously important.	1/24/2023 4:54 PM
51	Why is boucherie so special? You have no problem putting social housing and food banks in westbank towns centre, you treat westbank towns centre like the city of kelowna treats Rutland, there is a lack of fairness. Buildings should be same heights in all neighborhoods	1/24/2023 4:13 PM

Q7 1.3 Neighbourhood Centres additional heights and densities up to 6 storeys (e.g. Lakeview Village in Lakeview Heights). View the Neighbourhood Centres display board (2). Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 141 Skipped: 22



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	21.99% 31	13.48% 19	29.79% 42	34.75% 49	141	2.77

Q8 Tell us more:

Answered: 70 Skipped: 93

#	RESPONSES	DATE
1	Re: Gellatly. Great idea to get some commercial/mixed buildings. Parking is already a challenge all summer in the Willow Beach and marina area. Parkade would be a good addition.	2/7/2023 11:19 PM
2	Gellatly needs to be upgraded as the City's water front everything should be tied back to the City main centre via OLD OK Hwy /Angus drive connector. Some commercial space with residence in 20 tower buildings would be nice. Extra parking would have to be provided for the commercial which would help the current congestion. Gellatly should be one way with extra parking in closed lane	2/7/2023 9:42 PM
3	Love this !!!! I think This the best thing for Westbank ! I've loved the community it's brings	2/7/2023 9:01 PM
4	6 Storeys is too tall for Smith Creek. Smith Creek is a hillside community, why would we build 6 storey high buildings so everyone can see an urban jungle. This does not seem to support the goal of preserving natural areas and park space. Visually, people do not want to see tall buildings on a hill. As well, 6 storeys is too tall for Lakeview Village.	2/7/2023 6:48 PM
5	Please see summary in Item 4 below.	2/7/2023 4:10 PM
6	The housing height and the population density on Gellatly should be held down	2/7/2023 2:22 PM
7	Heights 5/6 limited to 4 stories	2/7/2023 2:16 PM
8	- Six stories work in some areas but not others - Extraordinary community benefit lacks transparency and gives way too much discretion to the City and MUST be removed - Gellatly Bay should NOT be a neighbourhood centre - Wellness of our people should drive our OCP not parking	2/7/2023 2:08 PM
9	Agree but in order for these to be neighborhood centres they need amenities! You need somewhere to get basic groceries, a doctor office, a pub or restaurant. Not just more houses or apartments. Keep a park and pleasant outdoor spaces. Footpaths!	2/7/2023 2:02 PM
10	The number of 6 storey buildings should be kept to a minimum.	2/7/2023 1:39 PM
11	Need to expand the recreation centre to meet the population growth. Parking and cafes (meeting place) and restaurants and bottom of condos is vital.	2/7/2023 8:40 AM
12	Adding any more density to Gellatly Bay will RUIN the waterfront! It is already well used and very busy in the summer. Parking is a problem and a solution needs to be made for boat trailers that line the street and are moving around the corner towards the horses and onto Whitworth Road. It is becoming dangerous! The waterfront needs to be treated with great thought! Putting apartments there will only benefit a few that get to live there and a builder that makes the money. A Center is not needed there. Maybe a coffee shop/bakery like Bliss or a food truck for all the walkers and bike riders to stop at but not another source of high density. Parking needs to be considered for all of West Kelowna residents that want to use the beach in the summer and the walking path all year long. This land will never be free again... Please do not cover it in 8-10	2/6/2023 9:19 PM

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storey apartments (even 6 is too high) and add to the busy area. This little bit of country/waterfront is what makes West Kelowna special and there is no more to add!! Please make sure to keep the agriculture too. Please keep what is special about the water front safe from further development and density and improve it for the years to come of enjoyment by all for recreational use.

13	The gellatly neighbour hood centre would decimate one of the nicest areas of our town. A peaceful, beautiful, and not too busy corridor that is beloved for recreation. This is not the place for density.	2/6/2023 8:21 PM
14	Gellatly development will not work it's too close to the lake with a lot of activity from citizens & tourist... the Cove and yacht club already have challenges for members, employees, guests, boaters, tours to support these venues for day to day to larger events, it's impossible to get parking all the time	2/6/2023 7:11 PM
15	We are residents of The Pointe on Gellatly Rd. We are agreeable to a 4 to 6 story condo/business complex across from Willow Beach (current boat storage facility) but we don't want to see higher structures built in this area. There was a proposal for two 12 story towers and one 10 story tower which would not fit in with our rural setting. We have concerns about increased traffic, parking issues (Gellatly Rd.) is already very busy with very limited parking during the summer. Developments really need to fit in with the current aesthetics of the area.	2/6/2023 4:04 PM
16	Six stories buildings are enormous when you stand next to them. Including the addition of added density/height for "extraordinary community benefit" lacks transparency and creates anxiety for neighborhoods that will be affected. Community benefit should be expected at any time buildings of a larger scale are placed in residential areas and for many of these areas four stories is plenty. In particular Gellatly Bay should not be a Neighborhood Centre and should revert to the 2017 amendment to the OCP. Any development in this area must prioritize preserving and improving recreational benefits for the people of West Kelowna. In West Kelowna our access to the lake is so limited we cannot compromise for the sake of commercial endeavours near the lake.	2/6/2023 3:50 PM
17	Why are only two of the five shown for feedback or am I missing something here? Agree overall on the Centres	2/6/2023 2:46 PM
18	Densities in neighbourhood centres should be a maximum of four stories.	2/6/2023 9:55 AM
19	I live on Beachview Dr. and back onto Angus Dr. only a few hundred metres from the Gellatly neighborhood proposed development. Apparently your council doesn't listen very well to our neighborhood that is not in favour of 10 story highrise buildings across the street from Willow Beach. I went to a meeting at the Cove last year and your people told me this will not happen. We voted for members of city council that were only in favour of 4-6 stories of residents on that property. We built a new house here because it was a spectacular place to live and a quiet neighborhood. Now you want to turn it into a huge development with way too many people. The infrastructure isn't big enough to support it. There's a lot of wildlife that would be effected, traffic in and out is bad now, I can't imagine what another 250 residents would impact this. Your Fire Department doesn't have the equipment or training to deal with this kind of structure. There is so much available land in West Kelowna for development I don't understand why you insist on cramming it all into our little neighborhood. I would support a small commercial development with a few condos above, but 3 10 story high rise buildings is out of the question. There isn't any building's in the whole city that big. I can't imagine anything 2 1/2 times as high as the Cove there. It will effect the views of people in the Point development as well as blocking the sunset in the afternoon. Our neighborhood association is strongly opposed to your community plan and you will be getting huge opposition to this development proposal. We have already spent many hours at council meetings and voicing our opposition. Maybe you should take some of our tax dollars and invest in some hearing aids for your planning department and city council so you can better hear our concerns. XXXX a very concerned resident.	2/5/2023 11:43 PM
20	Stick to 4 stories in all neighbourhood centers. The Gellatly neighbourhood center, which allows up to 12 stories in a small cluster, most of which are located immediately across Gellatly Road from Willow Beach Park, will dominate and literally	2/5/2023 11:30 PM

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overshadow this very popular swimming and walking area. As a key public jewel of the entire Gellatly Bay area, it is imperative that the zoning here limits building heights to 4 stories to achieve appropriate and respectful scale within the area. I am also concerned about the impact that large buildings will have on street-side parking for the waterfront users who need parking in the area. Where six story densification has been achieved along Gossett Road, vehicles line the street-side at all times of the day because the required parking for the buildings is inadequate so it spills onto the street.

21	Gellatly Bay Please do NOT destroy this treasure. All large scale development should be kept at least a block away from the waterfront.	2/5/2023 10:20 PM
22	Our neighbourhood is already overcrowded spring, summer and fall. There is no parking for tourists or locals. Many of us avoid our own neighbourhood in these seasons. Parking and traffic are presently a nightmare in this very small area. Why would you want to add to it?! Willow beach is such a tiny strip of beach and is packed with tourists in these seasons. Where will anyone park? What about safety of pedestrians not to mention boats, trailers and vehicle parking? There have been many days whe vehicles park up the side of Whitworth and even as far as the Glen Canyon park entrance! And remember: from Glen Canyon park trail head to Whitworth there is NO sidewalk. Many families with children, cyclists and pedestrians are living their lives dangerously while traversing this area. Where will we put these extra vehicles? How can we provide security for we the people who are presently using this corridor? And... what will this look like when more traffic and people are added to this already dangerous area?	2/5/2023 5:52 PM
23	The arteries leading into these centers are already overcrowded. The traffic is very busy as it is. The roadways do not support this level of population in these areas. I especially disagree with the proposed Gellatly neighbourhood center. Gellatly Bay is the jewel of the Westside. The corridor is used daily by many members of the community. As well as there is a lot of traffic due to the boat launch there. Adding higher density housing would overcrowd this area and diminish the park-like qualities of the neighbourhood. There is barely enough parking in the area as it is, especially in the summer when tourism is in full swing. Gellatly Bay only has one main road leading into it and it is just not enough to support the level of traffic that would be involved with this plan.there are not enough options to access this are as it is. We need more public walkways, bike paths and public transit for access to the Gellatly Bay corridor. Extreme care and caution must be used in planning the Gellatly Bay area as it is our best asset on the Westside and the gateway to the Westside Wine Trail. When looking at the map there is not even much population or available land (due to proximity of WFN lands) near proposed Gellatly Bay neighbourhood center. Why try to add housing there???? too crowded!! Seems to me Glenrosa could use a neighbour center judging by all the housing there.	2/5/2023 2:12 PM
24	Too high a density level for the area. Traffic - roads not equipped to handle more traffic. Gellatly should remain accessible to all residents of West Kelowna not just those who happen to live along the route. Many more residential buildings with quick and easy access to Gellatly beaches will mean residents from other areas will be unable to enjoy the area, especially during the summer months when it is already populated at a maximum. There is currently an obvious lack of boat parking let alone vehicle parking. What do you mean by access via Brown Road???	2/5/2023 1:15 PM
25	The inclusion of Gellatly Bay as a neighbourhood centre does not make any sense due to several factors. 1. Gellatly Bay is a unique prople first area which provides unmatched opportunities for swimming, sun bathing, walking, boating and simply enjoying on albeit on a very small land area. 2. The road through the Gellatly Bay area is narrow and multi-curved and there is no available land for major destination parking. The area is already congested during all seasons and in particular during warmer months and public safety becomes very concerning issue as attendance to the area grows. 3. Further development and construction of multi-family, multi-storied buildings and retail stores etc. in the Gellatly Bay area will simply exacerbate and vastly multiply the concern, previously stated, as to public safety, usage availability and general ambiance of the unique location.	2/5/2023 12:14 PM
26	My husband and I are particularly interested in the Gellatly Neighborhood Center, as we have lived in that area for over 10 years. We are both thrilled and excited for the possibility of having a neighborhood center that would allow us the option to	2/5/2023 8:08 AM

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visit, shop or dine out. The waterfront is one of West Kelowna's biggest assets, which is unfortunately not utilized. Our only concern is that the parking in that area is already an issue.

27	I'm very pro development but fear the city may be stretching the boundaries for higher density especially in Smith Creek. Is the fire department able to get to these areas in ample time. Also will the increase traffic on the roads be viable? Are there service upgrades to water/sewer/roads that will follow and is the city able to keep up to it.	2/4/2023 6:48 PM
28	Again, six the maximum.	2/4/2023 4:31 PM
29	Considering the rural environment we feel the maximum height of structures should be less than 6 storeys. Gellatly Bay does not qualify as a Neighbourhood Centre designation and should be tourist/recreational only.	2/4/2023 4:04 PM
30	Once again, ugly and certainly not in keeping with the neighbourhood	2/4/2023 3:54 PM
31	Has retail already in place	2/4/2023 3:19 PM
32	Not sure why you aren't showing boards for all neighborhoods. Putting a 6 storey in Lakeview heights is a huge mistake and should never have been approved. This area is already a traffic nightmare and there's no solution due to geography. You should try to keep the existing "feel" of each neighborhood. That's what attracted people there and to West Kelowna. Gentrification of West Kelowna is a huge mistake. The way that this concept is being "sold" is deceptive. The infill development approved on Anders and Skyline is a traffic nightmare in winter and will lead to accidents due to dangerous parking. These homes have no yards. Where are all of these new residents expected to recreate (there's at least 250 more residents expected to move into the new 6 storey)?	2/4/2023 8:03 AM
33	Neighbourhood centres are essential for the creation of walkable neighbourhoods. Whether or not these neighbourhoods were appropriate places to build is almost irrelevant. They are there now and the people should not be forced to drive into town to get what they need. Six storey buildings, while a bit of a change up, might be necessary to provide enough density to justify a commercial zone.	2/3/2023 8:05 PM
34	Tourism forward as it already draws them.	2/3/2023 7:36 PM
35	6 stories seems a bit too high	2/3/2023 6:17 PM
36	The marina area has a huge potential to be a beautiful and attractive place for people to gather and wanting to spend time.	2/3/2023 6:04 PM
37	Once again, if an area is established and you want to change the height that is allowed that is not fair for people who already own in the area. This could change a Lakeview house to one that is blocked by a condo. Having a neighbourhood pub, store, coffee shop is one thing but we don't need large shops everywhere - it does not take more than 10 min to get anywhere in West Kelowna. Also it looks the Boucherie Center includes the boat launch/ yacht club in the plans. I hope this is not something that is being proposed as a change. The other important factor is parking. Boucherie is West Kelownas main beach/ water area to be enjoyed by the residents. There is already very limited parking in the area, adding more density will just make this problem worse	2/3/2023 5:09 PM
38	See previous comments	2/3/2023 4:38 PM
39	I think each neighbourhood will have their own characteristics and limitations you will have to work with and unsure 6 stories is the right number - 4 may be more reasonable.	2/3/2023 3:39 PM
40	Ensuring there is a variety of mix use will be essential for these centers to thrive as neighborhood centers. In particular having grocery stores, not just corner stores is essential to the health of the community. Currently these areas are mostly food deserts and we need to change that.	2/3/2023 2:55 PM

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41	YES get those going, i want to see rose valley on here as well	2/3/2023 2:06 PM
42	A significant part of residential and most of the commercial developments and stores are on Westbank First Nations land. West Kelowna City must have more integrated planning with the Westbank First Nations Government Office so the 2 plans are compatible and complementary.	2/3/2023 8:59 AM
43	The Smith Creek area is being over built and the infrastructure will not sustain it.	2/2/2023 6:53 PM
44	The current OCP draft includes a loophole that enables an indeterminate increase to height and density in the Neighbourhood Centres. I am opposed to the inclusion of exception language in the OCP if building standards can be traded away. Specific to Gellatly Bay, future development should be consistent with the established character and limit building height to 4 stories. Significant and unreasonable increases to density at the lakefront will overwhelm the tiny public beach and park, as well as impact the safety of pedestrians and cyclists on the narrow corridors.	2/2/2023 11:34 AM
45	Gellatly village is proposed to go 12 storey's which I firmly disagree it should stay at 6 stories similar to the other neighbourhood centers.	2/2/2023 10:21 AM
46	Disagree Gellatly Village not 12 stories high. Only 6 stories.	2/2/2023 9:12 AM
47	GELLATLY WATERFRONT NEEDS TO BE KEPT FOR ALL RESIDENTS OF WEST KELOWNA. BUILDING HIGHRISE CONDOS WITHIN GELLATLY NEIGHOURHOOD CENTER IS WRONG. ZONING SHOULD BE CD2 SAME AS COVE RESORT, 4 STORY MAX WITH MIX OF COMMERCIAL AND RESIDENTIAL SAME AS COVE. PARKING PARKING PARKING..... IF BOAT YARD IS CLOSED WHERE WILL LAKE USERS PARK TRAILERS WHEN THEY WANT TO USE LAKE? CITY SHOULD CONSIDER FLAT LOT AT 4215 GELLATLY FOR PAID PARKING LOT WHICH ALSO BORDERS ON GELLATLY NUT FARM. REMOVE STREET PARKING ON GELLATLY BETWEEN WHITWORTH RD AND COVE = IMPROVED SAFETY.	2/2/2023 7:59 AM
48	An idea that looks nice on a map but is meaningless in the real world. Apart from Gelattly Bay, there are no drivers for developing these neighbourhood centres.	2/1/2023 4:14 PM
49	These are a neat idea but seem like they are going to function as urban islands that require vehicle access and be parking nightmares. Again increased transit service for urban/neighbourhood centres.	2/1/2023 2:42 PM
50	More density means more traffic on Shannon Lake Road with it's many twists and turns. Speed limits are suggested but rarely enforced and as people on in a hurry speeds exceed 70K most days. The senior mobile home park at Crystal Springs has no way of making a left turn safely either out or in the park. Vehicles pass on the right illegally and on the left illegally going very fast. It is a very unsafe exit and entrance with no plans to alleviate the problem. Site lines are fine if going 50K but are not fine at speeds over 70 or 80K. Flashing lights, more signs and more police enforcement is required.	2/1/2023 1:26 PM
51	Agree as long as the Transportation Master Plan and servicing bylaw are updated to ensure that these densities and heights can be supported by better road system that is age friendly.	2/1/2023 10:39 AM
52	6 max still too tall	1/30/2023 9:52 PM
53	Limit to 3 storeys.	1/29/2023 10:20 AM
54	I agree to limiting heights to 6 stories. This is in keeping with the neighbourhood and the vision for WK.	1/28/2023 9:25 AM
55	Again care must be given to building heights and the waterfront.	1/27/2023 2:48 PM
56	Continued chopping up of former large single detached properties & orchards for sub-divisions is not okay. These areas	1/27/2023 11:30 AM

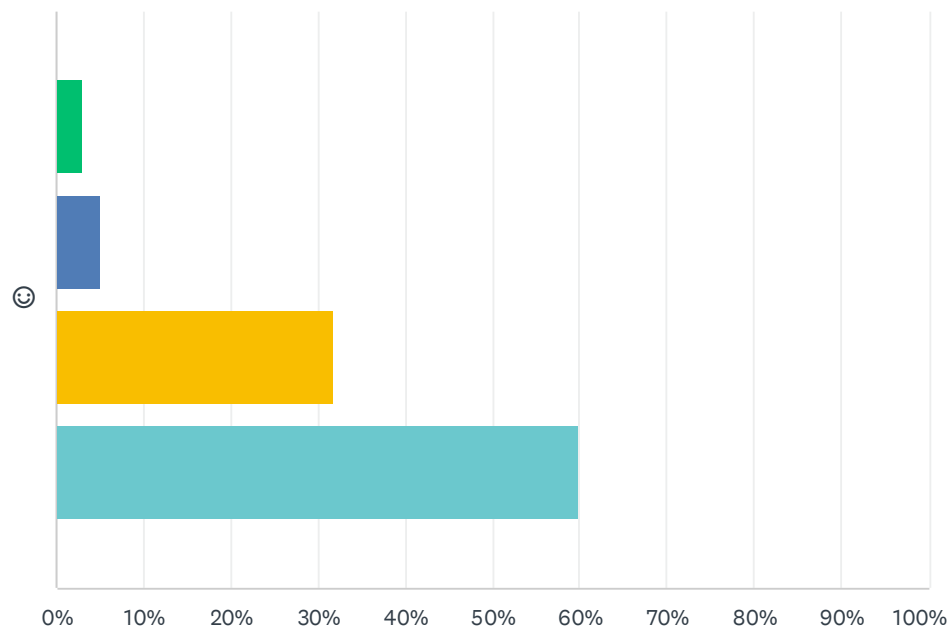
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have given West Kelowna/Lakeview Heights its unique flavour & seem only to be done to create a profit for speculators. There are lots of other "new areas" available for growth. When these "new growth" subdivisions happen, they should always use "NON POLLUTING street lights - very important! As a long-time resident, in-filling seems to be okay. One tower (on lands that used to be a school), to 6 stories is in process, but creating more similar towers is unacceptable. With plans to densify this area, where are all the children going to go to school? And what affect will new schools have on our taxes? Please look to other new areas for major expansion. Just because there is bus service & a small shopping centre doesn't mean that the whole area needs to be densified.

57	Knowing the rate at which roads and infrastructure is implemented, it would seem there will be many years before these things will happen. In the meantime, there will be hundreds of cars using the existing roads. THE INFRASTRUCTURE SHOULD BE DONE FIRST - rather than housing! That's why we have the terrible traffic situation all over West Kelowna.	1/26/2023 11:26 AM
58	Residential areas should keep their residential look and feel. Apartment buildings should only be allowed at or next to Lakeview village. We are not Kelowna and present our city as a good alternative in terms of liveability	1/26/2023 9:59 AM
59	Regarding Smith Creek, the drawings show the current SD23 property turning into mixed use space. What happened to building a school on this property?	1/26/2023 9:43 AM
60	Similar worries as above - the road and service infrastructure can't support, and I don't trust the city developers to plan accordingly as they have a bad history of miscalculating needs.	1/26/2023 9:32 AM
61	Change the Zoning Bylaw 0265 Part 4, Section 4.4, Standard Parking Spaces. Every condo/mixed use building must need to provide 2 off-street parking spaces for every unit.	1/26/2023 8:51 AM
62	Mandatory off street parking. 2 stalls per unit at least	1/26/2023 7:28 AM
63	I do not think re-zoning of any farm land should take place, I also think the downtown west Kelowna coat should be the primary focus.	1/26/2023 6:18 AM
64	Goats Peek neighborhood center should not exist you are taking business away from downtown West Kelowna	1/26/2023 12:36 AM
65	As with 1.2, 6 stories is too limited for the amount of growth. West Kelowna has no room for sprawl. Densities will need to be higher to accomodate further growth.	1/25/2023 10:45 PM
66	Really like the idea of the neighbourhood centres and the opportunity to have more residents in the vicinity with the option to walk to local services.	1/25/2023 8:31 PM
67	This is needed badly in the city. Would be refreshing to ideally wok or bike to an urban center instead of driving down to the 97 corridor. Absolutely love the idea	1/25/2023 8:15 PM
68	It's very challenging to get a grasp on actual areas. It would be helpful to have a map that one can zoom in to see up closer the boundary lines and plans for connected trails and walkways. I went to google maps for a better look but many people likely won't bother to do that. I think it's very important for youth and seniors in each area to have walkable cultural centres where they can participate in meaningful after school activities (art centres for after school and weekend art classes and access to outdoor and some indoor recreational activities and sports. The recreation centre and pool we have is overcrowded and it's very difficult to enrol children in swim lessons as there are never enough spaces.	1/24/2023 7:55 PM
69	Would be interested to see any future plans for the Neighbourhood Centres.	1/24/2023 4:54 PM
70	Build height should not be established by neighborhood.	1/24/2023 4:13 PM

Q9 2. Develop policies that support and encourage the development of the business park and industrial land in the economic core to help increase economic sustainability. View the Industrial and Business Park display board. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 135 Skipped: 28



Disagree Somewhat ... Somewhat ... Agree

	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	2.96%	5.19%	31.85%	60.00%	135	3.49
	4	7	43	81		

Q10 Tell us more:

Answered: 40 Skipped: 123

#	RESPONSES	DATE
1	Maintain regulations and policies of these industrial activities on environmental quality (noise/light/air/chemical etc pollution). Promote new, green technology development/businesses (for whatever industry- materials processing, waste management, whatever). Would be a beautiful concept if westbank could be a leader in sustainable development.	2/7/2023 9:54 PM
2	Glad to see the proposed Industrial area boundary expanded to accept more lands up Bartley and Stevens rd. The Ensign Bros Gravel pit Northwest of Lenz Rd on Bartley should also be included in this OCP as that is the current use and will be for the next 20 years	2/7/2023 9:42 PM
3	Love this space being created	2/7/2023 9:01 PM
4	Please see summary in Item 4 below.	2/7/2023 4:10 PM
5	We need this for both jobs and our tax base	2/7/2023 2:08 PM
6	I like the idea of expanding this area but with less "dirty" industry. More technology companies, office buildings and less noisy	2/7/2023 2:02 PM
7	Employment is needed in city for population growth. Good location	2/7/2023 8:40 AM
8	Yes this is where we need to grow !	2/6/2023 7:11 PM
9	This is essential to create taxes and jobs.	2/6/2023 3:50 PM
10	Agree	2/6/2023 2:46 PM
11	Create jobs in West Kelowna to reduce driving to Kelowna.	2/6/2023 9:55 AM
12	Yes this area is already more or less purposed for industrial use, I agree to continue the development in this area.	2/5/2023 2:12 PM
13	No concerns.	2/5/2023 8:08 AM
14	Most of that area has long term tenures and limited opportunities for change. You should consider allowing institutional uses	2/4/2023 8:03 AM
15	Though it is an eyesore, every community benefits from designated industrial zoning to provide room for large-scale employers. It is probably easier to enhance the existing zone than to develop one somewhere else and befoul some hitherto clean territory. If possible, put light industrial at the edges of the zone, because it can more easily be mixed with residential and commercial to make a walkable neighbourhood.	2/3/2023 8:05 PM
16	This area is a massive eye sore so anything to improve it would be beneficial	2/3/2023 6:04 PM
17	No comments	2/3/2023 4:38 PM

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18	There will definitely be growth within this area. It is a good section to plan and look out for.	2/3/2023 3:38 PM
19	We should also being more dense on bottom of bourcherie mountain off ross road as well	2/3/2023 2:06 PM
20	Most of the commercial developments and stores are on Westbank First Nations land. West Kelowna City must have more integrated planning with the Westbank First Nations Government Office so the 2 plans are compatible and complementary.	2/3/2023 8:59 AM
21	There are homes in this area and my concern is overdevelopment with a lack of planning for the roads to sustain this kind of development and the ability of residents to enjoy their outdoor areas with the additional noise, pollutants and hazards that come with this kind of development. We are already experiencing problems due to the homeless shelter at Bartley Road and the spill over of people who can't get a bed into the creek area cutting down trees and starting fires as well as leaving their trash and belongings behind when they leave.	2/2/2023 6:53 PM
22	Excellent concept to promote business development.	2/2/2023 11:34 AM
23	If a business park is in the plans along Steven's rd. You have to address relocating the homeless. Those businesses will experience the same problems business owners are having in kelowna - breaking, safety.	2/2/2023 9:39 AM
24	Agree	2/2/2023 9:12 AM
25	I support investment in an industrial land study.	2/1/2023 2:42 PM
26	I see a difficult situation here with the competition from WB nations band and affordability	1/29/2023 9:34 AM
27	Economic sustainability is essential to the growth of West Kelowna. Attracting and retaining industrial and commercial business will strengthen reserves and allow our city to meet its infrastructure needs. However, this development needs to be thoughtful and needs to maintain the existing characteristic of our community.	1/28/2023 9:25 AM
28	Seems okay.	1/27/2023 11:30 AM
29	it's about time	1/26/2023 9:43 AM
30	Depending on the industry, this could be beneficial. Clean business with low traffic would be best suited for these areas.	1/26/2023 9:32 AM
31	Build a multi use mall. Add a grocery store (Nesters is the only one in north end). Centralize shopping and give our local teens a place to work here in WK. Keep them engaged and off the streets.	1/26/2023 8:51 AM
32	Let's do more manufacturing here and support our own community.	1/26/2023 8:05 AM
33	Build a mall. Put our local teens to work so they stay focussed and not on streets.	1/26/2023 7:28 AM
34	Again the highway congestion needs to be looked at FIRST!	1/26/2023 6:54 AM
35	The Ensign Bros gravel pit that has been active since 2000 located at 2210 Bartley Rd should be included in the OCP as Industrial land. The City should acknowledge this approved Industrial use rather than ignoring it and by not designating the lands for any future use. This is what a Community plan is for so that community has a direction' The City should at the very least recognize the current uses though out the community..	1/26/2023 12:36 AM
36	With the expected growth of WK, the sand pit needs to be zoned out over the course of the next few years and the land needs to be zoned to allow for all forms of industry. Policies should be implamented to allow for tax incetives to be given to companies wanting to use the space for manfacturing and other growth sectors.	1/25/2023 10:45 PM
37	The gravel pit centrally located is a giant eyesore and potential danger as it continues to eat away at the mountain. Be done	1/24/2023 7:55 PM

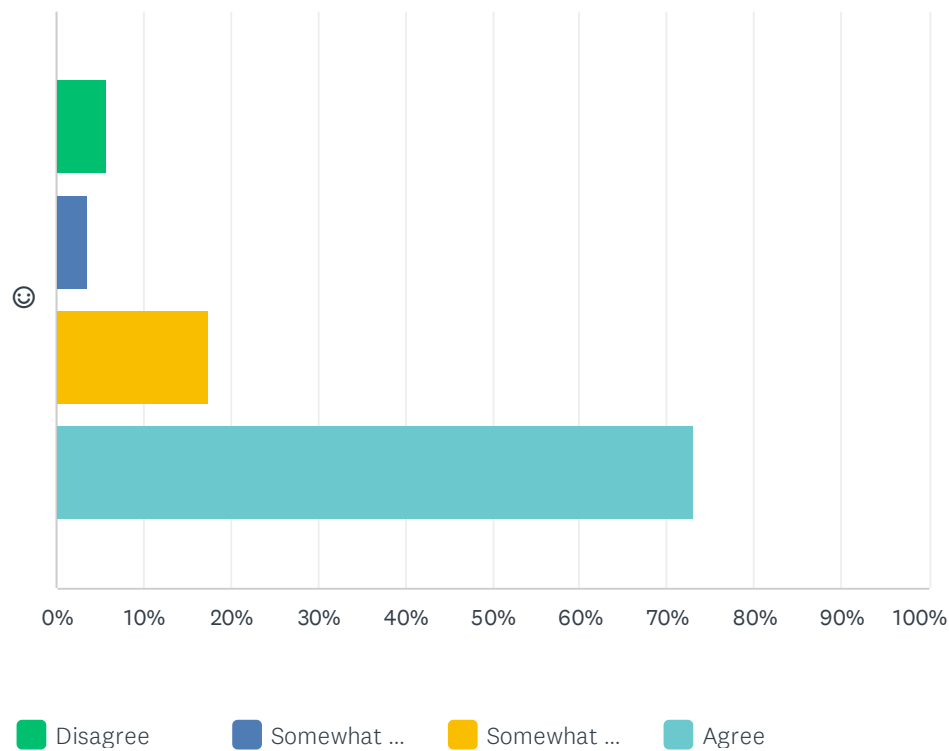
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already. This land could be perfect for a new pool and park (with water park and skate park for kids. An art centre here and artist studios as well as a business centre with trendy shops and cafes and restaurants will make it a stopping place for tourists rather than an area they want to zip through on the highway. Its central location and proximity to the Boucherie hub would be perfect for all the high school students and families living in the area.

38	West kelowna needs more stores and shops for our wonderful community if west Kelowna had more stuff here people wouldn't drive to other places to revive that service. Even having indoor things for children or a outdoor waterslide	1/24/2023 5:03 PM
39	Likewise as above,	1/24/2023 4:54 PM
40	Industrial land is at a premium, we have a huge potential there, do not let retail take over the industrial lands,they will pay off in the future	1/24/2023 4:13 PM

Q11 3. Protect and preserve major creeks, streams and lakes, and enhance green space opportunities in the community through land use buffers and other green space policies. View the Draft Land Use Plan interactive map. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 138 Skipped: 25



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	5.80% 8	3.62% 5	17.39% 24	73.19% 101	138	3.58

Q12 Tell us more:

Answered: 48 Skipped: 115

#	RESPONSES	DATE
1	All of our creeks, wetlands, lake are heavily confined and currently present high water capacity challenges. We are and will continue to be subject to increased frequency, intensity of extreme events like high ("flood"), and low ("drought") events. The water bodies NEED their floodplain capacity back. We can choose to be proactive and listen to what the water is currently telling us, or we can ignore and suffer. Work with agencies such as the Province, Okanagan Basin Water Board, Okanagan Nation Alliance, Westbank First Nation to plan land use around water bodies. Be very careful with hard infrastructure to contain water, this is expensive and has higher consequence of failure. Be very careful around how West Kelowna's land use plan fits into the future of the Okanagan Lake Regulation System (OLRS- Okanagan Lake/River and dams management). Anadromous salmon now have restored passage, adding complexity (but amazing opportunity) to West Kelowna's responsibilities to the fish, land, and water. Let's be a leader for good, not a contributor to environmental health decline.	2/7/2023 9:54 PM
2	The lands marked resource lands that are zoned single family, or country residential, Industrial, etc should be upgraded	2/7/2023 9:42 PM
3	If building on the hillsides, leave lots of trees so our hillsides don't look ugly like Coquitlam.	2/7/2023 6:48 PM
4	Please see summary in Item 4 below.	2/7/2023 4:10 PM
5	Increased traffic and density on Gellatly Bay will be detrimental to the beach, lake and creek	2/7/2023 2:22 PM
6	This is essential and our main priority. Preserve Gellatly Bay and its recreational contribution to West Kelowna. Adding in a parkade to Gellatly will not enhance it but ruin it. Boat trailer parking is Gellatly's major parking problem and a parkade will not solve it. Find a solution to boat trailers parking.	2/7/2023 2:08 PM
7	It is vital to keep our green spaces! However, please build FOOTPATHS so we can walk to those spaces without fear of getting run over (HORIZON DRIVE IN PARTICULAR!)	2/7/2023 2:02 PM
8	Love to see more green space	2/7/2023 1:50 PM
9	Green space is what makes west Kelowna	2/7/2023 8:40 AM
10	Protect the waterfront with those buffers! The space across from the Pier should be used for recreational purposes. How can it add to the future of the city to allow for more people to use the small amount of waterfront better? Especially if the city is growing? Further planning is required. Not 3 tall apartment buildings!	2/6/2023 9:19 PM
11	Preserving the natural ecosystem and beauty is paramount. Growth for growths sake is not the answer.	2/6/2023 8:21 PM
12	Yes please protect our green space, we need to take away WFN land which we have zero control, this could be built on & eliminate a lot of green space	2/6/2023 7:11 PM
13	This will protect our environment and make communities liveable.	2/6/2023 3:50 PM

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14	Somewhat disagree. It's not clear how the areas (such as proposed low density residential, yellow) that don't currently have housing on them were selected? There are some new additions in the proposed OCP that were not included previously. Are these under review or have proposals submitted?	2/6/2023 2:46 PM
15	Enhance connectivity within neighbourhoods by using greenbelts for walking paths. Where this access is non-existent within a neighbourhood, allocate funds to purchase the necessary easements to achieve this connectivity between the street and the greenbelt or park area. E.g. Sandstone Park between Ridgerock Way/Shannon Way and Sandstone Dr/Sandstone Cr lacks any access into the park from Ridgerock Way and upper Shannon Way. There was an access into this greenbelt off of Upper Sundance Drive but a new development may sever this (see EPP74050).	2/5/2023 11:30 PM
16	Gellatly Bay - Willow Beach should NOT be a neighborhood Center.	2/5/2023 10:20 PM
17	There needs to be more Green space near the lake. Theses areas need to be preserved for future. Do not build any high density near the lake!!!	2/5/2023 2:12 PM
18	We are a little concerned about the Goats Peak Development plan, as the water treatment facility and Gellatly Road South Road structure, does not support the additional planned population growth.	2/5/2023 8:08 AM
19	The water table at Willow Beach is very low and no buildings should be in that area and any building should not be adjacent to Rotary Park as that floods every year	2/4/2023 3:50 PM
20	If high density development is allowed along the Powers Creek and Gellatly Bay Area it will have a dramatic effect on green space, walkways. We have an excellent opportunity to Purchase additional lands in this area adjacent to Willow Beach and the Gellatly Nut Farm. This foresight will give future generations parks and walkways and protect the creek and foreshore. We only get one chance at a park system	2/4/2023 3:19 PM
21	Consider creating a trail system through neighborhoods that attach to businesses. West Kelowna needs more green space, especially in adjacent to city blocks where you are increasing density and putting in neighborhoods centres. You need to provide increased green spaces where people won't have yards (ie Anders and Teal new houses on the road with no parking).	2/4/2023 8:03 AM
22	Green and riparian spaces must be protected and should not be violated for any reason. They are not only aesthetically-pleasing recreational spaces, but essential providers of environmental services that every member of the community relies on for health and well-being. Any development of any existing green space must only be done in a way that enhances the biodiversity and health of the ecosystems that occupy that space. This includes remnant green spaces within the Growth Boundary.	2/3/2023 8:05 PM
23	Green space is highly important to sustain our ecosystem and its inhabitants	2/3/2023 7:36 PM
24	As a new Shannon lake resident, I would have like to have a trail that goes all around the lake. It would be just amazing and a great for the entire community to gather, exercise etc.	2/3/2023 6:17 PM
25	There is a lot med density housing but no mention of road changes. Round about, over passes. I live off Shannon Lake Rd and sometimes have to wait few minutes just to get out met driveway. The roundabout has helped the one way onto Bartley but if you drive the other way a lot of work needs to be done on the road system before anymore building should take place. A roundabout has been planned for Tallus Ridge for many years and that has not happened yet. Coming in and out of Smith Creek (and the landfill) is dangerous - another roundabout is needed there. Seeing how long it took to build the roundabout on Shannon Lake Rd this should be done before any more building in put in place. Overpasses and Underpasses need to be addressed on Hwy 97 as this is where all the extra backlog ends up. People live in West Kelowna for the small town feel. I do not want to loose this.	2/3/2023 5:09 PM

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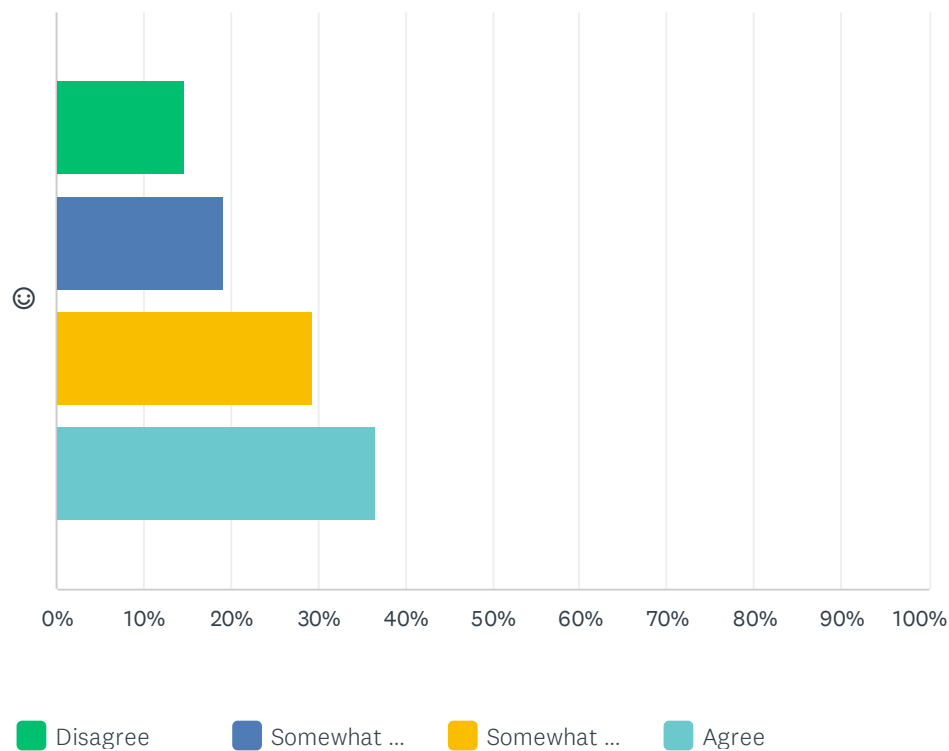
26	See previous comments and concerns	2/3/2023 4:38 PM
27	Need to ensure there is green space in all community areas, with the exception of industrial areas. Need to encourage more pocket parks and linear corridors connecting different areas together. Need policies to protect trees.	2/3/2023 2:55 PM
28	We need more mix of housing including duplexes, fourplexes and townhomes. We need to get creative on family housing	2/3/2023 2:06 PM
29	The land set aside to connect Menu Road to Pinot Noir Drive and Gregory Road needs to have paths, stairs and lights installed.	2/3/2023 8:59 AM
30	The City of West Kelowna doesn't seem to be protecting and preserving these areas that they currently have.	2/2/2023 6:53 PM
31	Agree	2/2/2023 9:12 AM
32	EXPAND ROTARY PARK WITHIN GELLATLY NEIGHBOURHOOD CENTER FOR FAMILY USE OF A PICNIC AREA ALONG SIDE POWERS CREEK	2/2/2023 7:59 AM
33	A nice idea, but there is no evidence that the City itself supports this.	2/1/2023 4:14 PM
34	There is some incredible space in Glen Canyon that the CoWK should look to incorporate in its future development plans in Westbank Urban Centre. I hope we can utilize this beautiful space in collaboration with the city's plans. I.e. this could be akin to Vancouver's Stanley Park for our community.	2/1/2023 7:45 AM
35	Think the restriction of cedar hedges would go a long way in fire safety and also preserving water as they are extreme water drinkers. Also feel there should be a restriction on the height trees are allowed to grow in residential areas- become too high, dangerous, and block views of neighbours.	1/31/2023 7:38 PM
36	Keeping green space in communities keeps us with the nature we are surrounded by. Our kids need nature to play in instead of having to commute towards nature	1/31/2023 3:08 PM
37	There needs to be wildlife considerations so wildlife still has the ability to feed and traverse with consideration to prevent human wildlife interactions.	1/29/2023 9:34 AM
38	Out of control growth, with no consideration for WATER, WATER, WATER. Just look at the rest of the planet. Coming here SOON!!	1/26/2023 11:26 AM
39	educational/institutional is showing on the former garbage dump - what does this mean? are we planning to build a school on the former garbage dump? I am seeing low density residential taking up a big piece of green space in Smith Creek - how does this preserve green space policies? it is also not clear how the West Kelowna waterfront is being preserved for recreational purposes.	1/26/2023 9:43 AM
40	It would be great if the city actually did this, but once again, this has not been the case. You are actively allowing Shannon Lake to be developed. One of the only pristine wildlife areas in our city is half a golf course, and will soon be half condos. Nobody believes that the city is planning on stopping there until most of the lake has been developed. Your trust is tarnished.	1/26/2023 9:32 AM
41	Put more pressure on the Province for aid. More natural resource officers and workers are needed.	1/26/2023 8:51 AM
42	Once this space is gone, it's gone. PLEASE make the choice to preserve green space.	1/26/2023 8:05 AM
43	Put more pressure on the Province for assistance. Need more natural resource officer aid and brush cleaning	1/26/2023 7:28 AM

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44	There are many small swaths of land that are unmanaged that would make excellent parks and recreation areas. The land between the campground and walmart is an example of this.	1/25/2023 10:45 PM
45	Institute a requirement for system of grey water or recycled water for public green spaces and ensure that drought tolerant plants and little grass is used in these centers.	1/25/2023 9:35 PM
46	It is absolutely essential to preserve and create more green spaces and community centres and access to resources. A thriving community is one that has green space and ways for community members to connect in meaningful ways. We need to encourage and create maker spaces and places for people to come together in a spirit of connection and forming a safe, respectful and equitable community. More art spaces are essential.	1/24/2023 7:55 PM
47	West kelowna needs to be more developed	1/24/2023 5:03 PM
48	Protecting green space & wildlife is also very important to the area.	1/24/2023 4:54 PM

Q13 Tell us on a scale of 1 (disagree) to 4 (agree) whether the Draft OCP Plan Update addresses these four focus areas. Scale 1 – 4 (disagree, somewhat disagree, somewhat agree, agree).

Answered: 136 Skipped: 27



	DISAGREE	SOMEWHAT DISAGREE	SOMEWHAT AGREE	AGREE	TOTAL	WEIGHTED AVERAGE
😊	14.71% 20	19.12% 26	29.41% 40	36.76% 50	136	2.88

Q14 Do you feel there is anything missed in the information presented that may contribute to the success of the Growth Concept? Tell us more:

Answered: 79 Skipped: 84

#	RESPONSES	DATE
1	Would be wonderful if park areas can maintain natural values, vs becoming groomed spaces. Would be even better to incorporate syilx land Stewardship practice and ways of seeing/known into land management. Otherwise, was very happy to see the pedestrian/bike route improvement plans and consideration for expanded public vehicle transportation. I *think UBC-O has a "Sustainable Communities" lab with some great ideas (Lead Professor escapes me)- have DWK staff connected with the UBC-O lab yet?	2/7/2023 9:54 PM
2	Yes by not designating single family residential areas outside of your proposed growth area the City is not providing more opportunity for attainable and affordable housing. This OCP does not provide areas of choice for all realms of home owners The City needs to upgrade the access roads and parking areas on the outskirts of the City promoting access to the Crown lands surrounding the City The City has to creat a road connection on the south side of the hwy from Hebert Rd to Gellatly Rd	2/7/2023 9:42 PM
3	Transportation bike trail ..sidewalks, connecting neighborhoods by foot / bike lanes everywhere !!	2/7/2023 9:01 PM
4	Why is there a lack of work being done on increased lighting by change over to LED lighting? Over 5 years is ridiculous. With crime increasing constantly, improving lighting has proved to help in other cities. Do it now.....capital cost which would be capitalized over the next 5-10-15 years. Especially downtown Westbank, proper lighting would led to people wanting to use the area more.	2/7/2023 8:27 PM
5	I couldn't find information on using the waterfront for recreational purposes. If it is in the OCP it isn't very obvious. Attainable and affordable housing is important, but has zoning for manufactured homes or tiny homes been addressed? All I am seeing is higher density in terms of infill and an increased number of storeys in buildings. This is affordable and attainable housing, that promotes higher density housing while allowing people to still feel like they live in their own place. Many people do not want to live in apartments that are made of wood due to sound issues.	2/7/2023 6:48 PM
6	Regarding the rezoning file Z 17-02 for the rezoning of the properties, 3290 GLENROSA RD, 3280 GLENROSA RD, and Salmon Road Plan KAP 777 Block 105, DL 3190 ODYD: Good afternoon XXXX, Further to XXXX email to XXXX that we understand was forwarded to Engineering (attached for reference). We understand that you are the contact for collecting feedback on the Traffic Management Plan. As you may know, we have an active rezoning application file with XXXX under West Kelowna file number Z 17-02. The application includes the three properties identified in the below list and image. The properties were granted an OCP amendment moving OCP from resource to Single and low density residential. The rezoning application sits at 3rd reading as the developer, XXXX has run into brick walls during land acquisition and negotiation processes with landowners. Refer to D.E. Pilling & Assocaites Ltd drawing 2463-P2 (rev.3) in the West Kelowna file Z 17-02. The land acquisition was deemed necessary to provide adequate right-of-way width to construct the modified road standard as shown on drawing 2463-X2 (rev.1). I note that the road standard meets the modified road design criteria agreed upon by West Kelowna as cooperatively determined with our office and the city's traffic consultants. The rezoning file is at a stalemate at 3rd reading due to the land acquisition roadblocks and largely due to the	2/7/2023 4:10 PM

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extent of the upgrade costs. You will see in the file that a requirement of this developer is to agree to upgrade and build Glenrosa Road from Gates Road approximately to the design presented on drawings 2463-R1 (rev.1) and 2463-R2 (rev.2). The construction estimate of this road upgrade from 3 years ago was too high to distribute amongst the yield of the 94 - R1 Single Family Lots and two multi-family sites of low density residential. The estimates showed a near breakeven project. This developer is looking to bring online over 100 units to West Kelowna with this project, but this Glenrosa Road upgrade has fallen on the lap of this developer stopping the project dead in its tracks. The file also includes a series of upgrades to Salmon Road. What is needed here to bring these homes online is to bring this development into the feasibility column by putting this section of Glenrosa Road into the DCC program so that DCC credits can be issued when this road is constructed. The development will be contributing largely by building a booster station that can one day be used to lift water to future reservoirs as well reservoir DCC's plus numerous other benefits including providing housing for 350 to 400 people. The city is wondering why this file sits still, but it's the extensive offsite upgrade costs that put this project in the non-feasible category which is preventing the developer from advancing to development permit and subdivision application. We ask West Kelowna to seriously consider this. This would almost certainly result in advanced applications and ultimately construction and the results will only greatly benefit the community. Please review and let us know your thoughts. D.E. Pilling & Associates Ltd. on Behalf of XXXX

The growth boundary should also be expanded to include 3275 Glenrosa Road and 3255 Glenrosa Road which are properties owned by the XXXX family, and 3349 Glenrosa Rd owned by XXXX. These owners and or interested buyers have contacted our office numerous times with interest in developing these lands. With Glenrosa Road from Gates to Turnbull as a DCC road, development will advance a proposed reservoir at elevation 801 which would serve a pressure zone up to elevation 773 with domestic pressure. This is shown on D.E. Pillings drawing 1540-W1 which is a water system concept plan provided to West Kelowna in 2011. An existing 400mm DI water main is aligned east west at the north end of the Trails and was installed in 1997 and was intended to feed reservoirs in the long term as shown on drawing 1540-W1. When we constructed the Glenrosa roundabout with the Trails in 2019, a water main crossing across the road was contemplated by West Kelowna so that this pipe would be in place to avoid future road cuts. The pricing came back and West Kelowna chose not to proceed, but that extension and this 400mm pipe is intended to feed these reservoirs. The line was to go west from the Trails and then northwest to Gates and straight west south of 3580 Gates Road to Gill Road and Turnbull to the proposed reservoir at 801 masl. As this route has become more unlikely due to adjacent parks and SRW's that were not acquired with development in general accordance with drawing 1540-W1, there is still realistic opportunity to get these mains and reservoirs planned and accounted for in the 2040 OCP to service upper Glenrosa by gravity water systems. To do this, we need this Glenrosa Road built and it starts with including Glenrosa road from Gates to Turnbull in the DCC program which gets a huge push forward with working towards that with file Z 17-02.

7	Well presented	2/7/2023 2:26 PM
8	Prevent more housing and commercial development on Gellatly Bay	2/7/2023 2:22 PM
9	The OCP must actually address preservation of parks and expand our waterfront for recreational purposes. Building high rises and parkades is actually doing the opposite in Gellatly Bay. - ADD direction on building on "KNOWN FLOODPLAINS" especially given our flooding in 2017 and recent atmospheric events in BC in Nov of 2021. -ADD that evacuation plans for fire and flood must be considered for new large developments	2/7/2023 2:08 PM
10	I am very disappointed that there is no commitment to look at safety improvements on Horizon Drive. There were 3 fatalities on this street in a 4 week period last summer. Every time I walk my dog I am risking my life Cars drive MUCH too fast on this road TWICE there has been a MVA outside my house and once ended up right in my driveway destroying landscaping and causing a lot of damage. There is a park across the street from my house (Horizon Park) with NO crossing NO warning signs and on a blind bend. Ridiculous. There is a beautiful park at the top of my street - Rose Valley Park - but	2/7/2023 2:02 PM

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to get there to hike or bike we have to walk or cycle in the road. There are not even any street lights. Please reconsider prioritizing before there are further fatalities.

11	Yes your idea of a community is money making and not very thoughtful of people	2/7/2023 2:00 PM
12	Opportunity to expand the West Kelowna waterfront would be lovely but not putting high density housing in. We already need more parking and have a high use with walkers, boaters, yacht club and parking in the area.	2/7/2023 1:50 PM
13	City Council has too much leeway in allowing for buildings 6 storeys and higher. Buildings of that height should be limited to the Westbank Town Center. Another extremely important and pressing issue is that there needs to be another bridge to connect with Kelowna. I realize this is out of City Council's control but they should be doing everything in their power to influence and impress upon those that do control this matter.	2/7/2023 1:39 PM
14	Recreation and parks is needed for the future growth.	2/7/2023 8:40 AM
15	I do not see where the "opportunities to expand the use of WK waterfront for recreational purposes" has been placed? I am only seeing those opportunities diminish with the Gellatly Bay neighbourhood Center. Please please please think very carefully for the waterfront use. Selling out to a builder will hurt the people of West Kelowna for the future and it will never be able to be replaced.	2/6/2023 9:19 PM
16	Transportation planning does not address the city's need to push the provincial government for a bypass or second bridge location. If there is continuous building of multi-storey housing it creates a further bottleneck on highway 97 into Kelowna. There needs to be a part of the plan that addresses how to work with local businesses to have West Kelowna as head offices instead of needing West Kelowna residents to drive into Kelowna every day.	2/6/2023 6:10 PM
17	Preservation of the waterfront and providing parks and natural areas has been less emphasized than growth and density. It is truly important to understand that these priorities are what have drawn people to West Kelowna and truly make our city special.	2/6/2023 3:50 PM
18	Disagree - on the parks and waterfront recreation aspects. Two suggestions: 1) New proposed low density housing in the OCP 2022 should have a green space requirement based on the size of the area and /or # of lots being developed. If a cluster of lots are being developed - for example, >5 lots there should be requirement to have a portion of the area as undeveloped green space. This would clearly show the city is committed to environment protection and continued housing development. 2) There is a dearth of safe, easy access launch points for the general public for smaller boats, canoes, kayaks to Okanagan Lake. Apologies if I missed it, but I did not see a strategy or action plan to identify and develop potential sites for lake access for recreation users on the Westside.	2/6/2023 2:46 PM
19	The Gellatly neighbourhood centre should be a much lower scale to fit into the neighbourhood better. Parking for the boat launch is a priority which will become even busier with a new development removing the boat storage business. Together with the Goats Peak development, this small beach will be over run. Also, the Community Plan should address a long term Highway bypass route which will change the downtown urban design.	2/6/2023 9:55 AM
20	Encourage cycling safety by creating bicycle lanes that are separated from the roadways by solid barriers. High priority: purchase lands still in a natural state for new park areas or to enlarge existing parks because these lands are scarce. The larger the park area, the better it will preserve the ecosystem and biodiversity. In larger parks with wildlife like deer present like Rose Valley and Goats Peak Parks, limit trail location and density to ensure there is sufficient undisturbed habitat for these non-human inhabitants to continue to thrive!	2/5/2023 11:30 PM
21	These items are addressed, but perhaps not to the betterment of West Kelowna. We need to preserve our waterfront and creeks. By adding hundreds of residences in close proximity to the waterfront does NOT preserve parks and natural areas,	2/5/2023 10:20 PM

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and will increase traffic in an already overburdened area.

22	Beware of 'affordable' housing! Will such housing continue to cater to the wealthy sector? Where is the advantage and for whom?	2/5/2023 5:52 PM
23	regarding focus areas: biking, hiking, walking trails!!! we need more! they have to connect to each other. West Kelowna residents should be able to safely bicycle into Kelowna or Peachland on designated paths. public transit needs to be more of a priority, and providing more parks..absolutely!!! West Kelowna's waterfront for recreational purposes, yes exactly. Not more housing and an urban center near waterfront. Recreation only and easy access by a network of biking, walking paths. People can park in the urban centers and access these areas by pathways or public transit. When you look at larger centers like Vancouver there are numerous ways to access waterfront but little parking nearby. Can't have both. Preserve the beauty of our waterfront and green spaces!!!! first and foremost.	2/5/2023 2:12 PM
24	See the letter from the Gellatly Bay association. Our Bay not not be everything to all people. We only have limited space. Priority must be people not more development.	2/5/2023 11:15 AM
25	We are particularly excited to finally see the waterfront growth plan going forward. Areas of concern for us are..... - Need for sidewalks on Whitworth Road and from Whitworth to the Glen Canyon Park access. - Expansion of the Boucherie Wine Trail to include bike trails from Gellately Road to Quails Gate.	2/5/2023 8:08 AM
26	I would suggest for the city to have focused density to urban centres in the first part of the OCP (first 2-5 years) until developers feel comfortable with the rezoning, city process, variances allowed and there is some progress made with development and then expand out in future years(Smith Creek, Glenrosa). Going to broad too soon may not give much direction if growth and I feel will bite the city in the end as developers will build on cheaper land(Smith Creek) before building in the city centre which has a bigger overall benefit to the community and will provide more commercial opportunities and investment to keep shopping in West Kelowna sun give more spaces for daycares a	2/4/2023 6:48 PM
27	Totally agree - which is why it is vital to preserve the Gellatly Bay waterfront for recreational purposes - we have very little other waterfront accessible and available for that purpose.	2/4/2023 4:31 PM
28	Missing is a detailed recreational plan for lakeshore development along Boucherie and Gellatly roads. As mentioned in several letters to City Council in the past , please read the Westside Weekly August 4/10 article regarding " doggy beach" and suggestions to relocate it to another location to make that beach available to children. Note July 27/2010 letter from District of West Kelowna for information as well as 2011-2012-2013 newspaper articles.	2/4/2023 4:04 PM
29	We continue to entertain density in areas with road systems not designed to handle the traffic. Gellatly/Boucherie corridors is going to be as traffic clogged as Kelowna's Lakeshore Road	2/4/2023 3:19 PM
30	In your transportation planning ideas, I do not see any plan for a sidewalk on Gregory Rd from Shiraz Ct to Boucherie Rd. I, and other people, have been appealing to City Hall for this sidewalk but keep getting basically stonewalled. The City has determined that a multi-purpose path be implemented along Boucherie Rd, and I agree that this is a good idea. However, many citizens (pedestrians) would like to use Gregory Rd to access this path, but do not have a sidewalk to do so. But even beyond this point, there should be a sidewalk along Gregory Rd for many other reasons. I have expressed these reasons previously to City Hall, but I will say them again. Gregory Rd is a residential street that gets high-volume, high-speed traffic. It is not safe to walk this street where there isn't a sidewalk. And there is no way, if this street were to be developed today, that it would be allowed to proceed without having a sidewalk. The City seems to be hiding behind a "grandfathered in" justification for not completing this sidewalk, which I find unconscionable. Yes, I know this will cost money, but sometimes the City has to just bite the bullet and get necessary things done. If taxes have to be raised somewhat to accommodate the needs of the community, then taxes should be raised. A sidewalk on Gregory Rd would not only be utilized by the people living along this street, but even more so by the people who live higher up on Boucherie	2/4/2023 1:08 PM

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Mountain areas, Ridge Blvd, etc. The only other way down to Boucherie Rd is Mission Hill Rd, and there is about a km between Gregory Rd and Mission Hill Rd. So, for pedestrians coming down Vineyard Dr from Boucherie Mountain areas, Gregory Rd is the only logical choice to get down to Boucherie Rd. Taking your dog for a walk, walking to visit someone, walking with your baby in a stroller, children walking to school, walking to a bus stop, walking to any destination, walking for exercise or just for enjoyment, these are all reasons to have a sidewalk in a community. The City should be doing all it can to encourage people to walk, and by not completing this sidewalk on Gregory Rd, the City is sending out an entirely different message. If the City is being truthful and sincere about improving walking conditions in communities, completing the sidewalk along Gregory Rd would be an obvious and much appreciated step.

31	I strongly agree with all four focus areas. Having the highway cutting West Bank in that way in my opinion is a major problem and hampers the growth and the development of that area. This side of the lake is as beauty as the Kelowna one but far less developed. It could create business, job opportunity and recreational areas if developed properly. Having more bike corridor would be nice.	2/3/2023 6:17 PM
32	It would be nice to have a comprehensive sidewalk network in places where people actually want to use them and that are not leading to nowhere as we see all over West Kelowna.	2/3/2023 6:04 PM
33	I still feel the OCP draft is addressing many areas that have already been developed. People have spend good money and do not want to see zoning changes where they live. Roads and infrastructure changes need to happen before added density can even be talked about. Although a vibrant Westbank Centre sounds great on paper, until the highway running through the middle of it can be addressed this is not easily obtainable. Once again - in theory better transit sound great but when a bus goes half empty is it really saving that much emissions. Unfortunately it is not just our transit system. If I took the bus into Kelowna could I easily get around once there as well. For students this would be a big plus as there is not much option for the majority to take a bus to and from work even in West Kelowna. It is pretty tough to live in the Okanagan without a vehicle so unless huge infrastructure changes happen (rapid train etc) I would sooner see money spent improving our roads -overpasses, round a bouts to keep traffic flowing. This would help the emissions far more than adding more busses that few use. Finally, not sure what you mean but make the most efficient use of the land. If it must means building up higher than I am not in favour of this.	2/3/2023 5:09 PM
34	Our growth is not sustainable due to lack of water. We need to stop over populating areas.	2/3/2023 4:06 PM
35	I don't see how there are plan to expand the waterfront for recreational use as it appears that the OCP is trying to open it up to denser development which will only decrease opportunities to use this already crowded recreational area.	2/3/2023 3:39 PM
36	Transportation planning and parks and natural areas are well planned for as far as I know. I have not seen anything about the West Kelowna Waterfront. The only way to create opportunities for attainable and affordable housing is to bring the housing market down. This is accomplished by establishing more competition (having more houses for sale). In order to do that, you need developers. And in order to have developers you need land. The OCP should really have consideration for land-owners and developers, especially if you are looking at the next 20 years. The city should be encouraging and working with developers, not making the process so hard it is almost not worth the effort. And you should definitely be looking at the WHOLE of the west kelowna district, not just certain areas.	2/3/2023 3:38 PM
37	The current and future Transportation plan is missing the obvious addition - Uber. The current for hire transportation available to West Kelowna (and in Kelowna) is awful and does not meet the demand of either residents or tourists - while road improvement, bike lanes, sidewalks are critical and major bus routes help many people get around, does council really think bus transportation is the future? When was the last time you took a bus? or hosted visitors to our city and decided that the bus was the best option to see it or get to a destination?. That is small town thinking. Kamloops has Uber... We have >13 wineries and countless restaurants - many of them world class destinations/venues visited by thousands of tourists from all over the world - but their experience in getting to and from these great locations where they would likely be	2/3/2023 3:27 PM

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imbibing (exception wine tour shuttles) is the bus? or a crappy taxi that takes ridiculous time to arrive for pick ups? Would the benefit to our city beyond the obvious better overall experience, be improved road safety (via reduced DUI drivers/accidents) and more tourists spending more \$ not also be a new and ongoing source of important tax revenue via licensing fees? Or are the taxi companies too big of a campaign donation contributor still? What the hell is holding up this move? especially looking 20 years out - its not even a consideration... be a leader and watch the balance of this region quickly follow or continue to be seen as small rural BC town (not a city) with an antiquated vision of the future.

38	Yes....council continues to ignore Boucherie Road. This is a major thoroughfare that is increasingly seeing vehicle usage with the addition of winery's, breweries, etc. The tax revenue generated from the business's along this thoroughfare is not proportionately being invested into the roadway improvements which is drastically required. The proposed pathway and bike lane does nothing to improve this vital transportation link in WK.	2/3/2023 3:13 PM
39	Need to increase sidewalks and bike paths on the west side.	2/3/2023 3:03 PM
40	I have one suggestion regarding achieving the desired "in-fill" into existing R1 zones neighborhoods. There are many properties I am certain that do not meet the required frontage of 16 meters and consequently would need to apply for a rezoning on the smaller frontage piece of the property, in order to facilitate a subdivision. This regulation, in my opinion, is hindering the attainability objectives contained within the draft OCP. I understand that there is a zoning regulation that requires an increased 'side yard setback' on the side abutting a neighboring R1 zoned property. I question the need for this regulation where an application involves a change of zoning only due to the lack of frontage to the property. There are some exceptionally large single family properties zoned R1, which would qualify for subdivision, creating another freehold titled property, should this regulation of increased sideyard setback be removed when involving a change to one of the "compact housing" zones.	2/3/2023 2:51 PM
41	more rec facilities like a new swimming pool and new arena more growth downtown a large city central park more parks in general	2/3/2023 2:06 PM
42	Why don't we enhance the parks we already have. There are parks within west kel that are great and others are crap. I drive by Shannon woods every day. What a mess that parking lot and area is. Revitalize and put money into what we already have and it will in turn pay dividends.	2/3/2023 2:05 PM
43	Additional boat launches need to be included in plans.	2/3/2023 8:59 AM
44	Safety for seniors. With percentage of seniors in WK ever increasing, communities such as Crystal Springs MUST have better access/egress to/from main arteries via automobile without putting our lives at risk. Access lanes to merge into traffic is a must and does not appear to be addressed in the current scheme.	2/2/2023 8:08 PM
45	I feel that the City of West Kelowna needs to grow and expand its tax base; however, I don't believe that this plan realistically allows that. It puts in place growth of housing without the necessary improvements to streets and access to the highway - which is our main thoroughfare. Highway 97 is congested frequently and the safety of the highway (while I realize this is the responsibility of the Province) is lacking - there are no street lights for long stretches, the lines and/or barriers can be difficult to see at night or in inclement weather. Roundabouts are built *everywhere* without realizing how traffic turning from the highway into them causes backups out onto the highway. Boucherie Road, which accesses our biggest tourist destinations of the wineries, is in great need of an overhaul. It could be such a beautiful drive if not for the very narrow road and the state of disrepair. If I didn't know better, I would think that all this planning is being done in Victoria instead of by people who live here.	2/2/2023 6:53 PM
46	It appears there are more plans to expand the development of Gellatly Bay for commercial rather than recreational purposes. How can you expand recreational use of Gellatly Bay without acquiring more land of which there is none to be had?	2/2/2023 11:34 AM

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47	why are you pushing a growth concept??	2/2/2023 10:44 AM
48	Better access in and out of Crystal Springs at 1850 Shannon Lake Road is required. Shannon Lake Road is now designated a main artery no. 1 priority road and traffic has substantially increased making it difficult, if not dangerous entering and exiting Crystal Springs.	2/2/2023 10:27 AM
49	Access in and out of Crystal Springs mobile home park is scary. Trying to get onto Shannon lake rd. In both directions is unsafe. Traffic moves at 70 clicks. Slowing down on Shannon Lake in order to turn into the park is a real issue for residents. Drivers are on your tail, swerve around you and pass, and then brake at the bridge. Turning lanes in both directions would help. It will continue to be u safe with increased traffic.	2/2/2023 9:39 AM
50	Agree	2/2/2023 9:12 AM
51	Focus more on sidewalks for students walking to school. They do not have to be fancy, gravel walkways will suffice. Let us provide safe walkways to our schools!!!	2/2/2023 9:05 AM
52	MORE THOUGHT IS REQUIRED ON HOW TO KEEP THE LAKE FRONT AVAILABLE TO ALL RESIDENTS OF WEST KELOWNA TO ENJOY LAKE NOT ONLY THE 1% THAT CAN AFFORD LAKE FRONT HOMES. KEEP DENSITY LOW NEAR LAKE, WE DON'T NEED HIGHRISE CONDOS ON LAKE.... KEEP HIGH DENSITY IN URBAN CENTERS.	2/2/2023 7:59 AM
53	Would be possible to have at least one sand court set up for beach tennis instead of all of them set up for beach volleyball ?	2/1/2023 2:45 PM
54	Celebration, enhancement and protection of existing agricultural lands.	2/1/2023 10:39 AM
55	Traffic is huge issue and what I see exacerbates the already terribke situation. E.g., i have counted as many as 72 cars backed up on Old Okanagan and Butt Rd.	1/30/2023 9:52 PM
56	Recreational facilities. I would argue recreational facilities need to be pushed for the EXISTING amount of housing let alone after "More opportunities for attainable and affordable housing" is pursued. Example; Johnson Bentley Aquatic Center - will hit it's capacity soon and is lacking for older kids/teenagers, the hockey arena needs updates. Sport facilities, and recreational activities for teenagers	1/30/2023 12:08 PM
57	I did not see plans for sidewalks, transportation and nothing about expanding the use of West Kelowna waterfront. There is so much missed opportunity, look at Peachland waterfront. We need at least a coffee shop or something. There are also no street lights, not nearly enough, its so dark everywhere.	1/29/2023 10:46 AM
58	Boucherie Road Multi-use Pathway needs to be completed up to Pritchard Road in this phase. Please do not stop at Green Bay. This is a serious safety issue.	1/29/2023 10:20 AM
59	When considering park and recreation areas Do NOT destroy the ambiance of the neighborhood like was done with Prichard Park. Ensure sufficient parking and tranquility of the neighborhood to ensure the residents are not affected by traffic noise and congestion.	1/29/2023 9:34 AM
60	The Growth Concept Plan seems to focus on building (residential, commercial, industrial) and omits infrastructure.	1/28/2023 9:25 AM
61	Be very careful on how you proceed!!	1/27/2023 2:48 PM
62	Sidewalks in an older established area are not needed. Affordable housing, if there is such a thing, should be in major centres.	1/27/2023 11:30 AM

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63	This OCP plan gives the illusion that these things are actually being considered. But it seems the community is being given lip service. SMART PLANNING needs to be done. Not all growth is good.	1/26/2023 11:26 AM
64	Please make sure that there is also consideration and investment for arts, culture, and heritage.	1/26/2023 10:01 AM
65	I do not have access to the growth concept. If we want a vibrant city a better use of our waterfront is vital.	1/26/2023 9:59 AM
66	where can we find out about sidewalk improvements/additions, new parks, and better transit?	1/26/2023 9:43 AM
67	This plan is lackluster and abstract at best. The Glenrosa neighborhood just had a death from a pedestrian walking on the street, due to lack of lighting and sidewalks. Children walk in these neighborhoods, and people speed in their vehicles on the same roads due to there being only a single residential road for entry and exist of this large area. I did not see any talk about adding road infrastructure to alleviate this problem. (A problem that has been talked about for several years now, and a problem I still see to not be recognized by the city).	1/26/2023 9:32 AM
68	I think you mean preserving parks?	1/26/2023 9:19 AM
69	Please look at Delray Rd.	1/26/2023 9:12 AM
70	Need to create a better flow for traffic on the highway	1/26/2023 6:54 AM
71	More bike paths/trails that are both safe and allow bikes to be used instead of cars to move around various areas in West Kelowna	1/26/2023 6:53 AM
72	#1 Is traffic - all the housing has caused major issues with downtown west Kelowna traffic flow, this needs to be addressed before more houses and apartments go in, also the time to get to Kelowna in traffic in the morning. The current issues need to be resolved. This was said to be 2019!? #2 development of business area in west Kelowna - many Main Street properties are holding on until they know what will happen with the current to develop, this needs to be addressed and pressed with the ministry of transportation, fixing this will help traffic and promote business in downtown keeping more west Kelowna people working on the Westside. It will help tourism and bring in higher tax revenue, taking some of the burden off the residential tax payers. I think in regards to green spaces we are really seeing issues now in downtown west Kelowna with all the new apartments that charge a large sum of money for parking so the streets are covered in cars, and they allow pets with no where to run them. This has caused major problems and expenses for the farm directly besides the apartments. Developers should have to address these issues in the future, low cost housing work around is \$250 parking spots... allowing dogs with no where to let them play should also be addressed in the development proposals. I agree with the development but with more thoughtfulness to traffic and effect of the people that live around these places and how it will effect them. In regards to preserving more parks green space I think this is good moving forward but I don't think expanding green areas on existing properties/lands adding rules to established properties and water adjacent homes is the way to do this. The waterfront plan that was previously done was great! Like to see it move forward. More	1/26/2023 6:18 AM
73	Needs to be more emphasis on planning ahead for massive road way restructuring and future planning. Westbank centre does not functionally manage traffic now. Adding additional volumes of people will only make this poorly managed area worse. Do more to take steps now to allow for better flow of vehicles through westbank.	1/25/2023 9:35 PM
74	More focus on density and housing, but less focus on existing roads or widening the existing roads especially to/from Kelowna. With increase population, expecting more traffic issues which needs to be dealt alongside new housing issues.	1/25/2023 9:07 PM
75	Biking corridors needs to be a priority. Improvements to access off road corridors to make it easier and safer to bike away from traffic throughout the city. Coordinate with City of Kelowna to determine NOW whether or not there will be a second crossing. Figure it out NOW and where it will be. Summer traffic is unbearable already even though "studies" I have read about indicate bridge is not at capacity yet. Living here tells us all the studies do not reflect reality in the summertime. If a	1/25/2023 8:43 PM

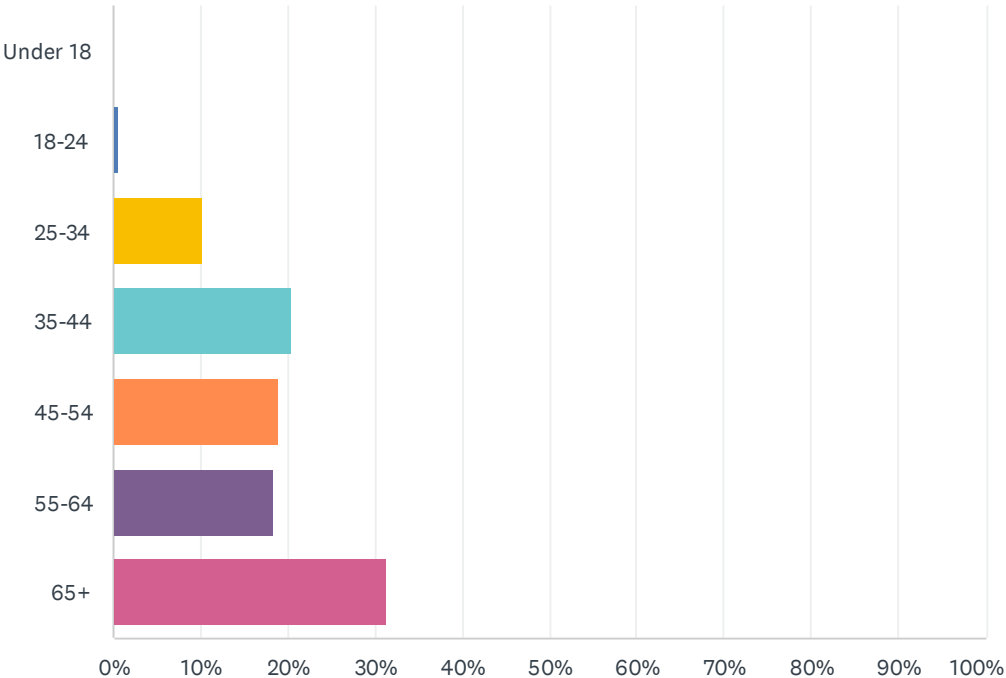
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solution is not decided upon now, I can't imagine what it will be like just a few short years from now. By the time decisions are made, designs are drawn, and building takes place, we are probably looking at at least 10 years until opening. And that's only if decided soon. Infrastructure must keep up with housing being built. Keep the area desirable and not a gridlock nightmare. I would rate this as #1 priority with bike lanes/pathways being priority #2.

76	I don't see anything here directly addressing the need for cultural spaces. Youth and seniors need somewhere to go and participate in creative endeavours. Our present recreation centre is already overcrowded. Each hub needs to have places where community members can come together and participate in activities of their choosing. The new City Hall should have spaces to hang and show artwork, spaces for all people in the community to come and create art together. West Kelowna is missing a cultural centre. This is key for a vibrant and healthy society and community. These creative spaces are also tourism attractions - especially because most of today's tourists want to participate in meaningful and sustainable tourism. A healthy and strong community is equitable, has green spaces for all to enjoy, takes into utmost importance the impact on climate and has places for community members to build community. More art spaces and parks are needed in the plan.	1/24/2023 7:55 PM
77	West kelowna needs all the four better transportation more parks and recreational activities more water recreational activities and more affordable housing for people	1/24/2023 5:03 PM
78	I think everything has been covered in your comprehensive plan.	1/24/2023 4:54 PM
79	How does it create affordable housing? My house which I built 20 years ago is now worth over 1000000, so if you think that is affordable then the only people who can afford to live here are overpaid firemen, teachers and nurses and police cannot afford it. Tell us what you think an affordable house is.	1/24/2023 4:13 PM

Q22 10. Select your age category

Answered: 147 Skipped: 16

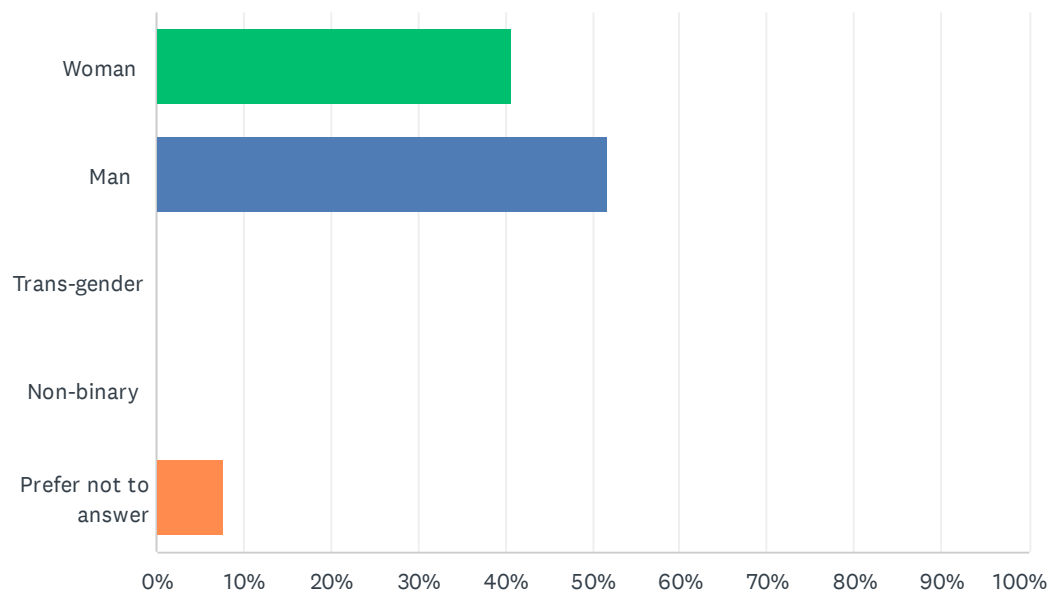


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ANSWER CHOICES	RESPONSES	
Under 18	0.00%	0
18-24	0.68%	1
25-34	10.20%	15
35-44	20.41%	30
45-54	19.05%	28
55-64	18.37%	27
65+	31.29%	46
TOTAL		147

Q23 11. Gender: What best describes your gender?

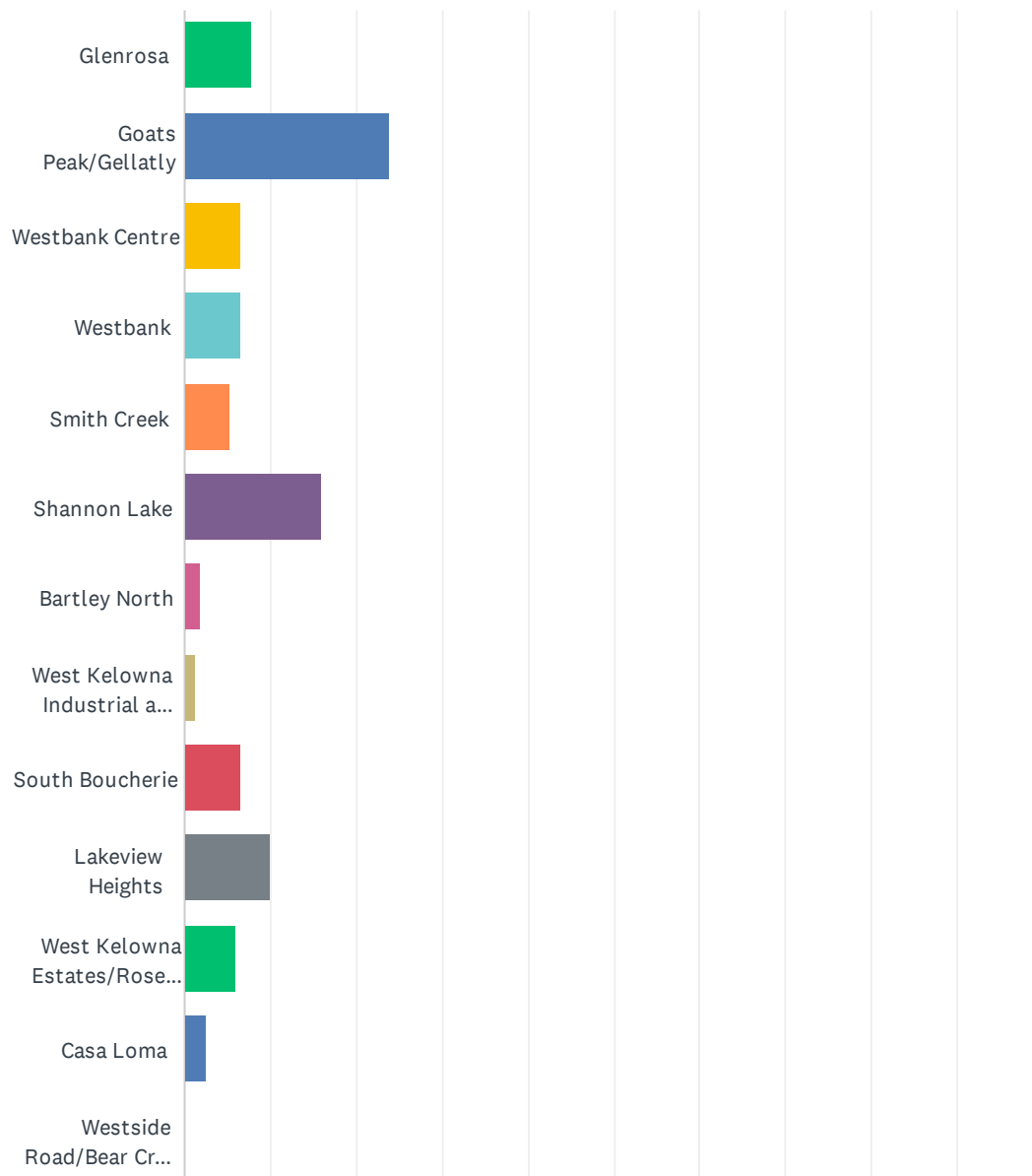
Answered: 145 Skipped: 18



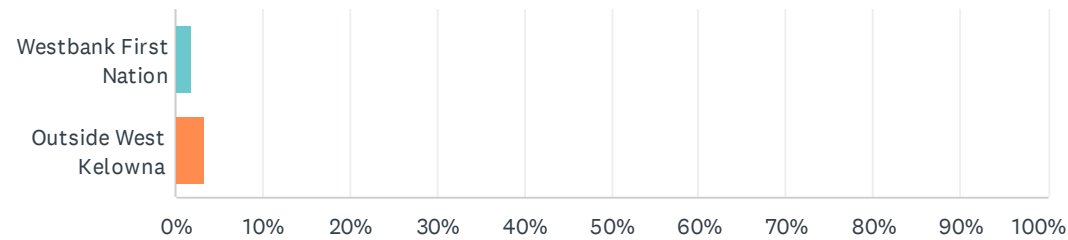
ANSWER CHOICES	RESPONSES	
Woman	40.69%	59
Man	51.72%	75
Trans-gender	0.00%	0
Non-binary	0.00%	0
Prefer not to answer	7.59%	11
TOTAL		145

Q24 12. What neighbourhood do you live in?

Answered: 151 Skipped: 12



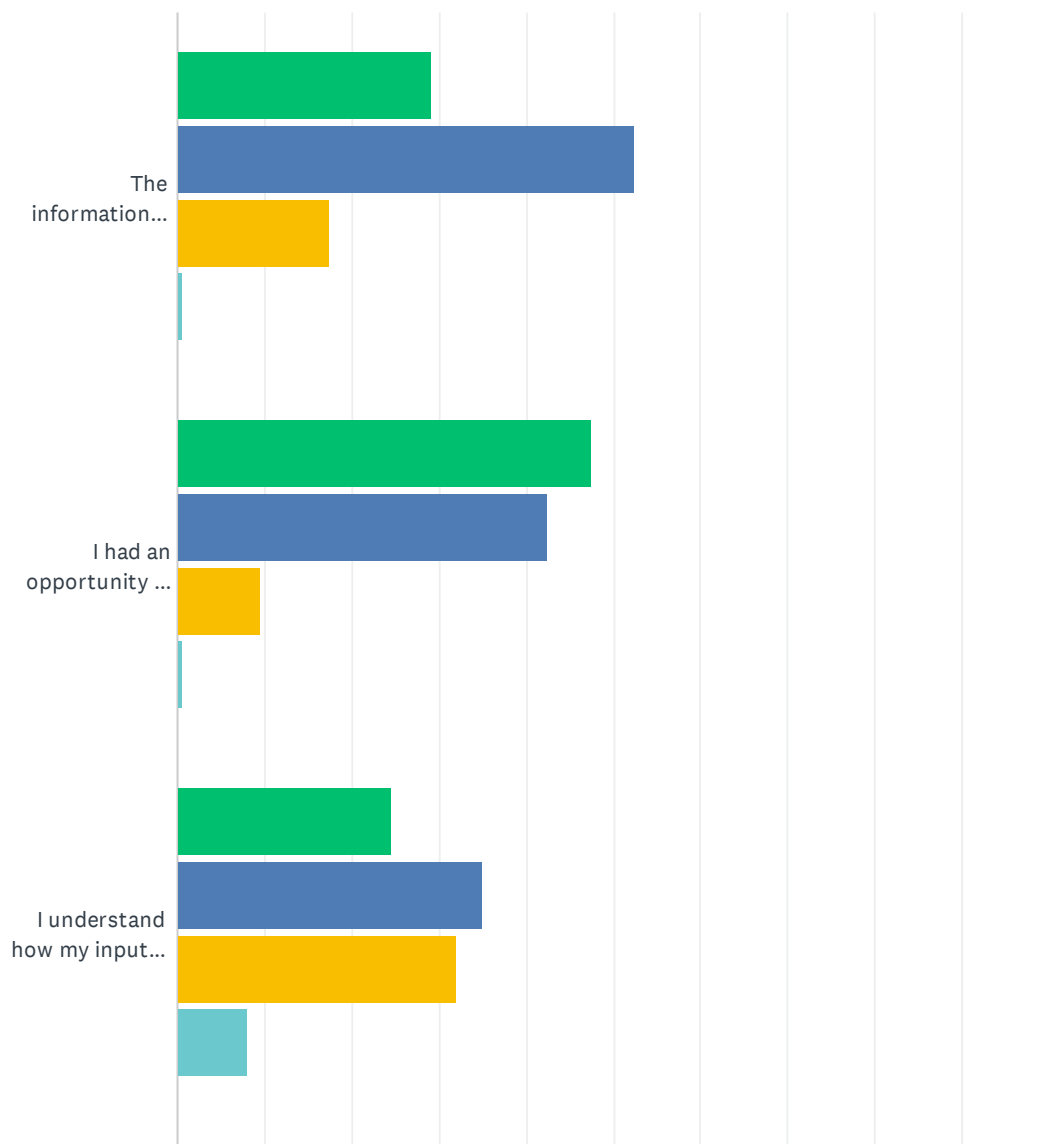
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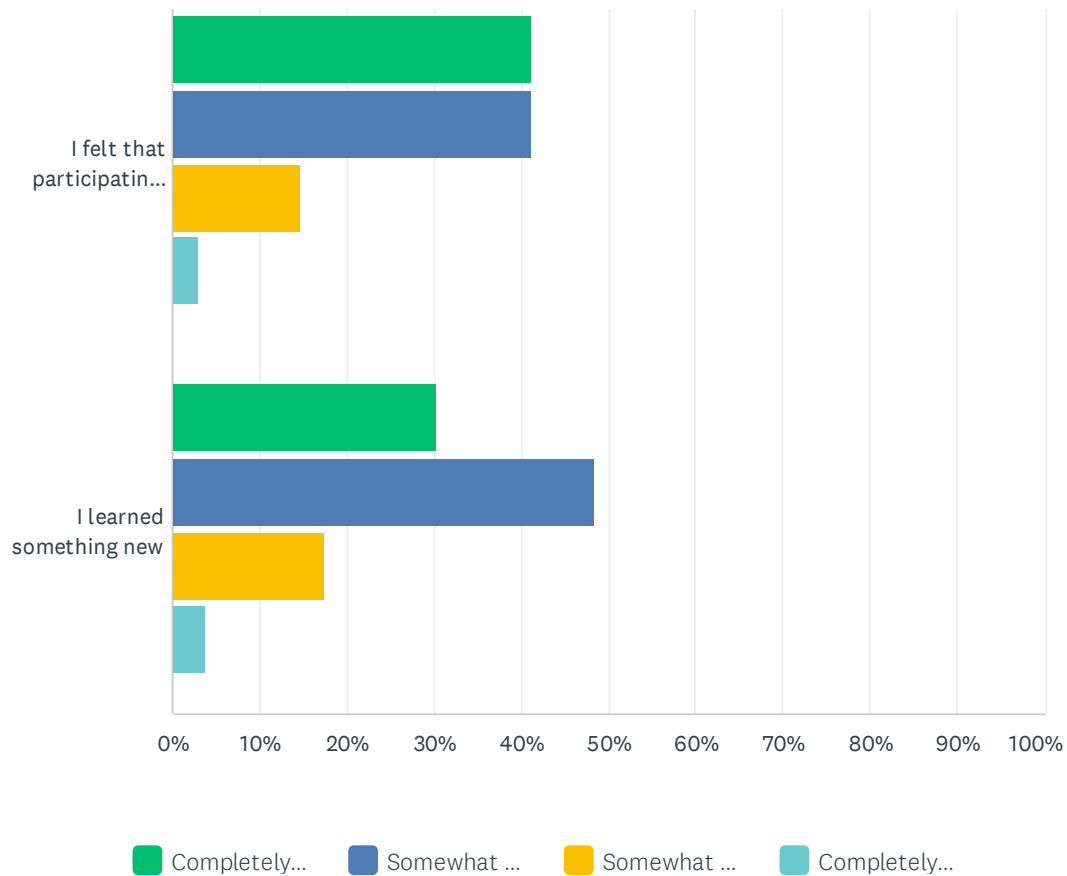
ANSWER CHOICES	RESPONSES	
Glenrosa	7.95%	12
Goats Peak/Gellatly	23.84%	36
Westbank Centre	6.62%	10
Westbank	6.62%	10
Smith Creek	5.30%	8
Shannon Lake	15.89%	24
Bartley North	1.99%	3
West Kelowna Industrial and Business Park	1.32%	2
South Boucherie	6.62%	10
Lakeview Heights	9.93%	15
West Kelowna Estates/Rose Valley	5.96%	9
Casa Loma	2.65%	4
Westside Road/Bear Creek Road	0.00%	0
Westbank First Nation	1.99%	3
Outside West Kelowna	3.31%	5
TOTAL		151

Q25 13. Please rank the following statements (completely agree, somewhat agree, somewhat disagree, completely disagree)

Answered: 137 Skipped: 26



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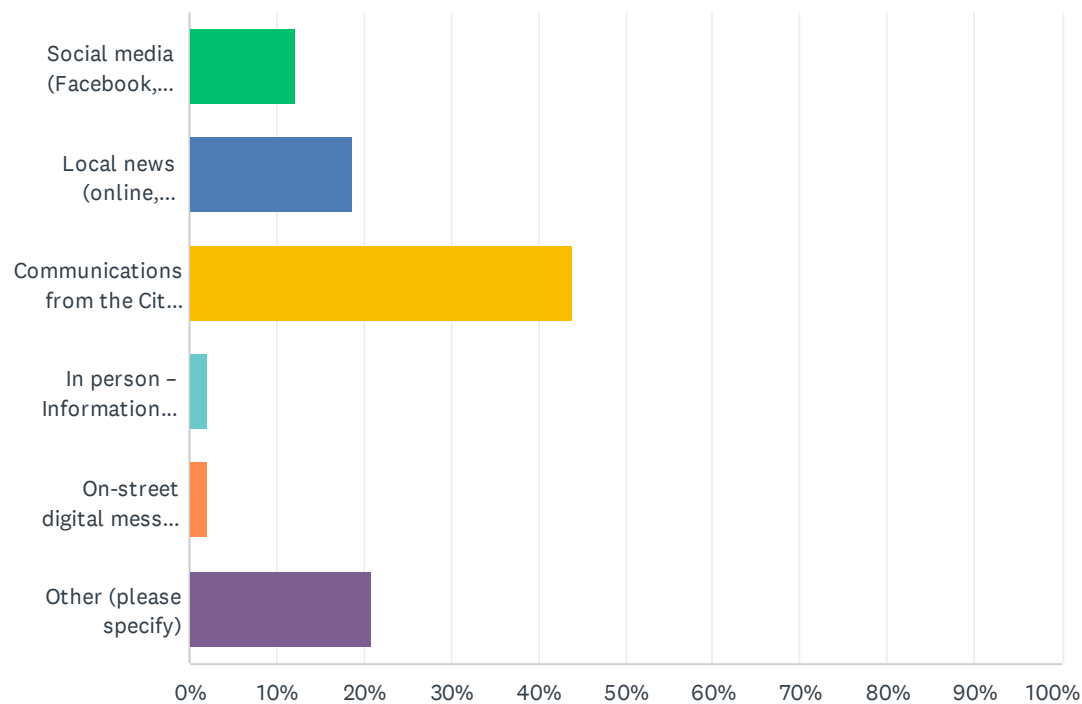


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	COMPLETELY AGREE	SOMEWHAT AGREE	SOMEWHAT DISAGREE	COMPLETELY DISAGREE	TOTAL
The information provided was clear and easy to understand	29.20% 40	52.55% 72	17.52% 24	0.73% 1	137
I had an opportunity to share my views accurately	47.45% 65	42.34% 58	9.49% 13	0.73% 1	137
I understand how my input will be used	24.63% 33	35.07% 47	32.09% 43	8.21% 11	134
I felt that participating was a valuable experience	41.18% 56	41.18% 56	14.71% 20	2.94% 4	136
I learned something new	30.30% 40	48.48% 64	17.42% 23	3.79% 5	132

Q26 14. How did you hear about public consultation for the OCP and TMP? (select all that apply)

Answered: 139 Skipped: 24



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ANSWER CHOICES	RESPONSES	
Social media (Facebook, Twitter, Instagram etc.)	12.23%	17
Local news (online, television, newspaper)	18.71%	26
Communications from the City (e-news, Public Service Announcements, News Releases etc.)	43.88%	61
In person – Information Session	2.16%	3
On-street digital message board	2.16%	3
Other (please specify)	20.86%	29
TOTAL		139

#	OTHER (PLEASE SPECIFY)	DATE
1	Castanet	2/7/2023 11:40 PM
2	From another resident of the city.	2/7/2023 7:21 PM
3	Friend	2/7/2023 6:59 PM
4	Forwarded from someone else a day before Feb 1 as the message from communications email went to spam folder.	2/7/2023 4:14 PM
5	Communication and in person	2/7/2023 2:24 PM
6	Survey question is broken - will only allow single response. Social Media, Local News, Communications from the City	2/7/2023 2:07 PM
7	Goats Peak/Gellatly Association	2/7/2023 1:54 PM
8	Concerned citizens	2/7/2023 1:39 PM
9	All of the above. The planners didn't incorporate my concerns into the draft OCP in respect to the Gellatly Bay area.	2/6/2023 10:07 AM
10	Community Association	2/5/2023 11:57 PM
11	Email from an organized group	2/5/2023 11:49 PM
12	Goat's Peak community newsletters	2/5/2023 6:03 PM
13	Gellatly Bay Neighbourhood Association	2/5/2023 2:40 PM
14	Gellatly Bay Goats Peak Community Association	2/5/2023 1:17 PM
15	Gallatly Bay association	2/5/2023 11:17 AM
16	From Goats Peak Community Association.	2/5/2023 1:17 AM
17	Neighbour	2/4/2023 2:49 PM

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18	email	2/3/2023 3:40 PM
19	social media and on street digital board (can't select more than one)	2/2/2023 7:06 PM
20	word of mouth	2/2/2023 10:47 AM
21	SLNA	2/2/2023 9:41 AM
22	Shannon Lake committee	2/1/2023 1:29 PM
23	Cowk website	1/31/2023 8:53 PM
24	Searched for it online/Went onto CWK website	1/29/2023 10:23 AM
25	there isn't the option to select more than one choice???	1/26/2023 10:28 AM
26	Castanet	1/26/2023 9:17 AM
27	Castanet	1/25/2023 10:55 PM
28	cyber	1/25/2023 10:28 PM
29	Email	1/24/2023 5:06 PM

Attachment 2

Summary of Agency and Stakeholder Referral Responses as follows:

- 1) Agricultural Land Commission
- 2) BC Transit
- 3) City of Kelowna
- 4) Fortis
- 5) Interior Health
- 6) Ministry of Agriculture and Food
- 7) Ministry of Housing / Ministry of Municipal Affairs
- 8) Ministry of Transportation and Infrastructure
- 9) Regional District of Central Okanagan
- 10) Central Okanagan Public Schools (School District No. 23)
- 11) Urban Development Institute
- 12) Westbank First Nation



Agricultural Land Commission
201 – 4940 Canada Way
Burnaby, British Columbia V5G 4K6
Tel: 604 660-7000 | Fax: 604 660-7033

April 6, 2023

Reply to the attention of Michael McBurnie
ALC Planning Review: 46831

Carla Eaton
Senior Planner, City of West Kelowna
carla.eaton@westkelownacity.ca

Re: 2040 Official Community Plan

Thank you for forwarding a draft copy of the City of West Kelowna 2040 Official Community Plan (the “OCP”) for review and comment by the Agricultural Land Commission (the “ALC” or “Commission”). The 2040 Official Community Plan is an update to the current OCP, which was referred to the ALC for review and comments 2011.

ALC staff provide the following comments to help ensure that the OCP is consistent with the purposes of the *Agricultural Land Commission Act* (the “ALCA”), the Agricultural Land Reserve (ALR) General Regulation, the ALR Use Regulation, and any decisions of the ALC. [Section 46 of the ALCA](#) requires local governments to ensure their bylaws are consistent with the ALCA, ALR regulations, and any orders of the Commission. Inconsistent bylaws include (but are not limited to) those which allow a use of land in the ALR that is not permitted under the ALCA or contemplate a use of land that would impair or impede the intent of the ALCA. This review by ALC staff aims to identify anything within the draft plan that is inconsistent with the ALCA and provide recommendations on how to improve them. Please note that ALC staff cannot endorse the use of ALR for non-farm uses.

2011 OCP Review

A draft of the current OCP was referred to the ALC for review in 2011. At that time, ALC staff provided three main comments:

1. That the Comprehensive Development zoning on ALR land in the Raymer area be removed and that all the Raymer ALR be identified in the Land Use Map as “Agricultural”.
2. That text amendments be inserted into the Raymer CD policy section referencing the protection of agricultural land through suitable buffering and setbacks as per the Ministry of Agriculture publication “*Guide to Edge Planning*” and the Commission’s “*Landscaped Buffer Specifications*”.
3. That the Commission did not support the proposed re-alignment of Elliot Road, which would bisect ALR properties.

For these reasons, ALC staff concluded that the 2011 OCP was inconsistent with the ALCA. It appears that only issue #2 was addressed in the final 2011 OCP and issues #1 and #3 were left unchanged. However, ALC staff are pleased to see that the current draft OCP update has removed the Comprehensive Development zoning on ALR land in the Raymer area (issue #1). As the City's transportation plan has yet to be updated, ALC staff are unclear whether the Elliot Road re-alignment (issue #3) has been addressed.

Transportation

ALC staff understand that an update to the transportation plan is ongoing, and that the updated transportation plan will be integrated into the OCP when it is completed. ALC staff will wait until the transportation draft plan is referred to provide specific comments on transportation.

In general, the ALC staff recommend that proposed new trails or roads (including the Elliott Road realignment) do not bisect ALR properties in order to maintain the continuity of ALR land for agricultural use.

ALC staff appreciate that policy #9 in the *Natural Areas and Public Parks Policies* section encourages consideration for linear parks and public right of ways adjacent to or along the edge of, but not necessarily within, agricultural lands.

Difficulty Reading Maps

ALC staff had difficulty reviewing the land use maps. The online maps do not include the ALR and growth boundary and the pdf maps provided are low resolution to the point of being illegible in some instances. In order for the ALC to provide an accurate response on a parcel-by-parcel review, clear maps would be required.

Agriculture Section

ALC staff appreciate that the OCP recognizes the importance of agriculture and strives to maintain a deep connection with the City's agricultural roots.

The City's Agricultural Objectives "support the preservation and enhancement of the agricultural land base through land use decisions consistent with identified agricultural policies, Agricultural Land Commission policies and decisions where applicable, and the Agricultural Plan." ALC staff appreciate the focus on supporting agriculture in the City, on all lands designated for agricultural use and not just those within the ALR.

ALC Staff also appreciate and support the following Agriculture Policies which prioritize Agriculturally-designated land for farm uses:

- 1. Support in principle the diversification of the agricultural economy in the City of West Kelowna through such activities as farm-gate marketing and other agri-tourism opportunities, which are ancillary to primary farming activities and do not impact the agricultural capability of farmland.*
- 2. Where conflict exists between non-agriculture uses and agriculture, attempt to resolve the conflict with minimal negative effect on agriculture, consistent with the Farm Practices Protection (Right to Farm) Act.*
- 4. Preserve agricultural land by directing development into designated Urban and Neighbourhood Centres, Residential, Commercial or Industrial land use designations.*
- 7. In order to minimize impact to agricultural lands and to protect resource and environmental values, subdivision is not supported within the Agriculture designation. Although the City endeavours to protect larger lot sizes, it also fully recognizes the contribution and viability of all sizes of farms, including existing small farms, in its jurisdiction.*
- 13. Discourage the proliferation of non-farm residential development or use, except as provided for by the Homesite Severance Policy within the ALR. Accessory homes and other housing will be limited according to Provincial ALR policies and regulations within the City's Zoning Bylaw.*

ALC Staff have comments on two other Agriculture policies:

- 11. Support the ALC objective of retaining agricultural lands and consolidating them in larger parcels to maintain their viability for agricultural use and further support consolidation of farmland.*

This policy, as worded, could be misinterpreted that the ALC has a policy to consolidate land. While the ALC supports retaining ALR land in large parcels to allow the broadest range of agricultural uses, it does not have a policy to consolidate land, nor the ability to consolidate land on its own initiative. ALC staff support the intention but recommend rewording it to make it clear that this is a City policy and not an ALC policy.

- 14. Encourage lower intensity and complementary agricultural uses as a transition between existing urban development and farm operations. Consideration of such uses should not be construed as support for subdivision to smaller parcels.*

It is unclear whether the lower intensity uses are suggested for the urban side or the agricultural side of the urban-agricultural interface. ALC staff advise that all forms of agriculture, including intensive agriculture, are permitted in the ALR. The

farm uses permitted in Part 2 Division 2 of the [ALR Use Regulation](#) can be regulated, but cannot be prohibited by a local government. ALC staff are concerned that this statement could unduly discourage certain types of protected agricultural uses on ALR parcels adjacent to urban uses.

Exclusions from the ALR

Section 3.6.6.2 *Food Security and Agriculture Policies*, states:

"ALR exclusion applications will generally not be supported, except at the discretion of the City consideration may be given to specific circumstances where there is significant community benefit consistent with the objectives and policies of the OCP and Community Vision. This may include the consideration of key transportation and infrastructure corridors with noted significant deficiencies affected by lands within the ALR, and existing City owned lands historically and currently operated as parks within the ALR."

ALC staff appreciate the City's intention to only support exclusion applications that are consistent with City planning objectives and have significant community benefit. Bill-15 amended the ALCA to require exclusions be submitted to the ALC only by local government, First Nation governments, or the Province to encourage exclusion applications be reviewed only as part of a thoughtful land-use planning process. ALC staff support the City using its discretion to only forward applications to the Commission that meet the City's planning objectives.

Housing

ALC staff appreciate the acknowledgement that redevelopment and infill will play a critical role in satisfying the need for future projected population growth, rather than identifying ALR land for future growth.

Growth Boundary

The majority of ALR land is included within the growth boundary. Further, some ALR properties are along the edge of, but within, the growth boundary (see examples below). No rationale was provided on why they were included in the growth area.



ALC staff find that including ALR land within the growth boundary is inconsistent with the ALCA and recommend amending the growth boundary so that it does not include ALR land within it. Including ALR land within the growth boundary can create a misperception that the permitted use of the land may change in the long-term, which can encourage speculation and discourage agricultural investment on agricultural land. However, ALC staff are open to discussing the rationale behind including certain ALR parcels within the growth boundary.

Edge Planning

ALC staff are pleased to see that a number of policies mention buffering and setbacks including:

2.15.2 Rural Residential Policies:

1. Development within the Rural Residential landscape should have no significant impact on adjacent natural areas or on the operations of adjacent agricultural areas.

2.19.4 Raymer CD Area Policies:

1(d) The identification of ALR lands and appropriate use of the Agricultural land use designation, as well as an evaluation of the necessity of buffering and setbacks between urban uses and ALR lands to help manage expectations of residents adjacent to working farmlands (with regard to odour, noise and spraying); and

3.6.6.2 Food Security and Agriculture Policies:

2. Minimize negative impacts of urban land uses on neighbouring agricultural land, including shade, wind tunnels and pollution.

The Ministry of Agriculture's [Guide to Edge Planning](#) describes best practices for edge planning along agricultural and urban interfaces to promote urban/rural compatibility. ALC staff note that the 2011 OCP did reference the *Guide to Edge Planning* (on the ALC's recommendation), in the *Raymer CD Area Policies* section. However, that section was removed from the current OCP and the guide is not referenced in the current OCP. ALC staff recommend considering the setback and buffer requirements outlined in the *Guide to Edge Planning*, including a 30 m setback for residential buildings (15 m for commercial or industrial) and an associated 15 m vegetated buffer (8 m for commercial or industrial).

ALC staff note that Table 3.15 in the City's Zoning Bylaw No. 0265 (2022) includes 2 levels of buffering, both of which fall well short of the Ministry's best practices:

Level 1 Buffer 3.0 m

Level 2 Buffer 6.0 m

ALC staff find the increased consideration of buffering is promising, but ultimately insufficient without referring to the Ministry's edge planning best practices.

Parks

Some ALR land is designated for park use. ALC staff note that the ALR Use Regulation permits two types of parks:

1. *Section 16 permits parks established under an act of the Provincial Government:*
 - (a) *a park established or continued under the Park Act or the Protected Areas of British Columbia Act;*
 - (b) *an ecological reserve established or continued under the Ecological Reserve Act or the Protected Areas of British Columbia Act;*
 - (c) *a wildlife management area designated under the Wildlife Act;*
 - (d) *a reserve established under section 15 of the Land Act for recreational use;*
 - (e) *a recreation site established under section 56 of the Forest and Range Practices Act;*
 - (f) *an area established by order under section 7 (1) of the Environment and Land Use Act to protect the environment or restrict land or resource use within the area.*
2. *Section 22 permits an open land park established by a local or first nation government for biodiversity conservation, passive recreation, heritage, wildlife or scenery viewing purposes as long as the area occupied by any associated structures does not exceed 100 m² for each parcel.*

As well, park uses that pre-date the ALR may be grandfathered.

Any park uses that do not meet the above criteria, including playing fields or constructed recreational trails, would require an application to the ALC.

Land Designations

Institutional

A number of ALR properties are designated *Institutional*. ALC staff are aware that there are some schools that were permitted by application or pre-date the ALR.

Resource Land

While most ALR land is designated *Agricultural*, some ALR land is designated *Resource Land*, which permits resource extraction and recreation uses. These uses are generally not permitted in the ALR and could cause speculation, create landowner confusion, and increase the chances of non-compliant activities in the ALR.

ALC staff recommend that all ALR land be designated *Agriculture*, unless it has been approved for a non-farm use by the ALC or has a grandfathering exception under [section 23 of the ALCA](#).

The ALC strives to provide a detailed response to all referrals affecting the ALR; however, you are advised that the lack of a specific response by the ALC to any draft provisions cannot in any way be construed as confirmation regarding the consistency of the submission with the ALCA, the Regulations, or any decisions of the Commission.

This response does not relieve the owner or occupier of the responsibility to comply with applicable Acts, regulations, bylaws of the local government, and decisions and orders of any person or body having jurisdiction over the land under an enactment.

If you have any questions about the above comments, please contact the undersigned at 236-468-3246 or by e-mail (ALC.Referrals@gov.bc.ca).

Yours truly,

PROVINCIAL AGRICULTURAL LAND COMMISSION

A handwritten signature in black ink, appearing to read 'M McBurnie', is written over a horizontal line.

Michael McBurnie, Regional Planner

Enclosure: Referral of 2040 Official Community Plan

CC: Ministry of Agriculture – Attention: Alison Fox

46831m1

To Carla Eaton,

RE: CWK Official Community Plan Referral

Thank you for the opportunity to review the City of West Kelowna's draft Official Community Plan (OCP). Please find BC Transit's recommendations below:

- Integration with BC Transit's new 0–3 year planning document. We are going to be utilizing transit recommendations from both the OCP and TMP in this document to ensure that we capture the needs of the community in West Kelowna. BC Transit requests the integration of our annual planning process within the OCP.
- The [Transit Service Guidelines for the Central Okanagan](#) outline minimum density targets that are used to determine when service to a new area is warranted. If average corridor density does not align with the targets outlined in the table below, it is unlikely that transit service will be considered.

Service Type	Average Corridor Density ⁵ (Activity Units / km ²)
Rapid Transit Route	5,000
Frequent Transit Route	4,000
Local Transit Route (Ridership)	2,000
Local Transit Route (Coverage) ⁶	1,000 - 1,500

- Section 2.3.1 (p. 28) indicates the intention of developing Brown Road as a high street with a mix of uses and activities. The [Central Okanagan Transit Future Action Plan](#) does not propose transit service on Brown Road in the future, so establishing accessible pedestrian connections to Elliott Road and Old Okanagan Highway, where transit service is provided, will be important to ensure transit connectivity is maintained as this corridor develops.
- When incorporating transit-oriented design elements into future developments, consider the reduction or elimination of parking minimums and, at minimum, consider avoiding the oversupply of parking, as this has a negative impact on promoting transit use.
- BC Transit is generally supportive of efforts to intensify land uses and avoid the introduction of both low-density and auto-oriented developments, as referenced in Section 2.4.2 (p. 31) and elsewhere in the draft plan. Similarly, BC Transit is generally supportive of efforts to intensify land uses close to transit hubs, as referenced in Section 2.6.2 (p. 35).
- 2.7.3.5 Gellatly Neighbourhood Centre Policies (p. 38) references the provision of additional public parking and the consideration of public parking as an extraordinary community benefit in this area where additional density or height is being considered. While BC Transit does not have plans to introduce additional transit service to Gellatly beyond what is proposed in the Transit Future Action Plan, it is recommended that the City of West Kelowna avoid the oversupply of parking as this has detrimental impacts on reducing vehicle reliance.
- Sections 2.11.2 (p. 47) and 2.12.2 (p. 49) reference promoting Bartley Road, Byland Road, Jennens Road, Ross Road, Stevens Road, Shannon Lake Road and Westlake Road as high

streets that encourage pedestrian mobility and access to businesses and community uses. BC Transit currently operates transit service on many of these roads and is supportive of efforts to improve the pedestrian environment for those accessing the industrial and business park areas by transit. Accessible sidewalk connections along these corridors will help ensure equitable access to these areas.

- 3.2.1.8 (p. 76) references ensuring that future developments maintain connections to, among other things, transit stops. BC Transit's development referral program will help ensure that future developments continue to strengthen the relationship between transit and land use. It is recommended that the City of West Kelowna continue to participate in the development referral program, and that consideration be given to noting this program as part of Point 8, Section 3.2.1 or under Point 2, Section 3.2.4 (p. 76).
- Point 8 of Section 3.9.2 (p. 103): Consider extending this policy to include payment for bus stop amenity upgrades as appropriate.
- Although mentioned throughout the document (3.2.4.2, 2.3.2.6, 2.4.2.5, 2.5.2.6), transit-oriented development does not have a definition and should be more clearly described to ensure appropriate utilization of the OCP.
- Section 3.2.4 (p. 76) references exploring new and expanded initiatives through the BC Transit-Kelowna Regional Transit System. Could this be more specific to referencing specific improvements from the TMP (routing, infrastructure) - 3.2.4.4 outlines this well for active transportation. Additionally.
- P.193 - the referenced sections should be swapped (3.2.4.2 and 3.2.4.1)

Thank you for this opportunity to provide comment to the draft City of West Kelowna Official Community Plan 2020-2040 dated January 27 2023. BC Transit is supportive of the direction of the document but requests further conversation regarding the highlighted points above. Ensuring continuity between BC Transit planning and local government planning is imperative to ensuring that we can continue to grow ridership in West Kelowna as well as make transit service more reliable and safer.

Thank you,

Erin Sparks
Senior Transit Planner
Esparks@bctransit.com

Bronson Bullivant
Senior Transit Planner
bbullivant@bctransit.com

March 7, 2023



City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

Attention: Carla Eaton, Senior Planner

Re: City of West Kelowna 2040 Official Community Plan

Thank you for the opportunity to review the draft West Kelowna Official Community Plan: 2020-2040. City of Kelowna staff have reviewed the draft Plan and offer the following comments:

Urban and Neighbourhood Centres. The City of Kelowna's 2040 Official Community Plan (OCP) and Transportation Master Plan (TMP) are committed to encouraging more use of sustainable transportation modes, including transit, aligned with an approach of focusing future growth in our Urban Centres. The draft West Kelowna OCP focuses growth in two Urban Centres along the Highway 97 transit corridor. This is a major step towards supporting investment in and use of transit for both communities. These two Urban Centres also support the creation of neighbourhoods where residents can meet most of their needs closer to home, reducing pressure on the Highway and the City of Kelowna's road network.

Growth Boundary. The designation of a growth boundary in West Kelowna's draft Plan is a positive step for both West Kelowna and Kelowna, which also has a growth boundary. A compact urban form in West Kelowna can reduce car dependency, positively impacting how residents travel to the City of Kelowna. This similar approach to land use in both cities means that residential infill and compact urban growth strategies are positioned to be more successful region wide.

Housing Variety and Infill. The draft OCP signals three areas for infill housing. In keeping with the comments above that the creation of complete communities in West Kelowna has an impact on Kelowna, the City of West Kelowna is encouraged to explore opportunities for more local services within the infill neighbourhoods.

Industrial and Business Park. Employment growth in the Industrial and Business Park along Highway 97 advances efforts to protect industrial land while focusing employment along key transit corridors. This approach benefits both West Kelowna and Kelowna, making it easier for residents of both communities to access employment by transit. The City of West Kelowna is encouraged to carefully consider the possible impacts of the proposed introduction of office space into this area on the region's industrial land base.

Equity and Inclusion. During the development of Kelowna's 2040 OCP, the City heard a lot about incorporating a greater lens of equity and inclusion into its policies. As both fast-growing cities tackle these challenges, it is important that equity and inclusion, along with Truth and Reconciliation, are addressed throughout our growth documents. For example, the draft West Kelowna OCP references shelters as part of the housing spectrum but provides little detail on how and where these types of services would be developed in West Kelowna.

Summary of Identified OCP Actions. West Kelowna and Kelowna share many common issues and challenges, and further collaboration between the two cities on implementing their respective OCPs increases the likelihood that their visions will be realized. As such, it is recommended both cities continue to share information, ideas and experiences from their respective implementation processes, and collaborate on projects where appropriate.

The City of West Kelowna should be commended for crafting a progressive and forward-thinking vision for future of the city. We are encouraged to see big picture alignment in how our two communities will continue to grow as part of one of Canada's fastest growing metropolitan areas: through compact urban development, strategically focused growth in Urban and Neighbourhood Centres, and retention and expansion of our employment lands.

If you have any questions or comments regarding this feedback, please do not hesitate to get in touch. We look forward to further collaboration efforts in the future.

Sincerely,



Robert Miles, MCIP, RPP
Long Range Policy Planning Manager

cc: R. Smith, Director of Planning and Development Services
D. Noble-Brandt, Department Manager, Policy and Planning
M. VanZerr, Strategic Transportation Planning Manager

1435 Water Street
Kelowna, BC V1Y 1J4
TEL 250-469-8500
FAX 250-862-3399
kelowna.ca

From: [Zone 5 Property Referrals](#)
To: [Carla Eaton](#)
Subject: RE: CWK Official Community Plan Referral - Fortis Property Referral #2023-177, Due Mar 7 - West Kelowna
Date: April-21-23 9:30:26 AM
Attachments: [image002.png](#)

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Morning,

With respect to the above noted file, FortisBC Energy Inc. (Gas) has reviewed the subject proposal and has no objections or concerns.

Thank you,

Morgan Barker, EIT

Planning & Design Technician | FortisBC Energy Inc.
☎ 250-868-4592 | ✉ morgan.barker@FortisBC.com
✉ 1975 Springfield Road, Kelowna BC V1Y 7V7

From: Referrals <Referrals@fortisbc.com>
Sent: Wednesday, February 8, 2023 11:21 AM
To: Zone 5 Property Referrals <Zone5PropertyReferrals@fortisbc.com>
Subject: CWK Official Community Plan Referral - Fortis Property Referral #2023-177, Due Mar 7 - West Kelowna

Fortis Property Referral #2023-177

Hello,

Please review the attached / below and provide your comments directly to Carla Eaton, by March 7, 2023.

Thank you,

Liz Dell

Lands Department, Property Services Assistant
16705 Fraser Highway | Surrey BC V4N 0E8
P: 778-578-8038 / liz.dell@fortisbc.com



From: Lynn Berndt <lynn.berndt@westkelownacity.ca>
Sent: Tuesday, February 7, 2023 1:39 PM
To: dev services <dev.services@westkelownacity.ca>
Cc: Carla Eaton <Carla.Eaton@westkelownacity.ca>
Subject: [External Email] - CWK Official Community Plan Referral

CAUTION: This is an external email.
Do not respond, click on links or open attachments unless you recognize the sender.

Hello,

Please find attached hereto the City of West Kelowna's referral for the Official Community Plan.

Please review and provide comment to Carla Eaton, Carla.eaton@westkelownacity.ca by March 7, 2023.

Thank you!



LYNN BERNDT | DEVELOPMENT SERVICES ASSISTANT | MUNICIPAL HALL
2760 Cameron Road, West Kelowna, BC V1Z 2T6
778.797.8830 | www.westkelownacity.ca

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We are a rapidly growing community.
Find out more about the [Development Cost Charges Update](#) and the City's
[Official Community Plan Update](#)

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March 10, 2023

Carla Eaton
City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6

Sent via email: Carla.Eaton@westkelownacity.ca

RE: File No. P21-01: Draft Official Community Plan

Dear Carla Eaton:

Thank you for the opportunity to provide comments for consideration regarding the draft Official Community Plan (OCP). This referral has been reviewed from a Healthy Community Development, Healthy Food Systems, and a Large Water System perspectives and each will be outlined below. The following information is for your consideration:

Healthy Community Development

The OCP presents an opportunity to improve the future health status of all residents by promoting healthy built environment principles. A healthy built environment is planned and built in a way which health evidence demonstrates a positive impact on people's physical, mental and social health. Chronic diseases, such as diabetes, some cancers and cardiovascular disease, are largely preventable and are influenced by citizens' levels of physical activity and food security, which are influenced by community planning. The [Healthy Built Environment Toolkit](#) (HBE Toolkit) is an evidence-based resource, which links planning principle to health outcomes. We were very pleased to see mention of the HBE Toolkit in policy 3.6.5.2.3.

Including HBE principles in community planning has been shown to support health equity. Health *inequity* occurs when there are differences in health status between people or populations due to social, political and economic factors which influence day-to-day life. The BC Centre for Disease [Control Fact Sheet: Supporting Health Equity Through the Built Environment](#) describes the vision for healthy, equitable built environments as: "safe, attractive, and complete neighbourhoods that support equitable opportunities for social connections and food security, access to protected natural environments, as well as accessible options for public and active transportation and housing." An equity lens can be used to distribute services and resources in a way that benefits people that need them the most so that the outcome for all residents is more equal. Using an equity approach contributes to developing sustainable, resilient and healthy communities by more effectively and systematically addressing community well-being.

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dākelh Dené, Ktunaxa, Nlaka'pamux, Secwépemc, St'át'imc, Syilx, and Tšilhqot'in Nations where we live, learn, collaborate and work together.

Housing is a key determinant of health. As noted in your OCP (pg 13), “short term rental units may remove housing from the general rental housing stock and reduce overall supply of available rental housing in the city;” this is a large concern given the limited housing stock and low vacancy rates. We are not aware of health research specifically about the long term implication of short term rentals on the health of communities. However, with short term rentals, there is the continual turnover of people within the neighbourhood, which discourages strong social connections to occur among neighbours. This can be detrimental for people of all ages, as social supports are an important buffer against stressful life circumstances and contribute to family and community resiliency. We encourage the city to look at the role short term rentals will play in the housing market and implement policy accordingly.

The following includes section specific comments:

Section 1: Introduction & Community Vision

Given the topographical and geographical constraints within the community, we commend the City of West Kelowna for their thoughtful identification of 2 urban centres and 5 neighbourhood centers. Ensuring that each neighbourhood center includes mixed use lands, such as the small-scale commercial centers as identified in the OCP, residents will have access to key amenities close to home.

Section 2: Land Use Designations

General Comments

We are very pleased to see the number of health promoting and equitable policies outlined in section 2.1. A few additional policy suggestions for this section include:

- Mitigate urban heat islands by expanding natural elements across the landscape
 - For example: The inclusion of green infrastructure to policy 2.1.5, 2.3.2.6 & 2.4.2.5 would contribute to climate change adaptation measures in helping to counteract the heat island effects of development.
- Prohibiting/limiting drive through services supports health and well being by encouraging increased physical activity.

There is use of the term ‘high street’ in multiple locations throughout the OCP, addition of this definition to the Glossary is suggested.

Healthy Natural Environments

We applaud the City of West Kelowna for having natural areas and public parks land use designations to be an allowable use in almost every part of the City. It is refreshing to see an OCP strive to have 20% of the city’s land area in the form of protected natural areas and/or publicly accessible parks, trails and greenspace.

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dākelh Dené, Ktunaxa, Nlaka’pamux, Secwépemc, St’át’imc, Syilx, and Tšilhqot’in Nations where we live, learn, collaborate and work together.

Accessing parks and green space increases social well-being by providing places for residents to make new connections and build relationships with friends and family. There is also a strong relationship between exposure to nature and reduced levels of stress, chronic disease and depression and anxiety.

With increased density utilizing infill (as identified in section 2.9 Low Density Residential), it is important to ensure that green spaces are also explicitly created along side these infill developments. Further to the health benefits mentioned above, expanded use of vegetation and natural elements mitigates air pollution and provides a cooling effect mitigating the impacts of extreme heat.

Healthy Neighbourhood Design

Unlike the Westbank Urban Centre sections (Area A & C), the Boucherie Urban Center objective indicates the encouragement of complete neighbourhoods (2.6.1.4). Creation of complete neighbourhoods can have a great impact on health and well-being. Complete neighbourhoods are more convenient, socially engaging, generally consume less energy and encourage regular physical activity and the use of active transportation. The Westbank Urban Centre sections, Area A & C, both have policy regarding the incorporation of transit-oriented and active transportation design elements, while the Boucherie Urban Center lacks similar policy. From a health and equity perspective, it would be great to see both the objective of complete neighbourhoods and the above mentioned policy align in all these sections.

Section 3: Policies

We are extremely pleased to see many policies that will promote the health of the community. The addition of extreme heat as an example to policy 3.4.1.2.1 would further strengthen the policy, as there are [many ways](#) that long range planning can mitigate the impacts of extreme heat.

As we have seen in the past few years, extreme weather events due to climate change are increasing and communities need to take additional steps to prepare for such emergencies. We strongly advocate that local governments create extreme weather emergency plans that cover both [extreme heat](#) and cold temperatures.

Radon gas is a naturally occurring health hazard of which local governments can mitigate harms through policy. [RDOS's Area G OCP's](#) Radon Gas Hazard Mitigation section (17.7) is a prime example articulating radon as a health hazard and identifying mitigation policies.

Section 3.6: Social Sustainability (pg 89) states that the creation of a “fair distribution of services” is essential to social sustainability. We advocate for the **equitable distribution** of services, for reasons previously stated above; this will ensure that all residents have the ability to thrive.

Section 4: Development Permit Areas (DPA)

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dākelh Dené, Ktunaxa, Nlaka'pamux, Secwépemc, St'át'imc, Syilx, and Tšilhqot'in Nations where we live, learn, collaborate and work together.

Climate change mitigation and adaptations strategies to extreme heat are key to community resiliency. In 2022, the University of Waterloo released [Irreversible Extreme Heat: Protecting Canadians and Communities from A Lethal Future](#) which speaks to both mitigation and adaptation strategies that can be taken by communities to protect the health of their residents. Recognizing that the City of West Kelowna's OCP already includes a number of the strategies outlined in this report, we applaud you for this. At the same time we strongly encourage you to review the publication and consider incorporating some of the other site specific strategies within your DPA section.

Section 5: Implementation

Section 5.3.1.3 speaks to the type of studies that might be required in order to evaluate development applications. We advocate for the addition of a '[Health Equity Impact Assessment](#)' to the list.

We are happy to see a breakdown of indicators identified within section 5.5: Monitoring and Performance Indicator. Some additional indicators for consideration include:

- % of agriculture land base (both ALR and non-ALR) that is farmed/in production;
 - Access through BC Assessment Authority Data: Actively Farmed Land Metric;
- % of population living within 1km of healthy food retail;
- % of people living within 400m (5 min walk) of a transit stop;
 - Access this data from BC Transit and the census;
- % of people who live within 1km of a park;
- Proportion of licensed child care facilities based on community demand/need;
- Monitoring and reporting out on water quality;
- Creation/upgrade of natural/green infrastructure (i.e. rain gardens, land swales, restoring wetlands, tree canopy etc.) annually

Healthy Food Systems

The following recommendations have been made by a Public Health Dietitian:

Section	Statement	Recommendation
Section 3.6.6.1 Food Security and Agriculture Objectives pg 97	1. Protect and enhance the existing agricultural character of West Kelowna as the city continues to grow.	Protect and enhance existing agricultural land of West Kelowna as the city continues to grow.
Section 3.6.6.1 Food Security and Agriculture		Consider addition of "include food security as part of community emergency response planning."

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dākelh Dené, Ktunaxa, Nlaka'pamux, Secwépemc, St'át'imc, Syilx, and Tšilhqot'in Nations where we live, learn, collaborate and work together.

Objectives pg 97 OR could be included in Emergency Response Section		
3.6.6 Food Security and Agriculture pg 97		Consider the development of a food security plan.

Large Water Systems

The following recommendations have been made by a Large Water System Environmental Health Officer:

Section	Statement	Recommendation
2.15 Rural Residential pg 54	The Rural Residential designation supports non-intensive farming operations that do not generate adverse impacts such as noxious odours or excessive dust. These areas are not considered growth areas and will be restricted to infill uses that can be served by on-site water and wastewater treatment systems.	Where possible, the City should work to extend water and sewer services to rural properties to deter the creation of small water systems that are not generally economically sustainable.
2.17 Resource Land pg 58	Sustainable farm practices (protection of surface and groundwater resources, aquatic habitat, woods, wetlands, wildlife habitat, and other natural characteristics) that do not place unreasonable constraints on the farming community, are also encouraged. As a result of the importance of agriculture in the City of West Kelowna, the protection of agricultural capacity, production and access to agricultural products is supported by this OCP.	Recommend including Sustainable Soil and Water Management Practices as an objective. This can be elaborated on at another time – but is an important concept for both agricultural water usage and soil conservation. This is a key issue for concerns regarding food security and water supply management during drought conditions.
2.18.1 Natural Areas and Public Parks Objectives:		Consider adding utilization of natural areas and public parks to support protection of water sources.

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dākelh Dené, Ktunaxa, Nlaka'pamux, Secwépemc, St'át'imc, Syilx, and Tšilhqot'in Nations where we live, learn, collaborate and work together.

Thank you for the opportunity to comment on your draft OCP. We welcome the opportunity to further our relationship with the City of West Kelowna by co-operatively identifying opportunities for collaboration and planning. For example, we are able to provide letters of support for funding opportunities, present HBE principles to Council or to the public, participate in stakeholder working groups, and provide a health perspective on policy documents, such as a revised Zoning Bylaw, and land development proposals.

If you have any questions or concerns, please feel free to contact Tanya Osborne at 778-214-0674 or Tanya.Osborne@interiorhealth.ca.

Sincerely,



Tanya Osborne, BAHS
Community Health Facilitator
Healthy Communities Healthy Families

Interior Health would like to recognize and acknowledge the traditional, ancestral, and unceded territories of the Dăkelh Dené, Ktunaxa, Nlaka'pamux, Secwépemc, St'át'imc, Syilx, and Tšilhqot'in Nations where we live, learn, collaborate and work together.



March 15, 2023

File: 0280-30

Local Government File: P 21-01

Carla Eaton, Senior Planner (Long Range)
City of West Kelowna
2760 Cameron Road
West Kelowna, BC V1Z 2T6
Via E-mail: Carla.Eaton@westkelownacity.ca

Dear Carla Eaton:

Re: West Kelowna Draft Official Community Plan (2020 – 2040)

Thank you for providing B.C. Ministry of Agriculture and Food (ministry) staff the opportunity to comment on the January 27, 2023 draft Official Community Plan Bylaw for West Kelowna. Overall ministry staff consider the objectives and policies affecting agriculture to be positive given the emphasis on protecting agricultural land and minimizing conflict. We offer the following comments that may help to provide increased clarity and suggestions for wording, additional objectives or policies that may support agriculture in the City:

Ministry name – We note that the document uses ‘Ministry of Agriculture’ throughout for the ministry name. It is currently the ‘Ministry of Agriculture and Food’, so we recommend updating this throughout the document.

2.1.11 General Land Use Policies – Agricultural Land Reserve exclusion applications will generally not be supported – the policy to generally not support exclusions in the Agricultural Land Reserve (ALR) is good, although it is somewhat unclear in the current draft what is meant by significant community benefit and whether or not exclusions would be supported only for the infrastructure mentioned or if private development applications that included these infrastructure components would also be supported.

2.1.12 – General Land Use Policies – Overall this is a good policy as it can have a positive impact by mitigating pest and disease exposure to commercial orchards. Technically, it should be referred to as the OKSIR Program as it is a service of four participating regional

districts: RDOS, RDCO, RDNO, and CSRD and funding is split between taxpayers and fruit growers. SIR residential resources are focused on urban host trees located within 200 m of commercial orchards, and so West Kelowna discouraging plantings, especially those within 200 m of commercial orchards is a great place to also focus on discouragement.

2.15 – Rural Residential – the focus on non-intensive farming operations that do not generate adverse impacts such as noxious odours or excessive dust is fine provided that there is no overlap with this designation and the ALR. If there is any overlap, it should be made clear that intensive agriculture is a permitted use in the ALR despite a local government zoning bylaw.

2.15.2.1 – Rural Residential Policies – Development within the Rural Residential landscape should have no significant impact on the operations of adjacent agricultural areas – this policy could provide a more guidance on how this should be achieved, particularly that many farm practice complaints come from rural residential areas.

2.16.1 – Resource Land Objectives – Protecting and enhancing agricultural resources is a good objective for this designation, as is protecting environmentally sensitive areas.

2.16.2 – Resource Land Policies – Where lands are in the ALR, protecting environmentally sensitive areas through the use of restrictive covenants may require approval of ALC.

2.17 – Agriculture – It appears from the map that this designation applies to all of the ALR, which is positive, although there are some areas outside the ALR which also have the designation.

2.17.1 – Agriculture Objectives – The objectives in this section are supportive of agriculture. There may be benefit in being more clear about what constitutes “innovative agricultural activities”, “innovative agricultural practices”, and “secondary activities” and to be clear that these should not be to the detriment of primary agricultural production in the ALR.

2.17.2 – Agriculture Policies – The policies in this section are generally very strong and support the objectives of the Agriculture designation

2.17.2.2 – This policy could be clearer with regard to West Kelowna’s role in resolving conflict between non-agriculture and agriculture uses. Also, the *Farm Practice Protection (Right to Farm) Act* only covers complaints relating to nuisance, so not all conflict may be resolved through formal complaint to the B.C. Farm Industry Review Board (B.C. FIRB). Perhaps it could be worded that City staff will work with ministry staff to resolve conflict, as Regional Agriologists try to assist in resolving complaints through education of the

public. Also, as a companion to this policy, another policy stating that land use planning tools will be utilized to minimize or prevent conflict could be useful.

2.17.2.9 – Although it is good to evaluate and consider the impact of development on drainage patterns on down slope agricultural areas, it is also necessary to mitigate any impacts. This may be happening through the various development permit area guidelines, but it would be stronger to include a reference to action here.

2.17.2.14 – Lower intensity residential uses are not always more compatible with agriculture than higher density residential. While strata corporations may sometimes band together to complain about neighbouring farm operations, higher density uses such as freehold row-housing may be more compatible than rural residential or suburban single-family dwellings provided adequate setbacks and vegetative buffers are utilized.

3.6.6.2.2 – Minimizing the impact of urban land uses on agriculture is a good policy. Perhaps it should be made clear that this will be done through development permit area guidelines (see DPA comment below as well).

3.6.6.2.3 – This policy should be more similar to the one in the Agriculture Policies section and be clear that these activities will not impact the ability of agricultural lands to continue to be farmed.

4.1 Development Permit Area Guidelines – While many of the Development Permit Areas (DPAs) contain guidelines which protect farming, none of the DPAs have been designated under . 488(1)(c) of the *Local Government Act*. Ministry staff think that the guidelines will be stronger if they are tied back to protection of farming through the designation, and that each DPA that is adjacent to farmland also contain objectives to protect farming.

4.2.3.8 General Guidelines Exemptions – The Ministry of Agriculture and Food does not determine or designate “normal farm practices”. B.C. FIRB is the only entity which can do this and it will typically only do so on a case-by-case basis as part of a formal nuisance complaint.

If you have any questions, please contact us directly at the email addresses or numbers below. We would be happy to review any future versions of the OCP as well.

Sincerely,



Alison Fox, P.Ag.
Land Use Agrologist
B.C. Ministry of Agriculture and Food
E-mail: Alison.Fox@gov.bc.ca
Office: (778) 666-0566



Philip Gyug, P.Ag.
Regional Agrologist
B.C. Ministry of Agriculture and Food
E-mail: Philip.Gyug@gov.bc.ca
Office: (250) 378-0573

Email copy: Michael McBurnie, Regional Planner, Agricultural Land Commission
ALC.Referrals@gov.bc.ca



March 20, 2023

Ref: 43865

Dear Carla Eaton,

Thank you for your February 7, 2023 formal referral of the City of West Kelowna's draft Official Community Plan (2040) (OCP) to the Ministries of Housing (HOUS) and Municipal Affairs (MUNI) for review and comment. Please consider this email as the joint ministry response to your request for feedback.

We continue to work with partners, including local governments, towards the achievement of a secure, low emission, sustainable economy, and a province where everyone can find a good home, in communities that are well-connected to services and transit. The Province is committed to meaningful reconciliation with Indigenous peoples and the implementation of the United Nations Declaration on the Rights of Indigenous Peoples (UNDRIP). As part of this, the provincial government continues to encourage [local governments](#) to work with the First Nations whose traditional territory they are situated within to ensure all voices are heard. I understand that the City of West Kelowna is referring the draft OCP to the local Westbank First Nation and has stated their commitment in the OCP to develop a strong working partnership with them.

As you know, the *Local Government Act* (LGA) requires local governments to consider their [housing needs report](#) when developing future OCPs (or making housing-related amendments to those plans). I acknowledge that the City of West Kelowna has completed a Housing Needs Assessment in 2022 and is using this information in conjunction with population projections and a 20-year housing distribution based on housing type analysis to inform planning and development decisions as part of the OCP policy direction.

With respect to the City of West Kelowna housing and growth concept objectives and policies with a focus on urban and neighbourhood centres and infill housing in the OCP, I encourage you to ensure that these are clearly reflected in your regional context statement (RCS) of the [regional growth strategy](#) (RGS) of Regional District of Central Okanagan that forms a portion of your municipal OCP and sets out the relationship between the RGS and the OCP to help local governments address regional issues. Sound asset management and development cost charges that are proactively reviewed will also support your community as it grows.

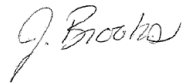
Please also keep HOUS and MUNI informed of any significant issues or concerns. The earlier we are aware of bylaw development or agency concerns, the better we can be of assistance. HOUS and MUNI have no concerns with the draft OCP at this time, subject to any provincial interests raised by other ministries and agencies. I trust this will help you with your ongoing work. If you have further questions, please feel free to contact me.

For more information or clarification on ministry programs and initiative for local governments in B.C. please contact the financial officer, governance and structure analyst, or planning analyst for your region: [Local Government Division Staff Finder](#).

Thank you again for referring the draft OCP to HOUS and MUNI.

Page 2

Jessica Brooks

A handwritten signature in black ink, appearing to read "J. Brooks". The signature is written in a cursive, flowing style.

Executive Director
Ministry of Housing
Planning and Land Use Management Branch

From: [Henry, Audrie L TRAN:EX](#)
To: [Carla Eaton](#)
Subject: City of West Kelowna - Draft Official Community Plan (2040)
Date: March-16-23 9:19:09 AM

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Our file: 2023-00659
Your file: P 21-01

Good morning Carla,

Thank you for the opportunity to comment on the “*West Kelowna DRAFT Official Community Plan (2040)*”, received February 8, 2023 via e-das. Please see the Ministry’s following comments:

- The Central Okanagan Integrated Transportation Study (CO-ITS) team has been working collaboratively with the City of West Kelowna for several years on the CO-ITS (previously the Central Okanagan Planning Study - COPS). We have facilitated a number of engagement sessions with City staff and will be presenting the findings/next steps for the CO-ITS to Mayor and Council over the next month. The CO-ITS has been a collaborative effort that provides a vision for the Highway 97 corridor through the Okanagan to 2040. It is important that the City of West Kelowna ensure that the Official Community Plan (OCP) aligns with the strategy for the CO-ITS. Many of the projects identified in the strategy will be partnerships between the Ministry and the City of West Kelowna and guide infrastructure improvements/investment in the community over the next 20 years.
- The Ministry is in the process of leading an initiative for Transit Oriented Development (TOD) to better integrate transportation and development to ensure greater alignment between transportation and land-use planning. The focus of this initiative will include placing a greater emphasis on complete, compact and connected communities as well as integrated transportation systems. This includes supporting a mix and choice of housing types in close proximity to amenities of daily life, enabling transportation choice, and supporting public and active transportation. The ministry recommends that the OCP focus on developing urban and neighbourhood centres with convenient access to services, a range of housing types and align with the Ministry’s integrated transportation and development initiative. It is also important that the OCP support opportunities for mode shift from vehicle reliance to transit/active transportation modes.
- The Provincial government released [The CleanBC Roadmap to 2030](#) in October 2021. *Roadmap to 2030* identifies actions across eight pathways one of which is the Communities Pathway. One key action of the Communities Pathway is supporting better land-use planning which includes supporting communities as they grow to “better align land-use and transportation planning to build connected, mixed-use communities where more people can

live closer to jobs, services and transportation choices, helping to reduce commute times and greenhouse gas emissions”.

- The Ministry would like to be included on development infill applications and other applications that utilize the Highway 97 network to better understand the impacts. Potential mitigative measures may be required depending on the applications. Please involve the Development Services staff early on with these applications.
- Please provide a clearer version of Schedule 5 “Future Road Network”, Schedule 6 “Future Transit Network”, Schedule 7 “Future Bicycle Network” and Schedule 8 “Future Pedestrian Network” plans shown on pages 210-217 to ensure that they are aligning with CO-ITS. I was not able to read the copies that have been included in the OCP. If these could be shared it would be appreciated.

Thanks again for the opportunity to comment, please feel free to reach out if there are any questions.

*I will also update the e-das file.

Have a great Thursday!

Regards,

Audrie Henry, Development Officer
Ministry of Transportation and Infrastructure
#300-1358 St. Paul Street
Kelowna, BC V1W 4T8
Phone: 236-766-7193 (UC) / 778-214-1666 (cell)
Fax: 250-712-3669

From: RDCO Planning Services
To: Carla Eaton
Subject: Revised comments from RDCO - CWK Official Community Plan Referral
Date: March-08-23 3:00:16 PM
Attachments: External Development Referral Feb 7 2023.pdf
Full Draft OCP Document_30JAN23.pdf

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Good afternoon,

Please disregard our earlier comments as we had a late submission. The comments below are our final comments. Should you have any questions, please contact Planning Services at 250-469-6227.

Thank-you for the opportunity to comment. RDCO staff have reviewed the application and have the following comments.

Planning Services:

- The Draft West Kelowna Official Community Plan shows alignment with the Regional Growth Strategy policies agreed upon by all member municipalities.
- Page 165: CWK staff may want to consider repositioning figure 47 and cross referencing it under section 4.9.5.26.b.
- Page 181: The Growth Management Strategy Bylaw No. 851 was the previous version of the bylaw. Please replace with "Regional Growth Strategy Bylaw No. 1336"
- Climate Action planning will likely be a strategic priority for the RDCO's Regional Board which may provide opportunities for collaboration in the near future.
- The RDCO's Regional Growth Strategy: Priority Projects Plan determined a five-year action plan for regional projects. One of the projects that was scheduled for 2021 but was not initiated was the Regional Employment Lands inventory Project. There may be an opportunity for collaboration when undertaking this project and West Kelowna's Economic Sustainability Action Items.
- Page 194: The CWK OCP mentions *Inventory, identify, and protect sensitive ecosystems and environmentally sensitive areas* as a short-term priority item. The 2022-2024 Central Okanagan SEI update will support WK's action item in relation to 3.5.2.7 for updating the inventory of sensitive ecosystem and environmentally sensitive areas.
- Page 196: The figure reference for Table 11. is misplaced and appears within the table.
- Page 206: On the southwestern boundary of CWK the Wildfire Interface DPAs for the District of Peachland and the RDCO cover more expansive areas than the CWK equivalent. City staff may wish to review.
- Staff would like to flag this for your attention: LGA s.446(1), *If a regional growth strategy applies to all or part of the same area of a municipality as an official community plan, the official community plan must include a regional context statement...*

Parks Services:

- Overall, the draft West Kelowna OCP 2040 has included well thought out and progressive objectives and policies to address future growth, climate change, natural areas and parks, and sustainable transportation. The draft OCP provides future direction and priorities towards increasing connectivity between the natural areas and public parks, community greenbelt and greenways and transportation corridors to ensure linkages with neighbourhood areas and the urban centres. It is encouraging to see the West Kelowna's draft OCP contain a target goal for 20% of the municipality's land area to consist of natural areas and public accessible parks.
- Parks Services recommends zoning changes to the following properties and revision to draft Schedule 1 – Land Use Plan:
 - Rezoning of RDCO owned park property for Glen Canyon Regional Park from A1 to P1.
Property Legal description: Parcel A, DL 3187, ODYD (2605 Hebert Road)
 - For consideration by City of West Kelowna to rezone CWK owned parcel adjacent to Kalamo Regional Park from R1 to P1.
Property Legal description Lot 15, Plan EPP67384, DL 3496, ODYD (3088 Collens Hill Road)

Sincerely,

Planning Services
Regional District of Central Okanagan
250-469-6227 | planning@rdco.com
Connect with us | rdco.com

The RDCO acknowledges our presence on the traditional, ancestral, and unceded *tm̓x̌'uúla'x̌w* (land) of the syilx / Okanagan people who have resided here since time immemorial. We recognize, honour, and respect the syilx / Okanagan lands upon which we live, work, and play.

From: RDCO Planning Services
Sent: March 8, 2023 9:43 AM
To: carla.eaton@westkelownacity.ca
Subject: FW: CWK Official Community Plan Referral

Good morning,

Thank-you for the opportunity to comment. RDCO staff have reviewed the application and have the following comments.

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- Page 181: The Growth Management Strategy Bylaw No. 851 was the previous version of the bylaw. Please replace with "Regional Growth Strategy Bylaw No. 1336"
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Sincerely,

Planning Services

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From: Lynn Berndt <lynn.berndt@westkelownacity.ca>
Sent: February 7, 2023 1:39 PM
To: dev services <dev.services@westkelownacity.ca>
Cc: Carla Eaton <Carla.Eaton@westkelownacity.ca>
Subject: CWK Official Community Plan Referral

CAUTION: This message was sent from outside the organization. Please do not click links, open attachments, or respond unless you recognize the source of this email and know the content is safe.

Hello,

Please find attached hereto the City of West Kelowna's referral for the Official Community Plan.

Please review and provide comment to Carla Eaton, Carla.eaton@westkelownacity.ca by March 7, 2023.

Thank you!



LYNN BERNDT | DEVELOPMENT SERVICES ASSISTANT | MUNICIPAL HALL

2760 Cameron Road, West Kelowna, BC V1Z 2T6

778.797.8830 | www.westkelownacity.ca

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March 7, 2023

Carla Eaton
Senior Planner
2760 Cameron Road,
West Kelowna, BC V1Z 2T6

Dear Ms. Eaton,

Re: 2040 OCP Comments

Thank you for the opportunity to comment on the draft Official Community Plan 2020-2040 (OCP). We would like to offer the following observations and comments related to the future plans in West Kelowna.

Future Growth

As the City is aware with the increased population growth in the City of West Kelowna, the enrolments in schools have increased rapidly over the recent years and has challenged the existing school facilities to accommodate the increased enrolments. The District has renovated all available spaces inside the schools to accommodate classrooms and have placed portables on school sites for additional classrooms.

The current draft OCP identifies 7,962 new residential units in West Kelowna by 2040. Based on current number of residential units on the Westside, the current student yield is 0.31 students per residential unit. There is a potential to have 2,500 new students in the school system and may potentially require two new elementary schools and one new middle/secondary school. The future planning for West Kelowna should consider the potential for new school sites and/or the need for additions to the current school facilities.

Schools and Agricultural Land Reserve

The School District currently has three school sites located within the Agricultural Land Reserve (ALR), Hudson Road Elementary, Shannon Lake Elementary, and Webber Road Elementary. Considering the projected growth identified by the City, the school sites will require additions over the length of the OCP and beyond. Recognizing the importance in the location of these schools within the community, school planning based on enrolment changes is challenging and require coordination among the School District, City of West Kelowna and the Agricultural Land Commission (ALC).

The three school sites were approved for school use by the ALC and two sites have been operating as elementary schools and one as a community use facility and now an elementary school for over thirty years. Since the properties were approved by the ALC, the three sites have been surrounded by residential developments.

In 2018, Council supported the exclusion of these three school sites from the ALR. This support was based on:

- The subject properties are currently operated as public institutional uses (two schools and a community center).
- The subject properties are located within and surrounded by existing residential neighbourhoods.

- West Kelowna's population is growing and putting pressure on existing school sites to physically accommodate this growth through building additions, renovations and the addition of portables.
- Two of the three subject properties maintain a Land Use Designation of *Institutional* which anticipates the use of schools.
- The ALC has supported the use of schools and community center on these sites for numerous years.

The School District's 2021 Long Range Facilities Plan confirms that all West Kelowna schools are operating beyond their operational capacity. With the anticipated population growth, these three schools, located in established residential neighbourhoods, are going to need substantial facility improvements and additions to accommodate this growth.

In the draft OCP, Shannon Lake and Hudson remain identified for future institutional land uses; however, Webber Road is still designated for future agricultural land uses.

In order to provide confidence to the Ministry of Education during budget deliberations, and support future discussions with the ALC, the School District requests that the City include the following policy and mapping changes to the draft OCP:

1. Change the future land use designation to Institutional for Webber Road Elementary School.
2. Include policy in the OCP to identify the importance of maintaining these specific school sites for educational services in the community and support future non-farm use and/or exclusions applications by the School District in the future.

School Site Land Use Designations

The School District staff and the City's planning staff over the years have continued to collaborate with each other to identify potential schools sites for new development in Area Structure Plans and the OCP land use map. This work has developed opportunities to have a school site incorporated into the Smith Creek development and identify a school site in the Goat's Peak development. However, the current draft OCP does not specifically identify a school site location in Goat's Peak development area. Current school sites and future school sites are included in the "Neighbourhood Centre" designation. While the designation identifies multiple uses for the sites, schools are not specifically identified and this exclusion of school may perceive the area to be more commercial in nature and future land use decisions may develop the site of other uses than a school.

Area Structure Plan's and OCP land uses provides an opportunity to designate the lands for a future school site. If local Government did not designate school sites at the point of adopting Area Structure Plans and/or OCPs there is a significant risk that appropriate sites will either not be available when needed or will be less desirable. However, due to provincial approvals process, the timing to secure the land through site acquisition can be undetermined due to budget pressures at the Ministry of Education level. The designation of a school site assists the school district in obtaining support and funding for site acquisition when the Ministry of Education is able to designate the funds or the School District has enough funds in the School Site Acquisition Charge account to acquire a designated site. Many school site acquisitions have been delayed in the past until a decision is made to move ahead with school construction and residential development can surround designated school sites before the Ministry authorizes the school site purchase. A school site acquisition is not authorized to proceed until the school district and Ministry of Education are relatively close to making a decision to build a new school.

In the current District 5 year plan, the site acquisition for Goat's Peak neighbourhood is identified and waiting for Ministry approval. The School District will continue to keep the site acquisition on the capital plan until the Ministry authorizes the District to proceed with the site acquisition.

School Site Requirements

In new areas of development it is important to ensure there is a sufficient supply of developable land available to meet public and school needs in appropriate locations. The early identification of potential development areas and increased consultation with the School District help ensure, if and where appropriate, the identification and designate lands for educational use in Comprehensive Development Plans and Neighbourhood Plans.

The standard size for a school site in developing areas is:

- 2.4 hectares (6 acres) for an elementary school;
- 4.0 to 4.8 hectares (10 – 12 acres) for a middle school;
- 5.7 hectares (14 acres) for a secondary school.

School sites should be:

- located as near as practical to the centre of the area being served;
- be relatively flat (less than 5% slope) and rectangular in shape;
- have adequate soils for building foundations;
- be located on a collector rather than an arterial road;
- be connected to the pedestrian walkways, sidewalks and public transit.

It is important that the City and the School District continue to work co-operatively to serve our common public citizens. We trust the foregoing will be helpful in addressing the public education component of the future planning in the City. Please don't hesitate to contact us if you wish more discussion on any of the items identified above.

Sincerely,



David Widdis
Planning Manager

Copy: Delta Carmichael, Secretary Treasurer
Rob Drew, Director of Operations
Stuart Kamstra, Assistant Director of Operations

WEST KELOWNA OCP REVIEW – UDI OKANAGAN FEEDBACK

March 24, 2023

Comments:

- 1) 6 storey: 2.75 FAR (equivalent to 2.35 net FAR)
- 2) 8 storey: 3.5 FAR (equivalent to 2.95 net FAR)
- 3) 12 storey: 5.5 FAR (equivalent to 4.65 net FAR)
- 4) 14 storey: 6.5 FAR (equivalent to 5.5 net FAR)

Additional comments about draft height and density:

- The feasibility of developing 8, 12, and 14 storeys is very limited. Recommendation to consider significant height and density proposals for areas where DWK wants densification beyond 6 storeys. The transition from wood frame to concrete is significant, and the density and height must be sufficient for higher density development to be feasible; otherwise, development will be limited to 6 storeys. For areas where concrete high-rise development is desired, minimum heights of 19 – 25 storeys should be considered.
- Similarly, low density neighbourhoods, planned for 1.0 and 1.5 FAR, need to be increased in order to provide the framework for apartment development. 3 storey apartment development is very challenging. 6 storeys should be considered base heights for apartments. For areas where DWK wants to see apartment development, FAR of 2.75 should be considered, otherwise, the predominant achievable form of housing within the 1.0 to 1.5 FAR is townhouse.

Comments about rental housing:

- West Kelowna should consider introducing a rental housing property tax incentive program to encourage more rental housing. The benefit of municipal tax exemptions is currently magnified by Provincial tax exemptions, thus increasing the benefit to the community.
- Rental housing is a very effective tool for providing affordable housing. Requirements for affordable housing in non-rental projects are very difficult to accommodate in condominium development (essentially a tax) and is also very difficult to administer from a municipal perspective. Rental housing incentives and healthy rental development is a superior solution to affordable housing, than regulated units within non-rental projects.

Impact of taxes, charges, and regulation on housing:

- Taxes, charges and regulation create barriers to developing housing. The feasibility of many projects often hinges on costs. Introduction of new costs and escalation of existing charges limits the volume of new housing that is feasible. Housing supply is the key to pricing. The

higher the volume of new housing, the more affordable prices are. As housing becomes more expensive to develop, prices must rise to off-set costs.

Growth

- Community Growth and housing targets are dependent on new housing. Barriers to new housing will limit growth and affordability.

Agriculture

- Sub divisions are not supported by eco-tourism
- There is nothing about agri-tourism in the OCP
- Farmers/Vineyard owners are looking for ways to generate income
- No mention of second residence on land

Section 1:

1.1 Purpose of the OCP (pg 12)

- A 227-page document is not a guiding policy

1.2 Frequently asked questions (pg 13)

- Why is an OCP important? To attract the right kind of community builders

1.5 Our West Kelowna Today and Tomorrow (pg 17)

- Where is this data coming from?
- refer to Andrew Ramlo growth projections, updated based on new
- immigration numbers, this only equates to 600/year
- WFN is experiencing 28% growth rate
- people don't like elevators, they like backyards or shared community amenities
- Covid has changed everything. from home office impacts, movement away from high rise, etc.

Pg 18 West Kelowna population projections graph:

- There is a supply issue that impacts these growth numbers. If more supply was available, more affordable, more people would move here

Pg 19 Housing Projections:

- Affordability will drive the future housing market into smaller townhomes and mf. This is based on total cost of construction and rising land values.

Pg 21 – 20 Year Housing Distribution (Based on Growth Concept

- not enough emphasis on single family - higher demand
- it's become unaffordable in City of Kelowna, so families will look to West Kelowna for growth

Pg 22 – Anticipated Housing Distribution (table 2)

- Growth rates are not attainable based on FAR and land buildable area. Must take into consideration roadways, other building inefficiencies, etc. (pops)

Pg 24 – Retail

- Requires a lot more density/growth to support these types of retail nodes in the Westbank urban center.

Pg 26 - 1.6.1 – Growth Strategy

- E-bikeable is more accurate than walkable

Pg 27 – Map 2

- Limited infill due to topography
- Growth needs to be re-allocated

Pg 28 – How Does Density Impact Communities:

- Incentivize these areas - help create critical mass

Pg 29 – Growth Concept Areas

- Requires offices - the offices support the retail, services, public transit, etc. Residential augments this as well.

Section 2:

- Concerned that experience and lifestyle retail is not going to be drawn to the two proposed urban centers in the near future - too much highway commercial focus/feel
- Focus more attention on expanding opportunity for Neighborhood Centers to take hold, helping overall supply and making Urban Centers more attractive to developers to invest in retail and high density housing
- Neighborhood centers too small and not enough density allowed - FAR of 1.5 will not materialize into a 6-storey building
- Case study of Goat's NC using areas summarized for Block E Goat's Peak CDP yields only half the units that OCP is indicating are needed to be built there; tried with 2.35 FAR full build-out residential (no commercial at all and no extra public spaces) and only got 415 units (not the >500 indicated in the OCP); expand NC to Block D (would also comment that 500 units is not really dense enough to function as an active neighborhood center - concern it won't support the neighborhood retail needed to bring a NC to life)
- Believe West Kelowna is missing a focus on semi-detached and townhouse in their overall product mix - make zoning more attractive for this product type
- Apartment numbers are unreliable as a view into the future - too small sample sizes (big spikes in graphs which show past year are associated with single apartment projects)
- In relation to above comment, what does West K have on file that they are confident is moving forward? Anything in the two Urban Centers? Anything of substantial height and unit count?
- West Kelowna Estates in-fill opportunities seem limited due to topography - likely not to materialize in the additional units that the City is hoping to see in that form
- Boucherie Urban Center on a busy highway across from industrial area - how can the City incentivize developers to invest by addressing those hinderances with Urban Center specific policies?
- The description of Neighborhood Center is great - the unit yield envisioned for it is not aligning.....a "Complete" neighborhood which can attract small scale service retail requires more than a few hundred units
- City should support medium density residential land use as more than 4-storey build form (consider proximity to transit route or neighborhood or urban center?)
- Do we need a Mixed Use term in the land use designation table?
- Adjust zones to allow for small-scale ground floor retail in Medium Density - speak to that support in the OCP update
- FAR for 6-storey building should be minimum 2.35
- POPS don't make sense - this seems like the City is simply finding a way to push burden onto stratas and homeowner associations, which doesn't make any sense in dense cores (Urban

Centers and Neighborhood Centers).....these spaces are an extension of the public realm and the City collects tax dollars to maintain this - this is further supported by Section 3 when speaking about public realm and "sense of place"

- A well-functioning "High Street" on Brown Road is hard to envision - the highway corridor getting stopped by proper High Street foot traffic will jam highway traffic more than it is already...envision MOTI being very difficult in this build out and opposed to applications regularly
- Bump up heights and FAR in Boucherie Urban Center, or consider creating an Urban Center specific density map to allow for the high-rise in certain areas i.e.. a height transitional approach from Boucherie Mountain towards highway, the same way there appears to be a proposed transitional approach for the Westbank Urban Center
- What is going to happen to existing City Hall site when new one is built? Opportunity for the City to plan for a fitting community amenity that isn't already there?
- Seeing Gellatly Neighborhood Center Policies, none for the others....is something missing?
- Thrifty Foods picture in Section 2.7.2 is absurdly out of place given the areas of land identified for these NC's, and the FAR's associated - the vision is great, but the size of the NC's and the policies proposed won't result in that vision
- Add Stacked Flats as a building type? (not really apartment, and not townhouse either)
- Can we get the rationale for the comment in Tourist Commercial about avoiding the stretch between Glenrosa and Crystal Mountain Resort?
- 10m is overly wide for public trail access adjacent creek etc. for trails
- Section 2.19.3.3 - this doesn't make any sense. Why is the OCP indicating a need to assess the need for the Goat's Peak development when the CDP is approved and the City is highlighting the area as a needed Neighborhood Center.....is this point an old point that is now irrelevant?
- Section 2.19.3.1 - believe this is done given the trail network plan and agreement to prep a parking lot for the trails

Pg 32 – 2.1 General Land Use Policies

- Net Tree Positive, Nonrestrictive

Pg 33 - #11. Agricultural Land Reserve

- Include consideration for lands that are currently fully serviced

Pg 33 - #13.

- New commercial land uses involving drive-through facilities need to be well thought out, not discouraged

Pg 34 – 2.2 Land Use Designations Summary:

- Wood frame
- Do not limit concrete height.
- Show >7 storeys instead

Pg 38 – 2.3 West Urban Centre Mixed-Use Corridor

- Revitalization of urban center through promotion and incentives
 - Public transit and transit-oriented development incentivizes density and other entitlements
- USES

- Includes amenity bonusing and affordable housing component - 0.5 additional far

MAXIMUM DENSITY

- NO NEED TO LIMIT
- 2.35 FOR 6 STOREY
- 2.95 FOR 8 STOREY
- 4.1 FOR 12 STOREY

- 4.7 FOR 14 STOREY

Pg 38 – 2.3.2 Policies

#3.

- Not required - this is the designated area for high-rise.
- The spacing/separation of towers should be a consideration

Pg 39 – Figure 9

- Pedestrian crossing requirements over/under passes Highway 97

Pg 39 #12&13

- Pops is ambiguous - refer to zoning? Public or private
- Is this a city maintenance concern?

Pg 40 – 2.4 Westbank Urban Centre – Commercial Core

- Maximum Density: Increase FAR from 2.5 to 3.0

Pg 41 – Figure 10

- Identify ministry of transportation impacts
- This is being ignored

Pg 41 - #10

- Downtown concept "big box" - should not be discouraged

Pg 42 – 2.5 Westbank Urban Centre – residential shoulders

- Maximum Density: Increase FAR from 2.0 to 2.5

Pg 43 – 2.5.2 Policies

#3

- Not required - this is the designated area for mid/high-rise.
- The spacing/separation of towers should be a consideration

Pg 44 – 2.6 Boucherie Urban Centre

- Maximum Density: Increase FAR from 2.0 to 2.5

Pg 46 – 2.7 Neighbourhood Centres

- Maximum Density: Increase FAR from 1.5 to 2.35
- 6 storey required to meet growth projections

Pg 47 – 2.7.2 policies

#5

- Not required

Pg 50 – 2.8 Medium Density Residential

- Maximum Density: Increase FAR from 1.0 to 1.85 for 4 storey, 2.35 for 6 storey

Pg 53 - 2.9.1 Low Density Residential Objectives

- Where is infill going to happen here?
- Infill in Kelowna estates is a challenge from a topographic perspective. This growth needs to be re-allocated

Pg 54 – 2.10 Commercial

- Maximum Density: Increase FAR from 1.5 to 1.85 for 4 storey, 2.35 for 6 storey

Pg 60 – 2.13 Educational/Institutional

- Maximum Density: why limit this?
- Consider incentives for university type projects, college, private school.

Section 3

- What does the City mean by encouraging the use of "edible landscaping"?

- Section 3.2.2.4 - Promoting higher degree of mixed land uses within growth centers to reduce the need for vehicles is a good goal - need those centers to have enough mass to support retail/office uses within them (speaks to FAR shortage again)
- Section 3.2.2.6 - Is MOTI going to support "traffic calming" on Brown Road where they are surely focused more on highway traffic flow? (a bi-product of traffic calming tends to be increased congestion)
- Are there any anticipated incentives related to promoting car-share programs that are City initiated, not just focused on what MF developers can do with those car-share companies?
- Section 3.2.3.1 - This goal of protected natural areas supports comments at recent round table, that the Remnant Land Policy doesn't work well in hillside areas - these areas of irregular shaped open/natural space should contribute to City owned/protected lands for the development of public trail networks
- Section 3.3.2.3 - this goal supports the development of regional stormwater management sites which would be beneficial when the alternative is often each and every developer installing underground tanks/infiltration galleys - where there are opportunities to support regional stormwater facilities, the City should assist developers in coordinating to develop these, just like they do a reservoir that services more than one development area
- Section 3.4.2.2.4 - while this is an initiative that all should support, during times of economic stress and when housing affordability is a serious issue, public buildings should be responsible in their choices of what design elements to incorporate, as many options have very high up front cost which will ultimately land on the tax payer at a time when that's the last thing that residents need; Focusing energy on retrofitting existing buildings as per Section 3.4.2.1.2 is likely to make a bigger difference in terms of energy consumption costs for public buildings
- Are there any initiatives that have already been started that are related to a "regional air quality program"??
- Section 3.4.4 - With Step Code implementation continuing and the building department already responsible for ensuring that is being followed, what would "Climate Action Coordinator" be responsible for overseeing?
- Section 3.5.2.1.3 - "Functional" access to the lakefront sometimes requires the City develop the lakefront - responsible development vs protection - will the City champion the build out of public realm for the purposes of enjoying the lakefront?? (West Kelowna Yacht Club and Gellatly Boat Launch and likely other waterfront access points will need some expansion/improvement at some point to support growth in Goat's and Gellatly and other areas)
- Section 3.5.2 - seems largely redundant to previous section in OCP which covers Greenbelt and Greenways - consider combining into one location
- Section 3.6.1.1.7/8 - this is another support for why the City should maintain ownership of significant public realm spaces - these types of events and programming are not fitting for POPS
- Section 3.6.3.2 - too much "explore opportunities" - would be more bold about how the City plans to immediately put into effect some of these policies (expedited approvals for in-fill through delegated authority is a good example)....."exploring opportunities" is easy to ignore/abandon. Language is too soft
- Section 3.6.5.1.2/2.1 - NCs will need to be bigger if public health and medical facilities are envisioned (even "smaller" services are generally not small floor areas associated for those uses)
- Section 3.7.1.5/2.6 - Support mixed use buildings in the industrial areas - light industrial is showing to successfully mix with commercial/office/residential - this promotes a live/work/play environment and CPTED through natural surveillance in these light industrial areas
- Section 3.8.2.3.a - Push for delegated authority through this OCP update!

Section 4

- Section 4.2.4.3 - Note something to do with affordability component - too much criticism of "simplistic" design which is something that needs to be supported when trying to bring price point sensitive product to market (let market be more of a driver)
- Section 4.2.5.7 - Re-vegetating proposed cut/fill slopes in hillside development should not require planting of shrubs/trees - doesn't provide additional stability above and beyond establish hydro-seeding and creates a maintenance issue on terrain that is difficult to walk in some areas - in addition this is not a requirement of nearby municipalities and things work perfectly fine in those areas - finally, trees on slopes ultimately block views which hillside residents value and the result is future residents wanting to remove the trees (this same comment applies to Section 4.9.5 Hillside DPA)
- Section 4.2.7 - recommend townhome projects that are 4 units or less DO NOT need to go through Form and Character DP (filing of Disclosure Statements is not required until strata projects hit 5 units or more, so it seems natural to cut it off there)
- Section 4.2.7 - if light industrial low-rise mixed use is going to be considered "mixed use", assuming it will be put through a Form and Character DP, but should specify
- Section 4.2.8.1 - a 1.5m setback above the 3rd storey doesn't seem necessary on side yards or rear yards when dealing with a 6-storey or less.....would argue it just complicates the design and reduces efficiency - can this requirement be limited to those builds that are above 6 storey? (or consider eliminating the need for this on side yards at least)
- Section 4.2.11.8 - Parking not permitted within front yard setback - there must be some exceptions to this that should be spoken to....wording like "unless site constraints require it" or "unless supported by City Staff as the most functional option" (not to mention point 10 in this section speaks to there being a scenario where parking does exist in the front yard setback)
- Section 4.2.11.14 - Gravel parking should be permitted in industrial setting if suited to the business that is operating, as well trailhead parking lots if being constructed by developer where dust control is not a concern
- Section 4.2.12.5.c - in cases of separated sidewalk, does the City not want street trees in the boulevard? Private property owners can maintain them if in a newer subdivision where irrigation sleeves cross the sidewalk, however where there are not private property owners with developed land, the City could either install an irrigation system themselves or defer tree planting by developer until private lands are built out
- Section 4.2.12.15 - significant cost savings using #1 pot size shrubs instead of #2 - consider revising min size requirement / all municipalities using 6cm caliper as min tree size has resulted in shortages in the past (consider going to 4-5cm to put the City in a position where supply issues are less a factor in streetscapes getting finished)
- Section 4.2.12.17 - why no rock mulch included in the list (least maintenance heavy option)
- Section 4.2.14.4 - confirm placement within road ROW is supported?
- Section 4.3.5 - when building commercial mixed use within Urban and Neighborhood Center, does this DPA also apply, or only the Urban and Neighborhood Center DP requirements?
- Section 4.3.5.13 - do Urban Centers have zero lot line front yard setbacks, or at least just the "High Street"? (if so, will the City be entertaining a hardscaped High Street boulevard that those businesses can use for seating to support their business and functionality for pedestrians?)
- Section 4.3.5.14 - within Urban and Neighborhood centers, thought towards a larger public realm plaza that serves this need for all the nearby businesses should be considered to avoid all the individual projects building minimum spaces that are seldomly used

- Section 4.4.4.4/15 - good spot to insert comment about light industrial mixed use with office/residential
- Section 4.5.5.3 - example image appears to show zero lot line setback on the frontage for the High Street - assuming the City will be planning for street furniture and/or allowing businesses to use the public realm for mobile seating/tables - are the cross sections being considered for this High Street going to have wide enough boulevard to support this function? If not and businesses want outdoor seating and other don't the result may be a lot of variation in front yard setback which can look goofy when combined with zero lot line side yard setback build outs
- Section 4.6.4.6 - could the existing City Hall site be a candidate for building this Urban Center entertainment hub? (DCC plaza/park space?)
- Section 4.6.5.22.a - this appears to be mandating POPS, or is it just highlighting the need for a connection i.e. gate or similar? (mixed use developers are going to respond well to POPS being mandatory)
- Section 4.7.5.11 - could see the odd scenario where site constraints make this a big challenge - consider adding language that speaks to potential exception to rule
- Section 4.7.5.12/7.4 - these items raises a question - is the City planning for any parkades in any of the Urban Centers at some certain massing?? Do they own any land that would be well suited?
- Section 4.7.5.14.a - the NCs don't appear large enough in size to achieve this.....need more than a few land parcels to do this successfully (Goat's and Smith Creek NCs are too small); the point about building "stepbacks" works, but to earlier points about the NCs being too small, this point is another to highlight as a good reason to expand them area wise
- Section 4.7.5.18 - may be growing pains, but this might not be possible if the adjacent low-density residential is within the future NC area - City needs to support when appropriate
- Section 4.7.5.25 - if the zoning associated with NCs will allow for zero lot line side yards to create smaller "High Street" feel, recommend setbacks above second storey only apply to frontages on local roads or public plazas (allow zero lot line on side yards and allow no stepback on lane frontages if that's the case)
- Figure 47 appears out of place - looks like it should be on the next page

Section 5

- Section 5.3.1.8.d/e - should note exception for intermittent material processing (crushing/screening) for materials sourced from and intended to be used within the same Urban or NC; City should support sustainability pros of processing onsite materials for use in road structure, when the material is deemed suitable by geo (can help cut/fill balance and significantly reduce hauling requirements from aggregate source sites, saving emissions and reducing demand on existing road network)
- Section 5.4.3.p.i - why is this a goal of the CDPAs? If the hillside area is entirely developable i.e. 20% slope or less, why would the City want or need more than the typical 5% parkland dedication requirement? (The City needs units to combat affordability issues)
- Section 5.4.3.q - supports prior notes that the Remnant Land Policy shouldn't really apply to hillside CDPAs



OVERVIEW

The City of West Kelowna is a growing and vibrant community. Between 2020 and 2040, we anticipate welcoming over 12,000 new residents. The Official Community Plan is the road map that will help us guide how and where we will shape this growth in the future.

This document contains objectives and policies that will guide the decisions of our Council and City staff towards creating a city that is reflective of the #OurWK Community Vision, a separate document that sets the vision for West Kelowna in 2040.

Summary of Comments on WFN May 2_2023 Ref Response - condensed PDF comments.pdf

Page: A

Author: kelli Subject: Sticky Note Date: 26/04/2023 3:16:33 PM
Interactive Map - possibly provide WFN zoning or land use instead of showing WFN as a white area. This will assist in showing what is adjacent to CWK
Author: carlaeaton Subject: Sticky Note Date: 08/05/2023 1:11:48 PM
We have amended the maps and schedules to include WFN parcels and roads in grey scale to address similar public comments as well

1.5 OUR WEST KELOWNA TODAY AND TOMORROW

The City of West Kelowna is located on the western shore of Okanagan Lake, on the traditional territory of the Syilx Okanagan people. The City of West Kelowna has an area totaling approximately 12,353 hectares (123.53 square kilometers) and spans from Highway 97C in the south to Rose Valley and Bear Creek Road in the north. This OCP encompasses the entirety of the City.

Our City has many neighbours, who we seek to collaborate with in implementing our vision:

- North: *Westbank First Nation Community Forest, the Regional District of Central Okanagan, including rural Crown land and large private land holdings.*
- East: *Okanagan Lake, The City of Kelowna*
- South: *The District Municipality of Peachland*
- West: *Regional District of Central Okanagan*

Our closest neighbours are the **Westbank First Nation**, who govern two adjacent reserves: **one in central West Kelowna**, and another on the City's eastern boundary. These areas, established in 1860 and overseen by the Westbank First Nation since 1963 are known as Tsinstikeptum Indian Reserves 9 and 10, respectively (referred to as IR#9 and IR#10 in this document), and comprise a total of approximately 980 hectares (2,420 acres). Westbank First Nation has been self-governing since 2005 and their administration operates independently.

West Kelowna values these neighbours as collaborative partners. As more people move to establish a career, develop connections with their neighbours and invest in the future of their community in the Okanagan Valley, our collaboration with neighbours to manage this growth is essential in fostering healthy, sustainable, and thriving communities while protecting our natural areas. A coordinated, collaborative planning approach, founded upon a shared vision, will enable West Kelowna to direct growth and manage change for the long-term health of the community, and the Okanagan Valley.

OUR PEOPLE

The City of West Kelowna is a vibrant and growing community. Our People is a foundation of the #OurWK vision as it reflects the people of West Kelowna's contributions to creating a more welcoming, equitable and sustainable community. The following section explores West Kelowna's anticipated population trends, as a baseline for the policies in the OCP.





Population Statistics

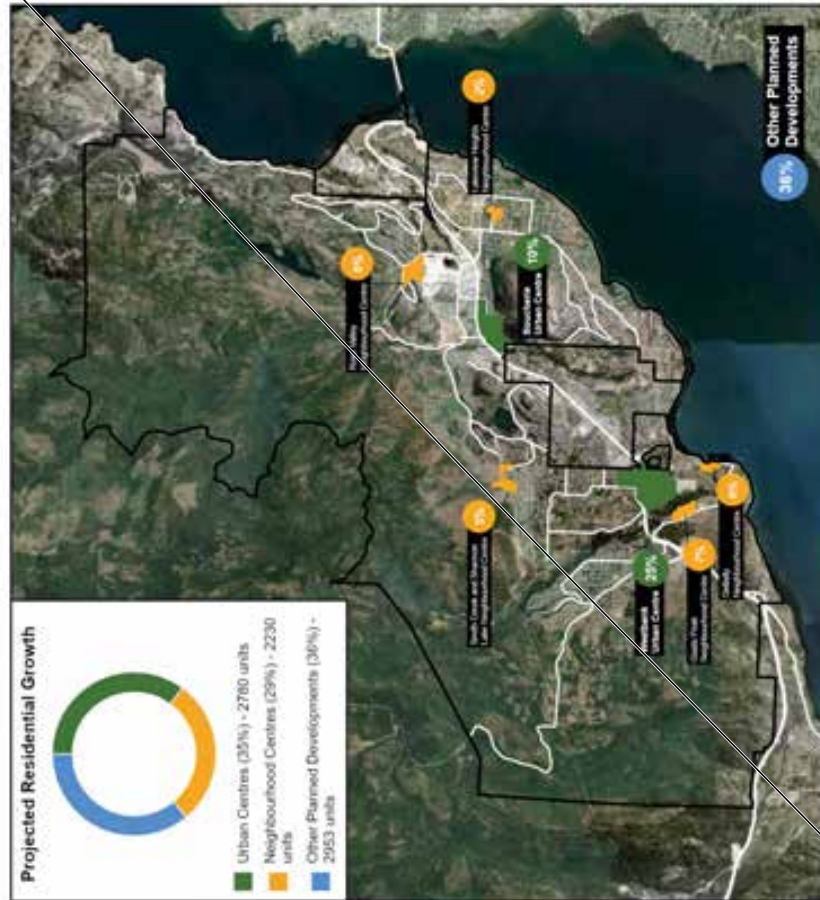
West Kelowna's growth has been a continuous story. According to the 2021 Statistics Canada Census, the population of West Kelowna is 36,078, which represents an increase of just over 10% from the 2016 Statistics Canada Census population of 32,655, and 32.3% from the 2008 population of 27,261.

Looking ahead, it is estimated that the population of West Kelowna will grow an average of 1.5% every year until 2050. This translates into an estimated future population of 41,961 in 2030, 48,223 in 2040, and 54,147 in 2050. These projections represent a slight increase from those made in the last version of the OCP. A growing population means that as we continue to grow as a community, we will need to create places to live, work, and play for existing residents and the approximately 12,000 additional people we anticipate welcoming by 2040.

West Kelowna, like many cities in Canada, has an aging population. In 2021, the median age of West Kelowna was 44, a slight decrease from the 2016 median age of 45.2. Further to this trend, 21.4% of residents are over the age of 65, with an additional 15% aged 55 to 64. This is slightly higher than the provincial average. By 2050, these percentages are expected to increase.

Page: 1

	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:11:03 PM
include WFN			
	Author: lalman	Subject: Sticky Note	Date: 27/04/2023 1:37:07 PM
Also include (WFN) in brackets after this so for the rest of the document WFN is clear			
	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:11:24 PM
WFN is not within West Kelowna			
	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:12:32 PM
add population comment (100008th). All are residents of the Westside and cross boundaries			



Map 1: Projected Residential Growth

Retail

The rapid adoption of online shopping and delivery services is continuing to change the retail market. Currently, West Kelowna's retail supply must be considered alongside the retail supply within the Westbank First Nation Lands, as both serve the same populations.

According to the Colliers Real Estate Inventory and Growth Projections report from 2021, it was estimated that West Kelowna and Westbank First Nation have a combined total of 147,716 square metres of retail space, and a vacancy rate of 6.5%, representing a healthy retail market. Westbank First Nation Land contains 64% of this retail space, which holds the majority of the big box stores. Smaller-scale retailers are more concentrated in West Kelowna, particularly in the Westbank Centre. While opportunities for big box retail are limited in West Kelowna, there is an opportunity to develop lifestyle and experience-oriented retail nodes. These often take the form of blocks of small, street-orientated shops.

Introducing small-scale Retail in Neighbourhoods

West Kelowna is estimated to see a demand for an additional 10,219 square metres of retail space by 2030, and 30,379 square metres by 2040. As West Kelowna's population continues to grow, and communities densify, opportunities begin to emerge to introduce small-scale commercial centres in neighbourhoods other than Westbank Centre. These types of uses can support the daily needs of residents within walking distance, reducing dependence on the automobile and enriching the neighbourhood experience.

Mixed-Use Development

West Kelowna's developable land is limited by topography and agricultural uses. Providing more opportunities for mixed-use development that combines commercial, office, entertainment, and residential in the same building is a way to supply West Kelowna with more retail space, while conserving land for other uses.




Office

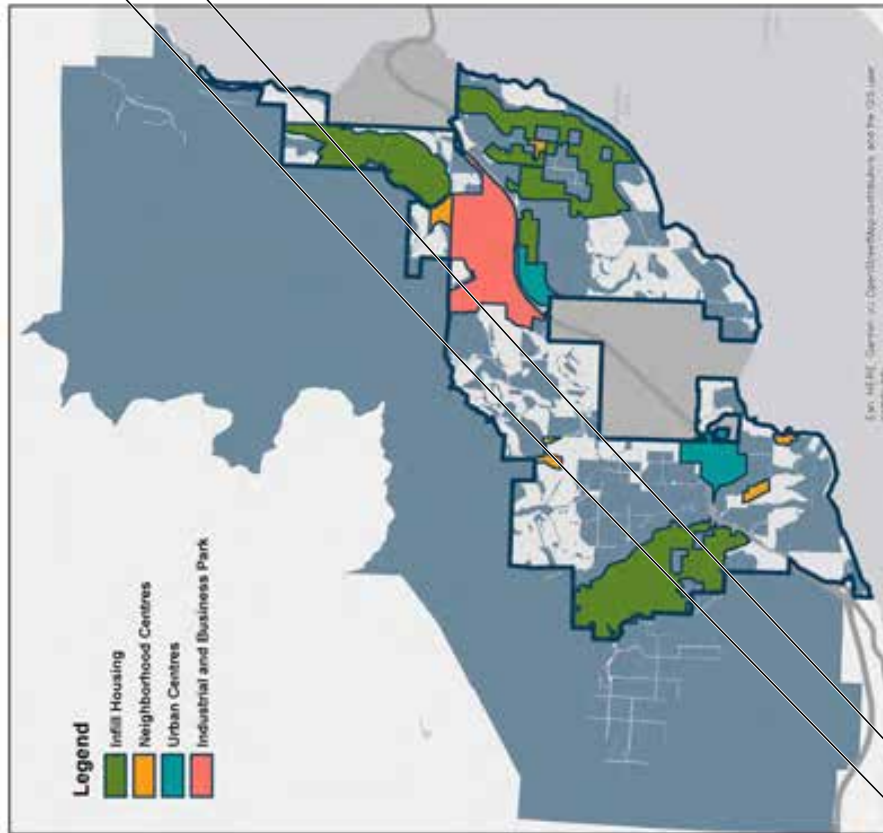
The rise in working remotely has softened the demand for office space in many jurisdictions across the country. West Kelowna currently has 33,972 square metres of office space with a vacancy rate of 5.9%. This market is relatively small, with ambulatory healthcare services, professional services, and public administration being the largest occupiers.

West Kelowna is estimated to see demand for 8,826 square metres of additional office space by 2030, and 14,493 square metres by 2040.



City of West Kelowna | Official Community Plan

	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:14:08 PM
		don't grey out WFN to the point where it looks like nothing is there.	
	Author: jallman	Subject: Sticky Note	Date: 27/04/2023 1:40:43 PM
		label WFN again - IR 9 and IR 10	
	Author: krell	Subject: Highlight	Date: 26/04/2023 3:14:12 PM
		Does not seem very inclusive when WFN Lands are gray or white and with no labels to identify WFN Lands	



Map 2. Growth Concept - Vibrant Centres and Complete Neighbourhoods

2.3 WESTBANK URBAN CENTRE - MIXED-USE CORRIDOR (AREA A)

The Westbank Urban Centre – Mixed Use Corridor land use designation applies to a specific sub-area within the Westbank Urban Centre, generally the lands located directly east and west of Brown Road between Gossett Road and Ingram Road.

The designation is intended to promote a high density, mixed-use precinct that encourages a walkable, pedestrian- scale experience through thoughtful urban design and active street-oriented uses. A high density of residential and office use in this area will support a successful and attractive **public realm**. This designation supports development with public-facing uses such as retail, hospitality and entertainment at the ground level, with more private uses such as office and residential on the upper storeys.

USES	MAXIMUM DENSITY	BUILDING HEIGHT
<ul style="list-style-type: none">Mixed-use buildingsInstitutional, and ancillary housing initiativesLive-work unitsParks and open space, and pedestrian connections	<ul style="list-style-type: none">Up to 3.5 Floor Area Ratio (FAR) or as guided by policy	<ul style="list-style-type: none">High-rise

2.3.1 Objectives

- To provide convenient access to services that meet the needs of residents for employment, shopping and amenities in the Mixed-Use Corridor by encouraging densities necessary to support a well-connected, walkable area and public transportation.
- To provide a centralized service centre that meets a range of community needs and which is conveniently accessible for the whole city.
- To encourage purpose-built rental and new multi-unit housing in the Mixed-Use Corridor.
- To have Brown Road develop into a **high street**, thereby being the most prominent area for a mix of activities with street fronting commercial uses.
- To secure and develop high-quality park and plaza space in Westbank Urban Centre.

2.3.2 Policies

- With the exception of Institutional uses, only mixed-use, mid- to high-rise development containing ground level commercial or office, which may include residential or hotel uses above, shall be permitted in the Mixed-Use Corridor. While permitted at the ground level, office use is encouraged to be located above ground level to ensure an activated street corridor.
- All development should design the building mass, form and details to create an interesting and pedestrian-oriented environment at the street level that is focused on active uses.
- Densities greater than 3.5 **FAR** and/or building heights above 12 storeys may be considered, where appropriate and if impacts to adjacent properties are sufficiently minimized, to promote transit-supportive development and/or specific community benefits including **affordable housing** units, childcare and recreation facilities, and others at the City's discretion. Shadow and view impacts to adjacent properties must be mitigated to the satisfaction of the City.

2.4 WESTBANK URBAN CENTRE - COMMERCIAL CORE (AREA B)

The Westbank Urban Centre – Commercial Core land use designation applies to the areas east and west of the Mixed-Use Corridor, with Ingram Road as its southern boundary and includes lands fronting Old Okanagan Highway at its northernmost extent.

The designation is intended to promote the development and redevelopment of higher density commercial uses and mixed-use buildings that contribute to active street frontages and a strong urban core.

USES	MAXIMUM DENSITY	BUILDING HEIGHT
<ul style="list-style-type: none">Commercial, including office and retailMixed-use buildingsInstitutional, and ancillary housing initiativesLive-work unitsParks and open space, and pedestrian connections	<ul style="list-style-type: none">Up to 2.5 Floor Area Ratio (FAR) or as guided by policy	<ul style="list-style-type: none">Mid-rise to high-rise



2.4.1 Objectives

- To encourage existing lots that are used for low density commercial to be redeveloped as high density commercial or mixed use to better utilize existing areas, improve street fronting design, and the pedestrian experience.
- To encourage future growth in this area to contribute to the creation of an employment and service centre for the community.
- That development in this area contributes to the overall vibrancy of the Westbank Urban Centre.
- To provide convenient access to services to meet the daily needs of residents and visitors.
- To secure and develop high-quality park and plaza space in Westbank Urban Centre.

What is a “pedestrian focused environment” or “pedestrian scale”?

These terms refers to designing buildings and spaces that specifically consider the needs and perspective of pedestrians and how people interact with the building and space, encouraging safer and more usually interesting places.

2.4.2 Policies

- Development should be primarily commercial or mixed use in a form that supports a transition to the residential shoulders from more intense land uses to less intense.

WPN lands are directly adjacent to area B (along Old Okanagan Hwy & north side of Hwy 97). The CWK model of "stepping down" in height/density towards residential neighbourhoods is a great idea, however the land directly adjacent to WPN here is not residential, it is currently low-rise commercial, and WFN plans on also allowing higher density (12 - 22+ storeys) in the future. This area would be better situated to be associated with area A.



Figure 10. Westbank Urban Centre - Commercial Core

2. All development should design the building mass, form and details to create an interesting and pedestrian-focused environment at the street level.
3. Densities greater than 2.5 FAR and/or building heights greater than 10 storeys may be considered, where appropriate and impacts to adjacent properties sufficiently minimized, to promote transit-supportive development and/or specific community benefits including **affordable housing** units, childcare and recreation facilities, and others at the City's discretion. Shadow and view impacts to adjacent properties must be mitigated to the satisfaction of the City.
4. Where Council is considering additional densities and heights for developments including an extraordinary community benefit, public consultation will be required despite any ability to waive public hearings under the *Local Government Act*.
5. Incorporate transit-oriented and active transportation design elements.
6. No new low-density residential zones or low density commercial zones are to be created or permitted within areas designated as Westbank Urban Centre – Commercial Core.
7. Support uses and activities that encourage both daytime and evening activities.
8. Consider the protection of views to Okanagan Lake and Mount Boucherie through any potential zoning amendment as applicable, and through building design during the development permit stage.
9. Support **pop-up** uses and structures that are temporary and transportable in order to enliven streets, public areas, and **publicly accessible private spaces**.
10. Discourage "big box" auto-oriented commercial development in the Westbank Urban Centre Commercial Core in favour of the establishment of compact, complete, pedestrian centres with a well-defined **streetscape** that supports pedestrian, bike and transit mobility.
11. Encourage uses which support and complement existing and planned institutional and civic functions within Westbank Urban Centre including City Hall and municipal facilities.
12. Consider additional residential uses in combination with the institutional use through a potential zoning amendment application if necessary, where the proposed residential use supports housing initiatives with a broader community interest such as supportive, seniors, affordable or non-market housing. Supportive housing initiatives will only be considered as part of Province led housing facilities and must include on-site support services.

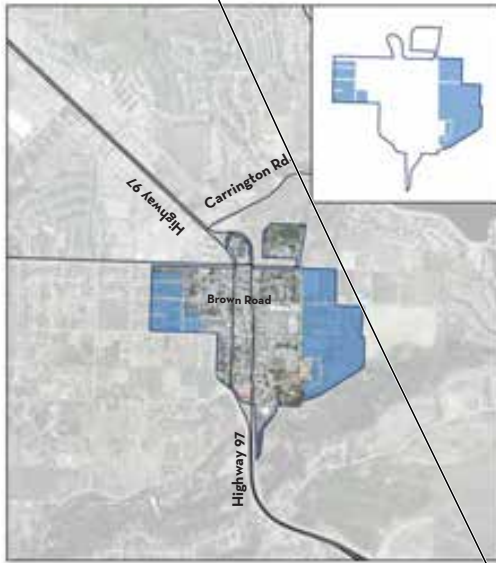


Figure 11. Westbank Urban Centre - Residential Shoulders

2.5.2 Policies

1. Low-rise residential development should take the form of townhouses (3 or greater units), New single-detached homes or duplexes are not permitted.
2. All multi-unit development should design the building mass, form and details to create an interesting and pedestrian-focused environment at the street level.
3. Densities greater than 2.0 **FAR** and heights greater than 6 storeys, may be considered, where appropriate and impacts to adjacent properties sufficiently minimized, to promote transit-supportive development and/or specific community benefits including **affordable housing** units, childcare and recreation facilities, and others at the City's discretion. Shadow and view impacts to adjacent properties from additional height must be mitigated to the satisfaction of the City.
4. Where Council is considering additional densities and heights for developments including an extraordinary community benefit, public consultation will be required despite any ability to waive public hearings under the *Local Government Act*.
5. Heights will transition downwards towards the edges of the Residential Shoulders to sensitively integrate with surrounding and anticipated development density.
6. Mixed-use development should create an appropriate transition from less intense to more intense land uses. Incorporate transit-oriented and active transportation design elements.
7. Use ground-oriented residential uses as a component to create sensitive transitional areas where the designation occurs next to lower scale uses, including institutional use such as a school.
8. Mitigate the impact of residential uses on non-compatible agricultural sites and ensure adequate **buffering** measures are utilized.
9. Encourage uses that support the existing and planned institutional uses within Westbank Urban Centre.
10. Consider additional residential uses in combination with the institutional use through a potential zoning amendment application if necessary, where the proposed residential use supports housing initiatives with a broader community interest such as supportive, seniors, affordable or non-market housing. Supportive housing initiatives will only be considered as part of Province led housing facilities and must include on-site support services.

2.6 BOUCHERIE URBAN CENTRE

The Boucherie Urban Centre land use designation applies to the parcels generally in the area between Mount Boucherie and Highway 97, between Ross Road and the west of Capri Road.

The designation is intended to allow for the redevelopment of the area into a secondary Urban Centre in West Kelowna, providing residential and employment densities that, while lower than the Westbank Urban Centre, support commercial and recreational opportunities and a pedestrian-friendly environment. Higher densities will be focused around the transit hub adjacent to Highway 97 and near the Mount Boucherie Community Centre, before transitioning towards more medium and low-density forms to ensure compatibility with the surrounding context.

USES	MAXIMUM DENSITY	BUILDING HEIGHT
<ul style="list-style-type: none">Mixed-use buildingsCommercial, including office and retailMulti-unit housingTownhousesInstitutional, and ancillary housing initiativesLive-work unitsParks and open space, and pedestrian connections	<ul style="list-style-type: none">Up to 2.0 Floor Area Ratio (FAR) or as guided by policy.	<ul style="list-style-type: none">Mid-Rise, with potential for low rise mixed use and townhouses at transition areas

2.6.1 Boucherie Urban Centre Objectives

- To promote a distinct and unique area, by focusing development characteristics that are appropriate and supportive of local civic services, parks, and natural amenities.
- To promote a diverse mix of housing to address housing accessibility and attainability.
- To promote mixed-use, commercial and medium density multi-residential development in Boucherie Centre as a secondary service hub for West Kelowna that supports alternative modes of transportation and walkability.
- To encourage a **complete neighbourhood** with commercial and mixed-use buildings offering convenient access to services to meet the needs of residents for employment, shopping and personal services in Boucherie Urban Centre by encouraging residential densities necessary to support walkable service.
- To provide services and amenities that support **infill** housing in the broader Boucherie neighbourhood outside of the Boucherie Urban Centre.
- To encourage lot consolidation to promote medium density multi-unit residential developments, as well as opportunities for commercial or mixed-use.
- To discourage "Big box" auto-oriented commercial development in Boucherie Centre in favour of the establishment of compact, complete pedestrian centres with a well-defined **streetscape** that supports pedestrian, bike and transit transportation.
- To secure and develop high-quality park and plaza space in Boucherie Urban Centre.

2.6.2 Boucherie Urban Centre Policies

- Provide a variety of housing layout, configuration, and tenure within mixed-use and multi-residential development that includes a focus on mid-rise buildings, where ground oriented townhouses are encouraged in podiums of mid-rise and taller buildings, where appropriate.
- Low-rise residential development may be considered to create a sensitive transition to areas of lower intensity use and form, but should take the form of townhomes (3 units or greater). No new low density, single-detached residential zones are to be created or permitted within areas designated as Boucherie Urban Centre.

2.16 RESOURCE LAND

The Resource Land land use designation applies generally to areas suitable for recreation, agriculture or forest or mineral extraction.

The developed areas of West Kelowna are bounded by private and publicly owned (Crown) lands which the OCP designates as Resource Land. Provincial forest, community watersheds, recreation tenures, open grazing, transportation networks and mineral resources are managed by provincial resource agencies through consultation with the local community and resource operators. The management of Resource Lands is complex, involving local, regional, provincial and WFN jurisdictions and multiple regulatory requirements.

USES	MAXIMUM DENSITY	BUILDING HEIGHT
<ul style="list-style-type: none">Natural resource extraction and forestryRecreationAgricultural usesParks and open space	<ul style="list-style-type: none">Established by zone but intended for low density	<ul style="list-style-type: none">Low-rise



- 2.16.1 Resource Land Objectives**
1. To protect environmentally sensitive areas to maintain their ecological function.
 2. To preserve significant view sheds of West Kelowna's natural areas.
 3. To protect and enhance agricultural resources.
 4. To protect community water resources and prevent negative downstream impacts.
 5. To support the use of Crown lands within the City boundary for rural resource values where they are consistent with the community's values.
 6. To demonstrate and encourage transparent and inclusive decision-making for Resource Lands with the WFN and other government agencies.



2.18.2 Natural Areas and Public Parks Policies

1. Apply the principles of **Crime Prevention Through Environmental Design (CPTED)** to parks and open space so that they are accessible in a safe and convenient manner.
2. Consider the policy and implementation direction established by the Parks and Recreation Master Plan.
3. Upon subdivision, the minimum 5% dedication of land or funds in lieu of dedication for parks will be used to complement the City's park system in accordance with the provincial regulations of the *Local Government Act*, and the City's Parks and Recreation Master Plan.
4. Seek to protect natural areas for its conservation value and where it complements the existing park and trail system through park dedication above the maximum 5% at subdivision, in line with the criteria and guidance provided by the Park Land Acceptance Policy.
5. Through a variety of strategies aim for 20% of land area within West Kelowna to be in the form of natural areas and **publicly accessible open spaces** such as parks, trails, and plazas. As areas develop, ensure sufficient open space is provided to offset the increased densities. (Refer also to Schedule 3, **Greenbelt** and **Greenways** Data table, which outlines the current area attributed to each of the seven types of **greenbelt** or **greenway** that contribute to the 20% goal within the **Growth Boundary** of the City of West Kelowna.)
6. Require Area Plans and Development Applications to include parks, open space, pedestrian and linear corridors and major recreation facilities. These must include consideration of the City's Parks and Recreation Master Plan, park pre-plans, community and staff input.
7. Encourage efforts by community organizations, Trusts and Service Groups to partner in the acquisition, maintenance or management of publicly owned spaces including parks, boulevards, foreshore, community gardens, trails, **greenways** and **greenbelts**.
8. **Coordinate park planning initiatives** with regional partners to ensure the efficient and effective provision of parks and recreation services, and to ensure that, whenever possible, linear parks connect across municipal boundaries.
9. Where appropriate, consider negotiation with agricultural landowners and appropriate provincial ministries and agencies, for provision of linear park linkages and public right of ways adjacent to or along the edge of, but not necessarily within, agricultural lands.
10. Consider acquiring significant large natural areas through purchase, donation, negotiation at the time of zoning or use of **density bonusing**, where such action is believed in the City of West Kelowna's best interest, and the land and processes comply with the Park Land Acceptance Policy.
11. At time of subdivision for all development types, and at time of zoning amendment for mixed-use, multi-residential, commercial, industrial and institutional developments, secure a **Statutory Right of Way (SRW)** for public access, up to 10 metres in width, where trails are included in a **Greenways** and **Greenbelt** Plan, or adjacent to creek corridors, or as otherwise identified during the development application process. The 10 metre corridor may be in addition to, and outside, any riparian management area requirements imposed through Environmental Development Permit Guidelines.
12. Continue working with RDCO, BC Parks and other community partners to foster the preservation and enhancements of West Kelowna's parks and open spaces.

3.1.3 Urban Design Policies

- 1. Where possible, design of new development should integrate visible links to the community's natural and cultural heritage.
- 2. New development should enhance the safety and security of community members by preventing and reducing opportunities for criminal activity through informed urban design, such as **Crime Prevention Through Environmental Design (CPTED)** principles.
- 3. New development should incorporate principles of universal accessibility.

3.1.4 Land Use along Major Roads Policies

- 1. Site design will generally orient primary building frontage to Major Roads, with parking and 'back-of-house' or servicing functions located to the rear.
- 2. Land uses adjacent to roads will be encouraged to enhance the visual character of the street through architecture, and urban design, including landscaping.
- 3. **Buffering** between Major Roads and other land uses will be required, where appropriate, such as through landscaping, plantings and open space.
- 4. Coordination with the Ministry of Transportation and Infrastructure, where appropriate, will be encouraged to improve the pedestrian environment along Highway 97 within the Westbank and Boucherie Urban Centres and at all major intersections and crossings.
- 5. Encourage the development of community gateways to include appropriate community signage, cultural and art displays within landscaped features, where possible through adjacent development application processes, or through City initiatives.
- 6. A Neighbourhood Identification Strategy and Signage Plan will be considered for implementation, where appropriate, across West Kelowna.



Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:25:34 PM
is this defined somewhere?		
Author: iallman	Subject: Sticky Note	Date: 27/04/2023 1:47:52 PM
maybe cultural heritage is the wrong word. This could open a can of worms. History of the land/area? (going from how the volcanoes were developed, how the sylix lived on the land, how the settlers moved here to farm, to present day with everybody here residing together?)		
Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:26:41 PM
coordinate with WFN along routes that run through both jurisdictions eg. Boucherie. While Boucherie is MOTI jurisdiction on reserve, it may not be forever and WFN has significant decision making authority over that route.		

3.2 TRANSPORTATION



Transportation and land use are fundamentally connected. The way we grow as a city will significantly impact the way we move. Cities which are made up of communities that have a mix of densities, including centres with residential, business and recreational opportunities, result in shorter trips for residents more likely to be taken through walking, cycling or transit, resulting in less traffic volume and vehicle emissions. Additionally, areas that have denser and more concentrated residential and commercial land uses tend to be more successful in supporting frequent and reliable transit service, as well as a wider range of businesses.

As West Kelowna grows, we are committed towards shifting to sustainable transportation options. While the Transportation Master Plan outlines the path forward for our transportation infrastructure, the OCP outlines land use and development strategies that can promote a shift towards more trips being taken through active modes or public transit. Shifting to more sustainable transportation options is not only an effective way to reduce our carbon emissions, but it results in cleaner air for residents, healthier lifestyles, and oftentimes a tighter-knit community.

Policies and objectives that help to address transportation issues including traffic calming, transit priority, improved cycling and pedestrian infrastructure, and parking management will also help to achieve a more efficient and safer transportation system.

This OCP was developed alongside with the latest Transportation Master Plan, and both share a consistent vision for West Kelowna. The Transportation Master Plan includes detail on how the transportation network in the City will evolve, and additional policies to support the OCP and overall vision of the City.



3.2.1 Transportation Objectives

1. Make it easier for everyone to choose sustainable and affordable options for traveling to, from and within West Kelowna.
2. Improve the overall experience, convenience, and safety for all modes and residents through the design of streets and neighbourhoods.
3. Promote development in the Urban and Neighbourhood Centres that contributes to a critical mass of population to make walking, cycling and public transit viable.
4. Improve the safety and reliability for people and goods to move around West Kelowna.
5. Reduce greenhouse gas emissions through the design and operation of the transportation network, in coordination with provincial mandates.
6. Design and adapt transportation infrastructure to be resilient to climate changes and natural disasters.

3.2.2 General Transportation Policies

1. All development must consider the Transportation Policies within the OCP, as well as applicable policies within the Transportation Master Plan.
2. Develop and maintain a traffic model of the City's transportation network.

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Author: gdimnickl Subject: Comment on Text Date: 26/04/2023 2:28:43 PM
coordinate with WRN to create multi-jurisdictional linkages
Author: lailman Subject: Sticky Note Date: 27/04/2023 1:49:34 PM
Should also discuss the regional connection here

3. Implement a Transportation Master Plan (TMP) that addresses current and future needs for all transportation modes, including pedestrian, bicycle, public transportation and automobile while continuing to improve transportation safety through the implementation of infrastructure, design, traffic calming and construction best practices.
4. Promote a higher degree and intensity of mixed land uses within identified growth centres to reduce the need for vehicular travel and longer trips, and support frequent public transit service, and encourage high density and affordable housing near the transit network, particularly the Route 97 transit exchanges.
5. Create enabling conditions to make active transportation and public transit competitive with driving, especially within the Urban and Neighbourhood Centres, including locating higher and medium density residential uses near jobs and services.
6. In Westbank Urban Centre, the Brown Road corridor will serve as a focal point for urban design through new developments and traffic calming, ensuring this area is walkable, accessible, and attractive.
7. Promote recreation and active transportation through the development of greenway trails.
8. At subdivision and zoning amendment stages of development, ensure that walking and cycling paths and lanes for all users link to adjacent major natural areas, parks, schools, other public institutions, employment nodes, transit stops, and community activity areas, and within and between new and established neighbourhoods. Streets created by subdivision will be integrated with existing roads and planned out in ways that are conducive to walking and cycling.
9. Support regulations that require pedestrian and cycling facilities as part of new developments, including bicycle parking, lockers and shower/change rooms, as appropriate.
10. New development will introduce new or enhance existing pedestrian connections, including through financial contributions to off-site pedestrian network improvements, where appropriate.
11. Promote the creation of a car-share policy, zero-emission vehicles, EV charging stations, and measures aimed at maximizing transportation options in new development, including through reductions to minimum parking requirements.
12. Explore shared public-private parking facilities and other efficiency strategies such as car-share spaces.
13. Leverage proposed development in Urban and Neighbourhood Centres to improve the active transportation network.
14. Development applications need to consider emergency access/egress, where appropriate.
15. Encourage an enhanced investment in pedestrian, cycling, and transit connections to and around schools and other locations with vulnerable users.
16. Continue work to reduce active transportation crossing barriers at key locations, such as intersections, creeks, and highways to increase options for people's movement in an effort to enhance community connectivity.

3.2.3 Greenways and Linear Trails Policies:

Greenbelts and Greenways have been identified as part of a concept for an interconnected network of existing and proposed community-wide linear trails, parks, greenspaces and/or protected areas (see Schedule 2 - Greenbelts and Greenways Concept map). The goal is to preserve natural areas and significant landscape features, **buffer** development edges, strengthen recreation opportunities and support **multi-modal transportation** alternatives. **Greenbelts and Greenways** consisting of trees and climate-appropriate vegetation serve to improve community health and livability. Additional linear trails may also be identified as part of the Transportation Plan, or at time of application review. Linear trails, and lands within proposed **Greenbelts** and **Greenways** may be protected, preserved, dedicated, or acquired at the time of zoning amendment and/or subdivision.

1. Linear trails and **Greenways** should significantly contribute to the City's goal for 20% of land area within West Kelowna to be in the form of protected natural areas and/or publicly accessible parks, trails, and greenspaces to ensure sufficient open space is provided to offset increased densities.
2. As a key component of the City's transportation network, support the protection and acquisition of linear

3. Continue to implement the Water Conservation Plan, and consider updates as necessary, to support water conservation and water quality objectives through educational tools and programs, water restriction policies, water loss management, rate and billing analysis, and others methods.
4. Reduce water and energy consumption by the City, residents, and businesses.
5. Continue to provide a cost-effective, safe, and sustainable supply of drinking water.

3.3.3.2 Water Policies

1. Ensure a safe supply of drinking water, including collaboration with **Westbank First Nation** regarding water service connections to the City's water system.
2. Balance the needs of a growing city with water availability for agriculture.
3. Explore the implementation of a range of measures to reduce community water consumption including outdoor water use restrictions, development and building bylaws, universal water metering, rainwater harvesting and conservation-oriented water rates.

3.3.4 Stormwater

The City owns, operates, and maintains the stormwater collection system within its boundary. As part of the vision for Our Adaptability to address the impacts of climate change and planning ahead for infrastructure needs, an increased emphasis on sustainable stormwater management is needed. This includes the proactive design of stormwater systems to protect natural habitats and water quality and mitigate flooding and other damage to public or private assets. The community seeks to manage stormwater through capture and ecological treatment, mimicking natural systems and demonstrating the City's leadership in sustainable design and asset management.

3.3.4.1 Stormwater Objectives

1. Proactively reduce the impact of stormwater runoff to downstream terrain, infrastructure and **watercourses** through planning and design.
2. Promote the benefits of **low impact design** and stormwater infiltration through educational initiatives and programs.
3. Consider watershed health and sustainability objectives at the beginning of all land use planning processes, to ensure policies support resiliency and water quality improvement.

3.3.4.2 Stormwater Policies

1. Ensure the design and management of storm water will avoid impact to downstream properties.
2. Support the integration of rainwater detention, infiltration and conveyance systems with community or natural amenity space where possible. Promote park and **streetscape** designs that serve as temporary rainwater detention and encourage rain gardens and other forms of low-impact development. Where a surface drainage system has been designed to function as a park or natural open space, it is not eligible as credit towards Local Government Act parkland dedication requirements at time of development, except at the City's discretion.
3. Reduce runoff through promoting permeable surfaces and discouraging impermeable surfaces.
4. Design new rainwater infrastructure to manage flows to pre-development rates including future climate change projections. This includes preventing frequently occurring small rainfall events from becoming surface run-off and ensuring the maintenance of minimum base flows, and in some instances augmented base flows, in water bodies.
5. Mimic natural ecosystem processes in rainwater system design and construction as much as possible. This includes minimizing runoff, maximizing infiltration, preserving and protecting the water absorbing capabilities of soil, vegetation and trees particularly along riparian corridors and minimizing impervious surfaces on both private and public lands.
6. Ensure stormwater meets applicable BC surface water objectives at the time it is discharged into receiving water bodies.
7. Apply best practices to land use management to prevent erosion and sedimentation during construction and for agricultural practices.

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- Author: gdimnick, Subject: Comment on Text Date: 26/04/2023 2:31:06 PM
clear that this isn't an open interconnection (non emergency) as WFN water cannot be 'sold' off reserve.
- Author: gdimnick, Subject: Comment on Text Date: 26/04/2023 2:31:52 PM
coordinate with WFN as stormwater, creeks and streams cross jurisdictional boundaries

3.5 NATURAL ENVIRONMENT AND HAZARDS



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Author: gdimnickl Subject: Comment on Text Date: 26/04/2023 2:33:37 PM
culturally significant places as well

The Okanagan is recognized and revered as one of Canada's most ecologically rich and diverse areas of Canada. **Sensitive ecosystems** such as the open grasslands, rocky outcroppings, pine savs, and dry benchlands are nourished by **watercourses** and are critical arteries for the health and wellbeing of the land and the lake.

The Okanagan has more threatened endangered and rare species than any other area of the Province. Both scenic and fragile, the natural environment is sensitive to the pressures associated with community development and the City of West Kelowna has such a wide diversity of affected landscapes ranging from aquatic to hillside. The health of the natural environment - ecosystems, biodiversity, and water and air quality - is a shared responsibility. The choices we make in managing the environment, such as the amount of water we use, the type of plants we landscape with, how and when we burn debris, the types of buildings we build and how we heat them and the way we design our community will directly affect the quality of our lives in the future.

3.5.1 Biodiversity and Environmentally Sensitive Areas

The biodiversity and unique and special habitats of the West Kelowna landscape are critical aspects of its intrinsic value to the community.

3.5.1.1 Biodiversity and Environmentally Sensitive Areas Objectives

1. Integrate measures to identify, protect, enhance and restore environmentally significant areas and local biodiversity in all land use decisions.
2. Encourage development and land uses that respond to the natural context of West Kelowna and are sensitive to existing environmental features
3. Protect the natural look and appeal of West Kelowna's prominent natural features.

3.5.2.1 Biodiversity and Environmentally Sensitive Areas Policies

1. Protect sensitive natural areas, while focusing growth in strategic areas.
2. Protect the natural look and appeal of West Kelowna's prominent natural features.
3. Protect the shoreline of Lake Okanagan, and maintain functional and visual access to the waterfront for the community.
4. New development along the shoreline, **riparian areas**, or other ecologically sensitive areas will require the protection of sufficient land needed to maintain protection and conservation of the area.
5. Require by way of development, restoration of already degraded ecosystems or sites and ensure ecological restoration work occurs on already disturbed sites.
6. Require that development plans consider and complement the natural topography of West Kelowna, while also considering universal accessibility and the retention of existing topographic features.
7. Encourage the preservation of culturally significant indigenous vegetation. Work with Westbank First Nation and other traditional knowledge holders to seek opportunities to identify, preserve, and celebrate culturally significant plants and ecology.
8. Encourage private stewardship of significant ecosystems through private property conservation tools such as conservation covenants, land trusts, and eco-gifting.

3.5.2 Greenbelts, Urban Forest, Parks and Natural Assets

Our Places celebrate the special areas that define the character and sense of community for West Kelowna and acknowledge that the green and blue spaces of the City must be protected and connected for future generations. The blue spaces consist of the City's watercourses (lakes, creeks, etc), and the spaces that connect to them. The green spaces consist of the City's natural areas, protected areas, as well as parks and linear trails. **Greenbelts and Greenways** (also noted in Section 3.2.4 and shown on Schedule 2 - Greenbelts and Greenways Concept map) identify much of these green spaces as part of a concept for an interconnected network of existing and proposed community-wide linear trails, parks, greenspaces and/or protected areas. Beyond the transportation goals tied to **Greenways**, **Greenbelts** form an integral part of the City's goal to preserve natural areas and/or provide recreational opportunities to mitigate increased density and anticipated population increases within the City and region. Protection of trees and climate-appropriate vegetation serve to improve community health and livability. Lands within proposed **Greenbelts** and **Greenways** may be protected, preserved, dedicated, or acquired at the time of zoning amendment and/or subdivision.

3.5.2.1 Greenbelts, Urban Forest, Parks and Natural Assets Objectives

1. Strive for 20% of land area within West Kelowna to be in the form of protected natural areas and/or publicly accessible parks, trails, and greenspaces to ensure sufficient open space is provided to offset increased densities and anticipated population growth within the City.
2. Connect existing and planned blue and green spaces to help establish a sustainable and significant natural asset network, including collaboration with other agencies to ensure completion of existing and future **Greenbelts** and **Greenways** within West Kelowna and to the region as a whole.
3. Work with community partners in protecting natural areas and waterways, including the Regional District of Central Okanagan, Westbank First Nation, and the Provincial Government.
4. Manage trees and forested areas within City owned or City controlled lands for the safety of residents.

3.5.2.2 Greenbelts, Urban Forest, Parks and Natural Assets Policies

1. As a key component of the City's green space goals, support the protection and potential acquisition of lands identified as **Greenbelts**, at the zoning amendment and subdivision stage, where:
 - a. Protection and acquisition may include some form of right of way dedication, registration of a Section 219 covenant, or zoning the area as park, as applicable; and may even include parkland dedication subject to the City's Park Land Acceptance Policy; and
 - b. Where consideration of any **Greenbelts** should also consider connectivity goals identified within the Transportation Section 3.2.4 Greenways and Linear Trails for additional related transportation network policies, as well as the Park and Recreation Master Plan update.
3. Designate publicly owned land, or support the acquisition of land to create a centrally located park and plaza space within the Westbank Urban Centre. The centrally located park/plaza space should be coordinated with policies and objectives of the Park and Recreation Master Plan, and/or updates to the Greenbelts and Greenways Concept Schedule 2 and related Greenways and Linear Trails policies, or Greenbelts, Urban Forests and Natural Assets policies.
4. Maintain existing vegetated areas in **Greenbelts** and **Greenways**, and restore vegetative cover to naturally forested hillside areas for the purposes of controlling erosion, providing habitat, and enhancing the natural beauty of the City utilizing wildfire best management practices.
5. Provide habitat and linkages within **Greenbelts** and **Greenways** for rare and native plants associated with the Okanagan Valley.
6. Encourage the use of native plant and tree species in landscaping and restoration on public and private lands, where appropriate.

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Author: gdimnickl Subject: Comment on Text Date: 25/04/2023 2:34:29 PM
and culturally significant places (better here or above?)

3.6 SOCIAL SUSTAINABILITY



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A socially sustainable city is one that promotes the physical and mental wellbeing of its residents, encourages healthy relationships between neighbours, and provides an accessible environment for all ages and abilities. The City of West Kelowna recognizes the importance of promoting a socially sustainable community. The #OURWK vision highlights several Key Actions which contribute to our city's social sustainability, as we strive to focus on Our People, Our Connectivity and Our Adaptability. While the responsibility for financing health and social programs rests with senior levels of government, The City of West Kelowna will support initiatives designed to meet the needs of all residents in the community for a range of social considerations, such as arts, culture, recreation and community services, heritage, public health and safety, food security, schools and housing attainability.

As West Kelowna grows, creating a fair distribution of services in facilities throughout the city is essential to its social sustainability. Social, recreational and cultural opportunities should be widely available in family, child, youth and elderly-friendly settings. There should be no barriers to access – physical, social or economic – including access to effective public transportation.

3.6.1 Arts, Culture, Community Services and Recreation

Arts, cultural and recreational development are key elements that add to the quality of community life. The City of West Kelowna recognizes the integral community contributions generated by these activities to local social and economic progress, as well as the value of artistic expression and enjoyment by its residents.

Cultural organizations are instrumental in further developing the aesthetic environment and rich diversity of ideas, activities and backgrounds that make communities interesting and thrive. The City of West Kelowna is committed to growing our cultural scene to connect us with one another and our community, including partnerships with organizations that promote and provide cultural programs and development. As we grow, we are committed to the provision of a range of recreational opportunities for residents through community programs and facilities and will consider investment in the arts and culture of our community.

3.6.1.1 Arts, Culture, Community Services and Recreation Objectives

1. Foster community interaction and bring together people of diverse backgrounds, lifestyles, generations, abilities, and interests through a variety of inclusive leisure, arts, cultural and recreational opportunities.
2. Create a network of community and recreational facilities which are conveniently accessible to all neighbourhoods, adaptable to changing needs, and with no barriers to entry (age, ability, religion, gender, etc.).
3. Encourage investments and partnerships in arts and culture for the purposes of strengthening the economic base, improving quality of life and enhancing community identity and pride.
4. Develop partnerships with government and non-government organizations to maximize the use and value of facilities through cooperative use arrangements.
5. Supply educational, recreational and cultural programming throughout the city that provides a full range of opportunities that are reasonably accessible at all stages of a person's life, regardless of socio-economic status.
6. Pursue other, innovative opportunities that supply, encourage and fund additional arts and recreational facilities and services that contribute to or augment the public system.
7. Generate new opportunities for creativity and vibrancy in the **public realm**, including through public art, cultural programming, festivals and other events.
8. Strengthen the role of Urban Centres and Neighbourhood Centres as hubs for cultural and communal activity.

Author: gdimnick. Subject: Comment on Text. Date: 26/04/2023 2:56:39 PM
recognize the unique cultural / FN elements to the social aspect of CWK
Author: gdimnick. Subject: Comment on Text. Date: 26/04/2023 2:58:35 PM
any reconciliation initiatives?

3.6.1.2 Arts and Culture Policies




1. In the Westbank and Boucherie Urban Centres, new development will support diverse community-oriented amenities including public art, cultural space and activities programming.
2. Development in Neighbourhood Centres, which are intended as nodes of local services and amenities, should consider facilitating opportunities for diverse businesses and spaces conducive to the arts and cultural sector.
3. Development will seek to integrate public art and culture opportunities with new and upgraded infrastructure or other improvements related to the project.
4. Where possible, locally sourced public art should be integrated with civic infrastructure, public spaces, plazas and in private development.
5. Where possible, design of new development will integrate links to the community's cultural and cultural heritage, especially its Indigenous history, to expand cultural, educational and recreational opportunities.
6. Support the efforts of volunteers and organizations that operate community facilities and services which strengthen West Kelowna's arts, cultural and historical character.
7. Pursue and continue to support public, community and private partnerships in facilities and services that maximize community benefit and promote artistic, cultural and recreational initiatives in West Kelowna.

3.6.1.3 Community Services and Recreation Policies






1. Support the expansion of a community facilities precinct, either in the Boucherie Urban Centre or Westbank Centre, while supporting smaller services within Neighbourhood Centres.
2. Support efforts to redesign the open space surrounding and between the Johnson Bentley Memorial Aquatic Center and City Hall as a public plaza and gathering space.
3. Continue to work with RDCO, BC Parks and other community partners to foster the preservation and enhancements of West Kelowna's parks and open spaces.
4. Provide public recreation and community facilities, including neighbourhood halls, in convenient locations, particularly within Urban and Neighbourhood Centres.
5. Encourage new development to contribute to or deliver, where appropriate, universally accessible community spaces and facilities including for childcare, recreation, and youth programming.
6. Continue the partnership with School District No. 23 for the joint use of school and West Kelowna facilities to maximize their value in the community.
7. Encourage government services and civic facilities, including schools, to remain and/or locate within identified Urban and Neighbourhood Centres, or in other areas within close proximity to transit.
8. Support the RDCO's Crime Stoppers Program, Crime Prevention Program, and Victim Witness Services Program.



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	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:56:17 PM
		What defines the culture of CWR?	
	Author: ialman	Subject: Sticky Note	Date: 27/04/2023 2:04:36 PM
		Not a municipality's job to define culture but the city of Vancouver has a great summary of their cultural plan (how they are supporting initiatives) guiding framework:	
		-Support for art and culture	
		-Champion creators	
		-Build on our commitments to Reconciliation and Equity	
		-Introduce bold moves to advance community-led cultural infrastructure	
		-Position Vancouver as a thriving hub for music	
	Author: gdimnick	Subject: Comment on Text	Date: 26/04/2023 2:37:38 PM
		that can be reflective of the region (all doesnt have to be, but it would be nice to be encouraged).	



	Author: gdimnick	Subject: Comment on Text and places	Date: 26/04/2023 2:39:01 PM
	Author: laliman	Subject: Sticky Note could just use WFN since we introduced this earlier in the document	Date: 27/04/2023 2:06:25 PM
	Author: gdimnick	Subject: Comment on Text one word	Date: 26/04/2023 2:38:10 PM
	Author: gdimnick	Subject: Comment on Text one word	Date: 26/04/2023 2:38:35 PM
	Author: gdimnick	Subject: Comment on Text include a policy to reflect objective 2 above.	Date: 26/04/2023 2:39:41 PM

3.6.2 Heritage and Archaeology

West Kelowna's heritage resources include historical and architecturally significant buildings, structures, trees, natural landscapes and archaeological features. These resources significantly contribute to the expression of our unique sense of place. West Kelowna recognizes the importance of identifying and protecting its natural and cultural heritage resources.

As West Kelowna grows, identifying and protecting our heritage will become more important. Heritage and conservation efforts cannot happen without our neighbours, West Bank First Nation, who are a valuable source of history. Continuing to develop a strong working partnership with them is essential for our community.

3.6.2.1 Heritage and Archaeology Objectives

1. Foster a sense of our past and a vision for our future through recognition of, and appreciation for, our natural and built heritage, including the City's rich agricultural history and contribution to it's development.
2. Create a deeper understanding of our local indigenous history in partnership with West Bank First Nation and other Indigenous peoples.
3. Identify and protect significant cultural and natural heritage resources.
4. Explore innovative and collaborative heritage conservation techniques that balance conservation with adaptability and growth.

3.6.2.2 Heritage and Archaeology Policies

1. Support initiatives in West Kelowna that acknowledges our local indigenous history.
2. Continue to support and encourage the continued growth and development of the Westbank Museum.
3. Celebrate West Kelowna's agricultural heritage by supporting agricultural festivals and events.
4. Increase public awareness, understanding and appreciation of West Kelowna's history through promotional and educational materials such as interpretive signage and brochures.

3.8 COLLABORATION AND PUBLIC INTERFACE



The City of West Kelowna values collaboration and partnership with its residents, neighbours, and other organizations that contribute to our community. We value engaging to make important decisions together and collaborating with our Central Okanagan Neighbours. As we continue to grow, we will utilize technology, best practices, and a transparent approach when working with others.

3.8.1 Collaboration and Public Interface Objectives

1. Continue to build upon and expand partnership and consultation opportunities with Westbank First Nation.
2. Support educational and socially oriented partnerships in the community, particularly across young and older generations.
3. Engage with the community on decisions and projects that are significant and where public input can influence outcomes.
4. Improve and streamline application processes and other workflows at City Hall.

3.8.2 Collaboration and Public Interface Policies

1. Before the initiation of a City project or initiative that may have a direct or indirect impact to Westbank First Nation's land operation, or the overall environment, the City will consult with Westbank First Nation to discuss partnership and collaboration opportunities.
2. Support the involvement of Youth Ambassadors in policy/program development processes.
3. Explore integrating new allowances provided by the *Local Government Act*, including:
 - a. Processing zoning amendment applications that are consistent with policies in this OCP without a Public Hearing.
 - b. Consider permitting relevant City Administration staff to approve minor development variance applications that are consistent with the goals and objectives of this OCP.
3. Continue to utilize online and other innovative methods to engage and communicate with the public and stakeholders on City projects and initiatives.
4. Explore the use of digital notifications for zoning amendment and development processes.
5. Utilize digital technologies to communicate with and listen to the public.

3.8.3 Collaboration and Public Interface Action Items

1. Create an online portal for property taxes, permit submissions and other City Hall activities that allows for online funding transfers, that in addition and does not replace the ability to perform activities at City Hall.

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Author: Ialiman Subject: Sticky Note
are you referring to just the reserve land or WFN's territory?
For larger initiatives I would recommend entire traditional territory so we can work together for the general Westside

Date: 27/04/2023 2:09:46 PM

4.2.2 Purpose

In accordance with the *Local Government Act*, Section 488(i)(a), (b), (c), (e), (f), (h), (i), and (j), the purpose of the General Guidelines is to promote a high standard of development across West Kelowna that contributes to the Community Vision (Section 1.4) and a safe, functional, and effective city that respects and preserves its environment. In addition, refer to each specific Development Permit Area for its designated purpose in accordance with Section 488 of the *Local Government Act* (LGA).

4.2.3 General Guidelines Exemptions

The following developments are exempt from the guidelines under the General Development Permit Area Guidelines. If the development is exempt from all other Development Permit Area Guidelines, then no Development Permit is required.

1. Proposed maintenance or repair of existing landscape that does not include excavation.
2. Emergency works, including tree cutting, if necessary to remove an immediate danger or hazard, where rehabilitation and restoration work to the satisfaction of West Kelowna will occur following the emergency.
3. The removal of trees and shrubs designated:
 - As hazardous by an **ISA Certified Arborist**; or
 - As host trees by the Sterile Insect Release Program and a report has been provided to the satisfaction of CWK that identifies the affected trees or shrubs prior to their removal.
4. Construction of, or regular and emergency CWK maintenance of municipal infrastructure or buildings, where the proposed works are conducted in a manner that is consistent with the objectives of the Development Permit Guidelines, and endorsed by the Director of Development.
5. The implementation of a fish habitat mitigation or restoration plan that is authorized by the senior government ministry or agency with jurisdiction.
6. The activity is conducted under direction of Emergency Management BC.
7. Maintenance and repair of building envelopes, so long as there are no changes to the previous design, colour scheme or materials used.
8. The site has been assessed by a qualified professional who has provided a report (to the satisfaction of CWK) which concludes that the proposed development would have no significant impact on the environment and/or is not subject to a hazardous condition; or the activity occurs on land designated provincial **Agricultural Land Reserve** and is considered normal farm practice as designated by the Ministry of Agriculture.

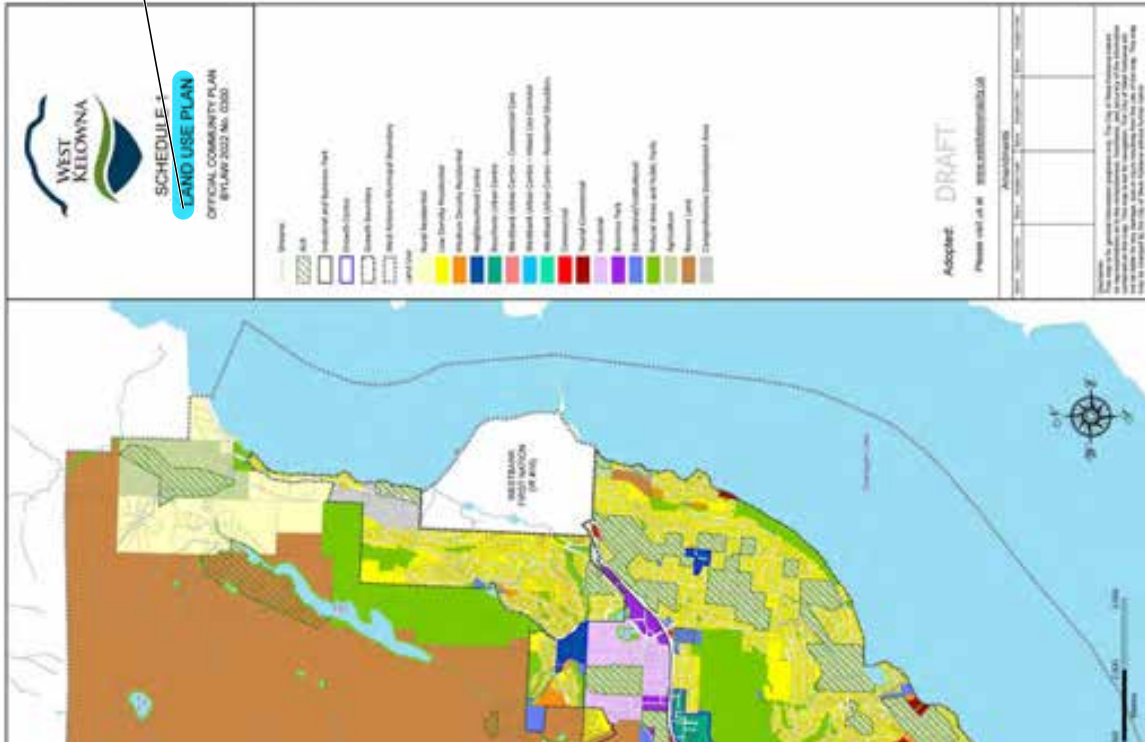
4.2.4 General Guidelines - All DPAs Design Principles

The General Guidelines - All DPAs Design Principles communicate the common shared high-level intentions of the entire Development Permit Guideline framework and reflect the vision and objectives of the OCP to promote a high standard of development across West Kelowna. All projects subject to a Development Permit will support the following Principles:

1. Ensure that the Community Vision is reflected in new growth and (re)development.
2. Ensure that policy direction of the OCP is realized through the fair implementation and administration of development guidelines.
3. Encourage attractive, built-forms that contribute to and enhance the general character of all development within the City, with a greater focus on guidelines for higher intensity mixed-use, multi-unit residential, commercial, business park or industrial development supporting walkable, complete neighbourhoods.
4. Encourage development that sensitively integrates and enhances with surrounding neighbourhoods.
5. Promote development that respects the natural environment, hillside and agricultural characteristics of the community.

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Author: gdimmiid. Subject: Comment on Text. Date: 26/04/2023 2:44:53 PM
perhaps there is a better place, but including a comment on require chance find procedures related to archaeology would be supportive of WFN arch requirements.



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Author: kiel Subject: Highlight Date: 26/04/2023 3:23:05 PM
 May be beneficial to show WFN Land Use as well - more inclusive and provides those who read the document with context even if it is at 50% transparency - if you are interested we can share this data with you.
 Same comment with the rest of the maps below.